

PD Modification Application:

PRS 22-1174

BOCC Land Use Meeting Date:

03/07/2023

1.0 APPLICATION SUMMARY

Applicant: Big Bend LA, LLC

FLU Category: UMU-20

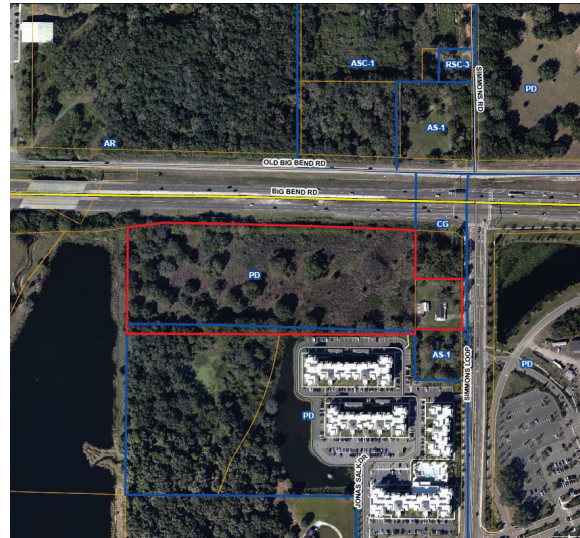
Service Area: Urban

Site Acreage: 11.74 acres

Community Plan Area: Riverview

Overlay: None

Request: Minor Modification to PD 16-0209



Existing Approvals:

PD 16-0209, MM 21-0865

MM 21-0865 was approved for a multi-family residential development with 211 units based on 10.57 acres at 20 units per acre. The project also included a 1.17-acre community dog park located on the western portion of the unified development. The residential building was approved for a maximum of 300,000 square feet of residential with a maximum height of 58 feet above the base floor elevation and up to four (4) stories.

Proposed Modifications:

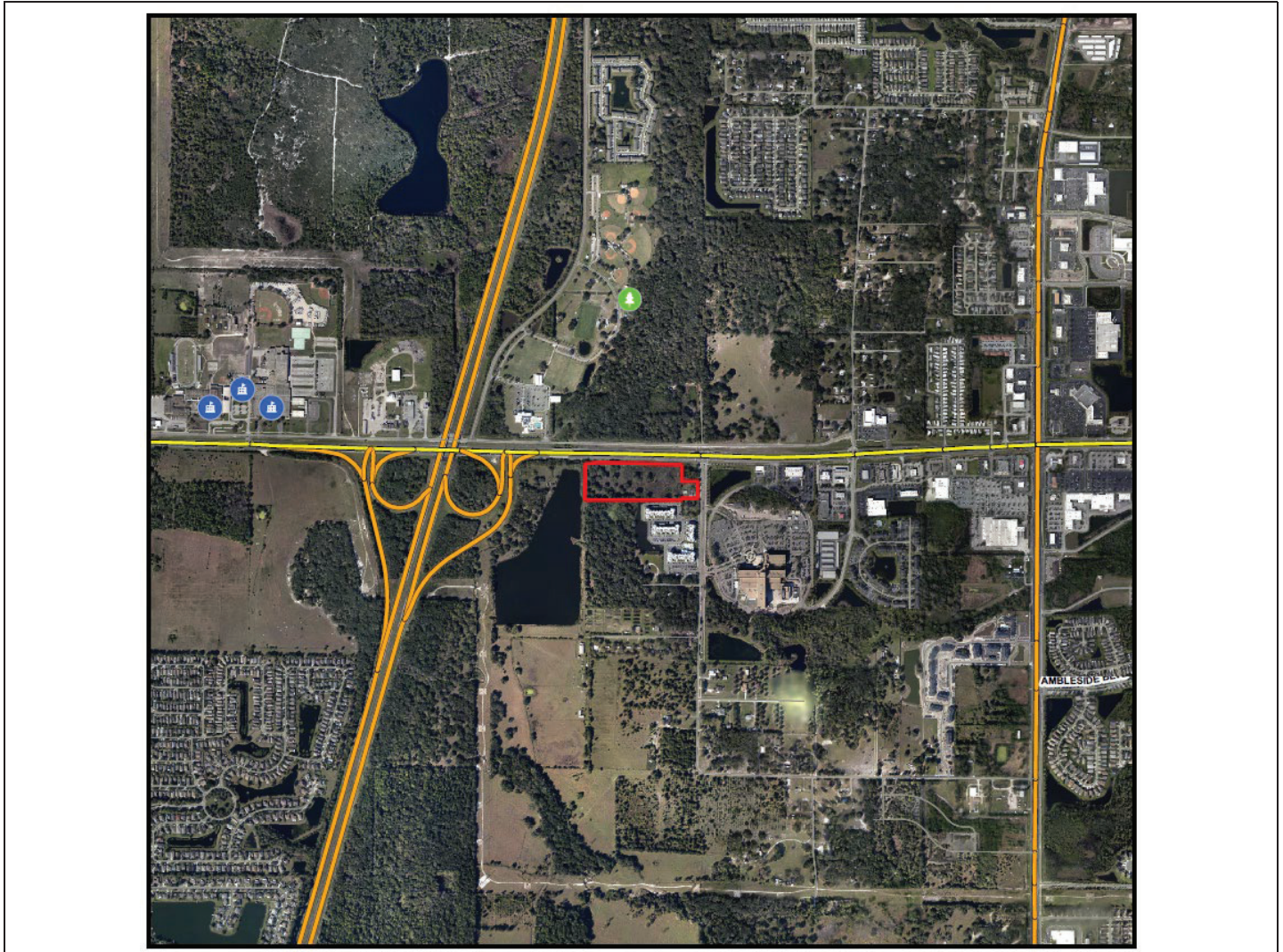
The applicant is seeking a minor modification to the existing Planned Development MM 21-0865. The applicant is proposing to modify approved condition no. 15 (proposed condition no. 17) to reduce the minimum throat depth requirement from 150 feet to 60 feet. The applicant submitted a traffic analysis supporting their request; however, given that the proposed entitlements are remaining unchanged, and no access points are proposed to be added or removed, a traditional trip generation and site access analysis was not required pursuant to the Development Review Procedures Manual. The applicant also proposes condition no. 27 pursuant to the wetland delineation requiring concurrent construction of the public dog park and cross access to the dog park to maintain comprehensive plan consistency.

Additional Information:

PD Variations	None requested.
Waivers	None requested.
Planning Commission Recommendation	Consistent, pursuant to conditions of approval.
Development Services Department Recommendation	Approvable, with conditions.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.1 Vicinity Map



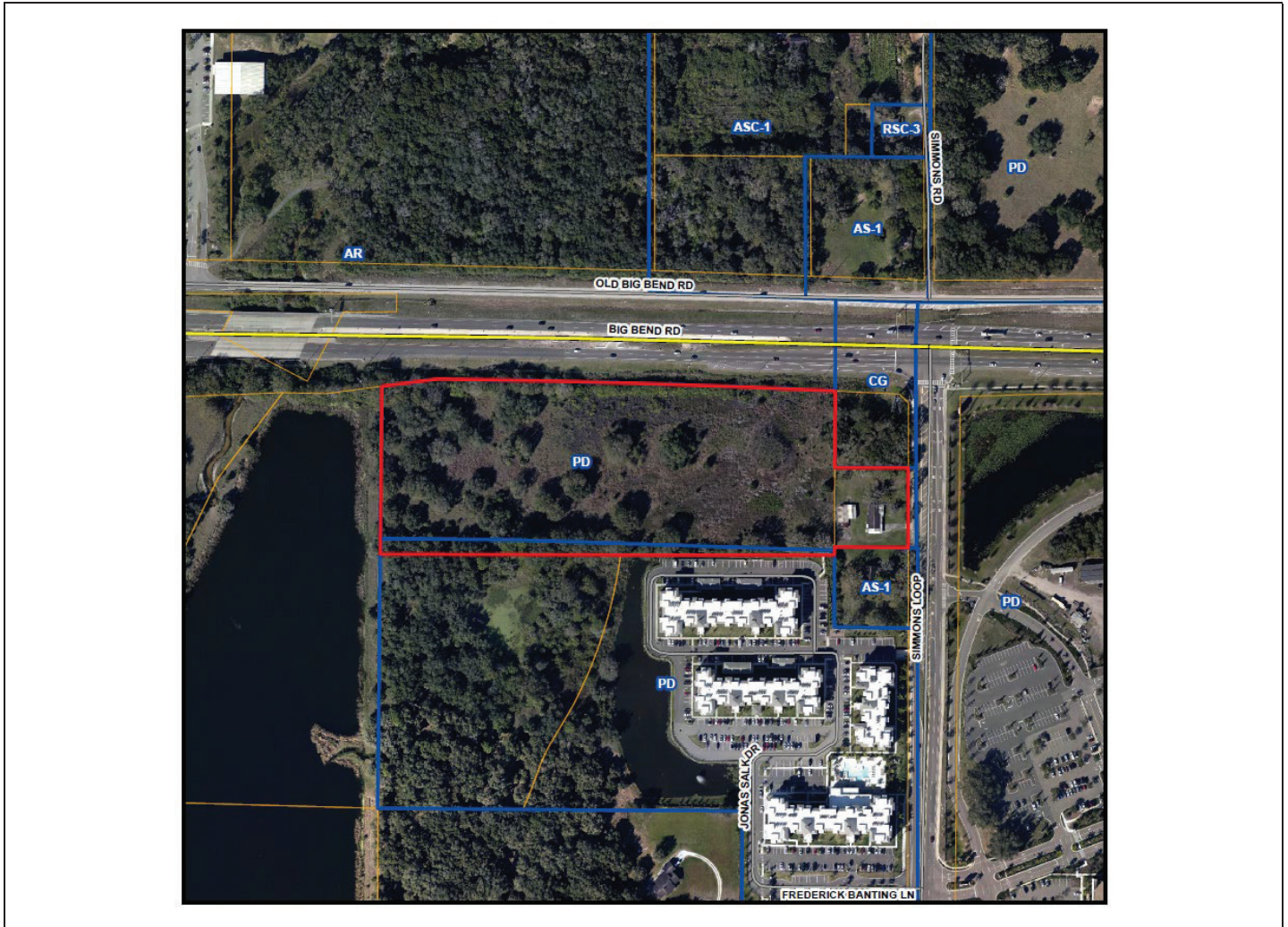
Context of Surrounding Area

The subject site is located off Big Bend Road approximately 900 feet to the west of the subject site is the on/off-ramp to I-75. The site is in the Urban Service Area and within the limits of the Riverview and Southshore Areawide Systems Community Plans. The surrounding area includes a mix of uses including:

- To the northeast is a vacant Commercial, General (CG) zoned property,
- A hospital and medical offices (St. Joseph's Hospital – South) to the east across Simmons Loop,
- A Planned Development (PD 15-0172) to the south was approved for a 400-unit multi-family development,
- To the west is zoned Agricultural Rural (AR) developed with an artificial lake (retention pond) to the west, and
- A County Park directly north across Big Bend Road, and northeast across Big Bend Road is vacant agriculturally zoned properties.

2.0 LAND USE MAP SET AND SUMMARY DATA

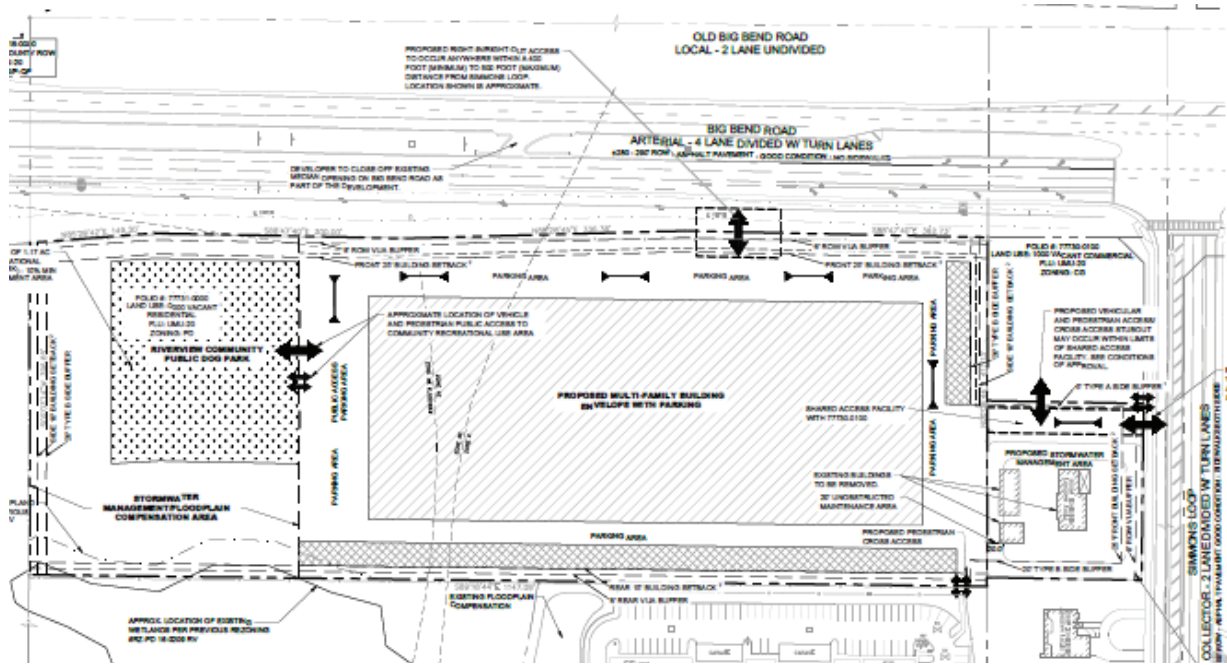
2.2 Immediate Area Map



Adjacent Zonings and Uses					
Location:	Zoning:	Future Land Use:	Density/F.A.R.	Permitted Use:	Existing Use:
North	AR, PD 04-1820, RSC-2, ASC-1, AS-1	UMU-20 (Urban Mixed Use – 20)	PD FAR: 0.40 ASC-1: 53,560 sq. ft.	Park, Agriculture, SF	Vance Vogel Park & Vacant
South	PD 15-0172	UMU-20 (Urban Mixed Use – 20)	PD: max. 400 multi-family units	Multi-family apartment buildings	Multi-family apartment buildings
West	AR	UMU-20 (Urban Mixed Use – 20)	Retention Pond	Retention Pond	Retention Pond
East	PD 17-0501	UMU-20 (Urban Mixed Use – 20)	FAR: 0.29 (excluding energy plant)	Hospital, medical offices and accessory medical uses.	Hospital, medical offices and accessory medical uses.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.4 Existing Site Plan



3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Simmons Loop	County Collector - Urban	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Big Bend Rd.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	1,258	87	104
Proposed	1,258	87	104
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North	X	Vehicular & Pedestrian	None	Meets LDC
South		Pedestrian	None	Meets LDC
East	X	Vehicular & Pedestrian	None	Meets LDC
West		None	None	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY

INFORMATION/REVIEWING AGENCY	OBJECTIONS	CONDITIONS REQUESTED	ADDITIONAL INFORMATION/COMMENTS
Environmental:			
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> N/A <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See EPC "Zoning Comment Sheet" dated January 18, 2023.
Natural Resources	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Conservation & Environmental Lands Mgmt.	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Check if Applicable: <input checked="" type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Coastal High Hazard Area <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Surface Water Resource Protection Area <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Other _____			
Public Facilities:			
Transportation <input type="checkbox"/> Design Exception Requested <input checked="" type="checkbox"/> Off-site Improvements Required	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Utilities Service Area/ Water & Wastewater <input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Hillsborough County School Board Adequate <input checked="" type="checkbox"/> K-5 <input checked="" type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input checked="" type="checkbox"/> 9-12 <input type="checkbox"/> N/A	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Applicant requests no change to unit count. The School Board completed a review for MM 21-0865. This was an analysis for adequate facilities only and is NOT a determination of school concurrency. A school concurrency review will be issued PRIOR TO preliminary plat or site plan approval. The applicant is not requesting a change of approved entitlements.
Impact/Mobility Fees No comments.			

Comprehensive Plan:

Planning Commission

- Meets Locational Criteria N/A
- Locational Criteria Waiver Requested
- Minimum Density Met N/A

- Inconsistent
- Consistent
- N/A

- Yes
- No

Condition 22 has been recommended to maintain comprehensive plan consistency with FLUE Policy 19.1.

5.0 IMPLEMENTATION RECOMMENDATION

5.1 Compatibility

The site is located west of the Big Bend Road and Simmons Loop intersection and is predominantly surrounded by residential with nearby medical uses, and vacant land. The underlying future land use (FLU) category of the subject parcel is Urban Mixed Use – 20 (UMU-20). The subject area consists of two parcels.

- The property is located just east of Interstate-75 within the Riverview Community Base Planning Area,
- Within the Urban Service Area,
- Residential as well as neighborhood serving uses such as a hospital and a park to the northwest across Big Bend Road.

The request is to modify Major Modification MM 21-0865, associated with two parcels totaling approximately 11.74 acres. The existing MM 21-0865 allows a multi-family development of 211 residential units located on 10.57-acres with a community dog park, located on the 1.17 acres located to the west of the multi-family building. The community dog park will be owned and maintained by the multi-family apartment complex. The application requests no additional variations to Land Development Code Parts 6.06.00 (Landscaping/Buffering). The site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land Development Code, Site Development and Technical Manuals.

The applicant is proposing to modify approved condition 15 (proposed condition no. 17) to lower the minimum throat depth requirement from 150 feet to 60 feet. Given the nature of the proposed change, the applicant submitted a traffic analysis supporting their request; however, given that the proposed entitlements are remaining unchanged, and no access points are proposed to be added or removed, a traditional trip generation and site access analysis was not necessarily required pursuant to the Development Review Procedures Manual (DRPM).

The applicant and County transportation staff coordinated with FDOT staff regarding the proposed changes. Staff notes that the 60-foot throat depth proposal exceeds the minimum requirements pursuant to LDC Sec. 6.04.03.G., which requires a minimum of 50 feet. The applicant submitted an analysis showing the 60-foot queue is anticipated to be sufficient. The applicant has also submitted a conceptual design for the internal access which ensures that entering vehicles are not confronted with vehicles pulling into or backing out of parking spaces and has shown stop-controlled movements on the side approaches which will ensure vehicles entering the project have priority when entering the site. The complete transportation review may be found in the Transportation Agency Review Comment Sheet at the end of this report.

On November 2, 2022, Hillsborough County Environmental Protection Commission (EPC) issued a Request for Additional Information letter for the Wetland Impact project. On January 18, 2023, EPC found that in the site plan's current configuration, a resubmittal is not necessary. If the zoning proposal changes and/or the site plans are altered, EPC staff will need to review the zoning again. This project as submitted is conceptually justified to move forward through the zoning review process, subject to conditions.

Projects that are 10 acres or greater within the Urban Mixed Use-20 future land use classification must demonstrate a mix of land uses in accordance with Comprehensive Plan Policy 19.1. Planning Commission staff reviewed the proposed Minor Modification site plan and conditions. Pursuant to the updated wetland delineation the applicant proposes condition no. 27, requiring concurrent construction of the public dog park and cross access to the dog park to maintain comprehensive plan consistency.

5.2 Recommendation

Based upon the above considerations, staff finds the request is **APPROVABLE, subject to conditions.**

6.0 PROPOSED CONDITIONS

Staff finds the request Approvable, subject to the following conditions listed below, and based on the general site plan submitted February 17, 2023.

1. Development shall be limited to 211 units of multi-family residential, with a 1.17-acre community dog park located on the western portion of the unified site plan.
2. Development shall comply with the following standards:

Minimum building setbacks:	25 feet from Big Bend Road and Simmons Loop Road rights-of-way, measured from the interior edge of right-of-way preservation areas where required
Maximum building height:	58 feet
Maximum impervious area:	70 percent project wide
Vehicular Use Area Buffer:	8-foot buffer along Big Bend Road and Simmons Loop 8-foot buffer along Rear Yard adjacent to folio no. 77730.0200
3. Buffering and screening between uses shall be provided in accordance with the requirements of LDC Section 6.06.06, except as follows:
 - 3.1 No buffering or screening shall be required on the south project boundary where adjacent to PD 11-0415.
 - 3.2 Buffering and screening adjacent to the southern boundary of parcel folio 77730.0100 shall include a 5-foot buffer with type "A" screening.
4. Buffering and screening for off-street vehicular use areas shall be provided in accordance with the requirements of LDC Section 6.06.04.
5. An evaluation of the property by Natural Resources staff has identified a number of mature trees that may include grand oaks. The potential stature of these trees warrants every effort to minimize their removal. The developer is encouraged to consult with Natural Resources staff for design input addressing these trees prior to submittal of initial site development plans.
6. The planting of required trees shall be sensitive to overhead electric utility lines. Trees that exceed a mature overall height of 20 feet shall not be planted within 30 feet of an existing or proposed overhead electric utility line.
7. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
8. The construction and location of any proposed wetland impacts are not approved by this correspondence but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed

in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.

9. Prior to the issuance of any building or land alteration permits or other development, the approved wetland / other surface water (OSW) line must be incorporated into the site plan. The wetland/ OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County Land Development Code (LDC).
10. Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.
- ~~8~~11. This site is located within the Hillsborough County Urban Service Area, therefore the subject property should be served by Hillsborough County Water and Wastewater Service. This does not guarantee water or wastewater service or a point of connection. Developer is responsible for submitting a utility service request at the time of development plan review and will be responsible for any on-site improvements as well as possible off-site improvements.
9. Water distribution system improvements will need to be completed prior to connection to the County's water system. No building permits that would create demand for water service shall be issued until completion by the County of funded Capital Improvement Program projects C32001 - South County Potable Water Repump Station Expansion and C32011 - Potable Water In-Line Booster Pump Station.
- ~~10~~12. As pedestrian cross-access is required between this and the adjacent project to the south, the developer shall construct a minimum a minimum 5-foot wide sidewalk connection between the internal sidewalk network and the existing pedestrian cross-access (sidewalk) stubout, which was previously constructed within folio 77730.0220. Given the need for public access through the project given the proposed publicly accessible dog park, such connection shall be ungated.
- ~~11~~13. The project shall be permitted one (1) right-in/ right-out connection onto Simmons Loop, and one (1) right-in/right-out connection onto Big Bend Rd. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, in the event the right-in/right-out driveway on Big Bend Rd. is being found to cause or contribute to unsafe conditions on Big Bend Rd. or operational issues at the I-75 and Big Bend Rd. interchange, the property owner shall be required (upon written request by Hillsborough County) to propose corrective measures, which could include but shall not be limited to revised onsite operational plans, revised site design, and/or changes to the external roadway/driveway access. Such measures will be subject to the review and approval of Hillsborough County. In the event an agreement on suitable corrective measures cannot be reached, Hillsborough County may require closure of the Big Bend Rd. access, which shall be at the property owners' sole cost and expense.
- ~~12~~14. As generally shown on the PD site plan, the Big Bend Rd. access shall be located a minimum of 400 feet and a maximum of 500 feet west of the Big Bend Rd. and Simmons Loop intersection, and the project access may occur anywhere within this area.
- ~~13~~15. As generally shown on the PD site plan, those portions of the internal project driveway and sidewalk system to Simmons Loop that runs along the southern boundary of folio 77730.0100 shall be considered a Shared Access Facility with folio 77730.0100. Prior to or concurrent with the initial increment of development, the developer shall record in the Official Records of Hillsborough County a construction easement and any other easement necessary to permit the owner/developer of folio 77730.0100 to

construct an utilize the required Shared Access Facility upon (re)development of the adjacent properties without further consultation. The design and location of all connections shall be subject to the review and approval of Hillsborough County.

- ~~14~~16. Since the developer is proposing a publicly accessible dog park which will be privately owned and maintained by the property owner, public access to the property must be ensured. As such, prior to or concurrent with the initial increment of development, the developer shall record in the Official Records of Hillsborough County a public access easement over the sidewalks and driveways which connect the proposed publicly accessible dog park with both vehicular and pedestrian entrances to the project (i.e. Big Bend Rd. and Simmons Loop) as well as the pedestrian cross access connection along the project's southern boundary.
- ~~15~~17. The internal driveway throat for the Big Bend Rd. access shall be free of parking spaces, internal connections or other conflicts for a minimum of distance of ~~150~~ 60 feet.
- ~~16~~18. The developer shall construct the following site access improvements:
 - ~~16~~18.1 The Big Bend. Rd. driveway connection shall be designed with a minimum radius of 50 feet; and,
 - ~~16~~18.2 The developer shall close the existing westbound directional median opening on Big Bend Rd. (located approximately 750 feet west of the intersection of Big Bend Rd. and Simmons Loop).
- ~~17~~19. Notwithstanding anything on the PD site plan or herein these conditions to the contrary, bicycle and pedestrian access may be permitted anywhere along the project boundaries.
20. Consistent with the determination made by the Administrator on 2/14/2023, the developer shall utilize a pervious pavement design (as approved within that determination) for that portion of the internal drive aisle within the wetland setback area.
21. Wetlands or other surface waters are considered Environmentally Sensitive Areas and are subject to Conservation Area and Preservation Area setbacks. A minimum setback must be maintained around these areas which shall be designated on all future plan submittals. Only items explicitly stated in the conditions of approval or items allowed per the LDC may be placed within the wetland setback. Proposed land alterations are restricted within the wetland setback areas.
22. Approval of this petition by Hillsborough County does not constitute a guarantee that Natural Resources approvals/permits necessary for the development, as proposed, will be issued, does not itself serve to justify any impacts to trees, natural plant communities, or wildlife habitat, and does not grant any implied or vested right to environmental approvals.
23. The construction and location of any proposed environmental impacts are not approved by this correspondence, but shall be reviewed by Natural Resources staff through the site and subdivision development plan process pursuant to the Land Development Code.
- ~~18~~24. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.

- ~~19~~25. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- ~~22~~26. Approval of this development order/permit does not constitute a guarantee that there will be public facilities at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.
27. Development will not occur in phases. A boardwalk will be constructed concurrently with the development of the community dog park and the multi-family uses to connect the community dog park to the eastern, multi-family development portion of the site, pursuant to Future Land Use Element Policy 19.1 of the Hillsborough County Comprehensive Plan. The boardwalk will be wide enough to allow utility vehicle access to and from the community dog park for maintenance and emergency purposes.

1.1 SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDNACE WITH HILLSBOROGUH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

Zoning Administrator Sign Off:

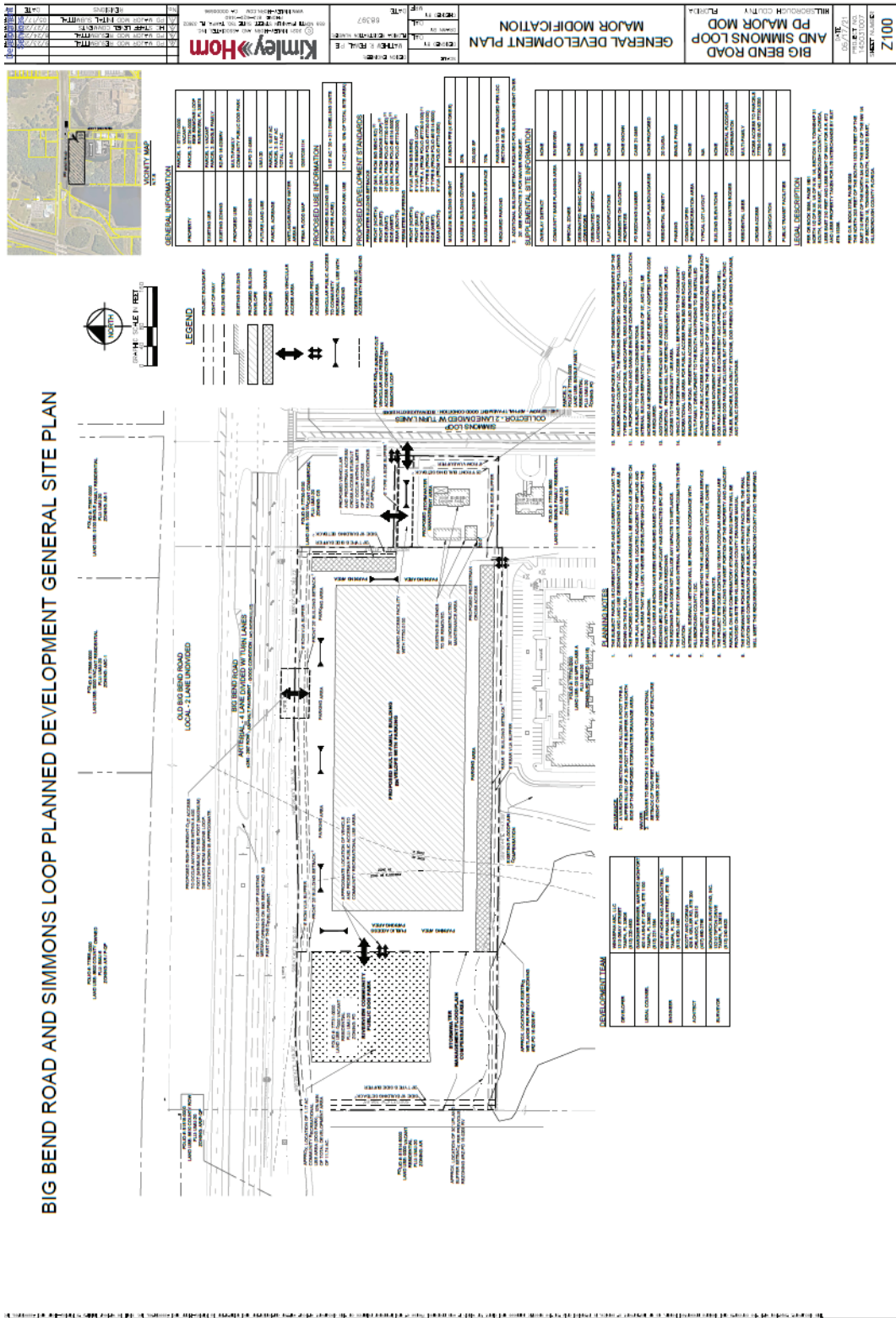
A handwritten signature in black ink, appearing to read 'J. Brian Grady', is written over a faint, semi-transparent watermark that says 'IntegrSign Desktop'.

J. Brian Grady
Thu Feb 23 2023 16:08:20

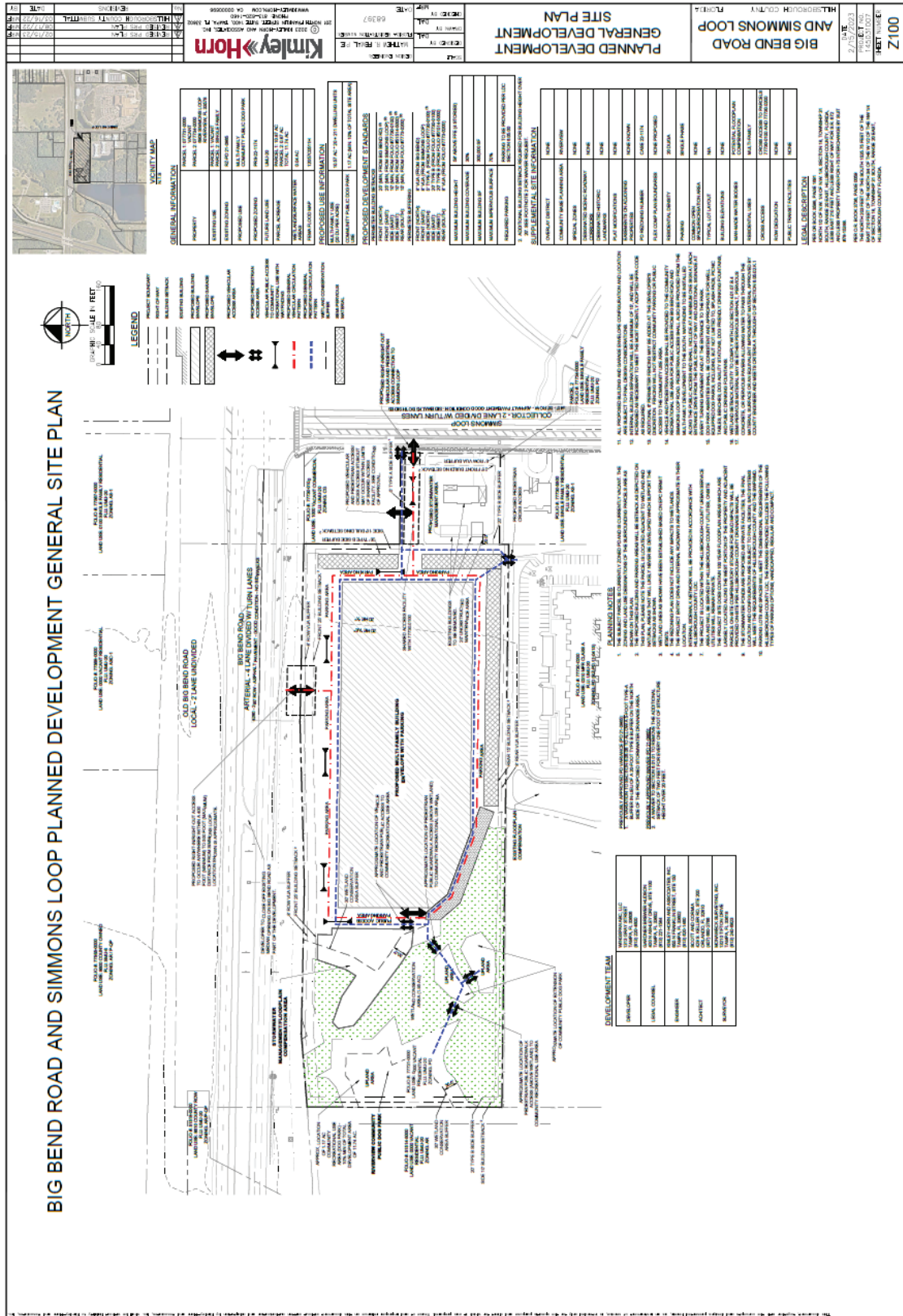
7.0 ADDITIONAL INFORMATION

8.0 Site Plans (Full)

8.1 Approved Site Plan



8.0 Site Plan
 8.2 Proposed Site Plan (Full)



8.0 FULL TRANSPORTATION REPORT

AGENCY REVIEW COMMENT SHEET

TO: Zoning Technician, Development Services Department

DATE: 11/17/2022

REVIEWER: James Ratliff, AICP, PTP

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: RV/ Central

PETITION NO: PRS 22-1174

- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to the listed or attached conditions.
- This agency objects for the reasons set forth below.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Condition

15. The internal driveway throat for the Big Bend Rd. access shall be free of parking spaces, internal connections or other conflicts for a minimum distance of ~~150~~60 feet.

New Condition

- Consistent with the determination made by the Administrator on 2/14/2023, the developer shall utilize a pervious pavement design (as approved within that determination) for that portion of the internal drive aisle within the wetland setback area.

PROJECT SUMMARY

The applicant is requesting minor modification request, also known as a Personal Appearance (PRS), to previously approved PD 16-0209, as was most recently modified via MM 21-0865. The project consists of two parcels totaling +/- 12.34 ac. The PD currently has approvals for up to 211 multi-family dwelling units and +/- 1.17 ac. publicly accessible dog park which will be owned and maintained by the multi-family apartments.

The applicant is proposing to modify approved condition 15 to lower the minimum throat depth requirement from 150 feet to 60 feet. Given the nature of the proposed change, the applicant submitted a traffic analysis supporting their request; however, given that the proposed entitlements are remaining unchanged, and no access points are proposed to be added or removed, a traditional trip generation and site access analysis was not necessarily required pursuant to the Development Review Procedures Manual (DRPM).

The applicant is also proposing a modification of the site plan with respect to the confirmation/location of publicly accessible dog park, wetland boundaries, internal circulation and parking areas, and garage parking areas. Staff discussed worked with the applicant to obtain clarity and/or modify the plan to address certain CPTED issues as well as issues related to the encroachment of the drive aisle into the wetland setback area (as further discussed below).

Staff has included a summary of the trips potentially generated under the existing zoning designations, utilizing a generalized worst-case scenario (and as was presented during MM 21-0865). The information below is largely based on data from the Institute of Transportation Engineer's Trip Generation Manual, 10th Edition; however, the Trip Generation Manual does not include data for a community dog park. As such, the applicant previously studied two Hillsborough County Dog Parks in the p.m. peak hour in order

estimate project traffic for the use. This data was utilized in the calculations below, and a.m. peak hour trips were conservatively assumed to equal p.m. peak hour trips. Average daily traffic was assumed to equal 10 times the peak hour trips. No changes in project trip generation will result from the proposed change.

Zoning, Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
211 Multi-Family Dwelling Units (ITE LUC 221)	1,148	76	93
1.17 ac. Dog Park (Rates by Special Study)	110 (est.)	11 (est.)	11
Subtotal:	1,258	87	104
<i>Less Internal Capture:</i>	<i>N/A</i>	<i>N/A</i>	<i>N/A</i>
<i>Less Pass-By Trips:</i>	<i>-0</i>	<i>-0</i>	<i>-0</i>
Net External Trips:	1,258	87	104

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Big Bend Rd. - Today

Big Bend Rd. is a 4-lane, divided, publicly maintained, arterial roadway characterized by +/- 11-foot wide travel lanes in average condition. The roadway lies within a +/- 257-foot wide right-of-way along the project's frontage. There are +/- 5-foot wide bicycle facilities (on paved shoulders) along both sides of Big Bend Rd. in the vicinity of the proposed project. There are +/- 5-foot wide sidewalks along the south side of Big Bend Rd. east of the proposed project.

Recently a 5th lane (i.e. eastbound to southbound right-turn drop lane) was added to Big Bend Rd. to connect the turn lane at the Simmons Loop intersection with the I-75 offramp to the project's west.

Big Bend Rd. - Future

There are several projects which are in various stages of planning and construction relative to Big Bend Rd., including the widening of Big Bend Rd. between US 41 and US 301 from 4-lanes to 6-lanes, as well as significant improvements to the interchange which are being funded by FDOT and the County to accelerate improvement of the interchange to its ultimate future configuration. CIP project information sheets as well as the latest conceptual rendering showing interchange related improvements have been attached to this report.

Other Area Projects

The Apollo Beach Blvd. (Paseo al Mar Blvd.) Flyover Project has been recently constructed. This project resulted in a 4-lane divided roadway between US 41 and US 301. The Belmont development has constructed Gate Dancer Rd. to its northern project boundary. The Gate Dancer Ext. within two PDs (22-0208 and 20-0311) is a fully developer funded project and has construction plan approval. These projects are anticipated to provide alternative routes throughout the area and significantly reduce traffic on certain segments of Big Bend Rd.

As a part of the FDOT CIGP grant application for the Flyover Project, County staff estimated the reduction in trips on existing area roadways which would result from completion of the Flyover Project and Gate Dancer Extension (collectively referred to as "the Projects"). Staff estimated that construction of the Projects would result in a trip reduction of 16.8% on the segment of Big Bend Rd. between US 41 and I-75, and a trip reduction of 5.7% on Big Bend Rd. between I-75 and US 301. Staff also estimated a 24.9% reduction of trips on 19th Ave. NE west of I-75, and a 10.4% reduction in trips on 19th Ave. NE east of I-75.

Simmons Loop

Simmons Loop is a 2-lane, divided, publicly maintained, collector roadway characterized by +/- 12-foot wide travel lanes in above average condition. The roadway lies within a +/- 95-foot wide right-of-way along the project's frontage. There are +/- 5 to 6-foot wide sidewalks along both sides of Simmons Loop in the vicinity of the proposed project. There are +/- 4-foot wide bicycle lanes along both sides of Simmons Loop in the vicinity of the proposed project.

Simmons Loop, along the project's frontage, is shown on the Hillsborough County Corridor Preservation Plan as a future 2-lane enhanced roadway. The roadway was recently constructed so as to accommodate its ultimate configuration and will allow for future intersection modifications as needed. As such, no additional right-of-way preservation was required.

SITE ACCESS AND CONNECTIVITY

Generally

The existing PD is approved for access to both Big Bend Rd. and Simmons Loop. There is also pedestrian cross access proposed to the south. Public access easements are required over certain project driveways and sidewalks to allow for public access to the privately maintained but publicly accessible dog park which is proposed.

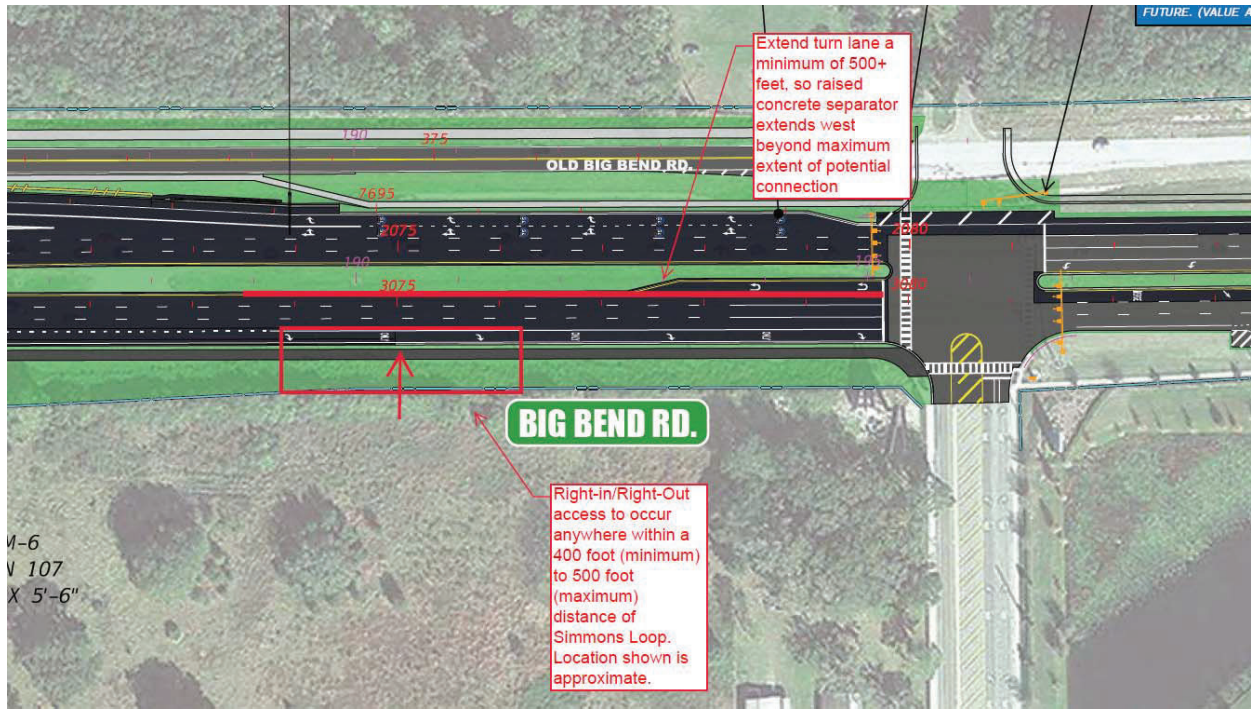
FDOT Coordination

Staff met extensively with County Public Works and Florida Department of Transportation staff during MM 21-0865 regarding the proposed project to work through FDOT concerns regarding the proximity of the proposed access to the interchange. After significant coordination it was agreed that the best solution for project access was to permit a right-in/right-out access to Big Bend Rd. within a 100-foot area located a minimum of 400-feet and a maximum of 500-feet west of the Simmons Loop and Big Bend Rd. intersection.

Given the stage of planning and design efforts for the interchange improvements, FDOT and the County agreed to incorporate improvements which will be needed to allow this access to safely and efficiently operate in proximity to the interchange in the future. Specifically, it was agreed that the County and FDOT would extend the westbound to northbound left turn (presently u-turn) lane a minimum of 500 ft and installation of concrete separator between the u-turn lane and the innermost through lane to avoid future potential conflicts with the right-in/right-out access. The general location and extent of the separator is shown below (although the graphic does not show the required turn lane extension).

It was also agreed that the right turn arrows for the eastbound drop lane which was recently constructed will start further to the west, close to the end of the Bullfrog Creek bridge. The consensus between FDOT and County staff was to start the arrows +/- 200 feet after the end of the ramp (west side of bridge).

FDOT staff also requested certain conditions, which were been included in the PD at their request. Specifically, conditions were added by staff regarding the minimum throat depth at the Big Bend Rd. access, closure of the existing directional median opening, as well as a condition requiring the developer to propose additional improvements and/or close the access should significant operational or safety problems arise in the future. Although no problems were anticipated based upon the agreed upon design, this condition provided FDOT staff with additional assurance that the interchange will continue to operate safety and efficiently once the significant infrastructure investments have been completed and the proposed project developed.



As a part of evaluating this request, the applicant and County coordinated with FDOT staff regarding the proposed changes. Staff notes that the 60-foot throat depth proposal exceeds the minimum requirements pursuant to LDC Sec. 6.04.03.G., which requires a minimum of 50 feet. The applicant submitted an analysis showing the 60-foot queue is anticipated to be sufficient. The applicant has also submitted a conceptual design for the internal access which ensures that entering vehicles are not confronted with vehicles pulling into or backing out of parking spaces and has shown stop-controlled movements on the side approaches which will ensure vehicles entering the project have priority when entering the site. While the graphic is not proposed to be binding (in order to ensure any additional changes necessary to meet site plan requirements can be made as needed), the applicant has committed to ensure that the final design reflects the conceptual layout.

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County staff identified an issue with the reconfigured drive aisle encroaching into the newly added wetland setback areas. Pursuant to Section 6.05.02.K.1. of the LDC, drive aisles for the requested use shall “Shall be constructed with a durable and dustless surface, including but not limited to asphalt, cement, or **equivalent improvement**. No slag, rock, pea gravel or other loose type of material shall be used.” [**emphasis added**]. Transportation Review Section staff understands that Natural Resources staff determined that an encroachment into the setback could be supported if certain pervious materials were used.

Pursuant to the above referenced LDC section, “In making a determination as to the suitability of an equivalent improvement, the Administrator shall find that such improvement:

- a. Provides a safe and permanent surface, suitable for the quantity and quality of traffic expected to use it;
- b. Provides a surface which will accept permanent delineation of parking spaces, aisles, accessways and maneuvering areas;
- c. Provides a surface that will not contribute to erosion or sedimentation, either on-site or off-site; and,
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The applicant submitted an email request to the County Engineer (i.e. the Administrator for above related issues), dated 2/14/2023 for approval of an equivalent pervious improvement, which was found to be

approvable by the County Engineer on 2/15/2023. Staff has proposed a condition which will require the developer to utilize the pervious pavement consistent with that determination, should the PD be approved.

ROADWAY LEVEL OF SERVICE (LOS) INFORMATION

Level of Service (LOS) information is reported below.

Roadway	From	To	LOS Standard	Peak Hour Directional LOS
Big Bend Rd.	I-75 N Ramp	US 301	D	C

Source: Hillsborough County 2020 Level of Service Report.

Ratliff, James

From: Williams, Michael
Sent: Wednesday, February 15, 2023 4:23 PM
To: Addie Clark
Cc: Ratliff, James; Lampkin, Timothy; Tirado, Sheida; Femal, Matt; Megan Smith; PW-CEIntake
Subject: FW: PRS 22-1174: Request for Review of Semi-Pervious Material as Equivalent Improvement
Attachments: Pervious Concrete Detail.pdf; DRAFT PRS General Development Plan_2023.02.13.pdf

Addie – the alternative pavement section in the attached will be considered approved for use should the proposed zoning modifications be approved by the Board of County Commissioners.

If you have any questions, please let me know.

Mike

Michael J. Williams, P.E.
Director, Development Review
County Engineer
Development Services Department

P: (813) 307-1851
M: (813) 614-2190
E: Williamsm@HillsboroughCounty.org
W: HCFLGov.net

Hillsborough County
601 E. Kennedy Blvd., Tampa, FL 33602

[Facebook](#) | [Twitter](#) | [YouTube](#) | [LinkedIn](#) | [HCFL Stay Safe](#)

Please note: All correspondence to or from this office is subject to Florida's Public Records law.

From: Addie Clark <aclark@gardnerbrewer.com>
Sent: Tuesday, February 14, 2023 2:04 PM
To: Williams, Michael <WilliamsM@HillsboroughCounty.ORG>
Cc: Ratliff, James <RatliffJa@hillsboroughcounty.org>; Lampkin, Timothy <LampkinT@hillsboroughcounty.org>; Femal, Matt <Matt.Femal@kimley-horn.com>; Megan Smith <msmith@gardnerbrewer.com>
Subject: PRS 22-1174: Request for Review of Semi-Pervious Material as Equivalent Improvement

External email: Use caution when clicking on links, opening attachments or replying to this email.

Good Afternoon Mike,

We appreciate your review of the request below. The attached draft site plan indicates that the area subject to the Administrative Request is located at the southwest portion of the multi-family area, specifically the southwest drive-aisle which will not include any parking spaces.

Request:

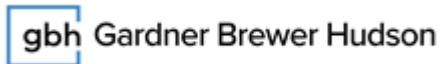
Administrative Request to Section 6.05.02.K (Required Paving) to allow for semi-pervious material as an equivalent improvement within a portion of the 30' wetland setback in accordance with Section 4.01.07.B.4 (Wetlands and Natural Water Bodies Setbacks):

- **Type of Material Proposed:** Pervious Concrete (See Attached Detail) or approved equivalent.
- **Proposed Use:** The semi-pervious area will be approximately 5,800 SF for a proposed drive-aisle without any parking spaces.
- **Justification for Request:** By allowing semi-pervious material within the drive-aisle along the perimeter of the multi-family area adjacent to the wetland, this will ensure continuous circulation and emergency access throughout the development. In doing so, this will result in reducing traffic congestion and driver confusion that may occur with a dead-end roadway within the development. Fire Department access does not allow dead-ends over 150'. The alternative fire truck turn around cannot be constructed due to the large area required which would increase the amount of wetland/buffer impacts.
- **Documentation:** The attached draft rezoning site plan contains a hatched area indicating the proposed limits of the semi-pervious material for the proposed drive-aisle.

Please let us know if you have any questions or if we can provide any additional information for your review.

Thank you so much,

Addie Clark, P.E.



O: (813) 733-0209 | M: (561) 319-4759

E: aclark@gardnerbrewer.com

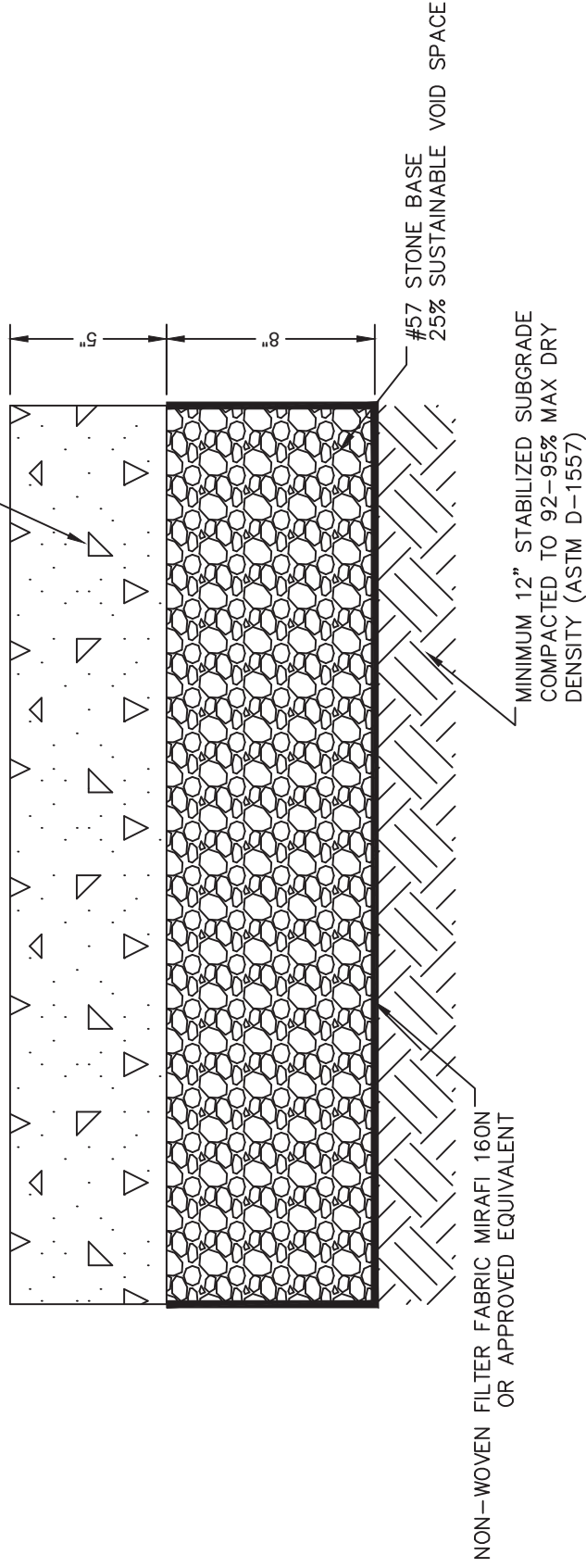
400 N. Ashley Dr., Ste. 1100, Tampa, FL 33602

gardnerbrewer.com

The preceding email message may be confidential or protected by the attorney-client privilege. It is not intended for transmission to, or receipt by, any unauthorized persons. If you have received this message in error, please (i) do not read it, (ii) reply to the sender that you received the message in error, and (iii) erase or destroy the message. Legal advice contained in the preceding message is solely for the benefit of the Gardner Brewer Hudson, P.A. client(s) represented by the Firm in the particular matter that is the subject of this message, and may not be relied upon by any other party.

- NOTE:
1. CONTRACTOR SHALL OBTAIN APPROVAL FROM CEC FOR MIX DESIGN AND PAVEMENT JOINT PLACEMENT PLAN PRIOR TO COMMENCEMENT OF WORK.
 2. ASPHALTIC CONCRETE SHALL MEET FDOT REQUIREMENTS AND PLACEMENT PROCEDURES AS OUTLINED IN THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (LATEST EDITION). THE ASPHALTIC CONCRETE SHALL BE COMPACTED TO A MINIMUM OF 93% (Gmm) OF THE MAXIMUM THEORETICAL SPECIFIC GRAVITY.

CHARGER ENTERPRISES PERVIOUS CONCRETE OR APPROVED EQUIVALENT. GC SHALL SUBMIT PERVIOUS CONCRETE SPECIFICATION TO ENGINEER FOR REVIEW PRIOR TO INSTALLATION.



STANDARD DUTY PERVIOUS CONCRETE SECTION

N.T.S.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Simmons Loop	County Collector - Urban	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Big Bend Rd.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	1,258	87	104
Proposed	1,258	87	104
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North	X	Vehicular & Pedestrian	None	Meets LDC
South		Pedestrian	None	Meets LDC
East	X	Vehicular & Pedestrian	None	Meets LDC
West		None	None	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

**CURRENTLY
APPROVED**

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Approval of the request, subject to the conditions listed, is based on the revised general site plan submitted September 28, 2021.

1. Development shall be limited to 211 units of multi-family residential, with a 1.17-acre community dog park located on the western portion of the unified site plan.
2. Development shall comply with the following standards:

Minimum building setbacks:	25 feet from Big Bend Road and Simmons Loop Road rights-of-way, measured from the interior edge of right-of-way preservation areas where required
Maximum building height:	58 feet
Maximum impervious area:	70 percent project wide
Vehicular Use Area Buffer:	8-foot buffer along Big Bend Road and Simmons Loop 8-foot buffer along Rear Yard adjacent to folio no. 77730.0200
3. Buffering and screening between uses shall be provided in accordance with the requirements of LDC Section 6.06.06, except as follows:
 - 3.1 No buffering or screening shall be required on the south project boundary where adjacent to PD 11-0415.
 - 3.2 Buffering and screening adjacent to the southern boundary of parcel folio 77730.0100 shall include a 5-foot buffer with type "A" screening.
4. Buffering and screening for off-street vehicular use areas shall be provided in accordance with the requirements of LDC Section 6.06.04.
5. An evaluation of the property by Natural Resources staff has identified a number of mature trees that may include grand oaks. The potential stature of these trees warrants every effort to minimize their removal. The developer is encouraged to consult with Natural Resources staff for design input addressing these trees prior to submittal of initial site development plans.
6. The planting of required trees shall be sensitive to overhead electric utility lines. Trees that exceed a mature overall height of 20 feet shall not be planted within 30 feet of an existing or proposed overhead electric utility line.
7. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
8. This site is located within the Hillsborough County Urban Service Area, therefore the subject property should be served by Hillsborough County Water and Wastewater Service. This does not guarantee water or wastewater service or a point of connection. Developer is responsible for submitting a utility service request at the time of development plan review and will be responsible for any on-site improvements as well as possible off-site improvements.

9. Water distribution system improvements will need to be completed prior to connection to the County's water system. No building permits that would create demand for water service shall be issued until completion by the County of funded Capital Improvement Program projects C32001 - South County Potable Water Repump Station Expansion and C32011 - Potable Water In-Line Booster Pump Station.
10. As pedestrian cross-access is required between this and the adjacent project to the south, the developer shall construct a minimum a minimum 5-foot wide sidewalk connection between the internal sidewalk network and the existing pedestrian cross-access (sidewalk) stubout, which was previously constructed within folio 77730.0220. Given the need for public access through the project given the proposed publicly accessible dog park, such connection shall be un gated.
11. The project shall be permitted one (1) right-in/ right-out connection onto Simmons Loop, and one (1) right-in/right-out connection onto Big Bend Rd. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, in the event the right-in/right-out driveway on Big Bend Rd. is being found to cause or contribute to unsafe conditions on Big Bend Rd. or operational issues at the I-75 and Big Bend Rd. interchange, the property owner shall be required (upon written request by Hillsborough County) to propose corrective measures, which could include but shall not be limited to revised onsite operational plans, revised site design, and/or changes to the external roadway/driveway access. Such measures will be subject to the review and approval of Hillsborough County. In the event an agreement on suitable corrective measures cannot be reached, Hillsborough County may require closure of the Big Bend Rd. access, which shall be at the property owners' sole cost and expense.
12. As generally shown on the PD site plan, the Big Bend Rd. access shall be located a minimum of 400 feet and a maximum of 500 feet west of the Big Bend Rd. and Simmons Loop intersection, and the project access may occur anywhere within this area.
13. As generally shown on the PD site plan, those portions of the internal project driveway and sidewalk system to Simmons Loop that runs along the southern boundary of folio 77730.0100 shall be considered a Shared Access Facility with folio 77730.0100. Prior to or concurrent with the initial increment of development, the developer shall record in the Official Records of Hillsborough County a construction easement and any other easement necessary to permit the owner/developer of folio 77730.0100 to construct an utilize the required Shared Access Facility upon (re)development of the adjacent properties without further consultation. The design and location of all connections shall be subject to the review and approval of Hillsborough County.
14. Since the developer is proposing a publicly accessible dog park which will be privately owned and maintained by the property owner, public access to the property must be ensured. As such, prior to or concurrent with the initial increment of development, the developer shall record in the Official Records of Hillsborough County a public access easement over the sidewalks and driveways which connect the proposed publicly accessible dog park with both vehicular and pedestrian entrances to the project (i.e. Big Bend Rd. and Simmons Loop) as well as the pedestrian cross access connection along the project's southern boundary.
15. The internal driveway throat for the Big Bend Rd. access shall be free of parking spaces, internal connections or other conflicts for a minimum of distance of 150 feet.
16. The developer shall construct the following site access improvements:

- 16.1 The Big Bend. Rd. driveway connection shall be designed with a minimum radius of 50 feet; and,
- 16.2 The developer shall close the existing westbound directional median opening on Big Bend Rd. (located approximately 750 feet west of the intersection of Big Bend Rd. and Simmons Loop).
17. Notwithstanding anything on the PD site plan or herein these conditions to the contrary, bicycle and pedestrian access may be permitted anywhere along the project boundaries.
18. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
19. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
20. Approval of this development order/permit does not constitute a guarantee that there will be public facilities at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.

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AGENCY COMMENTS

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AGENCY REVIEW COMMENT SHEET

TO: Zoning Technician, Development Services Department

DATE: 11/17/2022

REVIEWER: James Ratliff, AICP, PTP

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: RV/ Central

PETITION NO: PRS 22-1174

- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to the listed or attached conditions.
- This agency objects for the reasons set forth below.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Condition

15. The internal driveway throat for the Big Bend Rd. access shall be free of parking spaces, internal connections or other conflicts for a minimum distance of ~~150~~60 feet.

New Condition

- Consistent with the determination made by the Administrator on 2/14/2023, the developer shall utilize a pervious pavement design (as approved within that determination) for that portion of the internal drive aisle within the wetland setback area.

PROJECT SUMMARY

The applicant is requesting minor modification request, also known as a Personal Appearance (PRS), to previously approved PD 16-0209, as was most recently modified via MM 21-0865. The project consists of two parcels totaling +/- 12.34 ac. The PD currently has approvals for up to 211 multi-family dwelling units and +/- 1.17 ac. publicly accessible dog park which will be owned and maintained by the multi-family apartments.

The applicant is proposing to modify approved condition 15 to lower the minimum throat depth requirement from 150 feet to 60 feet. Given the nature of the proposed change, the applicant submitted a traffic analysis supporting their request; however, given that the proposed entitlements are remaining unchanged, and no access points are proposed to be added or removed, a traditional trip generation and site access analysis was not necessarily required pursuant to the Development Review Procedures Manual (DRPM).

The applicant is also proposing a modification of the site plan with respect to the confirmation/location of publicly accessible dog park, wetland boundaries, internal circulation and parking areas, and garage parking areas. Staff discussed worked with the applicant to obtain clarity and/or modify the plan to address certain CPTED issues as well as issues related to the encroachment of the drive aisle into the wetland setback area (as further discussed below).

Staff has included a summary of the trips potentially generated under the existing zoning designations, utilizing a generalized worst-case scenario (and as was presented during MM 21-0865). The information below is largely based on data from the Institute of Transportation Engineer's Trip Generation Manual, 10th Edition; however, the Trip Generation Manual does not include data for a community dog park. As such, the applicant previously studied two Hillsborough County Dog Parks in the p.m. peak hour in order

estimate project traffic for the use. This data was utilized in the calculations below, and a.m. peak hour trips were conservatively assumed to equal p.m. peak hour trips. Average daily traffic was assumed to equal 10 times the peak hour trips. No changes in project trip generation will result from the proposed change.

Zoning, Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
211 Multi-Family Dwelling Units (ITE LUC 221)	1,148	76	93
1.17 ac. Dog Park (Rates by Special Study)	110 (est.)	11 (est.)	11
Subtotal:	1,258	87	104
<i>Less Internal Capture:</i>	<i>N/A</i>	<i>N/A</i>	<i>N/A</i>
<i>Less Pass-By Trips:</i>	<i>-0</i>	<i>-0</i>	<i>-0</i>
Net External Trips:	1,258	87	104

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Big Bend Rd. - Today

Big Bend Rd. is a 4-lane, divided, publicly maintained, arterial roadway characterized by +/- 11-foot wide travel lanes in average condition. The roadway lies within a +/- 257-foot wide right-of-way along the project's frontage. There are +/- 5-foot wide bicycle facilities (on paved shoulders) along both sides of Big Bend Rd. in the vicinity of the proposed project. There are +/- 5-foot wide sidewalks along the south side of Big Bend Rd. east of the proposed project.

Recently a 5th lane (i.e. eastbound to southbound right-turn drop lane) was added to Big Bend Rd. to connect the turn lane at the Simmons Loop intersection with the I-75 offramp to the project's west.

Big Bend Rd. - Future

There are several projects which are in various stages of planning and construction relative to Big Bend Rd., including the widening of Big Bend Rd. between US 41 and US 301 from 4-lanes to 6-lanes, as well as significant improvements to the interchange which are being funded by FDOT and the County to accelerate improvement of the interchange to its ultimate future configuration. CIP project information sheets as well as the latest conceptual rendering showing interchange related improvements have been attached to this report.

Other Area Projects

The Apollo Beach Blvd. (Paseo al Mar Blvd.) Flyover Project has been recently constructed. This project resulted in a 4-lane divided roadway between US 41 and US 301. The Belmont development has constructed Gate Dancer Rd. to its northern project boundary. The Gate Dancer Ext. within two PDs (22-0208 and 20-0311) is a fully developer funded project and has construction plan approval. These projects are anticipated to provide alternative routes throughout the area and significantly reduce traffic on certain segments of Big Bend Rd.

As a part of the FDOT CIGP grant application for the Flyover Project, County staff estimated the reduction in trips on existing area roadways which would result from completion of the Flyover Project and Gate Dancer Extension (collectively referred to as "the Projects"). Staff estimated that construction of the Projects would result in a trip reduction of 16.8% on the segment of Big Bend Rd. between US 41 and I-75, and a trip reduction of 5.7% on Big Bend Rd. between I-75 and US 301. Staff also estimated a 24.9% reduction of trips on 19th Ave. NE west of I-75, and a 10.4% reduction in trips on 19th Ave. NE east of I-75.

Simmons Loop

Simmons Loop is a 2-lane, divided, publicly maintained, collector roadway characterized by +/- 12-foot wide travel lanes in above average condition. The roadway lies within a +/- 95-foot wide right-of-way along the project's frontage. There are +/- 5 to 6-foot wide sidewalks along both sides of Simmons Loop in the vicinity of the proposed project. There are +/- 4-foot wide bicycle lanes along both sides of Simmons Loop in the vicinity of the proposed project.

Simmons Loop, along the project's frontage, is shown on the Hillsborough County Corridor Preservation Plan as a future 2-lane enhanced roadway. The roadway was recently constructed so as to accommodate its ultimate configuration and will allow for future intersection modifications as needed. As such, no additional right-of-way preservation was required.

SITE ACCESS AND CONNECTIVITY

Generally

The existing PD is approved for access to both Big Bend Rd. and Simmons Loop. There is also pedestrian cross access proposed to the south. Public access easements are required over certain project driveways and sidewalks to allow for public access to the privately maintained but publicly accessible dog park which is proposed.

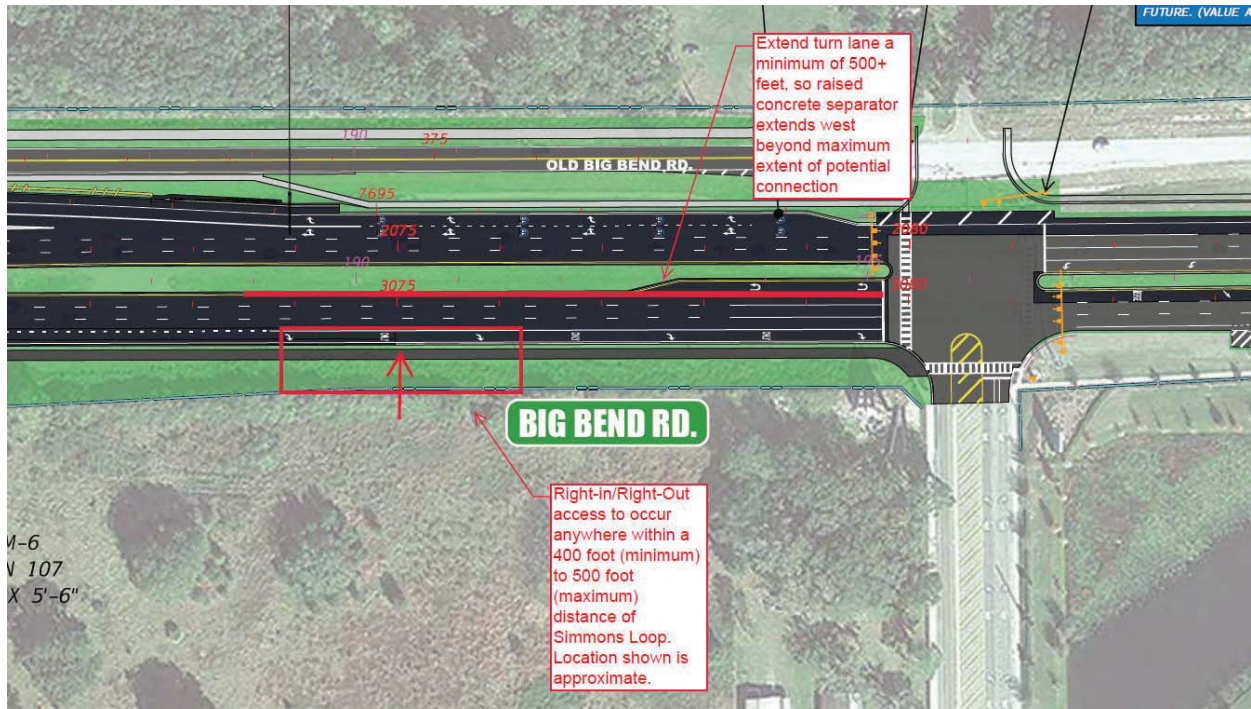
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Big Bend Rd.	I-75 N Ramp	US 301	D	C

Source: Hillsborough County 2020 Level of Service Report.

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If you have any questions, please let me know.

Mike

Michael J. Williams, P.E.
Director, Development Review
County Engineer
Development Services Department

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Hillsborough County
601 E. Kennedy Blvd., Tampa, FL 33602

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From: Addie Clark <aclark@gardnerbrewer.com>
Sent: Tuesday, February 14, 2023 2:04 PM
To: Williams, Michael <WilliamsM@HillsboroughCounty.ORG>
Cc: Ratliff, James <RatliffJa@hillsboroughcounty.org>; Lampkin, Timothy <LampkinT@hillsboroughcounty.org>; Femal, Matt <Matt.Femal@kimley-horn.com>; Megan Smith <msmith@gardnerbrewer.com>
Subject: PRS 22-1174: Request for Review of Semi-Pervious Material as Equivalent Improvement

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Good Afternoon Mike,

We appreciate your review of the request below. The attached draft site plan indicates that the area subject to the Administrative Request is located at the southwest portion of the multi-family area, specifically the southwest drive-aisle which will not include any parking spaces.

Request:

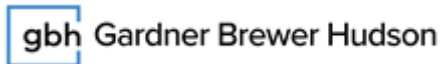
Administrative Request to Section 6.05.02.K (Required Paving) to allow for semi-pervious material as an equivalent improvement within a portion of the 30' wetland setback in accordance with Section 4.01.07.B.4 (Wetlands and Natural Water Bodies Setbacks):

- **Type of Material Proposed:** Pervious Concrete (See Attached Detail) or approved equivalent.
- **Proposed Use:** The semi-pervious area will be approximately 5,800 SF for a proposed drive-aisle without any parking spaces.
- **Justification for Request:** By allowing semi-pervious material within the drive-aisle along the perimeter of the multi-family area adjacent to the wetland, this will ensure continuous circulation and emergency access throughout the development. In doing so, this will result in reducing traffic congestion and driver confusion that may occur with a dead-end roadway within the development. Fire Department access does not allow dead-ends over 150'. The alternative fire truck turn around cannot be constructed due to the large area required which would increase the amount of wetland/buffer impacts.
- **Documentation:** The attached draft rezoning site plan contains a hatched area indicating the proposed limits of the semi-pervious material for the proposed drive-aisle.

Please let us know if you have any questions or if we can provide any additional information for your review.

Thank you so much,

Addie Clark, P.E.



O: (813) 733-0209 | M: (561) 319-4759

E: aclark@gardnerbrewer.com

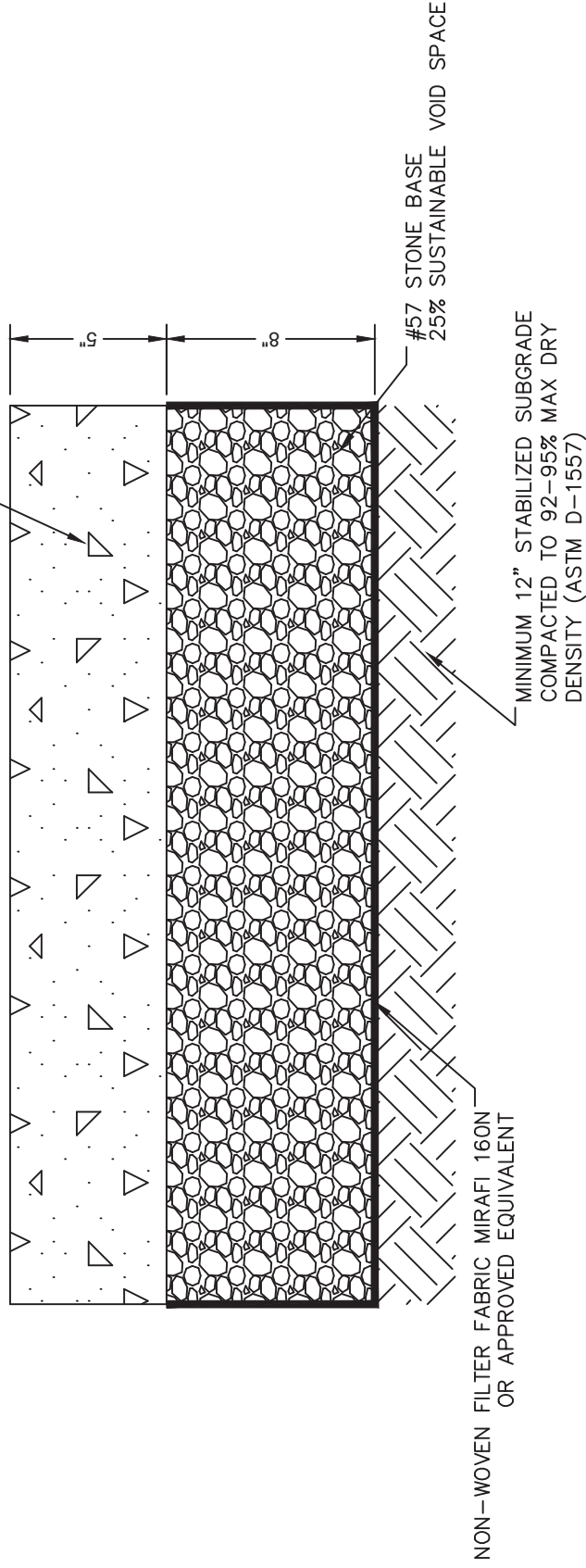
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- NOTE:
1. CONTRACTOR SHALL OBTAIN APPROVAL FROM CEC FOR MIX DESIGN AND PAVEMENT JOINT PLACEMENT PLAN PRIOR TO COMMENCEMENT OF WORK.
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CHARGER ENTERPRISES PERVIOUS CONCRETE OR APPROVED EQUIVALENT. GC SHALL SUBMIT PERVIOUS CONCRETE SPECIFICATION TO ENGINEER FOR REVIEW PRIOR TO INSTALLATION.



STANDARD DUTY PERVIOUS CONCRETE SECTION

N.T.S.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Simmons Loop	County Collector - Urban	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Big Bend Rd.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	1,258	87	104
Proposed	1,258	87	104
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North	X	Vehicular & Pedestrian	None	Meets LDC
South		Pedestrian	None	Meets LDC
East	X	Vehicular & Pedestrian	None	Meets LDC
West		None	None	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

From: [Williams, Michael](#)
To: [Addie Clark](#)
Cc: [Ratliff, James](#); [Lampkin, Timothy](#); [Tirado, Sheida](#); [Femal, Matt](#); [Megan Smith](#); [PW-CEIntake](#)
Subject: FW: PRS 22-1174: Request for Review of Semi-Pervious Material as Equivalent Improvement
Date: Wednesday, February 15, 2023 4:23:52 PM
Attachments: [image003.png](#)
[Pervious Concrete Detail.pdf](#)
[DRAFT PRS General Development Plan 2023.02.13.pdf](#)
[image001.png](#)

Addie – the alternative pavement section in the attached will be considered approved for use should the proposed zoning modifications be approved by the Board of County Commissioners.

If you have any questions, please let me know.

Mike

Michael J. Williams, P.E.
Director, Development Review
County Engineer
Development Services Department

P: (813) 307-1851
M: (813) 614-2190
E: Williamsm@HillsboroughCounty.org
W: HCFLGov.net

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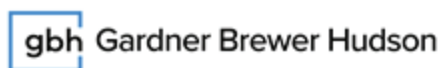
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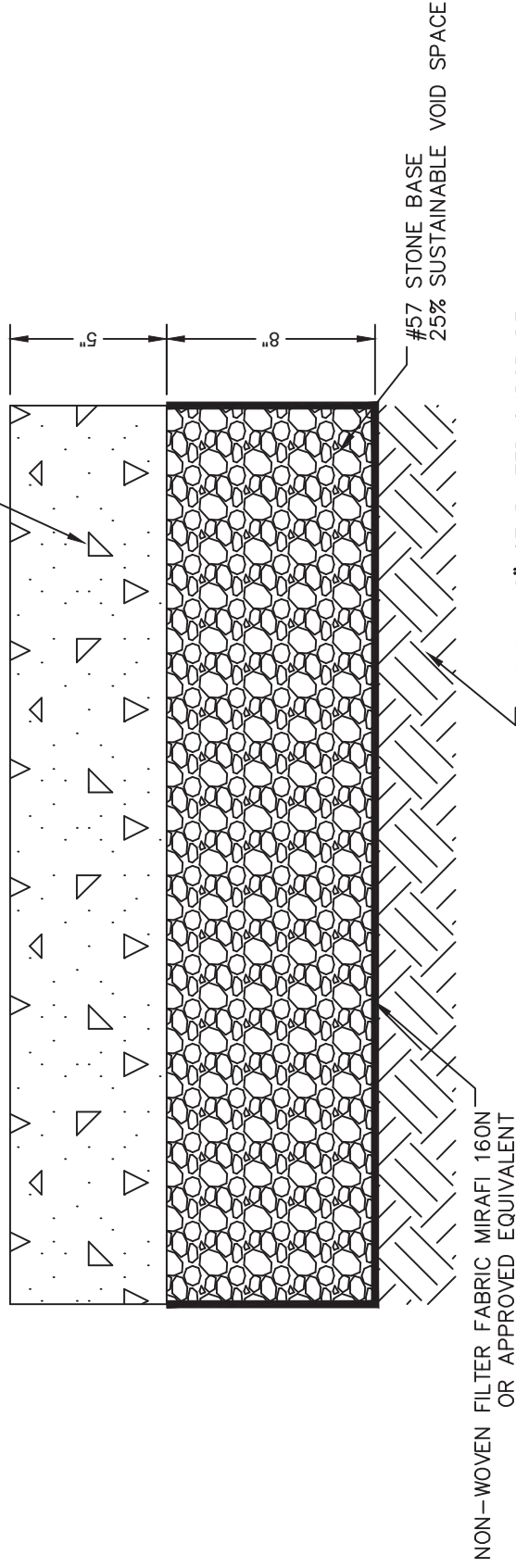
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STANDARD DUTY PERVIOUS CONCRETE SECTION

N.T.S.

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 Sterlin Woodard, P.E. WETLANDS DIVISION

AGENCY COMMENT SHEET

REZONING	
<p>HEARING DATE: March 7, 2023</p> <p>PETITION NO.: 22-1174</p> <p>EPC REVIEWER: Abbie Weeks</p> <p>CONTACT INFORMATION: (813) 627-2600 X1101</p> <p>EMAIL: weeksa@epchc.org</p>	<p>COMMENT DATE: January 18, 2023</p> <p>PROPERTY ADDRESS: Southwest corner of Big Bend Road & Simmons Loop</p> <p>FOLIO #: 0777310000, 0777340000</p> <p>STR: 18-31S-20E</p>
<p>REQUESTED ZONING: Minor Modification to PD</p>	
FINDINGS	
<p>WETLANDS PRESENT</p> <p>SITE INSPECTION DATE</p> <p>WETLAND LINE VALIDITY</p> <p>WETLANDS VERIFICATION (AERIAL PHOTO, SOILS SURVEY, EPC FILES)</p>	<p>YES</p> <p>05/19/2022</p> <p>Under SWFWMD review</p> <p>Wetlands are located in the northern and western portions of the property</p>
<p>The EPC Wetlands Division has reviewed the proposed rezoning. In the site plan's current configuration, a resubmittal is not necessary. If the zoning proposal changes and/or the site plans are altered, EPC staff will need to review the zoning again. This project as submitted is conceptually justified to move forward through the zoning review process as long as the following conditions are included:</p> <ul style="list-style-type: none"> • Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals. • The construction and location of any proposed wetland impacts are not approved by this correspondence but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property. • Prior to the issuance of any building or land alteration permits or other development, the approved wetland / other surface water (OSW) line must be incorporated into the site plan. The 	

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wetland/ OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County Land Development Code (LDC).

- Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.

INFORMATIONAL COMMENTS:

The following specific comments are made for informational purposes only and to provide guidance as to the EPC review process. However, future EPC staff review is not limited to the following, regardless of the obviousness of the concern as raised by the general site plan and EPC staff may identify other legitimate concerns at any time prior to final project approval.

- The subject property contains wetland/OSW areas, which have not been delineated. Knowledge of the actual extent of the wetland and OSW are necessary in order to verify the avoidance of wetland impacts pursuant to Chapter 1-11. Prior to the issuance of any building or land alteration permits or other development, the wetlands/OSWs must be field delineated in their entirety by EPC staff or Southwest Florida Water Management District staff (SWFWMD) and the wetland line surveyed. Once delineated, surveys must be submitted for review and formal approval by EPC staff.
- The site plan depicts wetland impacts that have not been authorized by the Executive Director of the EPC. The wetland impacts are indicated for the proposed multi-family development and dog park. Chapter 1-11, prohibits wetland impacts unless they are necessary for reasonable use of the property. Staff of the EPC recommends that this requirement be taken into account during the earliest stages of site design so that wetland impacts are avoided or minimized to the greatest extent possible. The size, location, and configuration of the wetlands may result in requirements to reduce or reconfigure the improvements depicted on the plan. If you choose to proceed with the wetland impacts depicted on the plan, a separate wetland impact/mitigation proposal and appropriate fees must be submitted to this agency for review.
- The Hillsborough County Land Development Code (LDC) defines wetlands and other surface waters as Environmentally Sensitive Areas. Pursuant to the LDC, wetlands and other surface waters are further defined as Conservation Areas or Preservation Areas and these areas must be designated as such on all development plans and plats. A minimum setback must be maintained around the Conservation/Preservation Area and the setback line must also be shown on all future plan submittals.
- Any activity interfering with the integrity of wetland(s) or other surface water(s), such as clearing, excavating, draining or filling, without written authorization from the Executive Director of the EPC or authorized agent, pursuant to Section 1-11.07, would be a violation of Section 17 of the Environmental Protection Act of Hillsborough County, Chapter 84-446, and of Chapter 1-11.

Aow/

ec: landuse@gardnerbrewer.com
lampkint@hillsboroughcounty.org

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AGENCY COMMENT SHEET

TO: **Zoning/Code Administration, Development Services Department**

FROM: **Reviewer:** Carla Shelton Knight **Date:** February 20, 2023

Agency: Natural Resources **Petition #:** 22-1174

- This agency has **no comment**
- This agency has **no objections**
- This agency has **no objections, subject to listed or attached conditions**
- This agency **objects, based on the listed or attached issues.**

1. Wetlands or other surface waters are considered Environmentally Sensitive Areas and are subject to Conservation Area and Preservation Area setbacks. A minimum setback must be maintained around these areas which shall be designated on all future plan submittals. **Only items explicitly stated in the conditions of approval or items allowed per the LDC may be placed within the wetland setback.** Proposed land alterations are restricted within the wetland setback areas.
2. Approval of this petition by Hillsborough County does not constitute a guarantee that Natural Resources approvals/permits necessary for the development, as proposed, will be issued, does not itself serve to justify any impacts to trees, natural plant communities, or wildlife habitat, and does not grant any implied or vested right to environmental approvals.
3. The construction and location of any proposed environmental impacts are not approved by this correspondence, but shall be reviewed by Natural Resources staff through the site and subdivision development plan process pursuant to the Land Development Code.
4. If the notes and/or graphics on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.

**WATER RESOURCE SERVICES
REZONING REVIEW COMMENT SHEET: WATER & WASTEWATER**

PETITION NO.: PRS22-1174 REVIEWED BY: Randy Rochelle DATE: 8/3/2022

FOLIO NO.: 77731.0000 & 77734.0000

WATER

- The property lies within the _____ Water Service Area. The applicant should contact the provider to determine the availability of water service.

- A 8 inch water main exists (adjacent to the site), (approximately 100 feet from the site) and is located east of the subject property within the east Right-of-Way of Simmons Loop. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.

- Water distribution system improvements will need to be completed prior to connection to the County's water system. The improvements include two funded CIP projects that are currently under construction, C32001 - South County Potable Water Repump Station Expansion and C32011 - Potable Water In-Line Booster Pump Station and will need to be completed by the County prior to issuance of any building permits that will create additional demand on the system.

WASTEWATER

- The property lies within the _____ Wastewater Service Area. The applicant should contact the provider to determine the availability of wastewater service.

- A 4 inch wastewater force main exists (adjacent to the site), (approximately _____ feet from the site) and is located within the west Right-of-Way of Simmons Loop. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.

- Wastewater collection system improvements will need to be completed prior to connection to the County's wastewater system. The improvements include _____ and will need to be completed by the _____ prior to issuance of any building permits that will create additional demand on the system.

COMMENTS: The subject rezoning includes parcels that are within the Urban Service Area and would require connection to the County's potable water systems.

Statement of Record

The South County service area (generally south of the Alafia River) has seen significant customer growth over the recent past. As new customers are added to the system there is an increased demand for potable water that is causing delivery issues during certain periods of the year. The greatest demand for water occurs during the spring dry season, generally the months of March through May. During the dry season of 2021 the Water Resources Department was challenged to deliver water to the southern portions of the service area to meet customer expectations for pressure and flow. While Levels of Service per the Comprehensive Plan were met, customers complained of very low pressure during early morning hours. Efforts to increase flow and pressure to the south resulted in unacceptably high pressures in the north portions of the service area. The Florida Plumbing Code limits household pressure to 80 psi to prevent damage to plumbing and possible injury due to system failure. The Department had to balance the operational challenges of customer demand in the south with over pressurization in the north, and as a result, water pressure and flow in the South County service area remained unsatisfactory during the dry period of 2021.

As a result of demand challenges, the Department initiated several projects to improve pressure and flow to the south area. Two projects currently under construction CIP C32001 - South County Potable Water Repump Station Expansion and CIP C32011 - Potable Water In-Line Booster Pump will increase the delivery pressure to customers.

These projects are scheduled to be completed and operational prior to the 2022 dry season, and must demonstrate improved water delivery through the highest demand periods before additional connections to the system can be recommended.

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