

PD Modification Application:

MM 23-0414

Zoning Hearing Master Date: 09/18/23

BOCC Land Use Meeting Date: 11/07/23



**Hillsborough
County Florida**

Development Services Department

1.0 APPLICATION SUMMARY

Applicant: Riverview Investment Group, LLC

FLU Category: SMU-6, CMU-12

Service Area: Urban

Site Acreage: +/- 5.6 acres

Community
Plan Area: Riverview

Overlay: None



Introduction Summary:

PD 06-0848 was approved in 2006 to allow (1) maximum of 351 MF residential units on 30.36 acres; (2) a maximum of 60,980-square-feet (“sf”) for Commercial Neighborhood zoning uses on 5.6 acres; (3) a maximum of 10,890-square-feet of Business Professional Office uses; and (4) a maximum of 32,670-square-feet, 150 room hotel in 3 acres, with a maximum height of 60 feet (5 stories).

MM 07-1378 approved an increase to the square footage of the hotel use from 32,670 sf to 65,340 sf.

PRS 08-0816 modified transportation conditions and increased the maximum square feet for the hotel from 65,340 sf to 68,880 sf, reduced the number of hotel rooms from 150 to 125 rooms, and increased the max. height of the hotel from 60 feet (5stories) to 74 feet (6 stories).

PRS 19-0778 modified certain transportation conditions, without changing the entitlements.

The applicant is requesting a major modification to the previously approved Plan Development (06-0848), as modified, generally as follows:

- (1) to expand the hotel’s square footage from 68,880 sf to 81,000 sf, through the use of Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel; and
- (2) to reduce the required parking spaces for the hotel from 138 to 125 spaces.

Existing Approval(s):	Proposed Modification(s):
1. Maximum of 68,880 sf hotel	1. Increase hotel to 81,000 sf
2. 138 hotel parking spaces	2. Reduce the hotel parking spaces to 125.
3. Maximum hotel height of 74 feet (6 stories)	3. No change.

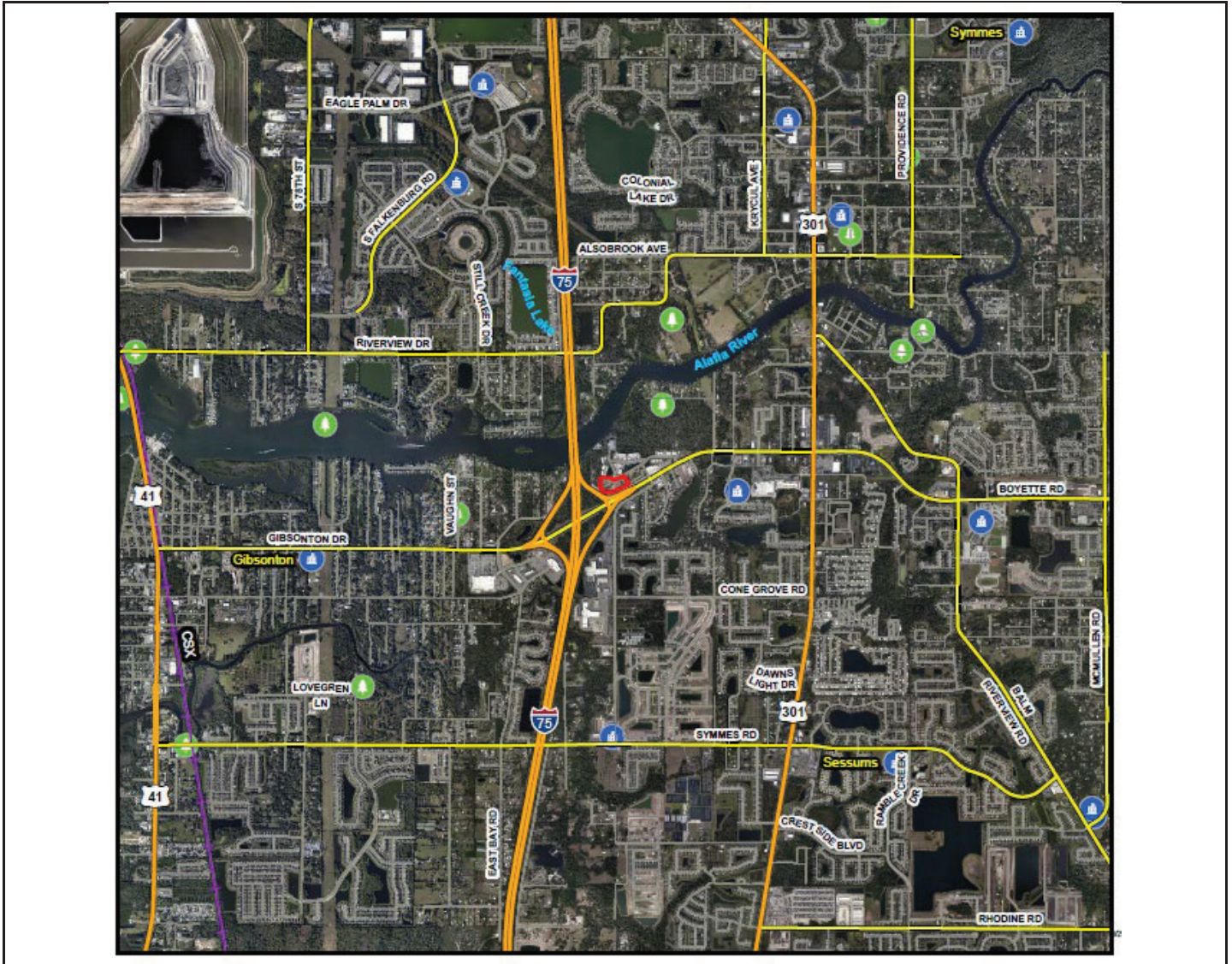
Additional Information:	
PD Variation(s):	LDC Part 6.05.00 (Parking/Loading)
Waiver(s) to the Land Development Code:	None requested.

Planning Commission Recommendation:
Consistent

Development Services Recommendation:
Approvable, subject to proposed conditions

2.0 LAND USE MAP SET AND SUMMARY DATA

2.1 Vicinity Map

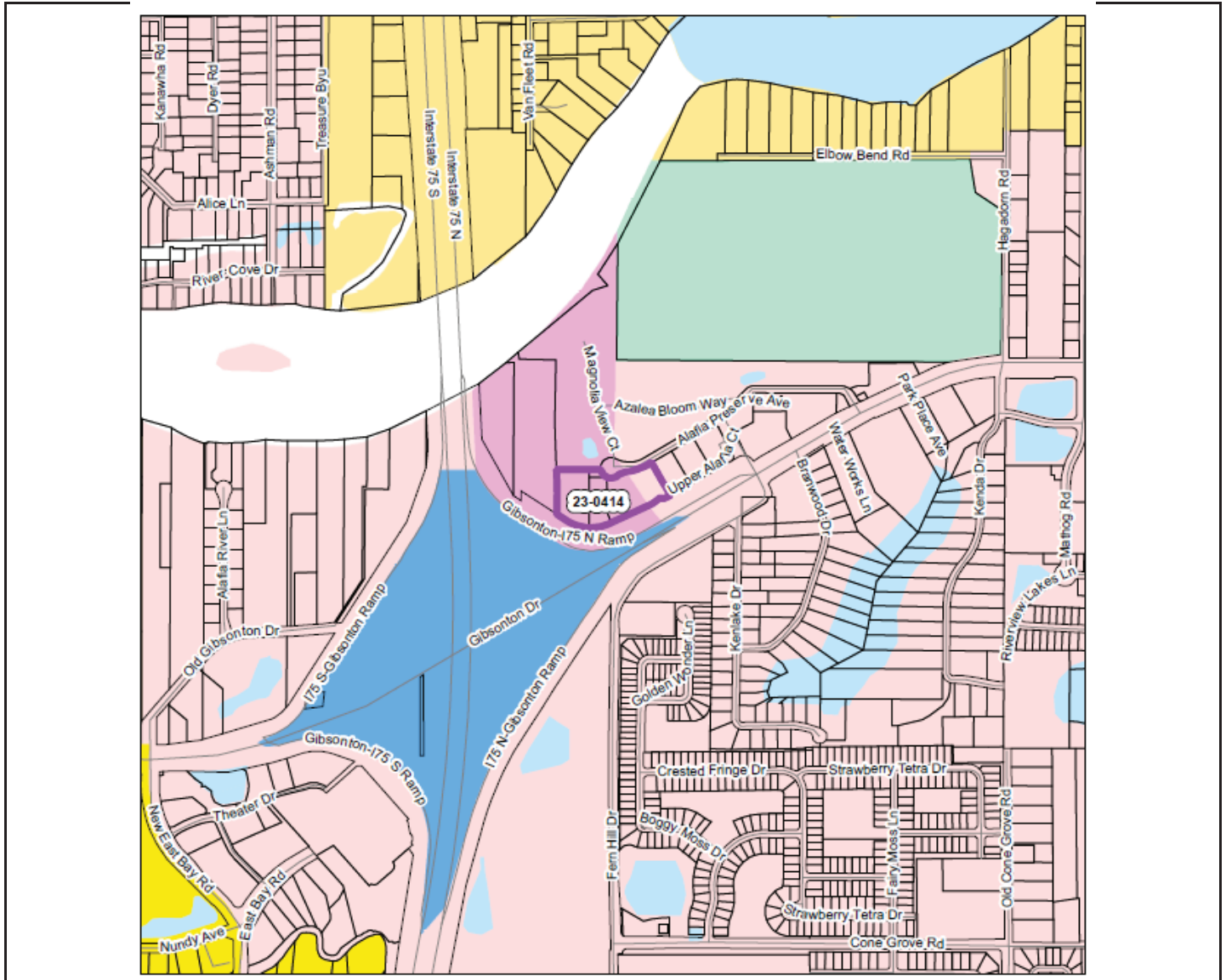


Context of Surrounding Area:

The subject property is located in the northeastern corner of Interstate 75 and Gibsonton Dr. The PD is approved for Multifamily and Commercial Neighborhood Uses. Existing land uses within the area include the multi-family residential development within the same overall PD, located to the immediate north of the portion of the site subject of this request.

2.0 LAND USE MAP SET AND SUMMARY DATA

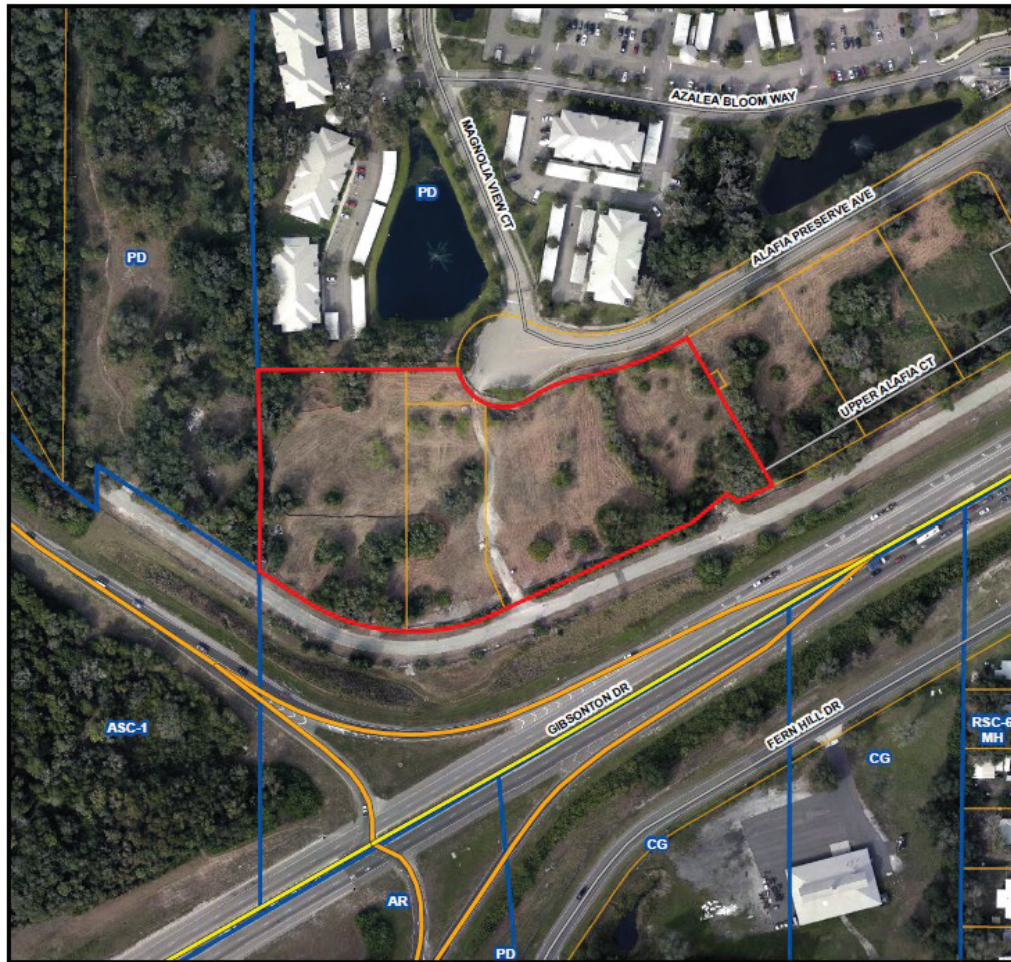
2.2 Future Land Use Map



Subject Site Future Land Use Category:	SMU-6, CMU-12
Maximum Density/F.A.R.:	SMU-6: 6 du per ac. /0.35 FAR CMU-12: 12 du per ac. / 0.5 FAR
Typical Uses:	SMU-6: Typical uses include residential, suburban commercial, offices, research parks, light industrial, multi-purpose, clustered residential, mixed-use. CMU-12: Typical uses include residential, community retail commercial, offices, research park, light industrial, multi-purpose, clustered residential, mixed-use.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Immediate Area Map

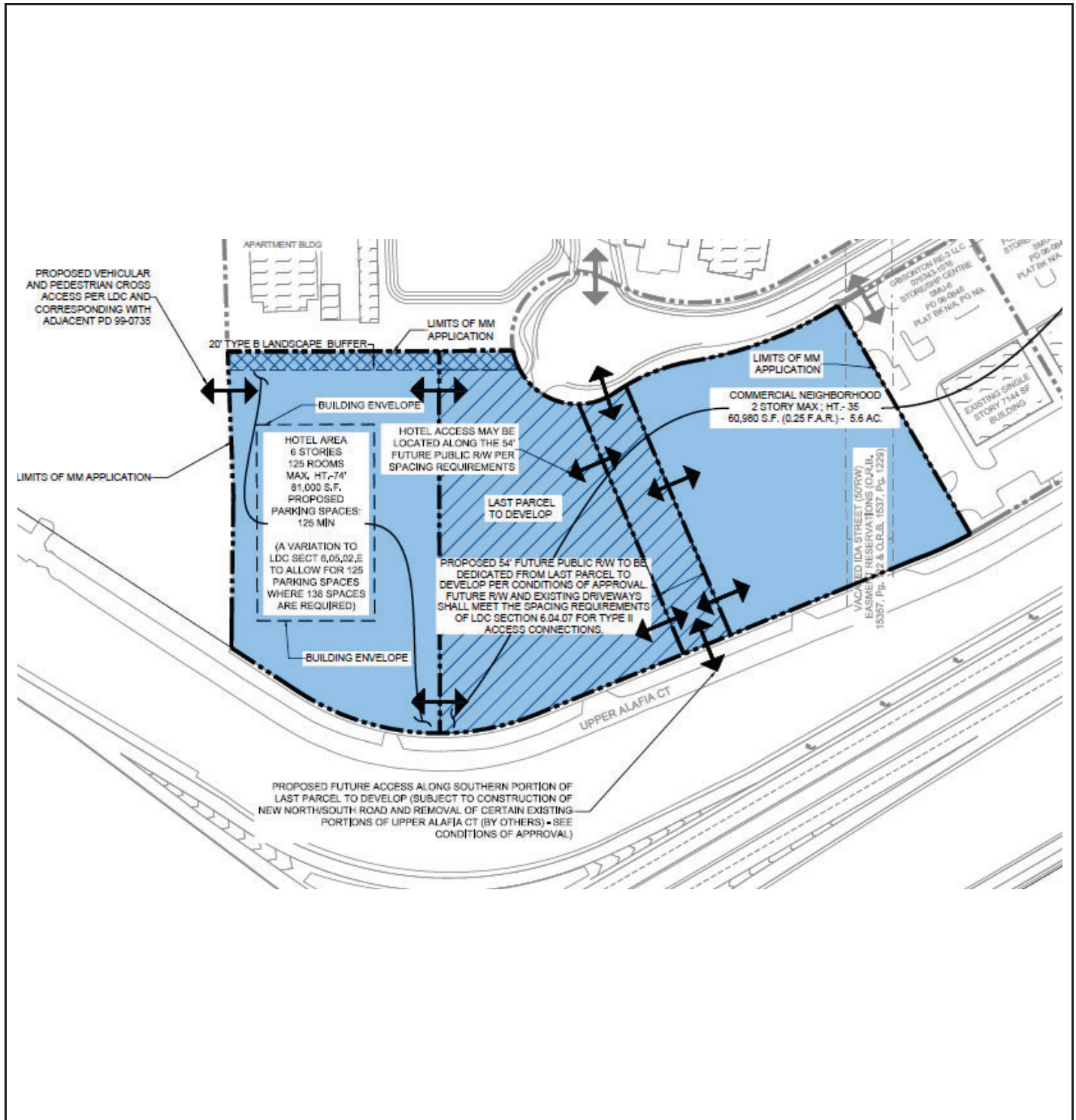


Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	PD 06-0848	351 MF residential units	Multi-family residential	Multi-family residential
South	Commercial General (CG)	Maximum 0.27 FAR	Uses allowed per Sec. 2.02.02 including commercial, offices.	Office building
East	PD 06-0848	Max. 60,980 sf CN; Max. 10,890 sf BPO	Neighborhood commercial/Office, Professional	Commercial strip center
West	PD 99-0735	Maximum 0.50 FAR	159 Dry Boat Storage, Max. 100 RV storage, 1,300 sf res., office, & retail for boat & RV maintenance, and 70-room hotel.	Vacant and boat ramp

2.0 LAND USE MAP SET AND SUMMARY DATA

2.5 Proposed Site Plan (partial provided below for size and orientation purposes. See Section 8.2 for full site plan)



3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibson Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.

4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY

INFORMATION/REVIEWING AGENCY				
Environmental:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Natural Resources	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Check if Applicable: <ul style="list-style-type: none"> <input type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area <input type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Other 				
Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Transportation				
<input type="checkbox"/> Design Exc./Adm. Variance Requested	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See "Transportation Summary Report"
<input checked="" type="checkbox"/> Off-site Improvements Provided				
Service Area/ Water & Wastewater				
<input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
<input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace				
Hillsborough County School Board				
Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A				
Impact/Mobility Fees				
Hotel (Per room (Mobility and Parks)) (Per 1,000 s.f. (Fire)) Mobility: \$4,168 * 0 = \$0 Parks: \$1,327 * 0 = \$0 Fire: \$313 * 12.12 = \$3,793.56 Urban Mobility, South Fire - Hotel Expansion from 68,880 to 81,000 (12,120 sq ft) - no additional rooms				

ZHM HEARING DATE: September 18, 2023
 BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

Comprehensive Plan:	Comments Received	Findings	Conditions Requested	Additional Information/Comments
Planning Commission <input checked="" type="checkbox"/> Meets Locational Criteria <input type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input type="checkbox"/> N/A <input type="checkbox"/> Density Bonus Requested <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	

5.0 IMPLEMENTATION RECOMMENDATIONS

5.1 Compatibility

The subject site is currently undeveloped and located at the northeastern quadrant of Interstate 75 and Gibsonton Drive. The site is surrounded on the north and east by other properties within the same PD. To the immediate west of the subject site is PD 99-0735, which is approved for up to 159 Dry Boat Storage spaces, 100 RV storage spaces, 1,300 sf residential, office, and retail for boat and RV maintenance, and a 70-room hotel. South of the subject site is right-of-way for Interstate 75 North and Gibsonton Road. Directly south across Gibsonton Road is commercial zoned property developed with an office building. Further south past the Commercial General is single-family residential and southeast across Gibsonton Drive are mobile homes. The site is located in the Urban Service Area and within the limits of the Riverview Community Plan.

The applicant's request is to expand the hotel buildable square footage to allow up to 81,000 square feet, through the use of a Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel, and to reduce the parking spaces required for the hotel use from 138 to 125 pursuant to a PD Variation for Parking. The nonresidential square footage for Commercial Neighborhood uses is nominally increased from 60,707 to 60,980. To address these increases and account for the additional hotel square footage. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The scale of the proposed development only increases the square footage of the hotel while maintaining the number of hotel rooms at 125. The applicant states, "The hotel floor area increase is requested to accommodate larger extended stay hotel type rooms. The parking reduction is sought to reflect current market conditions and is supported by the Institute of Transportation Engineer's Parking Manual."

The applicant does not request any variations to Land Development Code Parts 6.06.00 (Landscaping/Buffering). The applicant is required to have a 20 ft. buffer with Type "B" screening adjacent to the multi-family development located to the north of the hotel, which the applicant is providing, as shown on the site plan. This was also previously provided and is not a change.

Transportation PD Variation for Parking

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project's hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project's 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms). LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces (138 x .05 equals 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

The uses proposed are unchanged and remain compatible. Based on the adjacent zonings and uses identified above in the report, staff finds the proposed PD zoning district compatible with the existing zoning districts and development pattern in the area. The subject site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land Development Code. The proposed Planned Development

would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated Hillsborough County Comprehensive Plan.

5.2 Recommendation

Based on the above, staff finds the request approvable.

6.0 PROPOSED CONDITIONS

- Prior to PD Site Plan Certification, the developer shall revise the Site Plan to:
 - Under “Proposed Uses” update “352” to “351” Multifamily Units;
 - Under “Available Entitlement Calculation” update Total Units from “352” to “351”;
 - Under “Utilized Entitlement Calculation, Residential Density” update “352” to “351”;
 - Show the proper intersection configuration (i.e. the raised directional median) at the intersection of Gibsonton Dr. and Alafia Preserve Ave.);
 - Modify the label reading “PROPOSED 54’ FUTURE PUBLIC R/W TO BE DEDICATED FROM LAST PARCEL TO DEVELOP PER CONDITIONS OF APPROVAL. FUTURE R/W AND EXISTING DRIVEWAYS SHALL MEET THE SPACING REQUIREMENTS OF LDC SECTION 6.04.07 FOR TYPE II ACCESS CONNECTIONS.” to instead read “PROPOSED 54’ PUBLIC RIGHT-OF-WAY DEDICATION AND CONVEYANCE – SEE CONDITIONS OF APPROVAL.”

Approval-Approval of the request, subject to the conditions listed below, is based on the general site plan submitted August 28, 2023.

1. The multi-use development shall be limited as follows:

1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)

1.2 The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.

~~1.2~~ 1.3 The project shall be permitted a maximum of ~~60,707~~ 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.

~~1.3~~ 1.4 The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-0) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.

~~1.4~~ 1.5 The project shall be permitted a maximum of ~~68,880~~ 81,000 square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General

(CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).

- 1.5 1.6 Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
- 1.6 1.7 Location of order boxes for the drive-through facility shall be a minimum of 120 feet from the Residential Multi-family property line internal to the mixed use project as shown on the General Site Plan. Additionally, a solid fence or wall shall not be required to be placed between the site used for a drive-through facility and the Multi-family property. Buffering and Screening requirements for the vehicular use areas shall be in accordance with LDC Section 6.06.00.
2. The project may be permitted a maximum of 351 multi-family dwelling units, a ~~68,880~~ 81,000 square foot hotel, ~~60,707~~ 60,980 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.
3. Cross access shall be provided between all outparcels and Alafia Preserve Avenue ~~the major internal roadway~~. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.
4. The applicant shall show the ability to provide cross access to the western property as shown on the site plan. If any of the adjacent properties are developed by the same developer/owner, then cross access must be provided.
5. A temporary access easement shall remain over vacated portions of Ida Street until such time as a permanent easement is approved by Hillsborough County. A permanent paved easement approved by Hillsborough County shall provide access from the site to the western portion of the ELAPP preserve.
6. The following conditions are required access improvements to Gibsonton Drive:
 - 6.1 The Gibsonton Drive and Fem Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to ~~FOOT~~ FDOT approval.
 - 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right- in/right-out. [THIS CONDITION HAS BEEN SATISFIED].
 - 6.3 The site currently ~~has access to~~ abuts, but does not have access to, the ~~FOOT~~ FDOT frontage road that runs along Gibsonton Drive. ~~FOOT~~ FDOT has indicated that the portion of the frontage road east of the proposed 54-foot-wide public right-of-way and continuing to the Fem

Hill Drive Ext. access may will be restricted/modified removed in the future, once a new public roadway segment is constructed within the referenced right-of-way. Nothing herein this condition shall be construed as requiring the developer of the subject PD to construct such public roadway facility.

7. Gibsonton Drive and ~~Fem~~ Fern Hill Drive has been signaled. As a result, the connection from Old Gibsonton Drive to Fem Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive (i.e., the frontage road) currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:

7.1 ~~Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue~~ Developer shall dedicate and convey Fem Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. ~~The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and~~

Dedication shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan,
- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties, or
- c. September 12, 2025.

The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations.

7.2 To provide the alternative connection for Old Gibsonton Dr. (i.e., the frontage road) to the Fem Hill Drive ~~Extensions~~ Extension via Alafia Preserve Avenue, the Developer shall dedicate and convey a fifty-four (54) foot right-of-way within the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan, or

- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located ~~within the area labelled "Proposed Location of Future ROW"~~ on as generally shown on the Site Plan at the discretion of Developer consistent with applicable transportation technical and access management standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

8. Prior to or concurrent with the next increment of development, The the developer will provide a bus bay, and transit accessory pad including, shelter, seating, trash receptacle, and bicycle rack for existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.
9. Notwithstanding anything on the Site Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
10. Construction access shall be limited to those locations shown on PD site plan which are also proposed vehicular access connections. The developer shall include a note in each site/construction plan submittal which indicates same.
11. Parking shall be provided in accordance with Section 6.05 of the Hillsborough County Land Development Code. Notwithstanding the above, the project's hotel uses shall be permitted to provide parking at a rate of 1.0 parking space per room. Additionally, the developer will be required to construct 6 bicycle parking spaces which meet the location and design criteria found within Sec. 6.05.02.P. of the LDC.
- ~~9.~~ 12. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
- ~~10.~~ 13. Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.
- ~~11.~~ 14. A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to

Construction Site Development Review approval.

- ~~12.~~ 15. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise . References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
- ~~13.~~ 16. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- ~~14.~~ 17. Within 90 days of approval by the Hillsborough County Board of County Commissioners, the applicant shall submit to the Development Services Department a revised General Development Plan for certification which conforms the notes and graphic of the plan to the conditions outlined above and the Land Development Code (LDC). Subsequent to certification of the plan, if it is determined the certified plan does not accurately reflect the conditions of approval or requirements of the LDC, said plan will be deemed invalid and certification of the revised plan will be required.
- 18. In accordance with LDC Section 5.03.07.C, the certified PD general site plan for the 5.6-acre area subject to MM 23-0414 shall expire for the internal transportation network and external access points, as well as for any conditions related to the internal transportation network and external access points, if site construction plans, or equivalent thereof, have not been approved for all or part of the subject Planned Development within 5 years of the effective date of the PD unless an extension is granted as provided in the LDC. Upon expiration, re-certification of the PD General Site Plan shall be required in accordance with provisions set forth in LDC Section 5.03.07.C

ZHM HEARING DATE: September 18, 2023
BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

Zoning Administrator Sign Off:



J. Brian Grady
Mon Sep 11 2023 08:05:32

SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS

9.0 FULL TRANSPORTATION REPORT (see following pages)

AGENCY REVIEW COMMENT SHEET

TO: ZONING TECHNICIAN, Development Services Department

DATE: 09/09/2023

Revised: 9/11/2023

REVIEWER: James Ratliff, AICP, PTP

AGENCY/DEPT: Transportation

PLANNING SECTOR/AREA: South/ RV

PETITION NO: MM 23-0414

- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to listed or attached conditions.
- This agency objects, based on the listed or attached grounds.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Conditions

1. The multi-use development shall be limited as follows:
 - 1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary
	40 feet from ELAPP property
	50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2 The project shall be permitted a maximum of 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.
 - 1.4 The project shall be permitted a maximum ~~68,880~~[81,000](#) square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General (CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).

3. Cross access shall be provided between all outparcels and Alafia Preserve Ave.~~the major internal roadway~~. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.

[Transportation Review Section staff is proposing to modify this condition for clarity.]

6. The following conditions ~~are~~address required access improvements to Gibsonton Drive:
 - 6.1 The Gibsonton Drive and Fem Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to ~~FOOT~~FDOT approval.
 - 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right-in/right-out. [THIS CONDITION HAS BEEN SATISFIED].
 - 6.3 The site currently ~~has access to~~abuts, but does not have access to, the ~~FOOT~~FDOT frontage road that runs along Gibsonton Drive. ~~FOOT~~FDOT has indicated that the portion of the frontage road east of the proposed 54-foot-wide public right-of-way and continuing to the Fern Hill Dr. Ext. access may will be restricted/modified removed in the future, once a new public roadway segment is constructed within the referenced right-of-way. Nothing herein this condition shall be construed as requiring the developer of the subject PD to construct such public roadway facility.

[Transportation Review Section staff is proposing to modify this condition for clarity, to better reflect the FDOT and County's intentions/status of area improvements, and more accurately describe the planned future configuration of transportation facilities within the area.]

7. Gibsonton Drive and Fern Hill Drive has been signalized. As a result, the connection from Old Gibsonton Drive to Fern Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive (i.e. the frontage road) currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:

- 7.1 ~~Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue,~~ Developer shall dedicate and convey Fern Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. ~~The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and~~

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan; or,

- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties; or,
- c. September 12, 2025.

The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations.

- 7.2 To provide the alternative connection for Old Gibsonton Dr. (i.e. the frontage road) to the Fern Hill Drive ~~Extensions~~Extension via Alafia Preserve Avenue, the Developer shall dedicate and convey a fifty-four (54) foot right-of-way within the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan, or
- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located ~~within the area labeled "Proposed Location of Future ROW" on~~ as generally shown on the Site Plan, at the discretion of Developer consistent with applicable transportation technical and access management standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

[Transportation Review Section staff is proposing to modify this condition for clarity, to comport with the applicant's request, and to provide certainty for how and when these facilities will be provided.]

8. Prior to concurrent with the next increment of development, ~~The~~ the developer will provide a bus bay and ~~;~~ transit accessory pad including; shelter, seating, trash receptacle, and bicycle rack for the existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.

[Transportation Review Section staff is proposing to modify this condition for clarity, to make it clear when the required improvements are expected to be constructed, and to reflect the language of required transit improvements and appurtenances pursuant to Sections 6.02.17 and 6.03.09 of the LDC.]

New Conditions

- Notwithstanding anything on the Site Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
- Construction access shall be limited to those locations shown on PD site plan which are also proposed vehicular access connections. The developer shall include a note in each site/construction plan submittal which indicates same.
- Parking shall be provided in accordance with Section 6.05 of the Hillsborough County Land Development Code. Notwithstanding the above, the project's hotel uses shall be permitted to provide parking at a rate of 1.0 parking space per room. Additionally, the developer will be required to construct 6 bicycle parking spaces which meet the location and design criteria found within Sec. 6.05.02.P. of the LDC.

Other Conditions

- Prior to PD Site Plan Certification, the developer shall revise the Site Plan to:
 - Show the proper intersection configuration (i.e. the raised directional median) at the intersection of Gibsonton Dr. and Alafia Preserve Ave.);
 - Modify the label reading "PROPOSED 54' FUTURE PUBLIC R/W TO BE DEDICATED FROM LAST PARCEL TO DEVELOP PER CONDITIONS OF APPROVAL. FUTURE R/W AND EXISTING DRIVEWAYS SHALL MEET THE SPACING REQUIREMENTS OF LDC SECTION 6.04.07 FOR TYPE II ACCESS CONNECTIONS." to instead read "PROPOSED 54' PUBLIC RIGHT-OF-WAY DEDICATION AND CONVEYANCE – SEE CONDITIONS OF APPROVAL."

PROJECT OVERVIEW AND TRIP GENERATION

The applicant is requesting a Major Modification to a +/- 5.6 ac. portion of a +/- 39.96 ac. previously approved Planned Development (PD) #06-0848, and as most recently modified via PRS 19-0778. The existing PD is approved for 351 multi-family single-family dwelling units, 60,707 s.f. of Commercial Neighborhood (CN) uses, and 10,890 s.f. of Business Professional Office (BPO) uses, and a 68,880 s.f. hotel with a maximum of 125 rooms. The applicant is proposing to increase the allowable square-footage of hotel uses from 68,880 to 81,000 s.f. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

Given the above, and that there are no changes in proposed access configurations, staff did not require a trip generation and site access analysis to process this request, consistent with the Development Review Procedures Manual (DRPM). Staff prepared the below analysis, comparing trips potentially generally by development under the approved and proposed zoning, utilized a generalized worst-case scenario. Data provided below is based on information from the Institute of Transportation Engineer's Trip Generation Manual, 11th Edition.

Existing Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
60,980 s.f. Shopping Plaza (40-150k) (ITE LUC 821)	5,762	215	587
352 Multi-Family Dwelling Units (ITE LUC 220)	2,332	132	172
10,890 Medical Office Uses (ITE LUC 720)	392	33	41
125 Room Hotel (ITE LUC 310)	998	55	65
Subtotal:	9,484	435	865

Proposed Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
60,980 s.f. Shopping Plaza (40-150k) (ITE LUC 821)	5,762	215	587
352 Multi-Family Dwelling Units (ITE LUC 220)	2,332	132	172
10,890 Medical Office Uses (ITE LUC 720)	392	33	41
125 Room Hotel (ITE LUC 310)	998	55	65
Subtotal:	9,484	435	865

Difference:

	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
	No Change	No Change	No Change

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Alafia Preserve Ave. is a 2-lane, undivided, privately maintained, substandard, local roadway. The roadway is characterized by 11-foot-wide travel lanes in average condition. The roadway lies within a +/- 55-foot wide right-of-way. There are +/- 5-foot-wide sidewalks along portions of the roadway. There are +/- 5-foot-wide bicycle facilities along portions of the roadway.

The Fern Hill Dr. Ext. is a 2-lane, divided, privately maintained, substandard, local roadway. The roadway is characterized by 15-foot-wide travel lanes in above average condition. The roadway lies within a +/- 100-foot-wide right-of-way. There are +/- 6-foot-wide sidewalks along both sides of the roadway. There are no bicycle facilities along this segment of the roadway.

SITE ACCESS AND CIRCULATION

No changes to existing approved access or circulation are proposed.

CHANGES TO RIGHT-OF-WAY DEDICATION REQUIREMENTS

Staff notes that the project is currently in violation of its PD zoning conditions (see existing condition 7.1), due to Alafia Preserve Ave. and newly constructed Fern Hill Rd. extension not having been dedicated and conveyed to the County in accordance with current requirements. The applicant has proposed modifications which will cure that violation. Specifically, the applicant is proposing to modify the conditions to provide flexibility in the timing and location of the 54-foot-wide right-of-way dedication and conveyance area (i.e. for the lands needed to accommodate the new north/south public roadway necessary to allow for removal of certain portions of the existing frontage road). They are also proposing modifications to the condition governing the timing of the requirement to dedication and convey Alafia Preserve Ave. and Fern Hill Dr. Staff has reviewed these condition changes with the County Engineer and supports the proposed revisions offered by staff, hereinabove. Staff notes that conversations between the County and applicant regarding what data, analysis, and/or improvements may be required to both the Fern Hill Dr. Ext. and Alafia Preserve Ave., prior to the County being able to accept those facilities for ownership and maintenance, are ongoing.

REQUESTED PD VARIATION – PARKING

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project’s hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project’s 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms).

The 5th Edition of the Institute of Transportation Engineer’s Parking Generation Manual provides data indicating a range of rates between 0.43 and 1.47 of the 22 hotels studied. They also provide an 85th Percentile rate of 0.99 parking spaces per room, with a 95% confidence interval of 0.83 on the high end.

Staff has reviewed the ITE data and recommends approval the request, subject to conditions. LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces (138 x .05 = 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

ROADWAY LEVEL OF SERVICE (LOS) INFORMATION

LOS information for adjacent roadway segments are provided below.

Roadway	From	To	LOS Standard	Peak Hour Directional LOS
Gibson Dr.	I-75	US 301	D	C

Source: Hillsborough County 2020 Level of Service Report.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibsonton Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input checked="" type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

COUNTY OF HILLSBOROUGH

**RECOMMENDATION OF THE
LAND USE HEARING OFFICER**

APPLICATION NUMBER: MM 23-0414

DATE OF HEARING: September 18, 2023

APPLICANT: Riverview Hotel Investment Group, LLC

PETITION REQUEST: The Major Modification request is to modify PD 06-0848

LOCATION: 140 feet South of the Intersection of Alafia Preserve Ave. and Magnolia View Court

SIZE OF PROPERTY: 5.6 acres, m.o.l.

EXISTING ZONING DISTRICT: PD 06-0848

FUTURE LAND USE CATEGORY: CMU-12 and SMU-6

SERVICE AREA: Urban

COMMUNITY PLAN: Riverview

DEVELOPMENT REVIEW STAFF REPORT

***Note:** Formatting issues prevented the entire Development Services Department staff report from being copied into the Hearing Master's Recommendation. Therefore, please refer to the Development Services Department web site for the complete staff report.

1.0 APPLICATION SUMMARY



Applicant: Riverview Investment Group, LLC

FLU Category: SMU-6, CMU-12

Service Area: Urban

Site Acreage: +/- 5.6 acres

Community Plan Area: Riverview

Overlay: None

Introduction Summary:

PD 06-0848 was approved in 2006 to allow (1) maximum of 351 MF residential units on 30.36 acres; (2) a maximum of 60,980-square-foot ("sf") for Commercial Neighborhood zoning uses on 5.6 acres; (3) a maximum of 10,890- square-foot of Business Professional Office uses; and (4) a maximum of 32,670-square-foot, 150 room hotel in 3 acres, with a maximum height of 60 feet (5 stories).

MM 07-1378 approved an increase to the square footage of the hotel use from 32,670 sf to 65,340 sf.

PRS 08-0816 modified transportation conditions and increased the maximum square feet for the hotel from 65,340 sf to 68,880 sf, reduced the number of

hotel rooms from 150 to 125 rooms, and increased the max. height of the hotel from 60 feet (5stories) to 74 feet (6 stories).

PRS 19-0778 modified certain transportation conditions, without changing the entitlements.

The applicant is requesting a major modification to the previously approved Plan Development (06-0848), as modified, generally as follows:

(1) to expand the hotel's square footage from 68,880 sf to 81,000 sf, through the use of Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel; and

(2) to reduce the required parking spaces for the hotel from 138 to 125 spaces.

Existing Approval(s):

1. Maximum of 68,880 sf hotel
2. 138 hotel parking spaces
3. Maximum hotel height of 74 feet (6 stories)

Additional Information:

PD Variation(s):

Waiver(s) to the Land Development Code:

Proposed Modification(s):

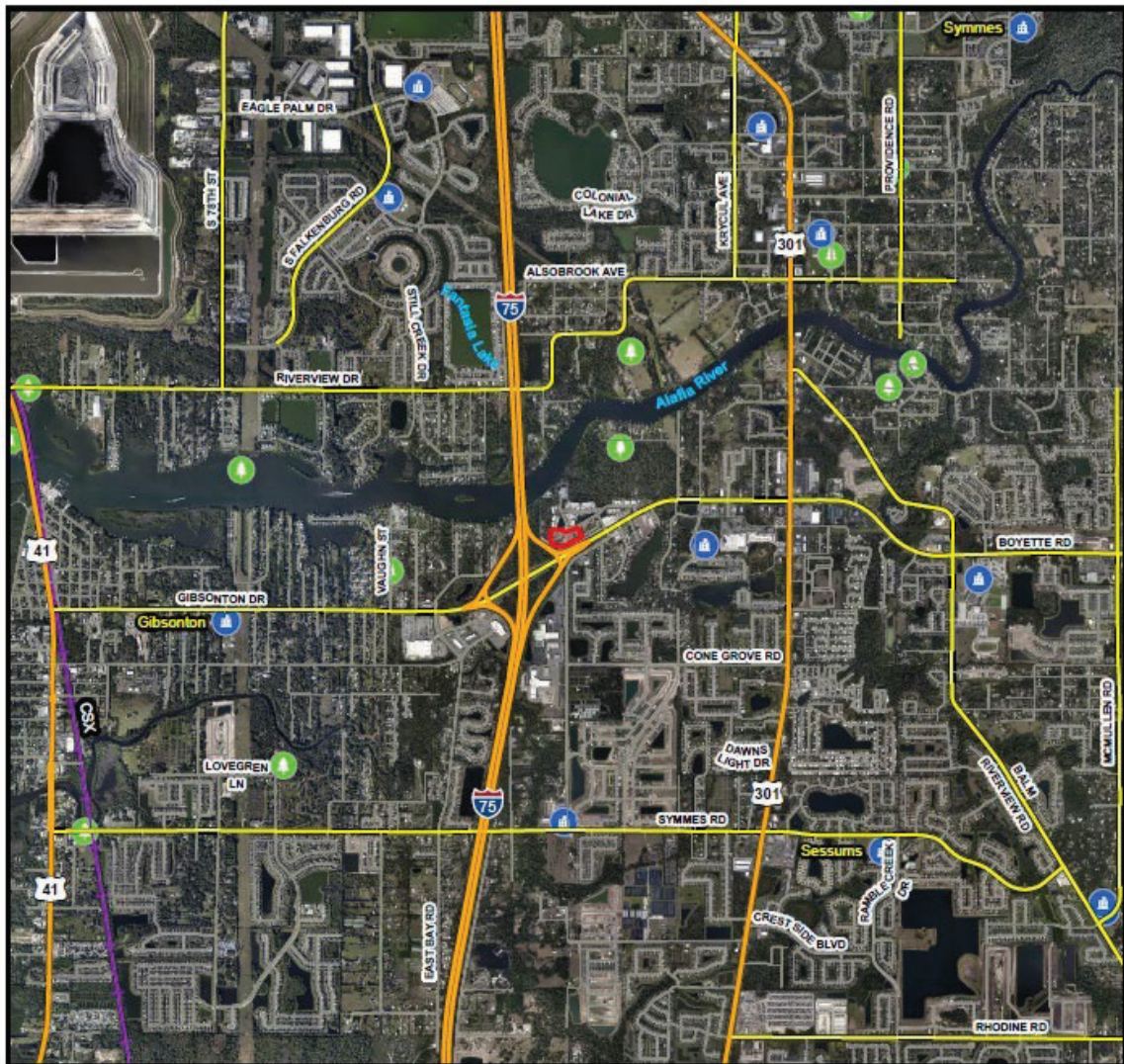
1. Increase hotel to 81,000 sf
2. Reduce the hotel parking spaces to 125.
3. No change.

LDC Part 6.05.00 (Parking/Loading) None requested.

Planning Commission Recommendation: Consistent

Development Services Recommendation: Approvable, subject to proposed conditions

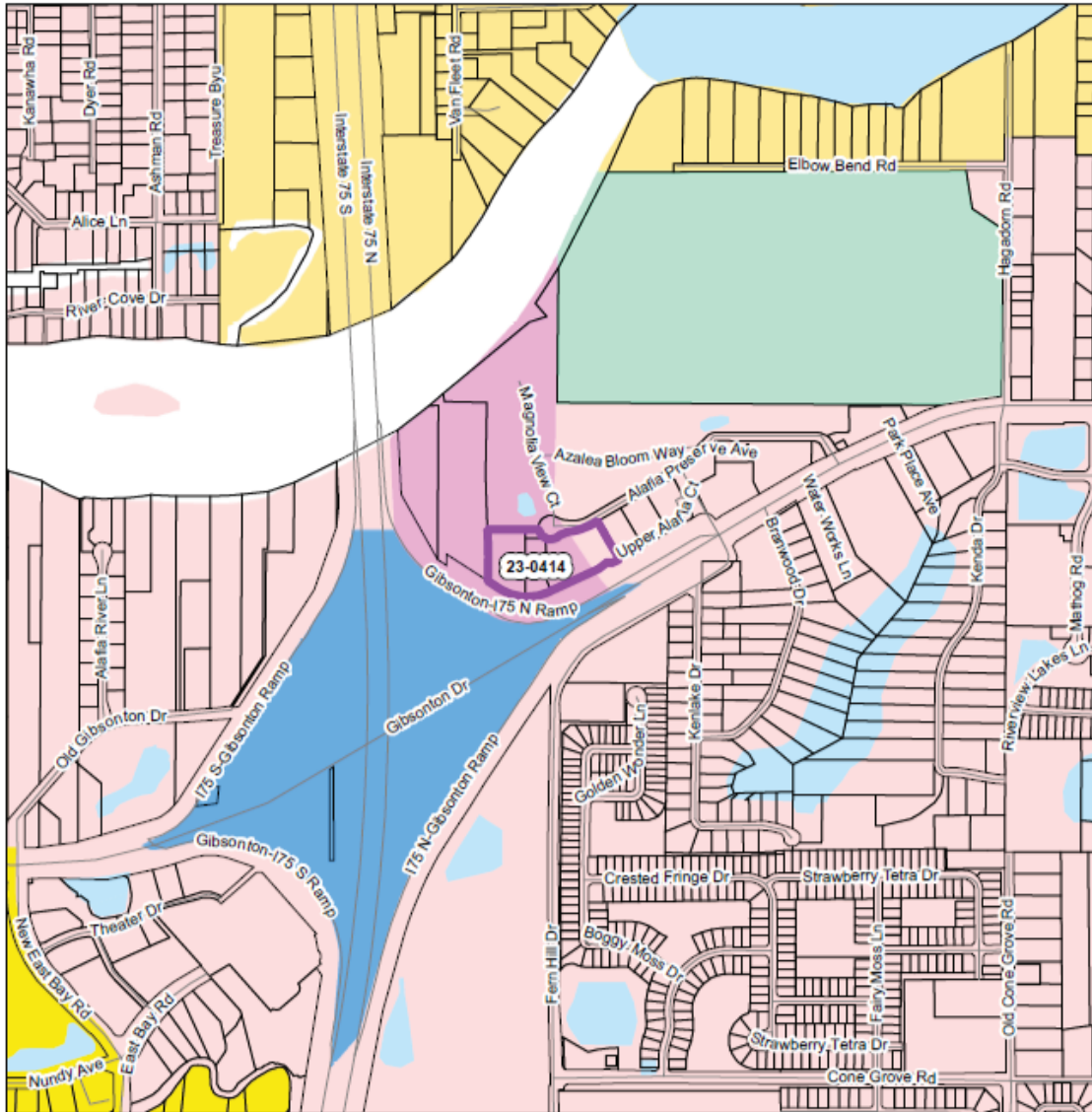
2.0 LAND USE MAP SET AND SUMMARY DATA 2.1 Vicinity Map



Context of Surrounding Area:

The subject property is located in the northeastern corner of Interstate 75 and Gibsonton Dr. The PD is approved for Multifamily and Commercial Neighborhood Uses. Existing land uses within the area include the multi-family residential development within the same overall PD, located to the immediate north of the portion of the site subject of this request.

2.0 LAND USE MAP SET AND SUMMARY DATA 2.2 Future Land Use Map



Subject Site Future Land Use Category:	SMU-6, CMU-12
Maximum Density/F.A.R.:	SMU-6: 6 du per ac. /0.35 FAR CMU-12: 12 du per ac. / 0.5 FAR
Typical Uses:	SMU-6: Typical uses include residential, suburban commercial, offices, research parks, light industrial, multi-purpose, clustered residential, mixed- use. CMU-12: Typical uses include residential, community retail commercial, offices, research park, light industrial, multi-purpose, clustered residential, mixed-use.

2.0 LAND USE MAP SET AND SUMMARY DATA 2.3 Immediate Area Map



Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
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South	Commercial General (CG)	Maximum 0.27 FAR	Uses allowed per Sec. 2.02.02 including commercial, offices.	Office building
East	PD 06-0848	Max. 60,980 sf CN; Max. 10,890 sf BPO	Neighborhood commercial/Office, Professional	Commercial strip center
West	PD 99-0735	Maximum 0.50 FAR	159 Dry Boat Storage, Max. 100 RV storage, 1,300 sf res., office, & retail for boat & RV maintenance, and 70- room hotel.	Vacant and boat ramp

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibsonston Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.

4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY

INFORMATION/REVIEWING AGENCY				
	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Environmental:				

Environmental Protection Commission Natural Resources Conservation & Environ. Lands Mgmt.

Check if Applicable:

Wetlands/Other Surface Waters

Use of Environmentally Sensitive Land Credit

Wellhead Protection Area

Surface Water Resource Protection Area

Potable Water Wellfield Protection Area Significant Wildlife Habitat

Coastal High Hazard Area

Urban/Suburban/Rural Scenic Corridor Adjacent to ELAPP property

Other

Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Transportation <input type="checkbox"/> Design Exc./Adm. Variance Requested <input checked="" type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See "Transportation Summary Report"
Service Area/ Water & Wastewater <input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Hillsborough County School Board Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	

Impact/Mobility Fees

Hotel
 (Per room (Mobility and Parks)) (Per 1,000 s.f. (Fire))
 Mobility: \$4,168 * 0 = \$0
 Parks: \$1,327 * 0 = \$0
 Fire: \$313 * 12.12 = \$3,793.56

Urban Mobility, South Fire - Hotel Expansion from 68,880 to 81,000 (12,120 sq ft) - no additional rooms

Comprehensive Plan:	Comments Received	Findings	Conditions Requested	Additional Information/Comments
Planning Commission <input checked="" type="checkbox"/> Meets Locational Criteria <input type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input type="checkbox"/> N/A <input type="checkbox"/> Density Bonus Requested <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	

5.0 IMPLEMENTATION RECOMMENDATIONS

5.1 Compatibility

The subject site is currently undeveloped and located at the northeastern quadrant of Interstate 75 and Gibsonton Drive. The site is surrounded on the north and east by other properties within the same PD. To the immediate west of the subject site is PD 99-0735, which is approved for up to 159 Dry Boat Storage spaces, 100 RV storage spaces, 1,300 sf residential, office, and retail for boat and RV maintenance, and a 70-room hotel. South of the subject site is right-of-way for Interstate 75 North and Gibsonton Road. Directly south across Gibsonton Road is commercial zoned property developed with an office building. Further south past the Commercial General is single-family residential and southeast across Gibsonton Drive are mobile homes. The site is located in the Urban Service Area and within the limits of the Riverview Community Plan.

The applicant's request is to expand the hotel buildable square footage to allow up to 81,000 square feet, through the use of a Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel, and to reduce the parking spaces required for the hotel use from 138 to 125 pursuant to a PD Variation for Parking. The nonresidential square footage for Commercial Neighborhood uses is nominally increased from 60,707 to 60,980. To address these increases and account for the additional hotel square footage. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The scale of the proposed development only increases the square footage of the hotel while maintaining the number of hotel rooms at 125. The applicant states, "The hotel floor area increase is requested to accommodate larger extended stay hotel type rooms. The parking reduction is sought to reflect current market conditions and is supported by the Institute of Transportation Engineer's Parking Manual."

The applicant does not request any variations to Land Development Code Parts 6.06.00 (Landscaping/Buffering). The applicant is required to have a 20 ft. buffer with Type "B" screening adjacent to the multi-family development located to the north of the hotel, which the applicant is providing, as shown on the site plan. This was also previously provided and is not a change.

Transportation PD Variation for Parking

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project's hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project's 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms). LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces ($138 \times .05$ equals 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

The uses proposed are unchanged and remain compatible. Based on the adjacent zonings and uses identified above in the report, staff finds the proposed PD zoning district compatible with the existing zoning districts and development pattern in the area. The subject site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land Development Code. The proposed Planned Development would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated Hillsborough County Comprehensive Plan.

5.2 Recommendation

Based on the above, staff finds the request approvable.

Zoning conditions were presented to the Zoning Hearing Master at the hearing and are hereby incorporated into the Zoning Hearing Master's recommendation.

SUMMARY OF HEARING

THIS CAUSE came on for hearing before the Hillsborough County Land Use Hearing Officer on September 18, 2023. Ms. Michelle Heinrich of the Hillsborough County Development Services Department introduced the petition.

Mr. Kevin Reali 401 East Jackson Street testified on behalf of the applicant. Mr. Reali showed a series of graphics and identified the boundary of the PD and the boundary of the Major Modification within the PD. He stated that the request is to increase the square footage of the hotel floor area from 68,880 square feet to 81,000 square feet with no change to the number of rooms. The increase in square footage is due to a change in the hotel type for an extended stay hotel. He added that the hotel rooms will be a bit bigger. Mr. Reali testified that a PD Variation is requested to reduce the amount of parking for the hotel. He explained that there are several changes to the approved zoning conditions are there have been three prior Personal Appearance applications and the conditions needed to be cleaned up. Policy 19.3 of the Future Land Use Element is proposed to be used for a mixed use density bonus in the existing flex approval which was part of the original approval. The waivers that were approved previously are also continuing to be requested and applied. Mr. Reali stated that because there is no increase in the number of hotel rooms being proposed, there is no change in the associated trip generation from the project. Regarding the reduction in parking, Mr. Reali testified that ITE data was used for hotel rooms which equates to less than one space (0.99) per room. County transportation staff supports the request as the applicant agreed to provide bicycle parking on-site. Mr. Reali concluded his presentation by stating that he was contacted by the property owner to the west asking about the stormwater permitting for the roadway. He explained that the zoning conditions are not clear who will construct the road. Whoever builds the road will be required to comply with SWFWMD regulations and the required amount of drainage.

Mr. Tim Lampkin of the Development Services Department, testified regarding the County staff report and stated that the proposed modification to PD 06-0848 to expand the hotel's buildable square footage up to 81,000 square feet. He described the surrounding land uses and stated that the applicant is requesting a PD Variation to reduce the amount of required parking for the hotel. The Land Development Code requires 1.1 spaces per room or 138 spaces and the applicant proposes to install 125 parking spaces and six bicycle spaces. Mr. Lampkin testified that there is no increase in the number of hotel rooms and the project is consistent with the development pattern in the area.

Ms. Jillian Massey of the Planning Commission testified regarding the Planning Commission staff report. Ms. Massey stated that the property is designated Community Mixed Use-12 and Suburban Mixed Use-6 by the Future Land Use Map and is located within the Urban Service Area and the Riverview Community Plan. She described the request and stated that it meets Objective 1 and Policy 1.2 regarding compatibility with the surrounding area. She cited numerous policies that shows consistency with the modification and stated that the project is compatible with the surrounding area. She concluded her presentation by stating that the development is consistent with the Comprehensive Plan.

Hearing Master Finch asked audience members if there were any proponents of the application. None replied.

Hearing Master Finch asked audience members if there were any opponents of the application. None replied.

County staff did not have additional comments.

Mr. Reali testified that the modification is supported by all reviewing agencies.

Hearing Master Finch then concluded the hearing.

EVIDENCE SUBMITTED

Ms. Heinrich submitted a revised staff report into the record.

Mr. Reali submitted a copy of his PowerPoint presentation which included zoning documents pertaining to prior modifications into the record.

PREFACE

All matters that precede the Summary of Hearing section of this Decision are hereby incorporated into and shall constitute a part of the ensuing Findings of Fact and Conclusions of Law.

FINDINGS OF FACT

1. The subject site is 5.6 acres and is zoned Planned Development (06-0848). The property is designated CMU-12 and SMU-6 by the Comprehensive Plan and located in the Urban Service Area and the Riverview Community Planning Area.
2. The Planned Development (PD) is currently approved for 125 hotel rooms with a maximum square footage of 68,880 square feet and a maximum height of 74 feet (6 stories).
3. The Major Modification requests to increase the maximum square footage of the hotel from 68,880 square feet to 81,000 square feet with no increase in the number of hotel rooms. Additionally, the applicant requests to reduce the required parking for the hotel from 138 spaces to 125 spaces.
4. The applicant's representative testified that the increase in square footage for the hotel is due to a change in the type of hotel as it will be developed as an extended stay hotel.
5. The reduction in the required number of parking spaces is requested through a Planned Development Variation to Land Development Code (LDC) Section 6.05.00. The applicant based the reduction in parking spaces from 138 to 125 on the Institute of Transportation Engineer's Manual which states that hotel parking is 0.99 spaces per room. Given the number of hotel rooms (125), the applicant is requesting to provide 125 parking spaces.

County Transportation staff reviewed the reduction request and asked the applicant to also provide the LDC required amount of bicycle parking spaces for the 138 rooms which equates to 6 bicycle parking spaces. The applicant agreed to a zoning condition for the bicycle parking spaces.

The PD Variation meets LDC Section 5.03.06.C(b) as the request is in harmony with the purpose and intent of the parking standards and the reduction in spaces is minimal such that it will not substantially interfere with adjacent property owners.

6. The Planning Commission found the request meets Objective 1 and Policy 1.2 regarding compatibility with the surrounding area. Staff cited numerous policies that shows consistency with the modification and stated that the project is compatible with the surrounding area. Staff found the modification consistent with the Comprehensive Plan.

7. The PD has been modified previously and the applicant and County staff have revised the zoning conditions to reflect the intended use of the hotel more accurately.
8. The Major Modification request is consistent with the Comprehensive Plan and Land Development Code and compatible with the surrounding area.

FINDINGS OF COMPLIANCE/NON-COMPLIANCE WITH THE HILLSBOROUGH COUNTY COMPREHENSIVE PLAN

The Major Modification request is in compliance with and does further the intent of the Goals, Objectives and the Policies of the Future of Hillsborough Comprehensive Plan.

CONCLUSIONS OF LAW

Based on the Findings of Fact cited above, there is substantial competent evidence to demonstrate that the requested Major Modification to the Planned Development zoning is in conformance with the applicable requirements of the Land Development Code and with applicable zoning and established principles of zoning law.

SUMMARY

The Planned Development (PD) is currently approved for 125 hotel rooms with a maximum square footage of 68,880 square feet and a maximum height of 74 feet (6 stories).

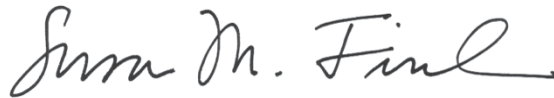
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The reduction in the required number of parking spaces is requested through a Planned Development Variation to Land Development Code (LDC) Section 6.05.00. The applicant based the reduction in parking spaces from 138 to 125 on the Institute of Transportation Engineer's Manual which states that hotel parking is 0.99 spaces per room. Given the number of hotel rooms (125), the applicant is requesting to provide 125 parking spaces. County Transportation staff reviewed the reduction request and asked the applicant to also provide the LDC required amount of bicycle parking spaces for the 138 rooms which equates to 6 bicycle parking spaces. The applicant agreed to a zoning condition for the bicycle parking spaces. The PD Variation meets LDC Section 5.03.06.C(b) as the request is in harmony with the purpose and intent of the parking standards and the reduction in spaces is minimal such that it will not substantially interfere with adjacent property owners.

The request is consistent with the Comprehensive Plan and Land Development Code and compatible with the surrounding area.

RECOMMENDATION

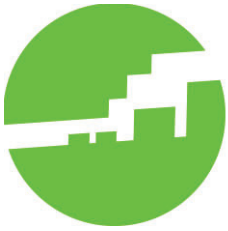
Based on the foregoing, this recommendation is for **APPROVAL** of the Major Modification to Planned Development 06-0848 as indicated by the Findings of Fact and Conclusions of Law stated above subject to the zoning conditions prepared by the Development Services Department.



October 9, 2023

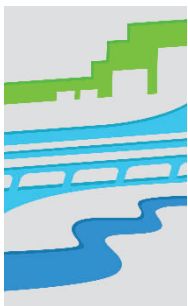
Susan M. Finch, AICP
Land Use Hearing Officer

Date



**Hillsborough County
City-County
Planning Commission**

Unincorporated Hillsborough County Rezoning	
Hearing Date: September 18, 2023 Report Prepared: September 6, 2023	Petition: PD 23-0414 Folios: 76324.0006, 76324.0010, 76343.1000, 76324.0005 <i>Northeast of I-75 and Gibsonton Drive</i>
Summary Data:	
Comprehensive Plan Finding:	CONSISTENT
Adopted Future Land Use:	Community Mixed Use-12 (12 du/ga; 0.50 FAR) Suburban Mixed Use-6 (6 du/ga; 0.35 FAR)
Service Area:	Urban
Community Plan:	Riverview
Request:	Major Modification to Planned Development (06-0848) to allow an increase in building size from 68,880 sq.ft. to 81,000 sq.ft. with no change to the number of rooms within the hotel site
Parcel Size (Approx.):	5.6 +/- acres (243,936 sq. ft.)
Street Functional Classification:	Interstate-75 – Arterial Gibsonton Drive – County Arterial
Locational Criteria:	Meets
Evacuation Zone:	C



Context

- The approximately 5.6± acre site is located Northeast of I-75 and Gibsonton Drive. The site is in the Urban Service Area and within the limits of the Riverview Community Plan.
- The subject site is located in the Community Mixed Use-12 (CMU-12) Future Land Use Category. Typical allowable uses in the CMU-12 include residential, community scale retail commercial, office, research corporate park uses, light industrial, multi-purpose and clustered residential and/or mixed-use projects at appropriate locations. Suburban Mixed Use-6 (SMU-6) allows for residential, suburban scale neighborhood commercial, office uses, research corporate park uses, light industrial multi-purpose and clustered residential and/or mixed use projects at appropriate locations. Projects that are 10 acres or greater within the CMU-12 Future Land Use classification and properties 20 acres or greater in the Suburban Mixed Use-6 (SMU-6) Future Land Use must demonstrate a mix of land uses in accordance with Policy 19.1.
- The CMU-12 Future Land Use category is located to the north, west, and northwest of the subject site. SMU-6 Future Land Use is located to the east, southeast, and northeast. Public /Quasi-Public Future Land Use is found to the south.
- Much of the area contains Planned Development zoning developed with various uses such as multi-family residential housing, and retail commercial.
- The applicant is requesting a Major Modification to Planned Development (06-0848) to allow an increase in building size from 68,880 sq.ft. to 81,000 sq.ft. with no change to the number of rooms for the hotel site.

Compliance with Comprehensive Plan:

The following Goals, Objectives, and Policies apply to this rezoning request and are used as a basis for a consistency finding.

FUTURE LAND USE ELEMENT

Urban Service Area (USA)

Objective 1: *Hillsborough County shall pro-actively direct new growth into the urban service area with the goal that at least 80% of all population growth will occur within the USA during the planning horizon of this Plan. Within the Urban Service Area, Hillsborough County will not impede agriculture. Building permit activity and other similar measures will be used to evaluate this objective.*

Policy 1.2: Minimum Density *All new residential or mixed-use land use categories within the USA shall have a density of 4 du/ga or greater unless environmental features or existing development patterns do not support those densities.*

Within the USA and in categories allowing 4 units per acre or greater, new development or redevelopment shall occur at a density of at least 75% of the allowable density of the land use category, unless the development meets the criteria of Policy 1.3.

Policy 1.4: *Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and architecture. Compatibility does not mean “the same as.” Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.*

Policy 7.3: *The land use category boundaries may be considered for interpretation as flexible boundaries in accordance with the Flex Provision as follows:*

- *Through application of the flex provision, the land use category boundaries shall be deemed to extend beyond the precise line to include property adjoining or separated by a man made or natural feature from the existing boundary line.*
- *The line may be relocated a maximum of 500 feet from the existing land use boundary of the adopted Land Use Plan Map. Right-of-Way is not included in the measurement of the 500 foot flex.*
- *No new flexes can be extended from an existing flexed area.*
- *All flexes must be parallel to the land use category line.*
- *Flexes are not permitted in the Rural Area or in areas specified in Community Plans.*
- *Flexes are also not permitted from the Urban Service Area into the Rural Area. All flexes in the Rural Area approved prior to July 2007 are recognized and are not to be considered non-conforming.*
- *Flexes to increase residential density are not permitted in the Coastal High Hazard Area. Flexes are not permitted from a municipality into the unincorporated county.*
- *A flex must be requested as part of planned development or site plan oriented rezoning application. Major Modification to approved zoning that changes the intensity, density or the range of uses will require that the previous flex request be re-evaluated for consistency and a new flex request may be required.*
- *Applicants requesting a flex must provide written justification that they meet the criteria for a flex as outlined below.*
- *The Board of County Commissioners may flex the plan category boundary to recognize or grant a zoning district which is not permitted in the land use category but lies within the distance of a conforming land use category, as described above. Prior to the determination by the Board of County Commissioner, the staff of the Planning Commission shall make a recommendation on the consistency of the request with the Comprehensive Plan.*

Objective 8: *The Future Land Use Map will include Land Use Categories which outline the maximum level of intensity or density and range of permitted land uses allowed and planned for an area. A table of the land use categories and description of each category can be found in Appendix A.*

Policy 8.8: *For projects whose boundaries encompass more than one land use category, density and intensity calculations will allow for the blending of those categories across the entire project. All portions of the project must be contiguous to qualify for blending. Blending of densities and intensities is not permitted across improved public roadways or between the Urban Service Area (USA) and Rural Service Area (RSA) boundary. The combined total number of dwelling units and/or FAR possible under all the land use categories within the project will be used as a ceiling for review purposes. This provides maximum design flexibility for those projects, because the location or clustering of those units on the project site need not conform to the land use category*

boundary on the site as long as the maximum number of dwelling units permitted for the entire project are not exceeded.

Relationship to Land Development Regulations

Objective 9: All existing and future land development regulations shall be made consistent with the Comprehensive Plan, and all development approvals shall be consistent with those development regulations as per the timeframe provided for within Chapter 163, Florida Statutes. Whenever feasible and consistent with Comprehensive Plan policies, land development regulations shall be designed to provide flexible, alternative solutions to problems.

Policy 9.1: Each land use plan category shall have a set of zoning districts that may be permitted within that land use plan category, and development shall not be approved for zoning that is inconsistent with the plan.

Policy 9.2: Developments must meet or exceed the requirements of all land development regulations as established and adopted by Hillsborough County, the state of Florida and the federal government unless such requirements have been previously waived by those governmental bodies.

Neighborhood/Community Development

Objective 16: Neighborhood Protection The neighborhood is a functional unit of community development. There is a need to protect existing neighborhoods and communities and those that will emerge in the future. To preserve, protect and enhance neighborhoods and communities, all new development must conform to the following policies.

Policy 16.1: Established and planned neighborhoods and communities shall be protected by restricting incompatible land uses through mechanisms such as:

- a) locational criteria for the placement of non-residential uses as identified in this Plan,
- b) limiting commercial development in residential land use categories to neighborhood scale;
- c) requiring buffer areas and screening devices between unlike land uses;

Policy 16.2: Gradual transitions of intensities between different land uses shall be provided for as new development is proposed and approved, through the use of professional site planning, buffering and screening techniques and control of specific land uses.

Policy 16.3: Development and redevelopment shall be integrated with the adjacent land uses through:

- a) the creation of like uses; or
- b) creation of complementary uses; or
- c) mitigation of adverse impacts; and
- d) transportation/pedestrian connections

Policy 16.5: Development of higher intensity non-residential land uses that are adjacent to established neighborhoods shall be restricted to collectors and arterials and to locations external to established and developing neighborhoods.

Mixed Use Land Use Categories

Objective 19: All development in the mixed use categories shall be integrated and interconnected to each other.

Policy 19.1: Larger new projects proposed in all mixed use plan categories shall be required to develop with a minimum of 2 land uses in accordance with the following:

- Requirements for 2 land uses will apply to properties 10 acres or greater in the RMU-35, UMU-20, and CMU-12 land use categories, and to properties 20 acres or greater in the SMU-6 and NMU-4 land use categories.
- At least 10% of the total building square footage in the project shall be used for uses other than the primary use.
- The mix of uses may be horizontally integrated (located in separate building). Horizontal integration may also be achieved by utilizing off-site uses of a different type located within $\frac{1}{4}$ mile of the project, on the same side of the street of a collector or arterial roadway connected by a continuous pedestrian sidewalk.
- The land uses that may be included in a mixed use project include: retail commercial, office, light industrial, residential, residential support uses, and civic uses provided that the use is permitted in the land use category.
- These requirements do not apply within $\frac{1}{2}$ of a mile of an identified Community Activity Centers (if other mixed use standards have been adopted for that area or when the project is exclusively industrial).

Policy 19.3: Incentives for Mixed Use

The following incentives are available to encourage mixed use and vertically integrated mixed use projects within the Urban Service Area:

- Parking structures shall not count towards the FAR for projects that include 3 or more land uses or vertically integrate two land uses.
- Projects that either include 3 or more land uses or vertically integrate two land uses may utilize a density bonus to the next higher land use category or the following FAR bonus:
- Property with a Future Land Use Category of 35 units per acre and/or 1.00 FAR and higher and within the USA – may increase up to 50 units and/or an additional .50 FAR
- Property within a Future Land Use Category of 9 units per acre and/or .5 FAR and higher and within the USA – Increase in FAR by .25
- Property within a Future Land Use Category of 4 units per acre and/or .25 FAR and higher and within the USA – Increase in FAR by .10

Community Design Component

2.1 Mixed Use Development

GOAL 1: Plan a pattern of compact, livable and walkable neighborhoods and communities within the urban service area which are supported by locally-oriented employment, goods and services.

OBJECTIVE 1-2: Promote a variety of uses in order to create vitality and bring many activities of daily life within walking distances of homes.

LIVABLE COMMUNITIES ELEMENT

Riverview Community Plan

Goal 1 *Achieve better design and densities that are compatible with Riverview's vision.*

- *Develop Riverview district-specific design guidelines and standards. The standards shall build on recognizable themes and design elements that are reflective of historic landmarks, architecture and heritage of Riverview. The mixed-use, residential, non-residential and roadway design standards shall include elements such as those listed.*

Mixed Use-Commercial-Residential

- *Enhance the ability to walk or bike between adjoining commercial areas.*

Transportation:

- *Develop distinctive roadway design and landscape standards for new developments and redevelopment projects that complement the community's uniqueness as well as encourage buffers to parking areas, water retention areas and sidewalks. Techniques may include landscaping, berming and median enhancements.*

Goal 2 *Reflect the vision of Riverview using the Riverview District Concept Map. The Riverview District Concept Map will illustrate the unique qualities and land uses related to distinct geographic areas identified as "districts". (See Figure 10)*

The following specific districts are incorporated into the Riverview District Concept Map. Require future development and redevelopment to comply with the adopted Riverview District Concept Map.

4. *Mixed Use – Focus and direct development toward walkable mixed-use town center locations throughout the community while respecting existing land use.*
5. *Residential – Encourage attractive residential development that complements the surrounding character and promotes housing diversity.*

Goal 4 *Provide safe, attractive, efficient multi-modal transportation, including vehicular, bicycle/pedestrian and transit.*

Goal 7 *Encourage economic development by supporting local business while attracting a variety of new uses and services, particularly high technology businesses.*

Goal 9 *Attract, locate and support appropriate industry and employment with state-of-the-art infrastructure.*

Goal 11 *Interconnect districts and public places with concepts for walkability, particularly schools and parks.*

- *Collaborate with private sector developers, government agencies and non-profit organizations to provide safe roadway, sidewalk and pathway connections, biking and equestrian linkages and other pedestrian amenities.*

- *Provide sidewalks, pathways and/or trails wide enough (wider than 5 feet) for people to easily pass each other or travel side-by-side.*

Staff Analysis of Goals, Objectives, and Policies:

The approximately 5.6 acre site is located Northeast of I-75 and Gibsonton Drive. The site is in the Urban Service Area and within the limits of the Riverview Community Plan. The applicant is requesting a Major Modification to Planned Development (06-0848) to allow an increase in building size from 68,880 sq.ft. to 81,000 sq.ft. with no change to the number of rooms for the hotel site.

The subject site has 4.59 acres designated Community Mixed Use-12 (CMU-12) and 1.01 acres Suburban Mixed Use-6 (SMU-6) Future Land Use Category. Projects that are 10 acres or greater within the CMU-12 Future Land Use classification and properties 20 acres or greater in the Suburban Mixed Use-6 (SMU-6) Future Land Use must demonstrate a mix of land uses in accordance with Policy 19.1-3. This property is part of a larger PD that meets the acreage threshold for the both the CMU-12 and SMU-6 Future Land Use categories. The CMU-12 Future Land Use category is located to the north, west, and northwest of the subject site. SMU-6 Future Land Use is located to the east, southeast, and northeast. Public /Quasi-Public Future Land Use is found to south.

Objective 1 and FLUE Policies 1.2, 1.3 and 1.4 as it provides growth in the Urban Service Area. The proposed major modification is compatible with the development pattern in the area as required in Policy 1.4. Much of the area contains Planned Development zoning developed with various uses such as multi-family residential housing, and retail commercial. Objective 8 enables the Future Land Use Map (FLUM) and Policy 8.1 mandates the range of acceptable land uses in each classification in the FLUM. Policy 8.2 requires all development to be compliant with the Future Land Use Element of the Comprehensive Plan and the proposed development does meet the intent of the FLU category.

The proposed Planned Development is consistent with the Neighborhood Protection policies under FLUE Objective 16. Specifically, FLUE Policy 16.1 focuses on the protection of neighborhoods through locational criteria for non-residential uses. FLUE Policy 16.2 calls for gradual transitions of intensities between land uses. The proposed development is consistent with this policy direction, as the non-residential land uses are proposed on to the end of the cul-de-sac and is closer to Interstate-75 serving as a buffer for the multi-family residential to the east. This arrangement of uses is also consistent with FLUE Policy 16.5, which seeks to focus more intensive development along arterials and collector roadways, and to areas outside of existing and developing neighborhoods.

The property is subject to Commercial Locational Criteria pursuant to FLUE Policy 22.2. The nearest qualifying intersection is Interstate-75 and Gibsonton Drive. The maximum building square footage permitted for neighborhood serving commercial uses per quadrant is 175,000 square feet for the SMU-6 Future Land Use category. The property occupies the northeast quadrant of the intersection and is within the required distance of the intersection. Therefore, the subject site meets Commercial Locational Criteria.

Due to the expected intensity of CMU-12 Future Land Use categories, locational criteria is not required for non-residential uses. However, commercial locational criteria is required for non-residential development within the SMU-6 Future Land Use category, the property is part of a 39.96 acre mixed use PD which overall meets commercial locational criteria.

The site is located within the limits of the Riverview Community Plan, specifically within the Residential District. The proposed development would not support the vision of the Riverview Community Plan by developing a mix of commercial uses on the southern portion of the site. Goal 4 calls for internal and external pedestrian connections for providing a safe and efficient multi-modal system within Riverview. The Community Plan also envisions connecting pedestrians with public places such as parks (Goal 11). The proposed project is consistent with the Riverview.

Overall, the proposed Major Modification would provide for a development pattern that is comparable to the mixed-use development pattern within the surrounding area and meets the intent or the vision of the Riverview Community Plan.

Recommendation

Based upon the above considerations, the Planning Commission staff finds the proposed Planned Development **CONSISTENT** with the *Unincorporated Hillsborough County Comprehensive Plan*.

HILLSBOROUGH COUNTY FUTURE LAND USE RZ MM 23-0414

<all other values>

Rezoning

STATUS

- APPROVED
- CONTINUED
- DENIED
- WITHDRAWN
- PENDING

Tampa Service

Urban Service

Shoreline

County Boundary

Jurisdiction Boundary

Roads

Parcels

WATER NATURAL LULC_Wee_Poly

AGRICULTURAL/MINING-1/20 (.25 FAR)

PEC PLANNED ENVIRONMENTAL COMMUNITY-1/2 (.25 FAR)

AGRICULTURAL-1/10 (.25 FAR)

AGRICULTURAL/RURAL-1/5 (.25 FAR)

AGRICULTURAL ESTATE-1/2.5 (.25 FAR)

RESIDENTIAL-1 (.25 FAR)

RESIDENTIAL-2 (.25 FAR)

RESIDENTIAL PLANNED-2 (.35 FAR)

RESIDENTIAL-4 (.25 FAR)

RESIDENTIAL-6 (.25 FAR)

RESIDENTIAL-9 (.35 FAR)

RESIDENTIAL-12 (.35 FAR)

RESIDENTIAL-16 (.35 FAR)

RESIDENTIAL-20 (.35 FAR)

RESIDENTIAL-35 (1.0 FAR)

NEIGHBORHOOD MIXED USE-4 (3) (.35 FAR)

SUBURBAN MIXED USE-6 (.35 FAR)

COMMUNITY MIXED USE-12 (.50 FAR)

URBAN MIXED USE-20 (1.0 FAR)

REGIONAL MIXED USE-35 (2.0 FAR)

INNOVATION CORRIDOR MIXED USE-35 (2.0 FAR)

OFFICE COMMERCIAL-20 (.75 FAR)

RESEARCH CORPORATE PARK (1.0 FAR)

ENERGY INDUSTRIAL PARK (.50 FAR USES OTHER THAN RETAIL, .25 FAR RETAIL/COMMERCE)

LIGHT INDUSTRIAL PLANNED (.75 FAR)

LIGHT INDUSTRIAL (.75 FAR)

HEAVY INDUSTRIAL (.75 FAR)

PUBLIC/QUASIPUBLIC

NATURAL PRESERVATION

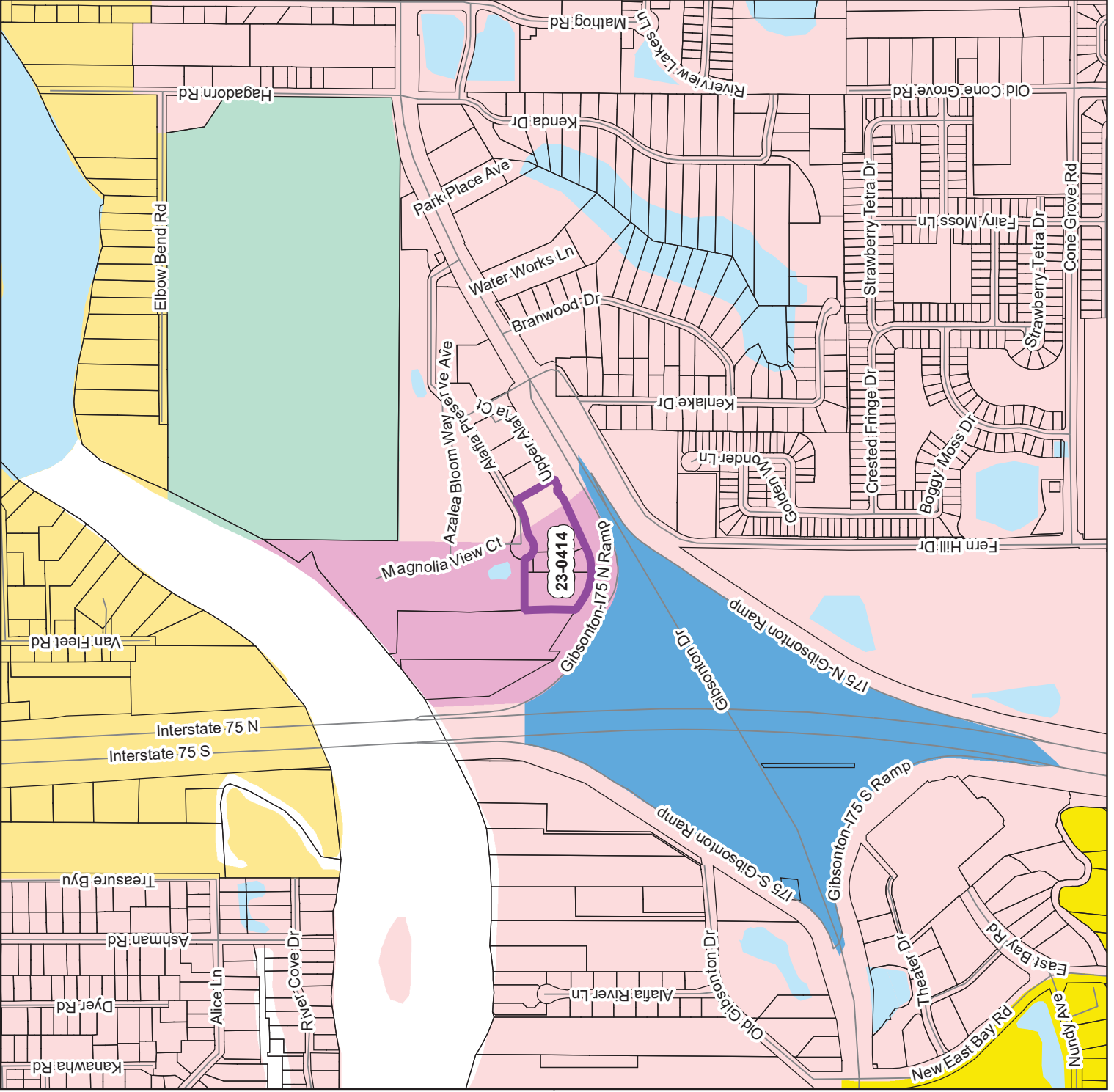
WIMAUMA VILLAGE RESIDENTIAL-2 (.25 FAR)

CITRUS PARK VILLAGE

DATA SOURCES: Rezoning boundaries from The Planning Commission and are not official. Parcel lines and data from Hillsborough County Property Appraiser. The rezoning status and author are for informational purposes only and are subject to the City/County Planning Commission. ACCURACY: It is intended that the information shown on this map is accurate to the best of our knowledge. However, such accuracy is not guaranteed by the Hillsborough County City/County Planning Commission. This map is for illustrative purposes only. For the most current data and information, visit the appropriate website.

0 460 920 1,380 1,840 Feet

Map Printed from Rezoning System: 4/26/2023
 Author: Beverly F. Daniels
 File: G:\Rezoning\System\MapProjects\HC\Gres_h\Rezoning - Copy.mxd





**GENERAL
SITE PLAN
FOR
CERTIFICATION**



**Hillsborough
County Florida**

DEVELOPMENT SERVICES

PO Box 1110, Tampa, FL 33601-1110
(813) 272-5600

**HILLSBOROUGH COUNTY
DEVELOPMENT SERVICES DEPARTMENT**

GENERAL SITE PLAN REVIEW/CERTIFICATION

**BOARD OF COUNTY
COMMISSIONERS**

Donna Cameron Cepeda
Harry Cohen
Ken Hagan
Pat Kemp
Gwendolyn "Gwen" Myers
Michael Owen
Joshua Wostal

COUNTY ADMINISTRATOR

Bonnie M. Wise

COUNTY ATTORNEY

Christine M. Beck

COUNTY INTERNAL AUDITOR

Peggy Caskey

DEPUTY COUNTY ADMINISTRATOR

Gregory S. Horwedel

Project Name: HDG Hotels

Zoning File: PD 06-0848 Modification: MM 23-0414

Atlas Page: None Submitted: 10/16/2023

To Planner for Review: 10/16/2023 Date Due: 10/23/2023

Contact Person: Kevin B. Reali Phone: 813.223.4800/kreali@stearnsweaver.com

Right-Of-Way or Land Required for Dedication: Yes No

The Development Services Department HAS NO OBJECTION to this General Site Plan.

The Development Services Department RECOMMENDS DISAPPROVAL of this General Site Plan for the following reasons:

Reviewed by: Tim Lampkin Date: 10-17-2023

Date Agent/Owner notified of Disapproval: _____



AGENCY COMMENTS

AGENCY REVIEW COMMENT SHEET

TO: ZONING TECHNICIAN, Planning Growth Management

DATE: 19 April 2023

REVIEWER: Bernard W. Kaiser, Conservation and Environmental Lands Management

APPLICANT: Elise Batsel

PETITION NO: MM 23-0414

LOCATION: Not listed

**FOLIO NO: 76324.0005,76324.0006, 76343.1000,
76324.0010**

SEC: _____ TWN: _____ RNG: _____

- This agency has no comments.

- This agency has no objection.

- This agency has no objection, subject to listed or attached conditions.

- This agency objects, based on the listed or attached conditions.

COMMENTS: _____.



VERBATIM TRANSCRIPT

1 HEARING MASTER: All right. Nothing at this time.
2 Thank you so much.

3 Then, with that, we'll close Rezoning 23-0109 and go
4 to the next case.

5 MS. HEINRICH: Our next item is Item D.4, Major Mod
6 application 23-0414. The applicant is requesting a major
7 modification to PD 06-0848. Tim Lampkin with Development
8 Services will provide staff findings. And you should have
9 received a revised staff report. In that report, it is amending
10 the cover page to include a parking variation; amending
11 condition 18 to make it specific to only apply to the area of
12 the major modification; and to add an underline to a missing new
13 condition, number 1.7.

14 HEARING MASTER: Okay. Thank you so much.

15 Is the applicant here? Good evening.

16 MR. REALI: Good evening. For the record, Kevin Reali
17 with the Stearns Weaver Miller law firm, 401 East Jackson
18 Street. The request this evening, as pointed out, is just a
19 portion of the PD. You can see the -- the thick gray line is
20 the boundary of the PD, and then the darker black line is the
21 boundary for the major modification which covers roughly five
22 and a half acres, generally located in the southwest portion of
23 the PD in the -- in the northeast quadrant of the intersection
24 with Gibsonton Drive and 75.

25 Future Land Use map, note that the PD itself has a mix

1 of CMU-12 and SMU-6. And then the zoning map which shows a
2 large PD in the area surrounded with the red line and then a mix
3 of various uses -- residential, nonresidential, commercial --
4 both north and south of Gibsonton Drive. This portion of the
5 area north of Gibsonton is not in the PD.

6 And so really this application is the story of two --
7 two different processes. The request that we have is very
8 straightforward. It's to increase the hotel floor area -- the
9 hotel is already a permitted use in this area of the PD -- from
10 68,880 square feet to 81,000 square feet, with no change in the
11 number of rooms. And that's simply to change the hotel type to
12 a extended stay hotel. So there's a -- the applicant is
13 intending to develop with that type of hotel, and the rooms are
14 bit bigger, and so they need a little bit more square footage.

15 And then along with that request, there's a variation
16 to the number of required parking spaces which is proposed at
17 one per room, consistent with the ITE. I'll go over that a
18 little bit later.

19 So that's the limit of the request here. And then
20 there are some other changes noted in the conditions. And in
21 working with staff, the Planning Commission staff and
22 transportation staff, in particular, were very patient with us.
23 It took us a long time to get through. This PD had three prior
24 PRS applications. And so, this being the fourth modification,
25 it was difficult to clean up the entitlements and the

1 transportation conditions to reflect the current conditions
2 today. And, by working with staff, we were able to get there.

3 So I'm going to go over the -- the cleanup request
4 briefly. It's touched on in the staff reports, but, really, I
5 just want to mention it. And if you have questions, we'll
6 answer them, but we don't need to go into the depth that we went
7 in with staff to -- to get here today.

8 So to deal with entitlements, we use Policy 19.3 for
9 mixed-use bonus in the existing flex approval which was part of
10 the existing -- the existing approval. And then with
11 transportation staff, you can see circled here in red, it says
12 last parcel to develop. And then we have this -- this future
13 right-of-way area here circled. And what's happening here is
14 FDOT and transportation staff want this Old Gibsonton Drive,
15 this roadway here, to eventually connect through the PD. And
16 there are conditions in place to make that happen, but as
17 development progresses, the opportunity to locate that
18 right-of-way gets smaller and smaller. And as that happens, we
19 have to show where that right-of-way is gonna be to ensure that
20 future access spacing can be met and future design can actually
21 be established because none of that is known here.

22 And so, in the prior version, it's a little confusing,
23 but this -- this larger area here was the last parcel to be
24 developed, and it gave a range of areas that the right-of-way
25 could be located. And what we've worked out with transportation

1 staff is the right-of-way is gonna be somewhere in here which
2 is -- it's almost exact. There's some extra feet there, but
3 it's pretty close. It's gonna be designed in that area. And
4 then this area here will be the last parcel to be developed.

5 You'll notice all the arrows added to the plan. There
6 wasn't enough space here to have all these access points. And
7 so what will happen eventually is the access will come in
8 through this future right-of-way, and then these parcels will
9 have cross access so that access spacing is properly achieved.

10 Oh, and I'll also note on that page there, there are
11 existing waivers. We're not changing any of the waivers. So
12 the -- the Development Services staff noted no waivers. There
13 are existing waivers; we're just not changing them, and some of
14 them are not applicable to the property.

15 As noted, Planning Commission staff worked with us on
16 the entitlements. And then once we got to a final place with
17 them, staff found the request consistent, noted compatibility
18 with the development pattern and with neighborhood protection
19 policies and commercial-locational criteria. And then, of
20 course, the flex and the 19.3 bonus that I already mentioned.

21 Transportation staff, I already noted the issues with
22 the right-of-way, the second point, but there are other
23 transportation parts of the application. With no change in
24 rooms, the way the ITE measures a hotel's impact to the
25 transportation network is by room count, not by the size of the

1 development. So with no changes to the room count, there are no
2 changes in trip generations. There's no study with this
3 application and no projected change in daily trips.

4 And then transportation staff reviewed our request for
5 the one space per room parking count. We submitted data
6 consistent with the ITE. ITE calculates the parking rate at
7 about one per room, and that's with, you know, the complete
8 study. There's actually a high level of confidence -- I'm going
9 from memory here, but something around 95 percent confidence
10 that it's closer to 0.8 per room. But that's not what the
11 applicant was after anyway, so we're happy to use the 0.99
12 number which is what was submitted. And transportation staff
13 supported that request with a condition to add some bicycle
14 parking.

15 Zoning staff concurred with the consistency mentioned
16 by the Planning Commission and the approvals that the
17 transportation staff that, you know, there really wasn't too
18 much to work through with compatibility or site design with
19 zoning staff. It was really more working with Planning
20 Commission and Transportation.

21 And, with that, we do have a transportation engineer
22 and civil engineer here if there are any questions. Oh, I
23 should -- I should note, while at the meeting today, I got a
24 call from the counsel for the landowner to the west asking about
25 the stormwater permitting for the roadway and stormwater

1 retention for that right-of-way that I had pointed out. And I
2 said that I would mention it on the record.

3 The way the conditions are written, it's not clear who
4 will construct that road. It's possible that the County would
5 ask for the right-of-way and construct it if development happens
6 in a certain way. And if it happens in another way, then the
7 applicant may be on the hook to -- or I should have said the
8 landowner, not necessarily the applicant -- may be on the hook
9 to build that road.

10 But whatever gets built and whoever builds it, it'll
11 have to comply with swift mud and provide the appropriate amount
12 of drainage, et cetera. So to alleviate the concern with
13 stormwater, we would just say that the future permitting will
14 require that that roadway have adequate stormwater capacity.

15 With that, we'll answer any questions. And I have a
16 packet to submit to the record, basically which is zoning
17 history and this presentation in case there are questions or
18 questions we didn't anticipate here at Board of County
19 Commissioners.

20 HEARING MASTER: No questions at this time. I read
21 all of the backup so I understand the request. So thank you for
22 that.

23 MR. REALI: Okay. Thank you.

24 HEARING MASTER: All right. Development Services.
25 Good evening.

1 MR. LAMPKIN: Good evening. Tim Lampkin, Development
2 Services, for Major Modification 23-0414. The subject site is
3 currently undeveloped. It's located in the northeast quadrant
4 of Interstate 75 and Gibsonton Drive. The site is surrounded on
5 the north and the east by other properties within the same PD.
6 The property to the west that was mentioned is subject to
7 PD 99-0735, which is approved for up to 159 dry boat storage
8 spaces, 100 RV storage spaces, some 1300 residential units,
9 office, retail, RV maintenance, and a 70-room hotel.

10 South of the subject site is a right-of-way for
11 Interstate 75 North and Gibsonton Road. Directly south across
12 Gibsonton is commercial zoned property developed with an office
13 building. The applicant's request is to expand the hotel's
14 buildable square footage to allow up to 81,000 square feet
15 through the use of comprehensive plan Policy 19.1, mixed-use
16 bonus, with no change, as the applicant stated, in the number of
17 rooms within the hotel, while maintaining the number of hotel
18 rooms at 125 and to reduce the parking spaces required for the
19 hotel use from 138 to 125 pursuant to the PD variation for
20 parking.

21 The applicant does not request any variation to the
22 land development codes part 6.0.6.00. The applicant submitted
23 any additional -- on top of the ones that have already been
24 approved -- the applicant submitted a PD variation for a
25 reduction in the required parking rate for the project's hotel

1 uses. The land development code, as the applicant stated,
2 requires 1.1 parking spaces per hotel room. As such, 138
3 parking spaces would normally be required. The applicant
4 proposed a parking rate of one space per room, for 125. Per LDC
5 section 6.05.02.P.1, which permits the administrator to reduce
6 the number of required parking spaces by 5 percent in exchange
7 for provisions such as bicycle parking.

8 Given the above, staff has proposed a condition which
9 would permit the developer to construct 125 parking spaces to
10 serve the proposed hotel uses, provided they also construct six
11 bicycle spaces, meeting LDC requirements.

12 The uses proposed are unchanged, as was stated, and
13 still remain compatible. Based upon the adjacent zonings, the
14 uses identified in the report, staff finds proposed PD zoning
15 district compatible with the existing surrounding zoning
16 districts and established development pattern in the area. The
17 subject site will comply with and conform with other applicable
18 policies and regulations. And the proposed development would
19 allow for development that's consistent with the County's
20 comprehensive plan.

21 Based upon the above, staff finds the request
22 approvable. Do you have any questions?

23 HEARING MASTER: Not at this time. Thank you so much.

24 MR. BALL: Okay. Thank you.

25 HEARING MASTER: Planning Commission.

1 MS. MASSEY: Jillian Massey, Planning Commission
2 staff. The subject property is in the community mixed-use-12
3 and suburban mixed-use-6 Future Land Use categories. It's in
4 the urban service area and within the limits of the Riverview
5 Community Plan.

6 Objective 1 in Future Land Use Element 1.2, 1.3, and
7 1.4, as it provides growth in the urban service area -- these
8 policies discuss growth in the urban service area. The proposed
9 major modification is compatible with the development pattern in
10 the area as required in Policy 1.4. Much of the area contains
11 plan development zoning developed with various uses such as
12 multifamily residential housing and retail commercial.
13 Objective 8 enables the Future Land Use map and Policy 8.1
14 mandates the range of acceptable land uses in each
15 classification. Policy 8.2 requires all development to be
16 compliant with the Future Land Use Element of the comprehensive
17 plan, and the proposed development does meet the intent of the
18 Future Land Use category.

19 The proposed plan development is consistent with the
20 neighborhood protection policies under the Future Land Use
21 Element Objective 16. Specifically, Future Land Use Element
22 Policy 16.1 focuses on the protection of neighborhoods through
23 locational criteria for nonresidential uses. Policy 16.2 calls
24 for the gradual transition of intensities between land uses.
25 The proposed development is consistent with this policy

1 direction, as the nonresidential land uses are proposed at the
2 end of the cul-de-sac and is closer to Interstate 75, serving as
3 a buffer for the multifamily residential to the east.

4 This arrangement of uses is also consistent with
5 Policy 16.5, which seeks to focus more intensive development
6 along arterials and clogged roadways into areas outside of
7 existing and developing neighborhoods.

8 The property is subject to commercial-locational
9 criteria pursuant to Future Land Use Element Policy 22.2. The
10 maximum building square footage permitted for the neighborhood
11 serving commercial uses per quadrant is 175,000 square feet in
12 the suburban mixed-use-6 Future Land Use category. The property
13 occupies the northeast quadrant of the intersection and is
14 within the required distance of the intersection.

15 The site is located within the limits of the Riverview
16 Community Plan in the residential district. Although the
17 proposed development would not support the vision of the
18 residential district by developing a mix of commercial uses on
19 the southern portion of the site, Goal 4 calls for internal and
20 external pedestrian connections for providing a safe and
21 efficient multi-modal system within Riverview. That Community
22 Plan also envisions connecting pedestrians with public places
23 such as parks, and, overall, the proposed project is consistent
24 with the intent of the Community Plan.

25 And based on these considerations, Planning Commission

1 staff finds the proposed development consistent with the
2 Unincorporated Hillsborough County Comprehensive Plan subject to
3 the conditions proposed by the Development Services Department.

4 HEARING MASTER: Thank you so much.

5 Is there anyone in the room or online that would like
6 to speak in support? Anyone in favor? Seeing no one. Anyone
7 in opposition? No one.

8 Ms. Heinrich.

9 MS. HEINRICH: Yes ma'am?

10 HEARING MASTER: Anything else?

11 MS. HEINRICH: No, ma'am.

12 HEARING MASTER: Okay. Thank you.

13 Sir, you have the last word if you'd like it.

14 MR. REALI: Again, Kevin Reali, for the record. Let
15 me just say we have support from all of the different staff
16 departments and request approval. Thank you.

17 HEARING MASTER: All right. Thank you so much. I
18 appreciate it.

19 And, with that, we'll close Major Modification 23-0414
20 and go to the last case.

21 MS. HEINRICH: Our next item is Item D.5, Major Mod
22 application 23-0578. The applicant is requesting a major
23 modification to PD 04-1476. Sam Ball with Development Services
24 has reviewed this and will provide staff findings after the
25 applicant's presentation.

1 Item A.5, Major Mod Application 23-0281. This
2 application is out of order to be heard and is being continued
3 to September 18, 2023 ZHM hearing.

4 Item A.6, PD 0287. This application is out of order
5 to be heard is being and is being continued to the
6 September 18, 2023 ZHM hearing.

7 Item A.7, PD 23-0369. This application is out of
8 order to be heard and is being continued to the
9 September 18, 2023 ZHM hearing.

10 Item A.8, PD 23-0406. This application is out of
11 order to be heard and is being continued to the October 16, 2023
12 ZHM hearing.

13 Item A.9, Major Mod Application 23-0407. This
14 application is being continued by Staff to the
15 September 18, 2023 ZHM hearing.

16 Item A.10, Major Mod 23-0414. This application is
17 being continued by the applicant to the September 18, 2023 ZHM
18 hearing.

19 Item A.11, PD 23-0472. This application is out of
20 order to be heard and is being continued to the
21 September 18, 2023 ZHM hearing.

22 Item A.12, PD 23-0516. This application is being
23 withdrawn from the ZHM process.

24 Item A.13, PD 23-0517. This application is out order
25 to be heard and is being continued to the September 18, 2023 ZHM

1 order to be heard and is being continued to the August 21, 2023
2 ZHM hearing.

3 Item A.10, Major Modification 23-0281. This
4 application is out of order to be heard and is being continued
5 to the August 21, 2023 ZHM hearing.

6 Item A.11 PD 23-0287. This application is out of
7 order to be heard and is being continued to the August 21, 2023
8 ZHM hearing.

9 Item A.12, Standard Rezoning 23-0324. This
10 application is out of order to be heard and is being continued
11 to the August 21, 2023 ZHM hearing.

12 Item A.13, PD 23-0369. This application is out of
13 order to be heard and is being continued to the August 21, 2023
14 ZHM hearing.

15 Item A.14 PD 23-0406. This application is out of
16 order to be heard and is being continued to the August 21, 2023
17 ZHM hearing.

18 Item A.15, Major Modification, 23-0407. This
19 application is out of order to be heard and is being continued
20 to the August 21, 2023 ZHM hearing.

21 Item A.16, PD 23-0408. This application has been
22 continued by the applicant to the August 21, 2023 ZHM hearing.

23 Item A.17, Major Mod 23-0414. This application is
24 being continued by Staff to the August 21, 2023 ZHM hearing.

25 Item A.18, PD Application 23-0422. This application



**EXHIBITS SUBMITTED
DURING THE ZHM HEARING**

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PAGE 1 OF 6

DATE/TIME: 9/18/2023 6pm HEARING MASTER: Susan Finch

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APPLICATION # <u>23-0369</u>	PLEASE PRINT NAME <u>Todd Pressman</u> MAILING ADDRESS <u>206 2nd Ave S. #451</u> CITY <u>St. Pete</u> STATE <u>FL</u> ZIP <u>33701</u> PHONE _____
APPLICATION # <u>23-0203</u>	PLEASE PRINT NAME <u>SUSAN SWIFT</u> MAILING ADDRESS <u>607 S. Alexander St #101</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>335</u> PHONE <u>813 247 9100</u>
APPLICATION # <u>23-0082</u>	PLEASE PRINT NAME <u>Todd Pressman</u> MAILING ADDRESS <u>200 2nd Ave S #451</u> CITY <u>St. Pete</u> STATE <u>FL</u> ZIP <u>33701</u> PHONE <u>727-824 1260</u>
APPLICATION # <u>23-0082</u>	PLEASE PRINT NAME <u>Michael Bernstein</u> MAILING ADDRESS <u>19537 Deer Lake Rd</u> CITY <u>Thutz</u> STATE <u>FL</u> ZIP <u>33549</u> PHONE <u>813 293 1930</u>
APPLICATION # <u>23-0082</u>	PLEASE PRINT NAME <u>JAY A MUFFLY</u> MAILING ADDRESS <u>102 5TH AVE SE</u> CITY <u>LUTE</u> STATE <u>FL</u> ZIP <u>33549</u> PHONE <u>813 949-2224</u>
APPLICATION # <u>23-0552</u> <u>VS</u>	PLEASE PRINT NAME <u>Gloria Linda Stewart</u> MAILING ADDRESS <u>6997-B Professional Parkway East</u> CITY <u>Sarasota</u> STATE <u>FL</u> ZIP <u>34246</u> PHONE _____

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PAGE 2 OF 6

DATE/TIME: 9/18/23 6pm HEARING MASTER: Susan Finch

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APPLICATION # <u>23-0552</u> <u>VS</u>	PLEASE PRINT NAME <u>Mollie Usher</u> MAILING ADDRESS <u>5513 Rain Frog Ln.</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33567</u> PHONE _____
APPLICATION # <u>23-0552</u>	PLEASE PRINT NAME <u>Jonathan Hoke</u> MAILING ADDRESS <u>5513 Rain Frog Ln</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33567</u> PHONE <u>813-7205151</u>
APPLICATION # <u>23-0552</u>	PLEASE PRINT NAME <u>Gretchen Genrich Hoke</u> MAILING ADDRESS <u>5513 Rain Frog Lane</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33567</u> PHONE <u>813-707-7039</u> <u>760-3981</u>
APPLICATION # <u>23-0552</u> <u>VS</u>	PLEASE PRINT NAME <u>Mollie Genrich</u> MAILING ADDRESS <u>5521 Rain Frog Ln.</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33567</u> PHONE _____
APPLICATION # <u>23-0552</u> <u>VS</u>	PLEASE PRINT NAME <u>Margaret Thompson</u> MAILING ADDRESS <u>5507 Rain Frog Ln</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33567</u> PHONE _____
APPLICATION # <u>23-0552</u> <u>VS</u>	PLEASE PRINT NAME <u>Charles Genrich</u> MAILING ADDRESS <u>5521 Rain Frog Ln.</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33567</u> PHONE _____

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PAGE 3 OF 6

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<p>APPLICATION # <u>23-0571</u></p>	<p>PLEASE PRINT NAME <u>Ruth Londono</u> MAILING ADDRESS <u>1502 W. Busch Blvd</u> CITY <u>Tpa</u> STATE <u>FL</u> ZIP <u>33612</u> PHONE <u>(813) 919-7802</u></p>
<p>APPLICATION # <u>23-0571</u></p>	<p>PLEASE PRINT NAME <u>Isabelle Albert</u> MAILING ADDRESS <u>1000 W. Ashley Dr.</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813 331-0976</u></p>
<p>APPLICATION # <u>23-6640</u></p>	<p>PLEASE PRINT NAME <u>Tu Mai</u> MAILING ADDRESS <u>14031 N. Dale Mabry Hwy</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33618</u> PHONE <u>(813) 962-6230</u></p>
<p>APPLICATION # <u>23-0792</u></p>	<p>PLEASE PRINT NAME <u>Tu Mai</u> MAILING ADDRESS <u>14031 N. Dale Mabry Hwy.</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33618</u> PHONE <u>(813) 962-6230</u></p>
<p>APPLICATION # <u>23-0792</u></p>	<p>PLEASE PRINT NAME <u>Hung Mai</u> MAILING ADDRESS <u>14031 N. Dale Mabry</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33618</u> PHONE <u>813 962-6230</u></p>
<p>APPLICATION # <u>23-0792</u></p>	<p>PLEASE PRINT NAME <u>Aleathea Hoskins</u> MAILING ADDRESS <u>2108 Siloam Springs Dr</u> CITY <u>Dover</u> STATE <u>FL</u> ZIP <u>33527</u> PHONE <u>813-431-9903</u></p>

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APPLICATION # <u>23-0792</u>	PLEASE PRINT NAME <u>Teri Wagner</u> MAILING ADDRESS <u>2108 Arch McDonald Dr</u> CITY <u>Dover</u> STATE <u>FL</u> ZIP <u>33502</u> PHONE <u>8134346722</u>
APPLICATION # <u>23-0792</u>	PLEASE PRINT NAME <u>Dana Wilson</u> MAILING ADDRESS <u>2102 Arch McDonald Drive</u> CITY <u>Dover</u> STATE <u>FL</u> ZIP <u>33527</u> PHONE <u>309-287-9739</u>
APPLICATION # <u>23-0792</u>	PLEASE PRINT NAME <u>Th Mai</u> MAILING ADDRESS <u>14031 N. Dale Mabry Hwy</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33618</u> PHONE <u>(813)962-6230</u>
APPLICATION # <u>23-0792</u>	PLEASE PRINT NAME <u>Justin Tillman</u> MAILING ADDRESS <u>2106 Siloam Springs</u> CITY <u>Dover</u> STATE <u>FL</u> ZIP <u>32227</u> PHONE <u>813335-484</u>
APPLICATION # <u>23-0792</u>	PLEASE PRINT NAME <u>Jow Berry</u> MAILING ADDRESS <u>1620 S Dover Rd</u> CITY <u>Dover</u> STATE <u>FL</u> ZIP <u>32227</u> PHONE <u>8132307536</u>
APPLICATION # <u>23-08416</u>	PLEASE PRINT NAME <u>Kami Corbett</u> MAILING ADDRESS <u>101 E Kennedy Blvd</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813-277-8421</u>

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PAGE 5 OF 6

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<p>APPLICATION # <u>23-0846</u></p>	<p>PLEASE PRINT NAME <u>Kathryn Barry</u> MAILING ADDRESS <u>3028 Colonial Ridge Dr</u> CITY <u>Brandon</u> STATE <u>FL</u> ZIP <u>33511</u> PHONE <u>540-419-5122</u></p>
<p>APPLICATION # <u>23-0846</u></p>	<p>PLEASE PRINT NAME <u>Steve Henry</u> MAILING ADDRESS <u>5023 W. LAMAR ST</u> CITY <u>TPA</u> STATE <u>FL</u> ZIP <u>33607</u> PHONE <u>813-289-0039</u></p>
<p>APPLICATION # <u>23-0059</u></p>	<p>PLEASE PRINT NAME <u>Mark Bentley</u> MAILING ADDRESS <u>401 E Jackson</u> CITY <u>TPA</u> STATE _____ ZIP _____ PHONE <u>813-225-2250</u></p>
<p>APPLICATION # <u>23-0059</u></p>	<p>PLEASE PRINT NAME <u>RYAN MANASSE</u> MAILING ADDRESS <u>401 E JACKSON ST</u> <u>STE 3100</u> CITY <u>TAMPA</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813-225-2500</u></p>
<p>APPLICATION # <u>23-0109</u> <u>VS</u></p>	<p>PLEASE PRINT NAME <u>Steve Schmitt</u> MAILING ADDRESS <u>5545 Wildwood Dr.</u> CITY <u>Reno</u> STATE <u>NV</u> ZIP <u>89511</u> PHONE _____</p>
<p>APPLICATION # <u>23-0414</u></p>	<p>PLEASE PRINT NAME <u>Kevin Reali</u> MAILING ADDRESS <u>401 E Jackson ST #2100</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813-222-5059</u></p>

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PAGE 6 OF 6

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<p>APPLICATION # 23-0578</p>	<p>PLEASE PRINT NAME <u>Alexandra Schaler</u> MAILING ADDRESS <u>400 N. Ashley Dr. Suite 1100</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813-319-0702</u></p>
<p>APPLICATION # 23-0578</p>	<p>PLEASE PRINT NAME <u>KATHY REYES</u> MAILING ADDRESS <u>10433 ALDER GREEN DR</u> CITY <u>RIVERVIEW</u> STATE <u>FL</u> ZIP <u>33578</u> PHONE <u>815-598-7541</u></p>
<p>APPLICATION # 23-0578</p>	<p>PLEASE PRINT NAME <u>ARMY ANTON</u> MAILING ADDRESS <u>10371 Scarlett Skimmer Dr.</u> CITY <u>Riverview</u> STATE <u>FL</u> ZIP <u>33578</u> PHONE <u>706-410-7933</u></p>
<p>APPLICATION # 23-0578 VS</p>	<p>PLEASE PRINT NAME <u>Cathy Aponte</u> MAILING ADDRESS <u>1340 Scarlett Skimmer Dr.</u> CITY <u>Riverview</u> STATE <u>FL</u> ZIP <u>33578</u> PHONE _____</p>
<p>APPLICATION # 23-0578</p>	<p>PLEASE PRINT NAME <u>STEVE HENRY</u> MAILING ADDRESS <u>5023 WILLOW ST</u> CITY <u>TPA</u> STATE <u>FL</u> ZIP <u>33607</u> PHONE <u>813-789-0039</u></p>
<p>APPLICATION #</p>	<p>PLEASE PRINT NAME _____ MAILING ADDRESS _____ CITY _____ STATE _____ ZIP _____ PHONE _____</p>

HEARING TYPE:

ZHM, PHM, VRH, LUHO

DATE: September 18, 2023

HEARING MASTER:


Susan Finch

PAGE: 1 OF 1

APPLICATION #	SUBMITTED BY	EXHIBITS SUBMITTED	HRG. MASTER YES OR NO
RZ 23-0203	Susan Swift	1. Applicant Presentation Packet	No
RZ 23-0082	Michelle Heinrich	1. Revised Staff Report – Email	No
RZ 23-0082	Todd Pressman	2. Applicant Presentation Packet	No
RZ 23-0552	Michelle Heinrich	1. Revised Staff Report – Email	No
RZ 23-0552	Jonathan Hoke	2. Opposition Presentation Packet	No
RZ 23-0552	Gretchen Hoke	3. Opposition Presentation Packet	No
RZ 23-0571	Michelle Heinrich	1. Revised Staff Report – Email	No
RZ 23-0571	Ruth Londono	2. Applicant Presentation Packet	No
RZ 23-0573	Michelle Heinrich	1. Revised Staff Report – Email	No
RZ 23-0573	Isabelle Albert	2. Applicant Presentation Packet	Yes (Copy)
RZ 23-0640	Michelle Heinrich	1. Revised Staff Report – Email	No
RZ 23-0792	Aleathea Hoskins	1. Opposition Presentation Packet	No
RZ 23-0792	Tu Mai	2. Applicant Presentation Packet	No
RZ 23-0846	Michelle Heinrich	1. Revised Staff Report – Email	No
RZ 23-0846	Kami Corbett	2. Applicant Presentation Packet	No
RZ 23-0059	Mark Bentley	1. Applicant Presentation Packet	Yes (Copy)
RZ 23-0109	Michelle Heinrich	1. Revised Staff Report – Email	No
MM 23-0414	Michelle Heinrich	1. Revised Staff Report- Email	No
MM 23-0414	Kevin Reali	2. Applicant Presentation Packet	No
MM 23-0578	Michelle Heinrich	1. Revised Staff Report – Email	No
MM 23-0578	Alexandra Schaler	2. Applicant Presentation Packet	No

SEPTEMBER 18, 2023 - ZONING HEARING MASTER

The Zoning Hearing Master (ZHM), Hillsborough County, Florida, met in Regular Meeting, scheduled for Monday, September 18, 2023, at 6:00 p.m., in the Boardroom, Frederick B. Karl County Center, Tampa, Florida, and held virtually.

 Susan Finch, ZHM, called the meeting to order at 6:00 p.m., led in the pledge of allegiance to the flag, and introduced Development Services (DS).

A. WITHDRAWALS AND CONTINUANCES

 Michelle Heinrich, DS, introduced staff, and reviewed changes/withdrawals/continuances.

 Susan Finch, ZHM, overview of ZHM process.

 Mary Dorman, Senior Assistant County Attorney, overview of oral argument/ZHM process.

 Susan Finch, ZHM, Oath.

B. REMANDS

B.1. RZ 23-0203

 Michelle Heinrich, DS, called RZ 23-0203.

 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0203.

C. REZONING STANDARD (RZ-STD):

C.1. RZ 23-0082

 Michelle Heinrich, DS, called RZ 23-0082.

 Testimony provided.


 Susan Finch, ZHM, closed RZ 23-0082.

MONDAY, SEPTEMBER 18, 2023

C.2. RZ 23-0552

 Michelle Heinrich, DS, called RZ 23-0552.

 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0552.

C.3. RZ 23-0571

 Michelle Heinrich, DS, called RZ 23-0571.

 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0571.

C.4. RZ 23-0573

 Michelle Heinrich, DS, called RZ 23-0573.


 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0573.

C.5. RZ 23-0640

 Michelle Heinrich, DS, called RZ 23-0640.


 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0640.

C.6. RZ 23-0792

 Michelle Heinrich, DS, called RZ 23-0792.

 Testimony provided.


 Susan Finch, ZHM, closed RZ 23-0792.

MONDAY, SEPTEMBER 18, 2023

C.7. RZ 23-00846

 Michelle Heinrich, DS, called RZ 23-0846.

 Testimony provided.


 Susan Finch, ZHM, closed RZ 23-0846.

D. REZONING-PLANNED DEVELOPMENT (RZ-PD) & MAJOR MODIFICATION (MM) :

D.1. RZ 23-0059

 Michelle Heinrich, DS, called RZ 23-0059.


 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0059.

D.2. RZ 23-0109

 Michelle Heinrich, DS, called RZ 23-0109.

 Testimony provided.

 Susan Finch, ZHM, closed RZ 23-0109.

D.3. RZ 23-0369

 Michelle Heinrich, DS, called RZ 23-0369.


 Testimony presented.

 Susan Finch, ZHM, continued RZ 23-0369 to November 13, 2023, ZHM.

D.4. MM 23-0414

 Michelle Heinrich, DS, called MM 23-0414.

 Testimony provided.


 Susan Finch, ZHM, closed MM 23-0414.

MONDAY, SEPTEMBER 18, 2023


D.5. MM 23-0578

 Michelle Heinrich, DS, called MM 23-0578.

 Testimony provided.

 Susan Finch, ZHM, closed MM 23-0578.

ADJOURNMENT

 Susan Finch, ZHM, adjourned meeting at 10:54 p.m.

PD Modification Application: MM 23-0414

Zoning Hearing Master Date: 09/18/23

BOCC Land Use Meeting Date: 11/07/23



**Hillsborough
County Florida**

Development Services Department

1.0 APPLICATION SUMMARY

Applicant: Riverview Investment Group, LLC

FLU Category: SMU-6, CMU-12

Service Area: Urban

Site Acreage: +/- 5.6 acres

Community Plan Area: Riverview

Overlay: None



Introduction Summary:

PD 06-0848 was approved in 2006 to allow (1) maximum of 351 MF residential units on 30.36 acres; (2) a maximum of 60,980-square-feet (“sf”) for Commercial Neighborhood zoning uses on 5.6 acres; (3) a maximum of 10,890-square-feet of Business Professional Office uses; and (4) a maximum of 32,670-square-feet, 150 room hotel in 3 acres, with a maximum height of 60 feet (5 stories).

MM 07-1378 approved an increase to the square footage of the hotel use from 32,670 sf to 65,340 sf.

PRS 08-0816 modified transportation conditions and increased the maximum square feet for the hotel from 65,340 sf to 68,880 sf, reduced the number of hotel rooms from 150 to 125 rooms, and increased the max. height of the hotel from 60 feet (5stories) to 74 feet (6 stories).

PRS 19-0778 modified certain transportation conditions, without changing the entitlements.

The applicant is requesting a major modification to the previously approved Plan Development (06-0848), as modified, generally as follows:

- (1) to expand the hotel’s square footage from 68,880 sf to 81,000 sf, through the use of Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel; and
- (2) to reduce the required parking spaces for the hotel from 138 to 125 spaces.

Existing Approval(s):	Proposed Modification(s):
1. Maximum of 68,880 sf hotel	1. Increase hotel to 81,000 sf
2. 138 hotel parking spaces	2. Reduce the hotel parking spaces to 125.
3. Maximum hotel height of 74 feet (6 stories)	3. No change.

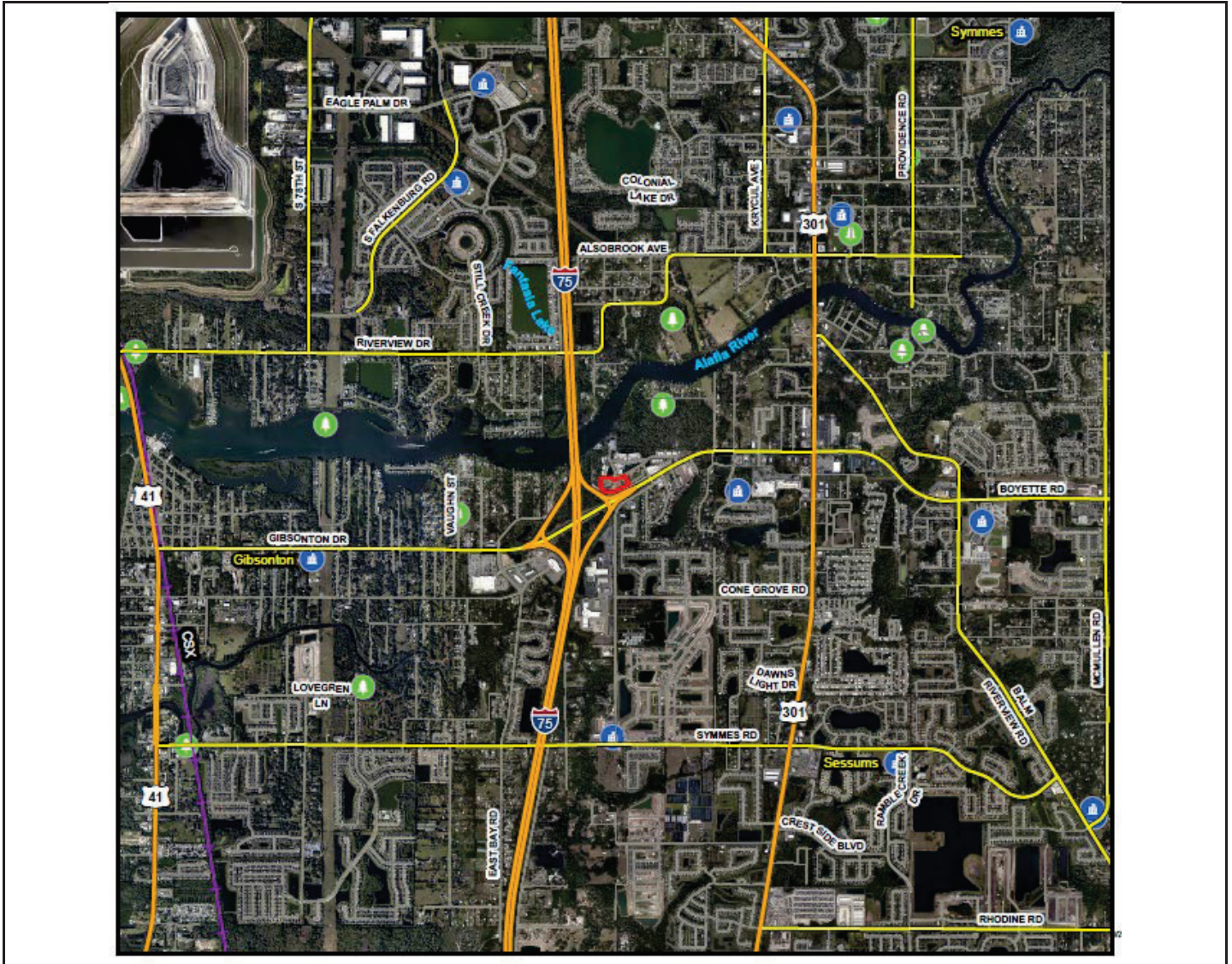
Additional Information:	
PD Variation(s):	None Requested as part of this application
Waiver(s) to the Land Development Code:	None requested.

Planning Commission Recommendation:
Consistent

Development Services Recommendation:
Approvable, subject to proposed conditions

2.0 LAND USE MAP SET AND SUMMARY DATA

2.1 Vicinity Map

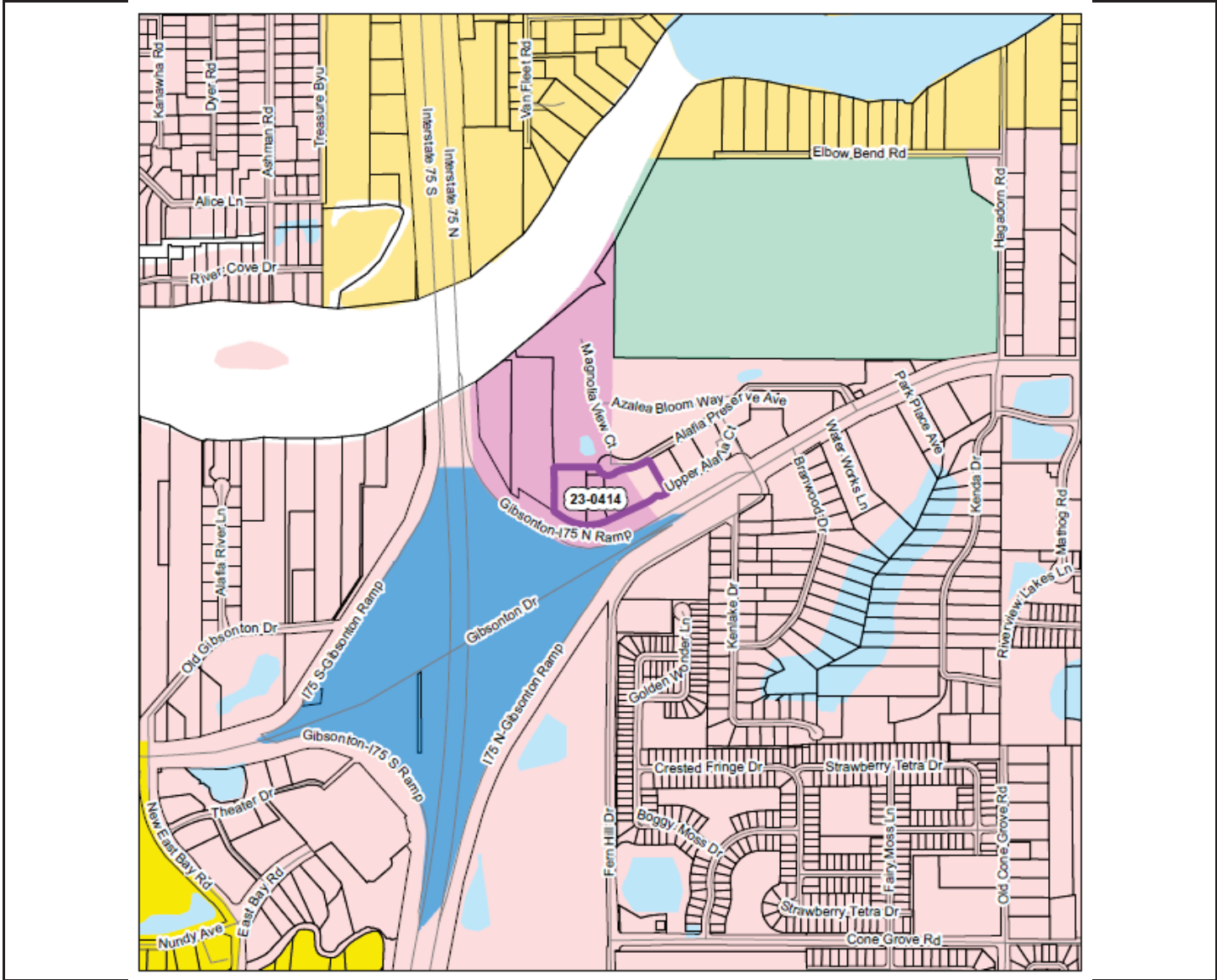


Context of Surrounding Area:

The subject property is located in the northeastern corner of Interstate 75 and Gibsonton Dr. The PD is approved for Multifamily and Commercial Neighborhood Uses. Existing land uses within the area include the multi-family residential development within the same overall PD, located to the immediate north of the portion of the site subject of this request.

2.0 LAND USE MAP SET AND SUMMARY DATA

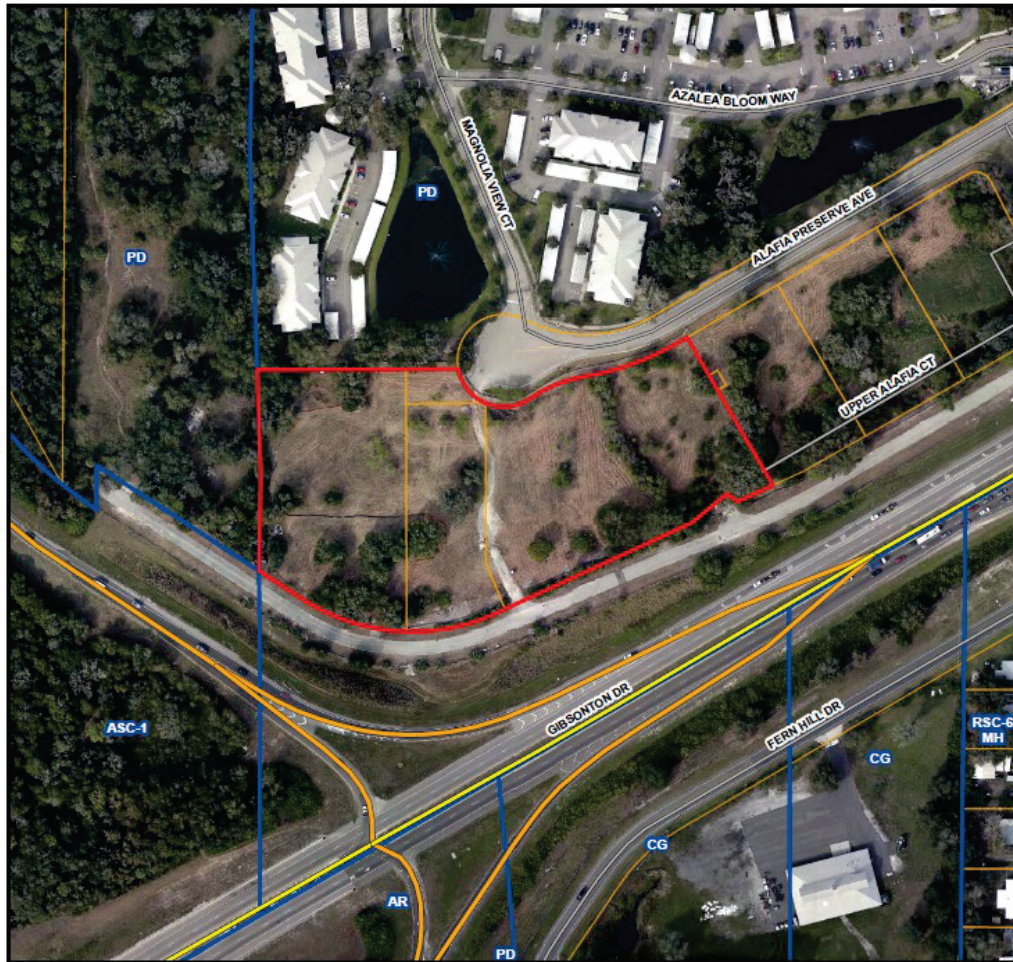
2.2 Future Land Use Map



Subject Site Future Land Use Category:	SMU-6, CMU-12
Maximum Density/F.A.R.:	SMU-6: 6 du per ac. /0.35 FAR CMU-12: 12 du per ac. / 0.5 FAR
Typical Uses:	SMU-6: Typical uses include residential, suburban commercial, offices, research parks, light industrial, multi-purpose, clustered residential, mixed-use. CMU-12: Typical uses include residential, community retail commercial, offices, research park, light industrial, multi-purpose, clustered residential, mixed-use.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Immediate Area Map

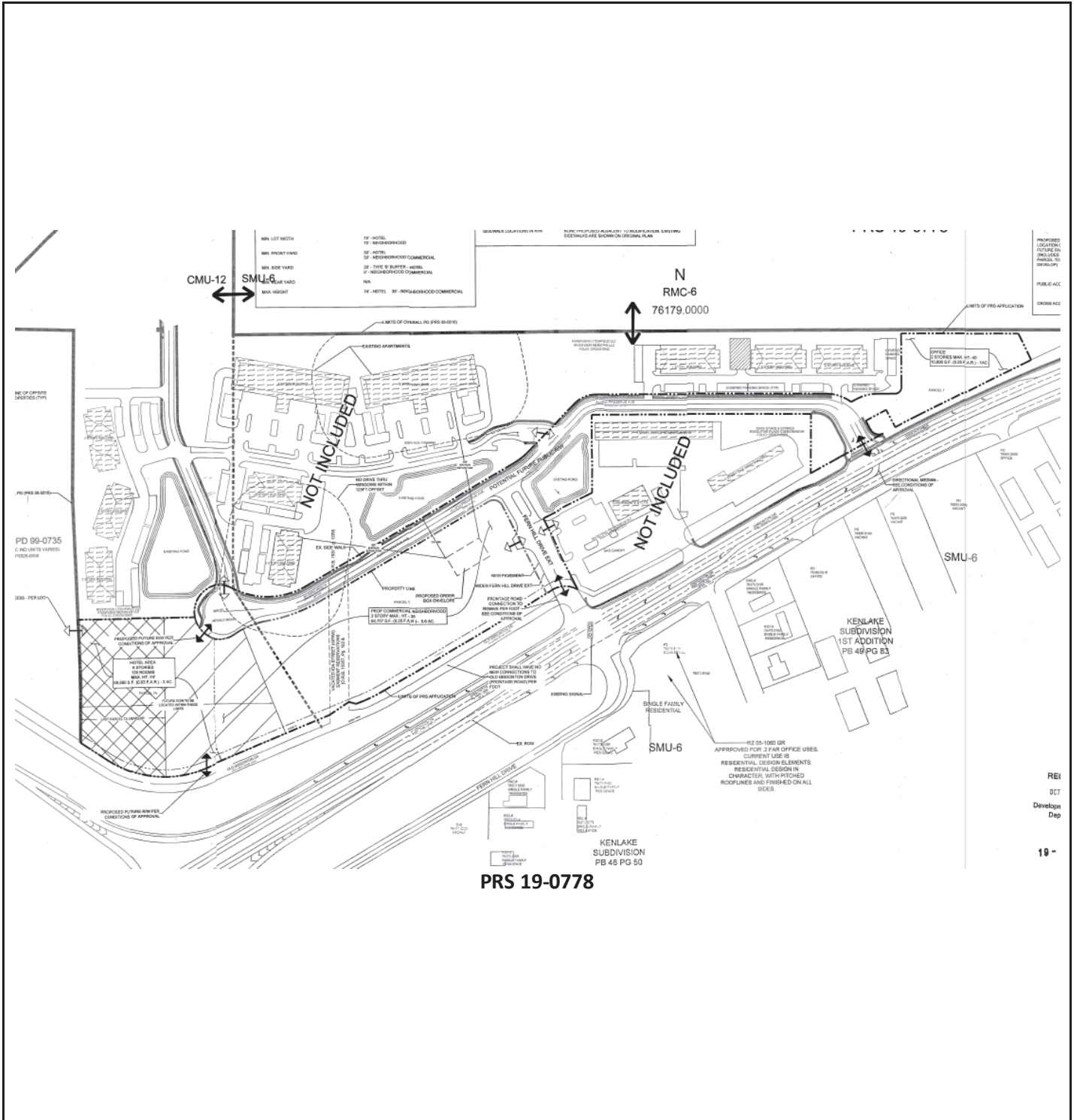


Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	PD 06-0848	351 MF residential units	Multi-family residential	Multi-family residential
South	Commercial General (CG)	Maximum 0.27 FAR	Uses allowed per Sec. 2.02.02 including commercial, offices.	Office building
East	PD 06-0848	Max. 60,980 sf CN; Max. 10,890 sf BPO	Neighborhood commercial/Office, Professional	Commercial strip center
West	PD 99-0735	Maximum 0.50 FAR	159 Dry Boat Storage, Max. 100 RV storage, 1,300 sf res., office, & retail for boat & RV maintenance, and 70-room hotel.	Vacant and boat ramp

2.0 LAND USE MAP SET AND SUMMARY DATA

2.4 Approved Site Plan (partial provided below for size and orientation purposes. See Section 8.1 for full site plan)

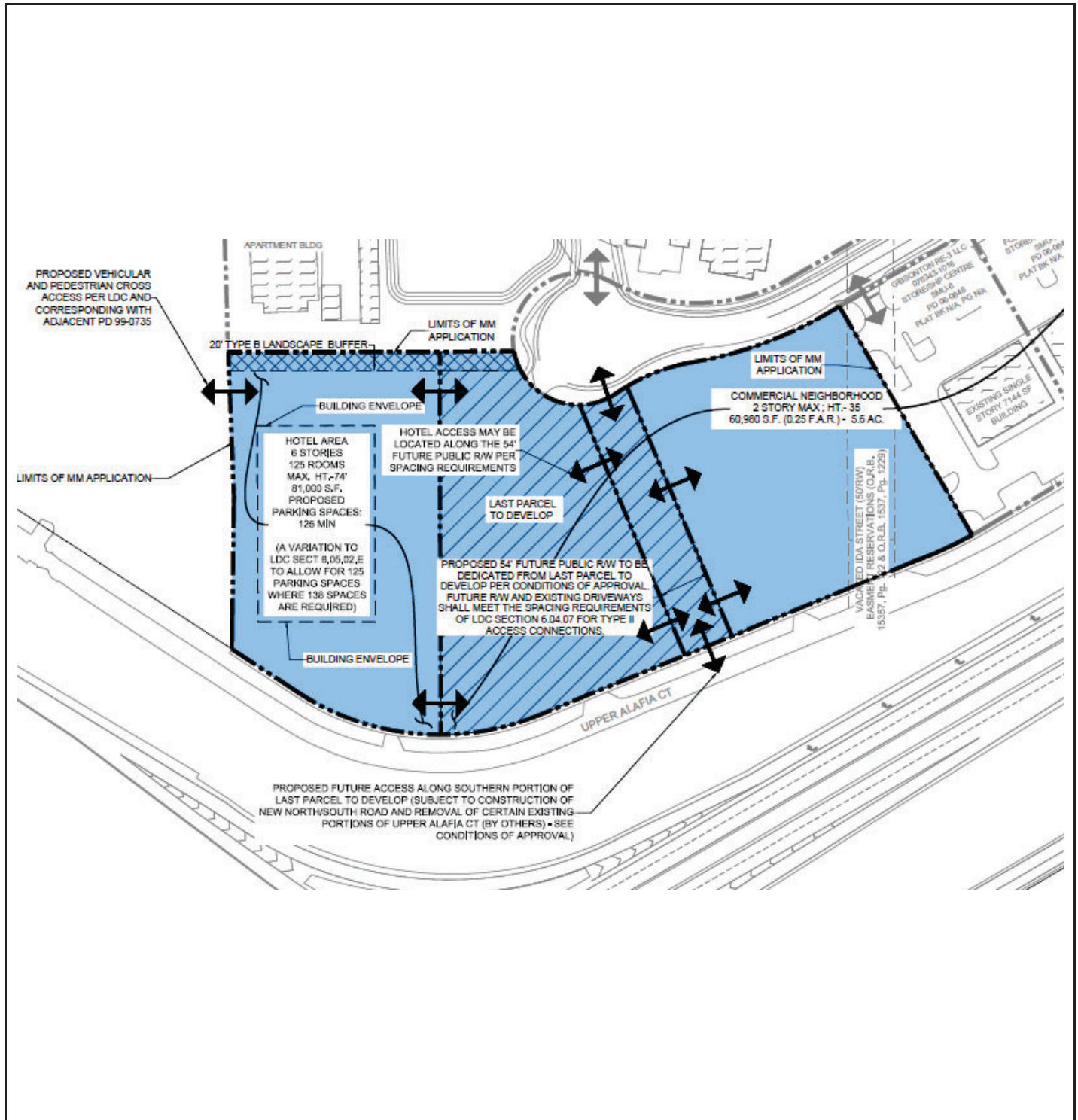


PRS 19-0778

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2.0 LAND USE MAP SET AND SUMMARY DATA

2.5 Proposed Site Plan (partial provided below for size and orientation purposes. See Section 8.2 for full site plan)



3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibson Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.

4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY

INFORMATION/REVIEWING AGENCY				
Environmental:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Natural Resources	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Check if Applicable: <ul style="list-style-type: none"> <input type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area <input type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Other 				
Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Transportation <input type="checkbox"/> Design Exc./Adm. Variance Requested <input checked="" type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See "Transportation Summary Report"
Service Area/ Water & Wastewater <input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Hillsborough County School Board Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Impact/Mobility Fees Hotel (Per room (Mobility and Parks)) (Per 1,000 s.f. (Fire)) Mobility: \$4,168 * 0 = \$0 Parks: \$1,327 * 0 = \$0 Fire: \$313 * 12.12 = \$3,793.56 Urban Mobility, South Fire - Hotel Expansion from 68,880 to 81,000 (12,120 sq ft) - no additional rooms				

ZHM HEARING DATE: September 18, 2023
 BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

Comprehensive Plan:	Comments Received	Findings	Conditions Requested	Additional Information/Comments
Planning Commission <input checked="" type="checkbox"/> Meets Locational Criteria <input type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input type="checkbox"/> N/A <input type="checkbox"/> Density Bonus Requested <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	

5.0 IMPLEMENTATION RECOMMENDATIONS

5.1 Compatibility

The subject site is currently undeveloped and located at the northeastern quadrant of Interstate 75 and Gibsonton Drive. The site is surrounded on the north and east by other properties within the same PD. To the immediate west of the subject site is PD 99-0735, which is approved for up to 159 Dry Boat Storage spaces, 100 RV storage spaces, 1,300 sf residential, office, and retail for boat and RV maintenance, and a 70-room hotel. South of the subject site is right-of-way for Interstate 75 North and Gibsonton Road. Directly south across Gibsonton Road is commercial zoned property developed with an office building. Further south past the Commercial General is single-family residential and southeast across Gibsonton Drive are mobile homes. The site is located in the Urban Service Area and within the limits of the Riverview Community Plan.

The applicant's request is to expand the hotel buildable square footage to allow up to 81,000 square feet, through the use of a Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel, and to reduce the parking spaces required for the hotel use from 138 to 125 pursuant to a PD Variation for Parking. The nonresidential square footage for Commercial Neighborhood uses is nominally increased from 60,707 to 60,980. To address these increases and account for the additional hotel square footage. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The scale of the proposed development only increases the square footage of the hotel while maintaining the number of hotel rooms at 125. The applicant states, "The hotel floor area increase is requested to accommodate larger extended stay hotel type rooms. The parking reduction is sought to reflect current market conditions and is supported by the Institute of Transportation Engineer's Parking Manual."

The applicant does not request any variations to Land Development Code Parts 6.06.00 (Landscaping/Buffering). The applicant is required to have a 20 ft. buffer with Type "B" screening adjacent to the multi-family development located to the north of the hotel, which the applicant is providing, as shown on the site plan. This was also previously provided and is not a change.

Transportation PD Variation for Parking

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project's hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project's 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms). LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces (138 x .05 equals 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

The uses proposed are unchanged and remain compatible. Based on the adjacent zonings and uses identified above in the report, staff finds the proposed PD zoning district compatible with the existing zoning districts and development pattern in the area. The subject site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land Development Code. The proposed Planned Development

would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated Hillsborough County Comprehensive Plan.

5.2 Recommendation

Based on the above, staff finds the request approvable.

6.0 PROPOSED CONDITIONS

- Prior to PD Site Plan Certification, the developer shall revise the Site Plan to:
 - Under “Proposed Uses” update “352” to “351” Multifamily Units;
 - Under “Available Entitlement Calculation” update Total Units from “352” to “351”;
 - Under “Utilized Entitlement Calculation, Residential Density” update “352” to “351”;
 - Show the proper intersection configuration (i.e. the raised directional median) at the intersection of Gibsonton Dr. and Alafia Preserve Ave.);
 - Modify the label reading “PROPOSED 54’ FUTURE PUBLIC R/W TO BE DEDICATED FROM LAST PARCEL TO DEVELOP PER CONDITIONS OF APPROVAL. FUTURE R/W AND EXISTING DRIVEWAYS SHALL MEET THE SPACING REQUIREMENTS OF LDC SECTION 6.04.07 FOR TYPE II ACCESS CONNECTIONS.” to instead read “PROPOSED 54’ PUBLIC RIGHT-OF-WAY DEDICATION AND CONVEYANCE – SEE CONDITIONS OF APPROVAL.”

Approval- Approval of the request, subject to the conditions listed below, is based on the general site plan submitted August 28, 2023.

1. The multi-use development shall be limited as follows:

1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)

1.2 The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.

~~1.2~~ 1.3 The project shall be permitted a maximum of ~~60,707~~ 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.

~~1.3~~ 1.4 The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-0) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.

~~1.4~~ 1.5 The project shall be permitted a maximum of ~~68,880~~ 81,000 square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General

(CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).

- 1.5 1.6 Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
- 1.6 1.7 Location of order boxes for the drive-through facility shall be a minimum of 120 feet from the Residential Multi-family property line internal to the mixed use project as shown on the General Site Plan. Additionally, a solid fence or wall shall not be required to be placed between the site used for a drive-through facility and the Multi-family property. Buffering and Screening requirements for the vehicular use areas shall be in accordance with LDC Section 6.06.00.
2. The project may be permitted a maximum of 351 multi-family dwelling units, a ~~68,880~~ 81,000 square foot hotel, ~~60,707~~ 60,980 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.
3. Cross access shall be provided between all outparcels and Alafia Preserve Avenue ~~the major internal roadway~~. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.
4. The applicant shall show the ability to provide cross access to the western property as shown on the site plan. If any of the adjacent properties are developed by the same developer/owner, then cross access must be provided.
5. A temporary access easement shall remain over vacated portions of Ida Street until such time as a permanent easement is approved by Hillsborough County. A permanent paved easement approved by Hillsborough County shall provide access from the site to the western portion of the ELAPP preserve.
6. The following conditions are required access improvements to Gibsonton Drive:
 - 6.1 The Gibsonton Drive and Fem Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to ~~FOOT~~ FDOT approval.
 - 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right- in/right-out. [THIS CONDITION HAS BEEN SATISFIED].
 - 6.3 The site currently ~~has access to~~ abuts, but does not have access to, the ~~FOOT~~ FDOT frontage road that runs along Gibsonton Drive. ~~FOOT~~ FDOT has indicated that the portion of the frontage road east of the proposed 54-foot-wide public right-of-way and continuing to the Fem

Hill Drive Ext. access may will be restricted/modified removed in the future, once a new public roadway segment is constructed within the referenced right-of-way. Nothing herein this condition shall be construed as requiring the developer of the subject PD to construct such public roadway facility.

7. Gibsonton Drive and ~~Fem~~ Fern Hill Drive has been signaled. As a result, the connection from Old Gibsonton Drive to Fem Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive (i.e., the frontage road) currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:

7.1 ~~Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue~~ Developer shall dedicate and convey Fem Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. ~~The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and~~

Dedication shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan,
- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties, or
- c. September 12, 2025.

The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations.

7.2 To provide the alternative connection for Old Gibsonton Dr. (i.e., the frontage road) to the Fem Hill Drive ~~Extensions~~ Extension via Alafia Preserve Avenue, the Developer shall dedicate and convey a fifty-four (54) foot right-of-way within the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan, or

- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located ~~within the area labelled "Proposed Location of Future ROW"~~ on as generally shown on the Site Plan at the discretion of Developer consistent with applicable transportation technical and access management standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

8. Prior to or concurrent with the next increment of development, The the developer will provide a bus bay, and transit accessory pad including, shelter, seating, trash receptacle, and bicycle rack for existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.
9. Notwithstanding anything on the Site Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
10. Construction access shall be limited to those locations shown on PD site plan which are also proposed vehicular access connections. The developer shall include a note in each site/construction plan submittal which indicates same.
11. Parking shall be provided in accordance with Section 6.05 of the Hillsborough County Land Development Code. Notwithstanding the above, the project's hotel uses shall be permitted to provide parking at a rate of 1.0 parking space per room. Additionally, the developer will be required to construct 6 bicycle parking spaces which meet the location and design criteria found within Sec. 6.05.02.P. of the LDC.
- ~~9.~~ 12. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
- ~~10.~~ 13. Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.
- ~~11.~~ 14. A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to

Construction Site Development Review approval.

- ~~12.~~ 15. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise . References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
- ~~13.~~ 16. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- ~~14.~~ 17. Within 90 days of approval by the Hillsborough County Board of County Commissioners, the applicant shall submit to the Development Services Department a revised General Development Plan for certification which conforms the notes and graphic of the plan to the conditions outlined above and the Land Development Code (LDC). Subsequent to certification of the plan, if it is determined the certified plan does not accurately reflect the conditions of approval or requirements of the LDC, said plan will be deemed invalid and certification of the revised plan will be required.
- 18. In accordance with LDC Section 5.03.07.C, the certified PD general site plan shall expire for the internal transportation network and external access points, as well as for any conditions related to the internal transportation network and external access points, if site construction plans, or equivalent thereof, have not been approved for all or part of the subject Planned Development within 5 years of the effective date of the PD unless an extension is granted as provided in the LDC. Upon expiration, re-certification of the PD General Site Plan shall be required in accordance with provisions set forth in LDC Section 5.03.07.C

ZHM HEARING DATE: September 18, 2023
BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

Zoning Administrator Sign Off:



J. Brian Grady
Mon Sep 11 2023 08:05:32

SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

APPLICATION NUMBER: MM 23-0414

ZHM HEARING DATE: September 18, 2023

BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS

9.0 FULL TRANSPORTATION REPORT (see following pages)

AGENCY REVIEW COMMENT SHEET

TO: ZONING TECHNICIAN, Development Services Department

DATE: 09/09/2023

Revised: 9/11/2023

REVIEWER: James Ratliff, AICP, PTP

AGENCY/DEPT: Transportation

PLANNING SECTOR/AREA: South/ RV

PETITION NO: MM 23-0414

-
- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to listed or attached conditions.
- This agency objects, based on the listed or attached grounds.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Conditions

1. The multi-use development shall be limited as follows:
 - 1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary
	40 feet from ELAPP property
	50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2 The project shall be permitted a maximum of 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.
 - 1.4 The project shall be permitted a maximum ~~68,880~~[81,000](#) square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General (CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).

3. Cross access shall be provided between all outparcels and Alafia Preserve Ave.~~the major internal roadway~~. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.

[Transportation Review Section staff is proposing to modify this condition for clarity.]

6. The following conditions ~~are~~address required access improvements to Gibsonton Drive:
 - 6.1 The Gibsonton Drive and Fem Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to ~~FOOT~~FDOT approval.
 - 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right-in/right-out. [THIS CONDITION HAS BEEN SATISFIED].
 - 6.3 The site currently ~~has access to~~abuts, but does not have access to, the ~~FOOT~~FDOT frontage road that runs along Gibsonton Drive. ~~FOOT~~FDOT has indicated that the portion of the frontage road east of the proposed 54-foot-wide public right-of-way and continuing to the Fern Hill Dr. Ext. access may will be restricted/modified removed in the future, once a new public roadway segment is constructed within the referenced right-of-way. Nothing herein this condition shall be construed as requiring the developer of the subject PD to construct such public roadway facility.

[Transportation Review Section staff is proposing to modify this condition for clarity, to better reflect the FDOT and County's intentions/status of area improvements, and more accurately describe the planned future configuration of transportation facilities within the area.]

7. Gibsonton Drive and Fern Hill Drive has been signalized. As a result, the connection from Old Gibsonton Drive to Fern Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive (i.e. the frontage road) currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:

- 7.1 ~~Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue,~~ Developer shall dedicate and convey Fern Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. ~~The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and~~

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan; or,

- b. within ninety (90) days from Developer’s receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties; or,
- c. September 12, 2025.

The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations.

- 7.2 To provide the alternative connection for Old Gibsonton Dr. (i.e. the frontage road) to the Fern Hill Drive ~~Extensions~~Extension via Alafia Preserve Avenue, the Developer shall dedicate and convey a fifty-four (54) foot right-of-way within the parcel labeled “Last Parcel to Develop” as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the “Last Parcel to Develop” as depicted on the Site Plan, or
- b. within ninety (90) days from Developer’s receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located ~~within the area labeled “Proposed Location of Future ROW”~~ as generally shown on the Site Plan, at the discretion of Developer consistent with applicable transportation technical and access management standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

[Transportation Review Section staff is proposing to modify this condition for clarity, to comport with the applicant’s request, and to provide certainty for how and when these facilities will be provided.]

8. Prior to concurrent with the next increment of development, ~~The~~ the developer will provide a bus bay and ~~;~~ transit accessory pad including; shelter, seating, trash receptacle, and bicycle rack for the existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.

[Transportation Review Section staff is proposing to modify this condition for clarity, to make it clear when the required improvements are expected to be constructed, and to reflect the language of required transit improvements and appurtenances pursuant to Sections 6.02.17 and 6.03.09 of the LDC.]

New Conditions

- Notwithstanding anything on the Site Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
- Construction access shall be limited to those locations shown on PD site plan which are also proposed vehicular access connections. The developer shall include a note in each site/construction plan submittal which indicates same.
- Parking shall be provided in accordance with Section 6.05 of the Hillsborough County Land Development Code. Notwithstanding the above, the project's hotel uses shall be permitted to provide parking at a rate of 1.0 parking space per room. Additionally, the developer will be required to construct 6 bicycle parking spaces which meet the location and design criteria found within Sec. 6.05.02.P. of the LDC.

Other Conditions

- Prior to PD Site Plan Certification, the developer shall revise the Site Plan to:
 - Show the proper intersection configuration (i.e. the raised directional median) at the intersection of Gibsonton Dr. and Alafia Preserve Ave.);
 - Modify the label reading "PROPOSED 54' FUTURE PUBLIC R/W TO BE DEDICATED FROM LAST PARCEL TO DEVELOP PER CONDITIONS OF APPROVAL. FUTURE R/W AND EXISTING DRIVEWAYS SHALL MEET THE SPACING REQUIREMENTS OF LDC SECTION 6.04.07 FOR TYPE II ACCESS CONNECTIONS." to instead read "PROPOSED 54' PUBLIC RIGHT-OF-WAY DEDICATION AND CONVEYANCE – SEE CONDITIONS OF APPROVAL."

PROJECT OVERVIEW AND TRIP GENERATION

The applicant is requesting a Major Modification to a +/- 5.6 ac. portion of a +/- 39.96 ac. previously approved Planned Development (PD) #06-0848, and as most recently modified via PRS 19-0778. The existing PD is approved for 351 multi-family single-family dwelling units, 60,707 s.f. of Commercial Neighborhood (CN) uses, and 10,890 s.f. of Business Professional Office (BPO) uses, and a 68,880 s.f. hotel with a maximum of 125 rooms. The applicant is proposing to increase the allowable square-footage of hotel uses from 68,880 to 81,000 s.f. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

Given the above, and that there are no changes in proposed access configurations, staff did not require a trip generation and site access analysis to process this request, consistent with the Development Review Procedures Manual (DRPM). Staff prepared the below analysis, comparing trips potentially generally by development under the approved and proposed zoning, utilized a generalized worst-case scenario. Data provided below is based on information from the Institute of Transportation Engineer's Trip Generation Manual, 11th Edition.

Existing Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
60,980 s.f. Shopping Plaza (40-150k) (ITE LUC 821)	5,762	215	587
352 Multi-Family Dwelling Units (ITE LUC 220)	2,332	132	172
10,890 Medical Office Uses (ITE LUC 720)	392	33	41
125 Room Hotel (ITE LUC 310)	998	55	65
Subtotal:	9,484	435	865

Proposed Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
60,980 s.f. Shopping Plaza (40-150k) (ITE LUC 821)	5,762	215	587
352 Multi-Family Dwelling Units (ITE LUC 220)	2,332	132	172
10,890 Medical Office Uses (ITE LUC 720)	392	33	41
125 Room Hotel (ITE LUC 310)	998	55	65
Subtotal:	9,484	435	865

Difference:

	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
	No Change	No Change	No Change

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Alafia Preserve Ave. is a 2-lane, undivided, privately maintained, substandard, local roadway. The roadway is characterized by 11-foot-wide travel lanes in average condition. The roadway lies within a +/- 55-foot wide right-of-way. There are +/- 5-foot-wide sidewalks along portions of the roadway. There are +/- 5-foot-wide bicycle facilities along portions of the roadway.

The Fern Hill Dr. Ext. is a 2-lane, divided, privately maintained, substandard, local roadway. The roadway is characterized by 15-foot-wide travel lanes in above average condition. The roadway lies within a +/- 100-foot-wide right-of-way. There are +/- 6-foot-wide sidewalks along both sides of the roadway. There are no bicycle facilities along this segment of the roadway.

SITE ACCESS AND CIRCULATION

No changes to existing approved access or circulation are proposed.

CHANGES TO RIGHT-OF-WAY DEDICATION REQUIREMENTS

Staff notes that the project is currently in violation of its PD zoning conditions (see existing condition 7.1), due to Alafia Preserve Ave. and newly constructed Fern Hill Rd. extension not having been dedicated and conveyed to the County in accordance with current requirements. The applicant has proposed modifications which will cure that violation. Specifically, the applicant is proposing to modify the conditions to provide flexibility in the timing and location of the 54-foot-wide right-of-way dedication and conveyance area (i.e. for the lands needed to accommodate the new north/south public roadway necessary to allow for removal of certain portions of the existing frontage road). They are also proposing modifications to the condition governing the timing of the requirement to dedication and convey Alafia Preserve Ave. and Fern Hill Dr. Staff has reviewed these condition changes with the County Engineer and supports the proposed revisions offered by staff, hereinabove. Staff notes that conversations between the County and applicant regarding what data, analysis, and/or improvements may be required to both the Fern Hill Dr. Ext. and Alafia Preserve Ave., prior to the County being able to accept those facilities for ownership and maintenance, are ongoing.

REQUESTED PD VARIATION – PARKING

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project’s hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project’s 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms).

The 5th Edition of the Institute of Transportation Engineer’s Parking Generation Manual provides data indicating a range of rates between 0.43 and 1.47 of the 22 hotels studied. They also provide an 85th Percentile rate of 0.99 parking spaces per room, with a 95% confidence interval of 0.83 on the high end.

Staff has reviewed the ITE data and recommends approval the request, subject to conditions. LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces (138 x .05 = 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

ROADWAY LEVEL OF SERVICE (LOS) INFORMATION

LOS information for adjacent roadway segments are provided below.

Roadway	From	To	LOS Standard	Peak Hour Directional LOS
Gibson Dr.	I-75	US 301	D	C

Source: Hillsborough County 2020 Level of Service Report.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibsonton Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input checked="" type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

PD Modification Application:

MM 23-0414

Zoning Hearing Master Date: 09/18/23

BOCC Land Use Meeting Date: 11/07/23



**Hillsborough
County Florida**

Development Services Department

1.0 APPLICATION SUMMARY

Applicant: Riverview Investment Group, LLC

FLU Category: SMU-6, CMU-12

Service Area: Urban

Site Acreage: +/- 5.6 acres

Community Plan Area: Riverview

Overlay: None



Introduction Summary:

PD 06-0848 was approved in 2006 to allow (1) maximum of 351 MF residential units on 30.36 acres; (2) a maximum of 60,980-square-feet ("sf") for Commercial Neighborhood zoning uses on 5.6 acres; (3) a maximum of 10,890-square-feet of Business Professional Office uses; and (4) a maximum of 32,670-square-feet, 150 room hotel in 3 acres, with a maximum height of 60 feet (5 stories).

MM 07-1378 approved an increase to the square footage of the hotel use from 32,670 sf to 65,340 sf.

PRS 08-0816 modified transportation conditions and increased the maximum square feet for the hotel from 65,340 sf to 68,880 sf, reduced the number of hotel rooms from 150 to 125 rooms, and increased the max. height of the hotel from 60 feet (5stories) to 74 feet (6 stories).

PRS 19-0778 modified certain transportation conditions, without changing the entitlements.

The applicant is requesting a major modification to the previously approved Plan Development (06-0848), as modified, generally as follows:

- (1) to expand the hotel's square footage from 68,880 sf to 81,000 sf, through the use of Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel; and
- (2) to reduce the required parking spaces for the hotel from 138 to 125 spaces.

Existing Approval(s):	Proposed Modification(s):
1. Maximum of 68,880 sf hotel	1. Increase hotel to 81,000 sf
2. 138 hotel parking spaces	2. Reduce the hotel parking spaces to 125.
3. Maximum hotel height of 74 feet (6 stories)	3. No change.

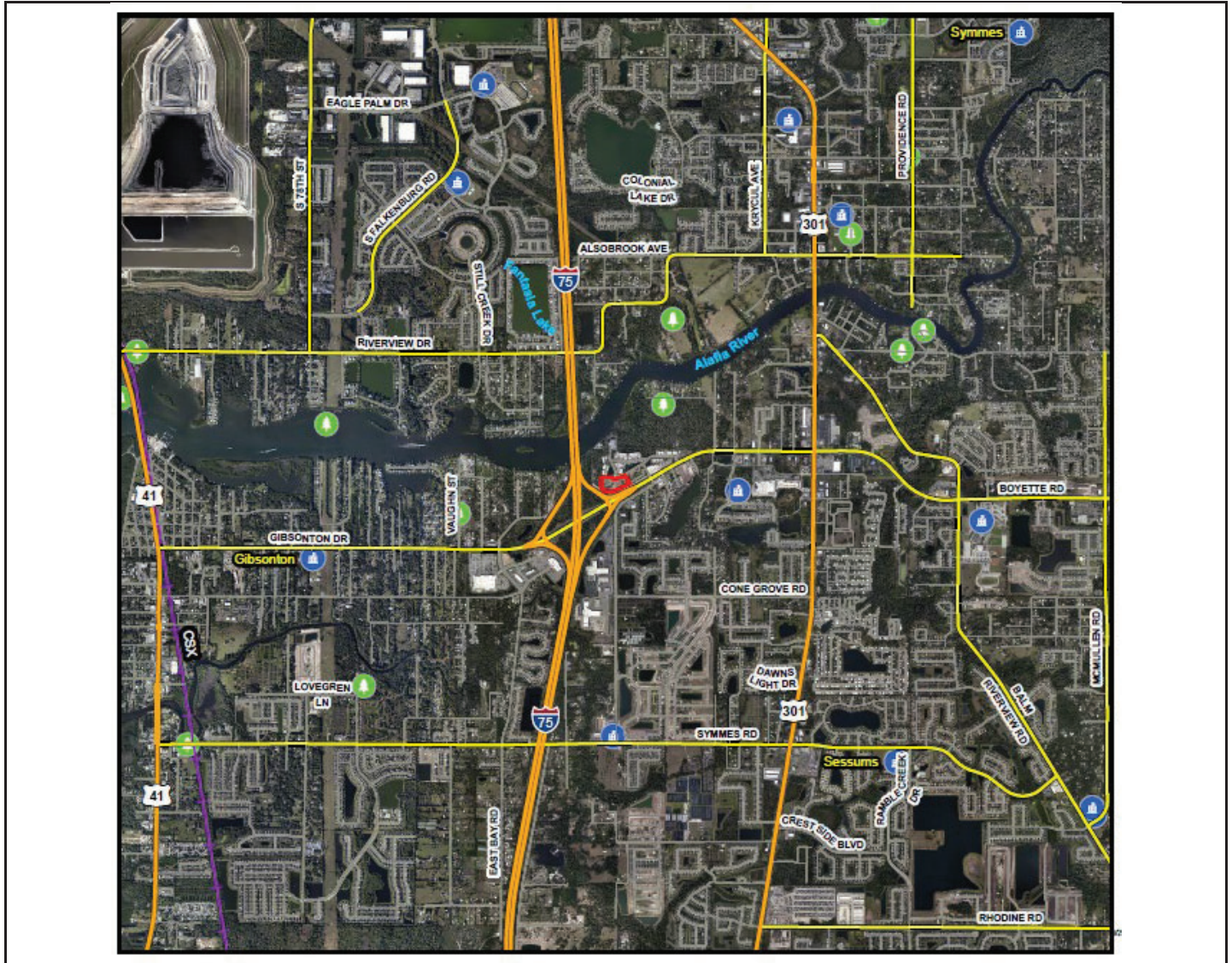
Additional Information:	
PD Variation(s):	LDC Part 6.05.00 (Parking/Loading)
Waiver(s) to the Land Development Code:	None requested.

Planning Commission Recommendation:
Consistent

Development Services Recommendation:
Approvable, subject to proposed conditions

2.0 LAND USE MAP SET AND SUMMARY DATA

2.1 Vicinity Map

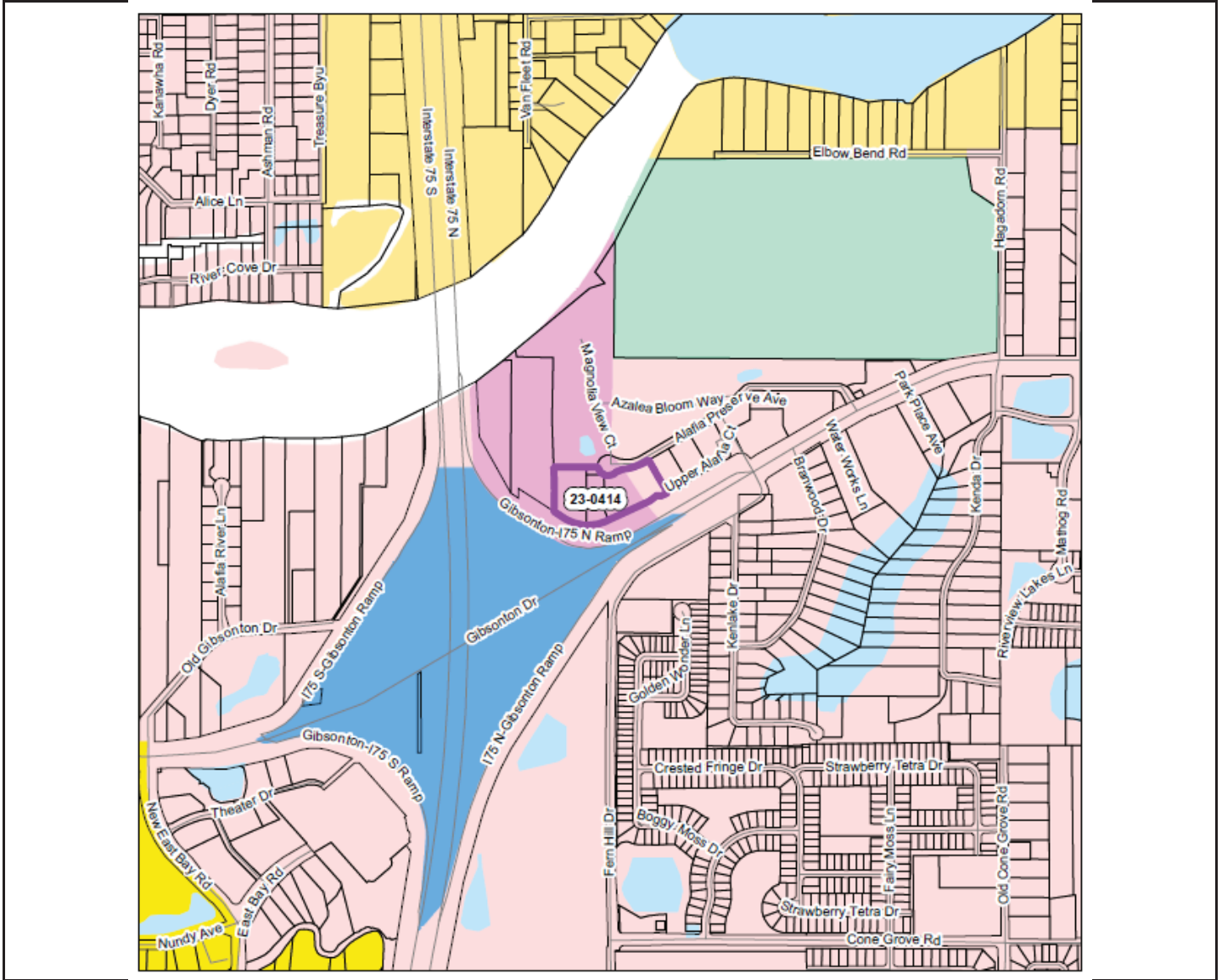


Context of Surrounding Area:

The subject property is located in the northeastern corner of Interstate 75 and Gibsonton Dr. The PD is approved for Multifamily and Commercial Neighborhood Uses. Existing land uses within the area include the multi-family residential development within the same overall PD, located to the immediate north of the portion of the site subject of this request.

2.0 LAND USE MAP SET AND SUMMARY DATA

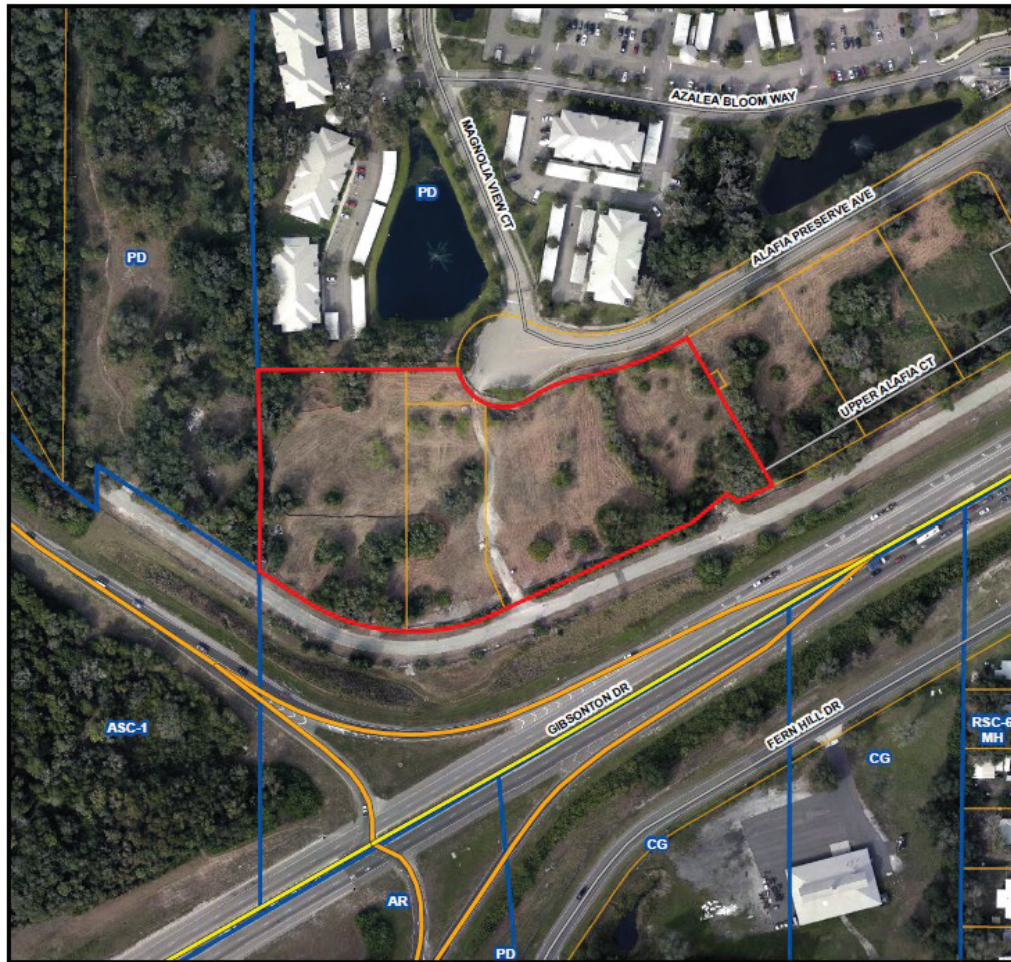
2.2 Future Land Use Map



Subject Site Future Land Use Category:	SMU-6, CMU-12
Maximum Density/F.A.R.:	SMU-6: 6 du per ac. /0.35 FAR CMU-12: 12 du per ac. / 0.5 FAR
Typical Uses:	SMU-6: Typical uses include residential, suburban commercial, offices, research parks, light industrial, multi-purpose, clustered residential, mixed-use. CMU-12: Typical uses include residential, community retail commercial, offices, research park, light industrial, multi-purpose, clustered residential, mixed-use.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Immediate Area Map

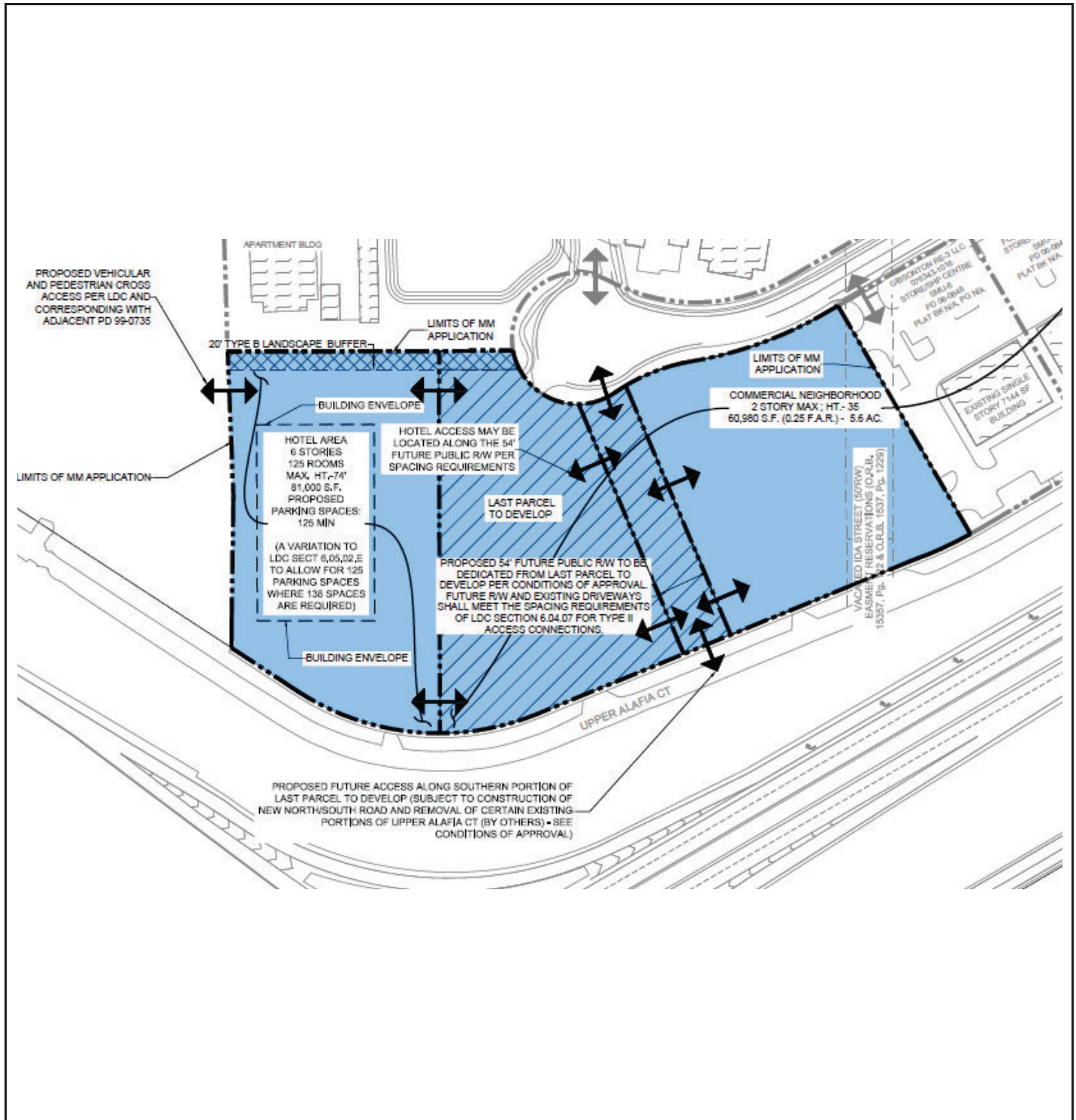


Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	PD 06-0848	351 MF residential units	Multi-family residential	Multi-family residential
South	Commercial General (CG)	Maximum 0.27 FAR	Uses allowed per Sec. 2.02.02 including commercial, offices.	Office building
East	PD 06-0848	Max. 60,980 sf CN; Max. 10,890 sf BPO	Neighborhood commercial/Office, Professional	Commercial strip center
West	PD 99-0735	Maximum 0.50 FAR	159 Dry Boat Storage, Max. 100 RV storage, 1,300 sf res., office, & retail for boat & RV maintenance, and 70-room hotel.	Vacant and boat ramp

2.0 LAND USE MAP SET AND SUMMARY DATA

2.5 Proposed Site Plan (partial provided below for size and orientation purposes. See Section 8.2 for full site plan)



3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibson Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.

4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY

INFORMATION/REVIEWING AGENCY				
Environmental:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Natural Resources	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Check if Applicable: <ul style="list-style-type: none"> <input type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area <input type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Other 				
Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Transportation <input type="checkbox"/> Design Exc./Adm. Variance Requested <input checked="" type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See "Transportation Summary Report"
Service Area/ Water & Wastewater <input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Hillsborough County School Board Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Impact/Mobility Fees Hotel (Per room (Mobility and Parks)) (Per 1,000 s.f. (Fire)) Mobility: \$4,168 * 0 = \$0 Parks: \$1,327 * 0 = \$0 Fire: \$313 * 12.12 = \$3,793.56 Urban Mobility, South Fire - Hotel Expansion from 68,880 to 81,000 (12,120 sq ft) - no additional rooms				

ZHM HEARING DATE: September 18, 2023
 BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

Comprehensive Plan:	Comments Received	Findings	Conditions Requested	Additional Information/Comments
Planning Commission <input checked="" type="checkbox"/> Meets Locational Criteria <input type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input type="checkbox"/> N/A <input type="checkbox"/> Density Bonus Requested <input checked="" type="checkbox"/> Consistent <input type="checkbox"/> Inconsistent	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	

5.0 IMPLEMENTATION RECOMMENDATIONS

5.1 Compatibility

The subject site is currently undeveloped and located at the northeastern quadrant of Interstate 75 and Gibsonton Drive. The site is surrounded on the north and east by other properties within the same PD. To the immediate west of the subject site is PD 99-0735, which is approved for up to 159 Dry Boat Storage spaces, 100 RV storage spaces, 1,300 sf residential, office, and retail for boat and RV maintenance, and a 70-room hotel. South of the subject site is right-of-way for Interstate 75 North and Gibsonton Road. Directly south across Gibsonton Road is commercial zoned property developed with an office building. Further south past the Commercial General is single-family residential and southeast across Gibsonton Drive are mobile homes. The site is located in the Urban Service Area and within the limits of the Riverview Community Plan.

The applicant's request is to expand the hotel buildable square footage to allow up to 81,000 square feet, through the use of a Comprehensive Plan Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel, and to reduce the parking spaces required for the hotel use from 138 to 125 pursuant to a PD Variation for Parking. The nonresidential square footage for Commercial Neighborhood uses is nominally increased from 60,707 to 60,980. To address these increases and account for the additional hotel square footage. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The scale of the proposed development only increases the square footage of the hotel while maintaining the number of hotel rooms at 125. The applicant states, "The hotel floor area increase is requested to accommodate larger extended stay hotel type rooms. The parking reduction is sought to reflect current market conditions and is supported by the Institute of Transportation Engineer's Parking Manual."

The applicant does not request any variations to Land Development Code Parts 6.06.00 (Landscaping/Buffering). The applicant is required to have a 20 ft. buffer with Type "B" screening adjacent to the multi-family development located to the north of the hotel, which the applicant is providing, as shown on the site plan. This was also previously provided and is not a change.

Transportation PD Variation for Parking

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project's hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project's 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms). LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces (138 x .05 equals 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

The uses proposed are unchanged and remain compatible. Based on the adjacent zonings and uses identified above in the report, staff finds the proposed PD zoning district compatible with the existing zoning districts and development pattern in the area. The subject site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land Development Code. The proposed Planned Development

would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated Hillsborough County Comprehensive Plan.

5.2 Recommendation

Based on the above, staff finds the request approvable.

6.0 PROPOSED CONDITIONS

- Prior to PD Site Plan Certification, the developer shall revise the Site Plan to:
 - Under “Proposed Uses” update “352” to “351” Multifamily Units;
 - Under “Available Entitlement Calculation” update Total Units from “352” to “351”;
 - Under “Utilized Entitlement Calculation, Residential Density” update “352” to “351”;
 - Show the proper intersection configuration (i.e. the raised directional median) at the intersection of Gibsonton Dr. and Alafia Preserve Ave.);
 - Modify the label reading “PROPOSED 54’ FUTURE PUBLIC R/W TO BE DEDICATED FROM LAST PARCEL TO DEVELOP PER CONDITIONS OF APPROVAL. FUTURE R/W AND EXISTING DRIVEWAYS SHALL MEET THE SPACING REQUIREMENTS OF LDC SECTION 6.04.07 FOR TYPE II ACCESS CONNECTIONS.” to instead read “PROPOSED 54’ PUBLIC RIGHT-OF-WAY DEDICATION AND CONVEYANCE – SEE CONDITIONS OF APPROVAL.”

Approval- Approval of the request, subject to the conditions listed below, is based on the general site plan submitted August 28, 2023.

1. The multi-use development shall be limited as follows:

1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)

1.2 The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.

~~1.2~~ 1.3 The project shall be permitted a maximum of ~~60,707~~ 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.

~~1.3~~ 1.4 The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-0) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.

~~1.4~~ 1.5 The project shall be permitted a maximum of ~~68,880~~ 81,000 square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General

(CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).

- 1.5 1.6 Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
- 1.6 1.7 Location of order boxes for the drive-through facility shall be a minimum of 120 feet from the Residential Multi-family property line internal to the mixed use project as shown on the General Site Plan. Additionally, a solid fence or wall shall not be required to be placed between the site used for a drive-through facility and the Multi-family property. Buffering and Screening requirements for the vehicular use areas shall be in accordance with LDC Section 6.06.00.
2. The project may be permitted a maximum of 351 multi-family dwelling units, a ~~68,880~~ 81,000 square foot hotel, ~~60,707~~ 60,980 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.
3. Cross access shall be provided between all outparcels and Alafia Preserve Avenue ~~the major internal roadway~~. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.
4. The applicant shall show the ability to provide cross access to the western property as shown on the site plan. If any of the adjacent properties are developed by the same developer/owner, then cross access must be provided.
5. A temporary access easement shall remain over vacated portions of Ida Street until such time as a permanent easement is approved by Hillsborough County. A permanent paved easement approved by Hillsborough County shall provide access from the site to the western portion of the ELAPP preserve.
6. The following conditions are required access improvements to Gibsonton Drive:
 - 6.1 The Gibsonton Drive and Fem Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to ~~FOOT~~ FDOT approval.
 - 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right- in/right-out. [THIS CONDITION HAS BEEN SATISFIED].
 - 6.3 The site currently ~~has access to~~ abuts, but does not have access to, the ~~FOOT~~ FDOT frontage road that runs along Gibsonton Drive. ~~FOOT~~ FDOT has indicated that the portion of the frontage road east of the proposed 54-foot-wide public right-of-way and continuing to the Fem

Hill Drive Ext. access may will be restricted/modified removed in the future, once a new public roadway segment is constructed within the referenced right-of-way. Nothing herein this condition shall be construed as requiring the developer of the subject PD to construct such public roadway facility.

7. Gibsonton Drive and ~~Fem~~ Fern Hill Drive has been signaled. As a result, the connection from Old Gibsonton Drive to Fem Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive (i.e., the frontage road) currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:

7.1 ~~Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue~~ Developer shall dedicate and convey Fem Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. ~~The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and~~

Dedication shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan,
- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties, or
- c. September 12, 2025.

The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations.

7.2 To provide the alternative connection for Old Gibsonton Dr. (i.e., the frontage road) to the Fem Hill Drive ~~Extensions~~ Extension via Alafia Preserve Avenue, the Developer shall dedicate and convey a fifty-four (54) foot right-of-way within the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan, or

- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located ~~within the area labelled "Proposed Location of Future ROW"~~ on as generally shown on the Site Plan at the discretion of Developer consistent with applicable transportation technical and access management standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

8. Prior to or concurrent with the next increment of development, The ~~the~~ developer will provide a bus bay, and transit accessory pad including, shelter, seating, trash receptacle, and bicycle rack for existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.
9. Notwithstanding anything on the Site Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
10. Construction access shall be limited to those locations shown on PD site plan which are also proposed vehicular access connections. The developer shall include a note in each site/construction plan submittal which indicates same.
11. Parking shall be provided in accordance with Section 6.05 of the Hillsborough County Land Development Code. Notwithstanding the above, the project's hotel uses shall be permitted to provide parking at a rate of 1.0 parking space per room. Additionally, the developer will be required to construct 6 bicycle parking spaces which meet the location and design criteria found within Sec. 6.05.02.P. of the LDC.
- ~~9.~~ 12. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
- ~~10.~~ 13. Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.
- ~~11.~~ 14. A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to

Construction Site Development Review approval.

- ~~12.~~ 15. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise . References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
- ~~13.~~ 16. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- ~~14.~~ 17. Within 90 days of approval by the Hillsborough County Board of County Commissioners, the applicant shall submit to the Development Services Department a revised General Development Plan for certification which conforms the notes and graphic of the plan to the conditions outlined above and the Land Development Code (LDC). Subsequent to certification of the plan, if it is determined the certified plan does not accurately reflect the conditions of approval or requirements of the LDC, said plan will be deemed invalid and certification of the revised plan will be required.
- 18. In accordance with LDC Section 5.03.07.C, the certified PD general site plan for the 5.6-acre area subject to MM 23-0414 shall expire for the internal transportation network and external access points, as well as for any conditions related to the internal transportation network and external access points, if site construction plans, or equivalent thereof, have not been approved for all or part of the subject Planned Development within 5 years of the effective date of the PD unless an extension is granted as provided in the LDC. Upon expiration, re-certification of the PD General Site Plan shall be required in accordance with provisions set forth in LDC Section 5.03.07.C

ZHM HEARING DATE: September 18, 2023
BOCC LUM MEETING DATE: November 7, 2023

Case Reviewer: Tim Lampkin, AICP

Zoning Administrator Sign Off:



J. Brian Grady
Mon Sep 11 2023 08:05:32

SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS

9.0 FULL TRANSPORTATION REPORT (see following pages)

AGENCY REVIEW COMMENT SHEET

TO: ZONING TECHNICIAN, Development Services Department

DATE: 09/09/2023

Revised: 9/11/2023

REVIEWER: James Ratliff, AICP, PTP

AGENCY/DEPT: Transportation

PLANNING SECTOR/AREA: South/ RV

PETITION NO: MM 23-0414

-
- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to listed or attached conditions.
- This agency objects, based on the listed or attached grounds.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Conditions

1. The multi-use development shall be limited as follows:
 - 1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary
	40 feet from ELAPP property
	50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2 The project shall be permitted a maximum of 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.
 - 1.4 The project shall be permitted a maximum ~~68,880~~81,000 square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General (CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).

3. Cross access shall be provided between all outparcels and Alafia Preserve Ave.~~the major internal roadway~~. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.

[Transportation Review Section staff is proposing to modify this condition for clarity.]

6. The following conditions ~~are~~address required access improvements to Gibsonton Drive:
 - 6.1 The Gibsonton Drive and Fem Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to ~~FOOT~~FDOT approval.
 - 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right-in/right-out. [THIS CONDITION HAS BEEN SATISFIED].
 - 6.3 The site currently ~~has access to~~abuts, but does not have access to, the ~~FOOT~~FDOT frontage road that runs along Gibsonton Drive. ~~FOOT~~FDOT has indicated that the portion of the frontage road east of the proposed 54-foot-wide public right-of-way and continuing to the Fern Hill Dr. Ext. access may will be restricted/modified removed in the future, once a new public roadway segment is constructed within the referenced right-of-way. Nothing herein this condition shall be construed as requiring the developer of the subject PD to construct such public roadway facility.

[Transportation Review Section staff is proposing to modify this condition for clarity, to better reflect the FDOT and County's intentions/status of area improvements, and more accurately describe the planned future configuration of transportation facilities within the area.]

7. Gibsonton Drive and Fern Hill Drive has been signalized. As a result, the connection from Old Gibsonton Drive to Fern Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive (i.e. the frontage road) currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:

- 7.1 ~~Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue,~~ Developer shall dedicate and convey Fern Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. ~~The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and~~

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan; or,

- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties; or,
- c. September 12, 2025.

The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations.

- 7.2 To provide the alternative connection for Old Gibsonton Dr. (i.e. the frontage road) to the Fern Hill Drive ~~Extensions~~Extension via Alafia Preserve Avenue, the Developer shall dedicate and convey a fifty-four (54) foot right-of-way within the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication and conveyance shall occur upon the earlier of the following:

- a. prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan, or
- b. within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located ~~within the area labeled "Proposed Location of Future ROW" on~~ as generally shown on the Site Plan, at the discretion of Developer consistent with applicable transportation technical and access management standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

[Transportation Review Section staff is proposing to modify this condition for clarity, to comport with the applicant's request, and to provide certainty for how and when these facilities will be provided.]

8. Prior to concurrent with the next increment of development, ~~The~~ the developer will provide a bus bay and ~~;~~ transit accessory pad including; shelter, seating, trash receptacle, and bicycle rack for the existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.

[Transportation Review Section staff is proposing to modify this condition for clarity, to make it clear when the required improvements are expected to be constructed, and to reflect the language of required transit improvements and appurtenances pursuant to Sections 6.02.17 and 6.03.09 of the LDC.]

New Conditions

- Notwithstanding anything on the Site Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
- Construction access shall be limited to those locations shown on PD site plan which are also proposed vehicular access connections. The developer shall include a note in each site/construction plan submittal which indicates same.
- Parking shall be provided in accordance with Section 6.05 of the Hillsborough County Land Development Code. Notwithstanding the above, the project's hotel uses shall be permitted to provide parking at a rate of 1.0 parking space per room. Additionally, the developer will be required to construct 6 bicycle parking spaces which meet the location and design criteria found within Sec. 6.05.02.P. of the LDC.

Other Conditions

- Prior to PD Site Plan Certification, the developer shall revise the Site Plan to:
 - Show the proper intersection configuration (i.e. the raised directional median) at the intersection of Gibsonton Dr. and Alafia Preserve Ave.);
 - Modify the label reading "PROPOSED 54' FUTURE PUBLIC R/W TO BE DEDICATED FROM LAST PARCEL TO DEVELOP PER CONDITIONS OF APPROVAL. FUTURE R/W AND EXISTING DRIVEWAYS SHALL MEET THE SPACING REQUIREMENTS OF LDC SECTION 6.04.07 FOR TYPE II ACCESS CONNECTIONS." to instead read "PROPOSED 54' PUBLIC RIGHT-OF-WAY DEDICATION AND CONVEYANCE – SEE CONDITIONS OF APPROVAL."

PROJECT OVERVIEW AND TRIP GENERATION

The applicant is requesting a Major Modification to a +/- 5.6 ac. portion of a +/- 39.96 ac. previously approved Planned Development (PD) #06-0848, and as most recently modified via PRS 19-0778. The existing PD is approved for 351 multi-family single-family dwelling units, 60,707 s.f. of Commercial Neighborhood (CN) uses, and 10,890 s.f. of Business Professional Office (BPO) uses, and a 68,880 s.f. hotel with a maximum of 125 rooms. The applicant is proposing to increase the allowable square-footage of hotel uses from 68,880 to 81,000 s.f. Staff notes that despite this increase, there will be no corresponding increase to the number of hotel rooms.

The Institute of Transportation Engineer's provides data for hotel rooms trip generation using the number of hotel rooms as the independent variable. Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.

Given the above, and that there are no changes in proposed access configurations, staff did not require a trip generation and site access analysis to process this request, consistent with the Development Review Procedures Manual (DRPM). Staff prepared the below analysis, comparing trips potentially generally by development under the approved and proposed zoning, utilized a generalized worst-case scenario. Data provided below is based on information from the Institute of Transportation Engineer's Trip Generation Manual, 11th Edition.

Existing Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
60,980 s.f. Shopping Plaza (40-150k) (ITE LUC 821)	5,762	215	587
352 Multi-Family Dwelling Units (ITE LUC 220)	2,332	132	172
10,890 Medical Office Uses (ITE LUC 720)	392	33	41
125 Room Hotel (ITE LUC 310)	998	55	65
Subtotal:	9,484	435	865

Proposed Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
60,980 s.f. Shopping Plaza (40-150k) (ITE LUC 821)	5,762	215	587
352 Multi-Family Dwelling Units (ITE LUC 220)	2,332	132	172
10,890 Medical Office Uses (ITE LUC 720)	392	33	41
125 Room Hotel (ITE LUC 310)	998	55	65
Subtotal:	9,484	435	865

Difference:

	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
	No Change	No Change	No Change

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Alafia Preserve Ave. is a 2-lane, undivided, privately maintained, substandard, local roadway. The roadway is characterized by 11-foot-wide travel lanes in average condition. The roadway lies within a +/- 55-foot wide right-of-way. There are +/- 5-foot-wide sidewalks along portions of the roadway. There are +/- 5-foot-wide bicycle facilities along portions of the roadway.

The Fern Hill Dr. Ext. is a 2-lane, divided, privately maintained, substandard, local roadway. The roadway is characterized by 15-foot-wide travel lanes in above average condition. The roadway lies within a +/- 100-foot-wide right-of-way. There are +/- 6-foot-wide sidewalks along both sides of the roadway. There are no bicycle facilities along this segment of the roadway.

SITE ACCESS AND CIRCULATION

No changes to existing approved access or circulation are proposed.

CHANGES TO RIGHT-OF-WAY DEDICATION REQUIREMENTS

Staff notes that the project is currently in violation of its PD zoning conditions (see existing condition 7.1), due to Alafia Preserve Ave. and newly constructed Fern Hill Rd. extension not having been dedicated and conveyed to the County in accordance with current requirements. The applicant has proposed modifications which will cure that violation. Specifically, the applicant is proposing to modify the conditions to provide flexibility in the timing and location of the 54-foot-wide right-of-way dedication and conveyance area (i.e. for the lands needed to accommodate the new north/south public roadway necessary to allow for removal of certain portions of the existing frontage road). They are also proposing modifications to the condition governing the timing of the requirement to dedication and convey Alafia Preserve Ave. and Fern Hill Dr. Staff has reviewed these condition changes with the County Engineer and supports the proposed revisions offered by staff, hereinabove. Staff notes that conversations between the County and applicant regarding what data, analysis, and/or improvements may be required to both the Fern Hill Dr. Ext. and Alafia Preserve Ave., prior to the County being able to accept those facilities for ownership and maintenance, are ongoing.

REQUESTED PD VARIATION – PARKING

The applicant submitted a PD Variation, requesting a reduction in the required parking rate for the project’s hotel uses. The Land Development Code requires 1.1 parking spaces per hotel room. As such, the project’s 125 rooms would require 138 parking spaces. The applicant is proposing parking at a rate of 1.0 spaces per room (or 125 rooms).

The 5th Edition of the Institute of Transportation Engineer’s Parking Generation Manual provides data indicating a range of rates between 0.43 and 1.47 of the 22 hotels studied. They also provide an 85th Percentile rate of 0.99 parking spaces per room, with a 95% confidence interval of 0.83 on the high end.

Staff has reviewed the ITE data and recommends approval the request, subject to conditions. LDC Sec 6.05.02.P.1. permits the Administrator to reduce the number of required automobile parking spaces by 5% in exchange for the provisions of bicycle parking meeting the locational/design criteria specified in the LDC. As such, staff believes it is appropriate that the first 5% of the requested reduction is offset by the provision of bicycle parking spaces (138 x .05 = 6.9, which rounds down to 6). Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.

ROADWAY LEVEL OF SERVICE (LOS) INFORMATION

LOS information for adjacent roadway segments are provided below.

Roadway	From	To	LOS Standard	Peak Hour Directional LOS
Gibson Dr.	I-75	US 301	D	C

Source: Hillsborough County 2020 Level of Service Report.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Gibsonton Dr.	County Arterial - Rural	4 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Alafia Preserve Ave.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other - TBD
Fern Hill Dr. Ext.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
	Choose an item.	Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	9,484	435	865
Proposed	9,484	435	865
Difference (+/-)	No Change	No Change	No Change

*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	None	Meets LDC
East		None	None	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input checked="" type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

Application No. 23-0414
Name: Kevin Reif
Entered at Public Hearing: ZHM
Exhibit # 2 Date: 9/18/23

**TAMPA
CIVIL**
CIVIL ENGINEERING



LINCKS & ASSOCIATES, INC.
*Engineers - Planners
Tampa, Florida*

STEARNS
WEAVER
MILLER

Major Modification MM 23-0414

Hillsborough County
Zoning Hearing Master
September 18, 2023

Team Members

- **Applicant:** Riverview Hotel Investment Group, LLC
- **Civil Engineer:** Tampa Civil Design
- **Traffic Engineer:** Lincks & Associates, Inc.
- **Legal and Land Planning:** Stearns Weaver Miller
 - S. Elise Batsel, Esq.
 - Kevin B. Reali, Esq.

General Information

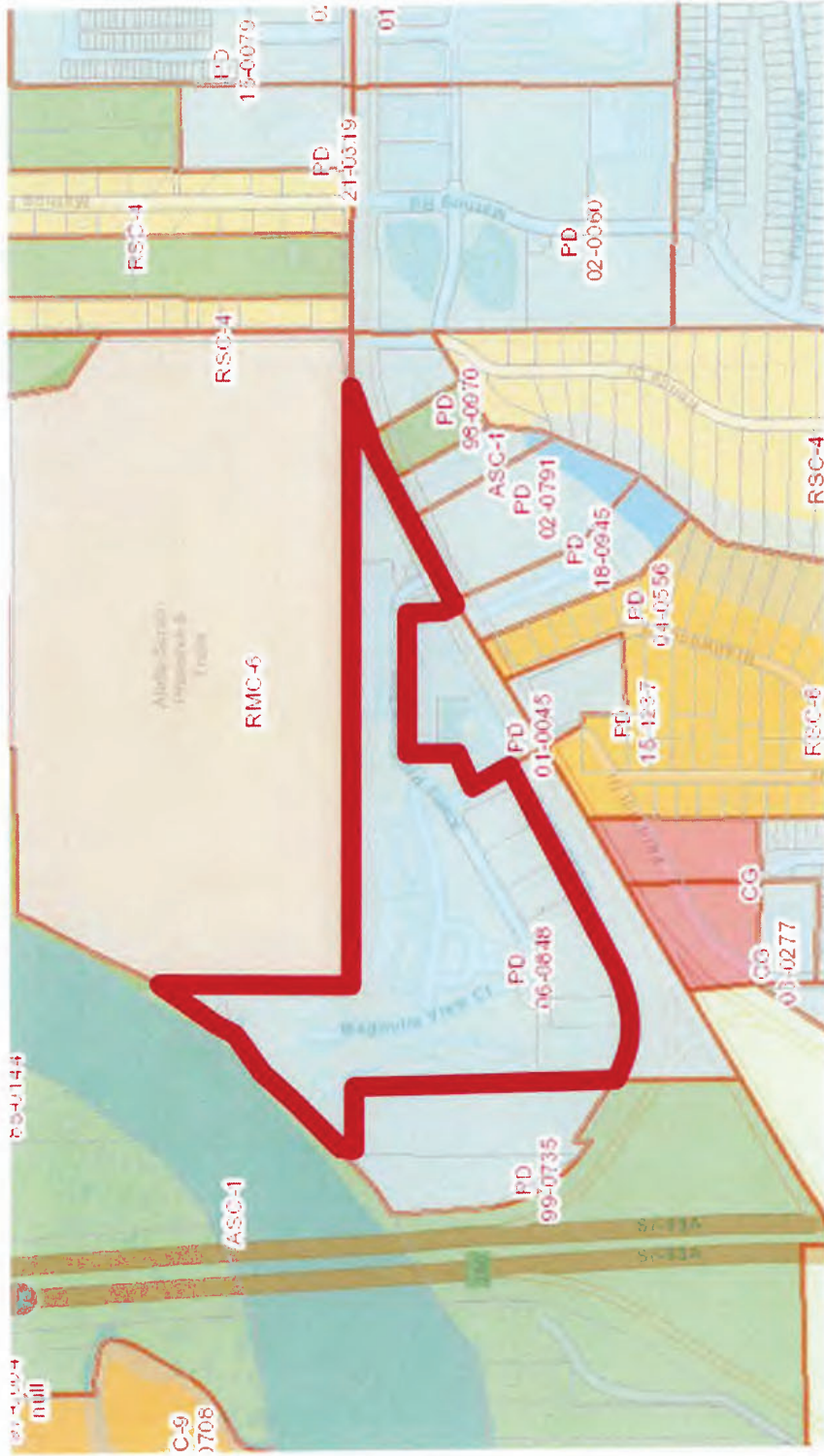
- Portion of PD 06-0848 as most recently modified by PRS 19-0778
- +/- 5.60 acre site in Riverview
- Generally located northeast of I-75 and Gibsonton Dr. intersection



Future Land Use Map

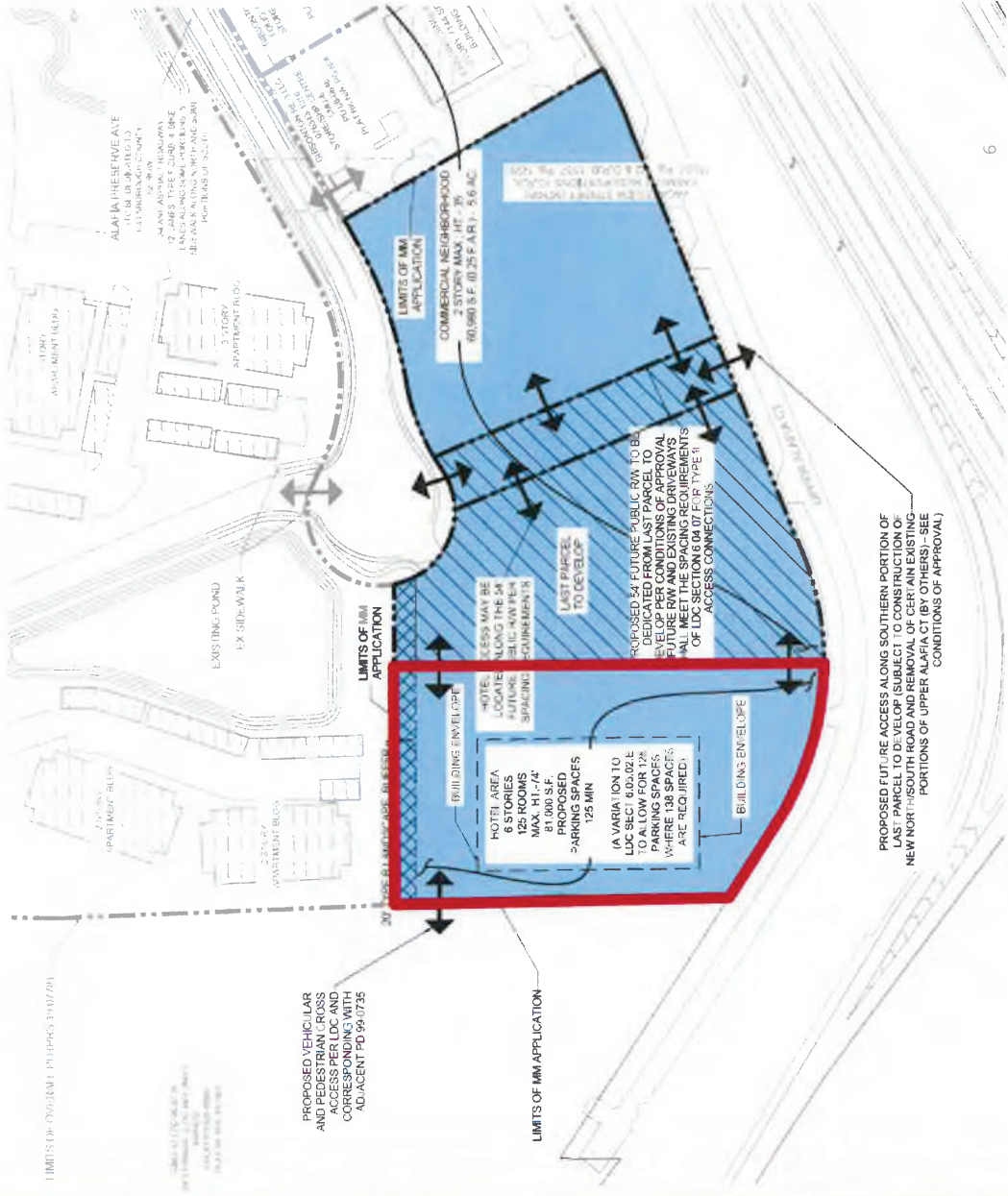


Zoning Map



Basic Request/Scope

- Increase Hotel floor area from 68,880 S.F. to 81,000 S.F.
- No Change to 125 room count for Hotel
- Variation to 6.05.02 to require 125 parking spaces (one per room)



Planning Commission

- Reviewed for consistency and compatibility with Comprehensive Plan and Riverview Community Plan
 - “The proposed major modification is compatible with the development pattern in the area as required in Policy 1.4”
 - “The proposed Planned Development is consistent with the Neighborhood Protection policies under FLUE Objective 16”
 - “The subject site meets Commercial Locational Criteria”
- Entitlements
 - FLEX and Policy 19.3 Bonus
 - Proper uses, mix of uses, and integration

“The Planning Commission staff finds the proposed Planned Development **CONSISTENT** with the *Unincorporated Hillsborough County Comprehensive Plan*”

Transportation

- “Given that the number of rooms is not changing (only the size of facility overall), there is no resultant increase in the maximum trip generation potential of the subject project.”
- Negotiated conditions to cure outstanding zoning requirement
 - Show location of future ROW
 - Add specificity to timing of dedication requirement
- “Staff has reviewed the ITE data and recommends approval the [parking waiver], subject to conditions.”
 - ITE calculates parking rate at 0.99 spaces per room (high confidence that rate is closer to 0.83)
 - Conditioned on providing bicycle parking

Zoning Staff

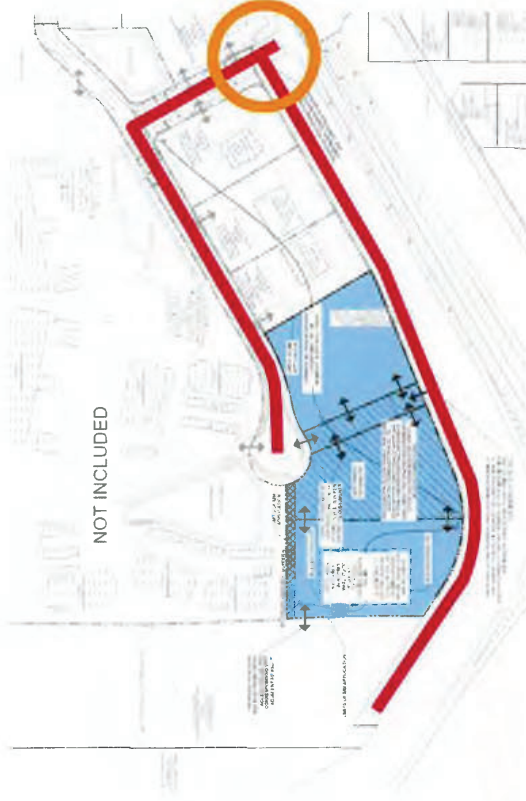
- “Given the above, staff has proposed a condition which would permit the developer to construct 125 parking spaces to serve the proposed hotel uses, provided they also construct 6 bicycle parking spaces meeting LDC requirements.”
- “The uses proposed are unchanged and remain compatible. Based on the adjacent zonings and uses identified above in the report, staff finds the proposed PD zoning district compatible with the existing zoning districts and development pattern in the area.”
- No Waivers

Consistent - “Approvable, subject to proposed conditions”

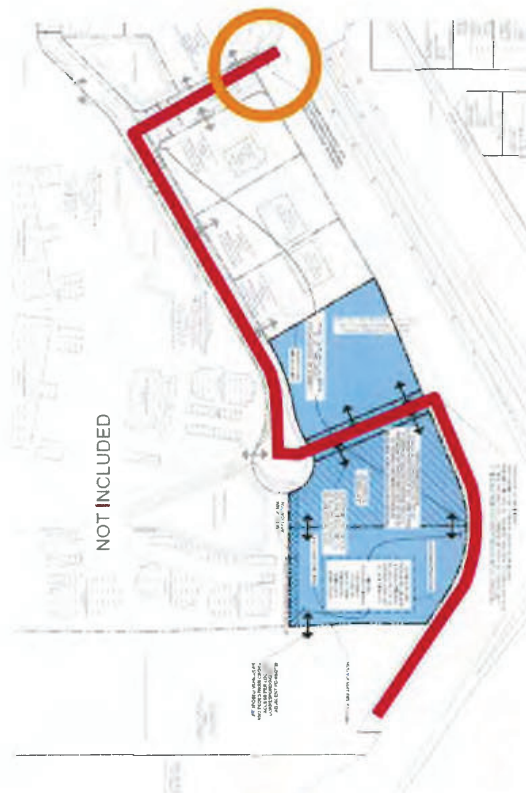
Questions/Comments?

Transportation Conditions

EXISTING ACCESS TO WESTERN PROPERTIES



PROPOSED ACCESS TO WESTERN PROPERTIES



MM 23-0414

**PROJECT NARRATIVE TO APPLICATION
FOR MAJOR MODIFICATION TO
PLANNED DEVELOPMENT ZONING**

**Applicants:
Alafia Crossing Acquisition, LLC, a Florida
limited liability company
and
SVA Land Holding, LLC, a Florida limited
liability company**

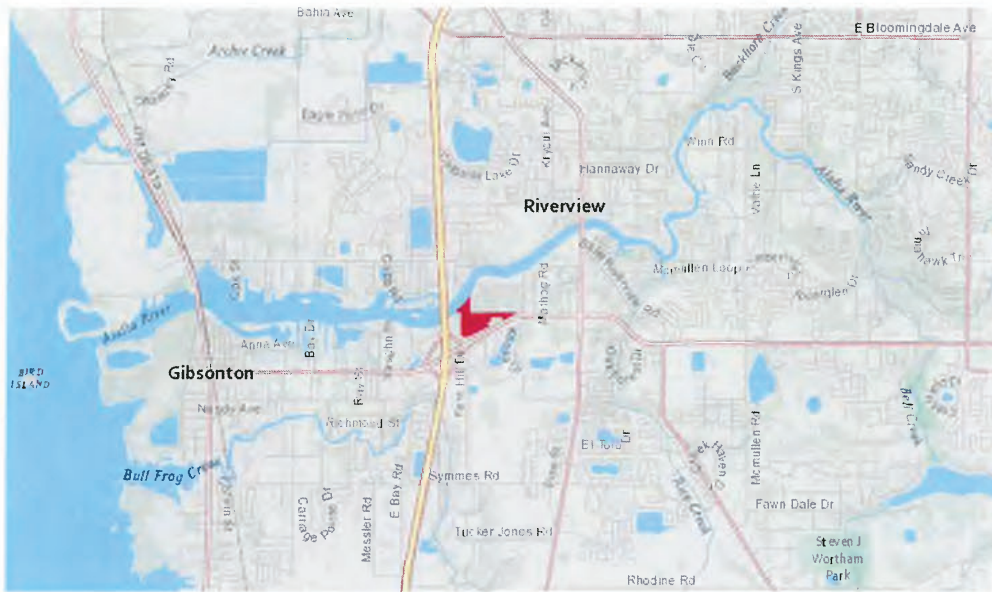
**Submitted by:
S. Elise Batsel, Esq.
Kevin B. Reali, Esq.
Stearns Weaver Miller
401 East Jackson Street, Suite 2100
Tampa, Florida 33602
(813) 223-4800**

Updated, as of September 18, 2023

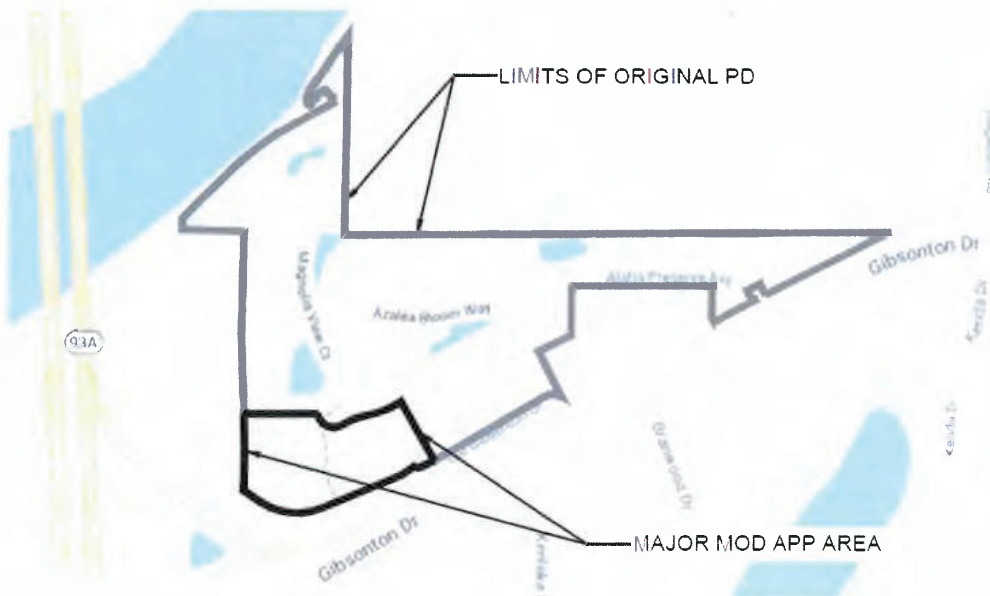
Project Narrative

This is an application for a Major Modification (“**Major Mod**”) to a portion of an existing Planned Development (PD 06-0848), as modified by PRS 07-1378, PRS 08-0816 and PRS 19-0778 (the “**Existing PD**”). The Existing PD includes +/- 39.96 acres located in unincorporated Hillsborough County and is generally located in the northeast quadrant of I-75 and Gibsonton Drive as shown below.

Vicinity Map:

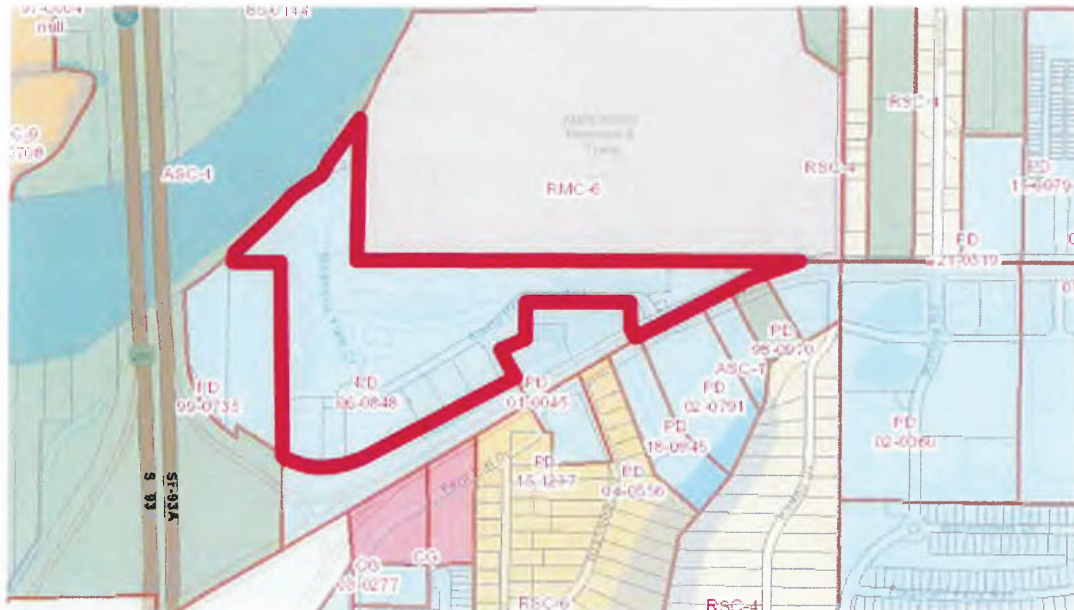


The property subject to the Major Mod includes +/- 5.59 acres (the “**Property**”), as shown below.

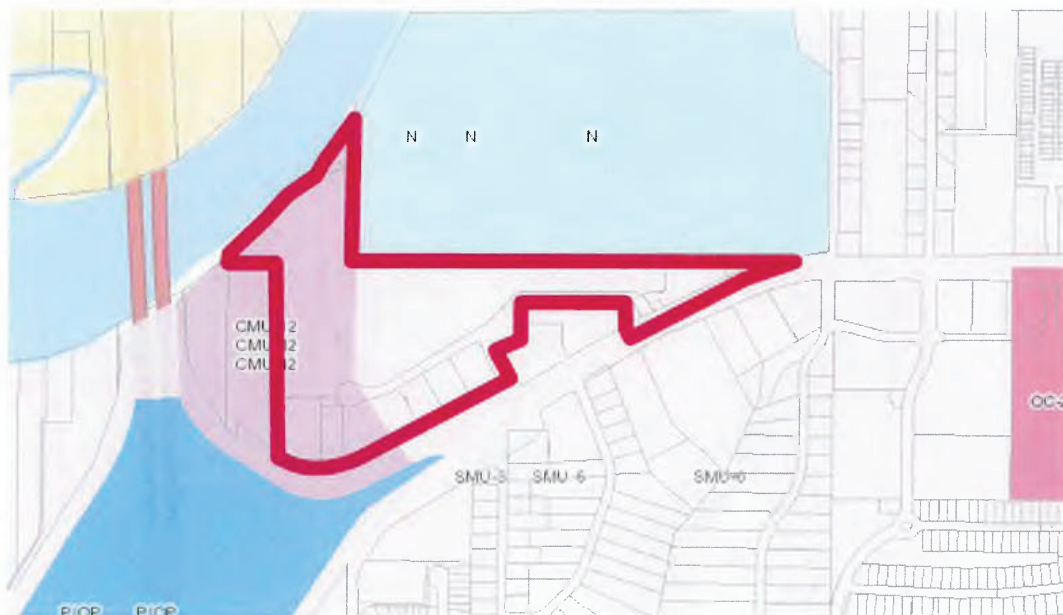


The Property includes Folio Nos. 076324-0005; 076324-0006; 076324-0010; and 076343-1000, which are all vacant. The Property is within the Urban Service Area and the Riverview Planning Area. The Existing PD and Major Mod boundaries include both the CMU-12 (“CMU-12”) and SMU-6 (“SMU-6”) Future Land Use categories. The CMU-12 is located on the western portion adjacent to I-75, and the remaining property to the east is within the SMU-6 Future Land Use category, as shown below.

Zoning Map:



Future Land Use Map:



I. Introduction:

While the application is made complicated by the zoning and development history of the Property, the goal of this application is simple. The request is limited to: (1) to expanding the hotel's square footage from 68,880 square feet to 81,000 square feet, through the use of a Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel; and (2) to reducing the required parking spaces for the hotel from 138 to 125 ("**Project**").

II. Background:

The Existing PD has been modified a number of times since the first approval in 2006. The Existing PD approval permitted (i) 351 multi-family units, (ii) 60,980 square feet of neighborhood commercial uses, (iii) 10,890 square feet of office uses, and (iv) a 150 room hotel with 32,670 square feet of building area. The basis for the original entitlement calculations for the Existing PD was: (i) the area of the Existing PD within CMU-12, (ii) the area of the Existing PD within SMU-6, and (iii) a flex of CMU-12. The Existing PD includes a waiver to the Commercial Locational Criteria ("**CLC**") from Hillsborough County Comprehensive Plan ("**Comprehensive Plan**") FLUE Objective 22 and Policy 22.2, as well as a waivers from Hillsborough County Land Development Code ("**LDC**") Sec. 6.11.35.B.1, to reduce the distance separation between drive thru windows to residentially zoned property from 200 feet to 120 feet, and Sec. 6.11.35.B.2, eliminate a screening wall. No change to the LDC waivers is requested as a part of this Major Modification.

2007 PRS:

In 2007, PRS 07-1378 approved an increase to the square footage of the hotel use from 32,670 square feet to 65,340 square feet. The justification for the application was that the hotel was within CMU-12, which permits a Floor Area Ratio ("**FAR**") of 0.5, but only permitted for a FAR of 0.25 at 32,670 square feet. On its face, PRS 07-1378 was simply expanding the hotel consistent with the underlying Future Land Use category. However, the Existing PD was not designed on a per parcel basis. The entitlement/flex calculation in the Existing PD started by allocating entitlements to the multi-family units and then allocated the remaining entitlements to the non-residential uses. All of the entitlements available under the Comprehensive Plan within the Existing PD were allocated as part of the Existing PD. In short, PRS 07-1378 permitted more total entitlements over the Existing PD than the Comprehensive Plan permits with just a flex provision ("**2007 Error**").

2008 PRS:

In 2008, PRS 08-0816 (i) further increased the size of the hotel from 65,340 square feet to 68,880 square feet by reallocating entitlements, (ii) reduced the number of rooms in the hotel to 125, and (iii) modified certain transportation conditions.

2019 PRS 19-0778:

PRS 19-0778 modified certain transportation conditions, without changing the entitlements.

III. Detailed Request:

As stated, the general request under this application is to expand the hotel buildable square footage to +/- 81,000 square feet, through the use of a Policy 19.3 mixed-use bonus, with no change in the number of rooms within the hotel, and (2) reduce the parking spaces required for the hotel use from 138 to 125. Additional changes to the site plan are transportation related. They include adjusting the location of the “last parcel to develop” indicated on the site plan to exclude the hotel area, and to shift the area for the future right-of-way to connect Alafia Preserve Avenue to Old Gibsonton Drive east of the hotel (the proposed is still within the original area set aside for future right-of-way, but now limited to the remaining area). Shifting the right-of-way requires changes to internal access points to ensure adequate spacing for the proposed right-of-way. As such, the hotel will take access through the proposed right-of-way. Additional cross-access points are also estimated on the updated site plan.

The hotel floor area increase is requested to accommodate larger extended stay hotel type rooms. The parking reduction is sought to reflect current market conditions and is supported by the Institute of Transportation Engineer’s Parking Manual.

In preparation for this application, significant review and computation was conducted to ensure that a new application could be proposed that would comply with the Comprehensive Plan density/intensity limitations. This Major Mod proposes to use the same flex applied in 2006 in combination with Policy 19.3 mixed-use bonus (with the Policy 19.3 mixed-use bonus limited to the Property subject to this Major Mod). By applying both the flex and Policy 19.3 mixed-use, this application will (1) correct the 2007 Error, and (2) increases the maximum square footage of the hotel, while conforming to the Comprehensive Plan. Further, the entitlements for all the uses other than the hotel are rolled back to the original 2006 approval, to ensure the Major Mod does not impact other property within the Existing PD.

IV. Entitlement Calculation:

While the only substantive requested entitlement change is to increase the hotel square footage to 81,000 square feet, the site plan and conditions show other changes to ensure no other property owners have adverse impacts due to this Application. First, while the multifamily is built out, the Property Appraiser records show 352 units instead of the 351 units permitted, so the PD is updated to 352 to be conservative. Second, the nonresidential square footage for Commercial Neighborhood uses is nominally increased from 60,707 to 60,980 to match the original approval. To achieve these increases, and account for the additional hotel square footage, the Application applies a Policy 19.3 mixed-use bonus. The result conservatively corrects the 2007 Error with no negative impacts to other landowners in the Existing PD. The calculations are shown on the site plan.

The entitlements calculation shown in the original and first updated narrative used a top down approach to calculate entitlements. After several discussions with staff, this resubmission proposes a new calculation form. The new calculation uses portions of each category for residential and non-residential uses, and accounts for both the Policy 7.3 flex and Policy 19.3 bonus all at once, as shown below.

The calculation ensures entitlements previously approved are properly accounted for using conservative assumptions. Each of the uses is fully entitled through the implementation of the Policy 19.3 bonus, which is minimally applied to achieve the stated goals of this application and ensure no negative impacts on property within the PD that are not party to this Application.

<u>AVAILABLE ENTITLEMENT CALCULATION</u>						
	<u>RESIDENTIAL ENTITLEMENTS</u>			<u>NON-RESIDENTIAL ENTITLEMENTS</u>		
	ACRES	DU/AC	UNITS	ACRES	FAR	CONV UNITS
CMU-12 (WITHOUT BONUS)	12.5 AC	X 12	= 150	0.37 AC	X 0.5 X 43,560 SF	= 8,058 SF
CMU-12 (WITH BONUS)	0.5 AC	X 16	= 8	3.92 AC	X 0.75 X 43,560 SF	= 128,066 SF
CMU-12 FLEX	10.75 AC	X 12	= 129	0 AC	X 0.5 X 43,560 SF	= 0 SF
SMU-6 (WITHOUT BONUS)	9.84 AC	X 6	= 6	0.91 AC	X 0.25 X 43,560 SF	= 9,909 SF
SMU-6 (WITH BONUS)	0.67 AC	X 9	= 6	0.5 AC	X 0.35 X 43,560 SF	= 7,623 SF
TOTAL UNITS			352			153,656 SF
TOTAL ACRES ATTRIBUTABLE TO RESIDENTIAL ENTITLEMENTS				34.26 AC		
TOTAL ACRES ATTRIBUTABLE TO NON-RESIDENTIAL ENTITLEMENTS				5.7 AC		
TOTAL PD ACRES				39.96 AC		

<u>UTILIZED ENTITLEMENT CALCULATION</u>		
	<u>RESIDENTIAL DENSITY</u>	<u>NON-RESIDENTIAL FAR</u>
TOTAL AVAILABLE IN PD	352 DWELLING UNITS	153,656 SF
MULTI-FAMILY USED	352 DWELLING UNITS	N/A
NEIGHBORHOOD COMMERCIAL USED	N/A	60,980 SF
OFFICE USED	N/A	10,890 SF
HOTEL USED	N/A	81,000 SF
REMAINING	0	786 SF

The calculations correct prior errors and account for the request set forth in this application. This application has no negative impacts on the remaining property owners within the Existing PD.

V. Character and Compatibility:

The area surrounding the Existing PD has continued to develop since the 2006 approval, but remains a consistent mix of uses including single-family, multi-family, retail, offices, and self-storage. The Property is immediately adjacent to the Alafia River, the Preserve at Alafia apartment complex, and I-75. The uses proposed are unchanged, and remain compatible. The scale of the proposed development only increases slightly the square footage of the hotel without any increase in the number of hotel rooms. The minor increase in floor area will have no impact on the compatibility with the surrounding area. The consistent 125 hotel rooms ensures the impact of the development on public services remains constant. In short, while the Major Mod requires a complicated review of the entitlements, the ultimate result is a minor change to the actual development pattern and impact to the public.

VI. Consistency with the Comprehensive Plan:

This Major Mod is consistent with the Comprehensive Plan because there are no changes to the approved uses, and the requested entitlements changes comply with the Comprehensive Plan. With regard to uses, the Property is located within both the CMU-12 and SMU-6 Future Land Use categories. The CMU-12 category allows for residential, community scale retail commercial,

office uses, research corporate park uses, light industrial multi-purpose, clustered residential, and mixed use projects. The SMU-6 category allows for residential, suburban scale neighborhood commercial, office uses, research corporate park uses, light industrial multi-purpose, and mixed use projects. All of the proposed uses fit within the contemplated use types of the existing Future Land Use categories. Additionally, while the hotel square footage is slightly increased with this request, total entitlements comply with the previously approved flex and Policy 19.3 mixed-use bonus, as calculated above and confirmed below.

The consistency of the Major Mod with the Comprehensive Plan is illustrated through the following policies:

Policy 1.4: Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and architecture. Compatibility does not mean "the same as." Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.

Discussion: As mentioned, the Major Mod will slightly increase the floor area of the hotel. The hotel is located on the southwest corner of the Existing PD, which is adjacent to Gibsonton Road and I-75, an ideal location for a hotel use. The hotel will be similar in height and bulk to the surrounding multi-family development (and similar in height and use to the existing approval, since only a minor change is requested). It will serve as a transitional area from commercial developments next to the Major Mod and across Gibsonton Drive. Additionally, the traffic circulation will be similar to that of the surrounding development. The proposed hotel is consistent with the character of existing developments in the area.

Policy 16.2: Gradual transitions of intensities between different land uses shall be provided for as new development is proposed and approved, through the use of professional site planning, buffering and screening techniques and control of specific land uses.

Discussion: The hotel will be part of a transition from the I-75/Gibsonton interchange to the less intense uses to the north and east. The hotel will have the appropriate buffers along its perimeter to ensure the surrounding areas are not negatively impacted by the development.

VII. Policies 7.3 and 7.4 Flex Provisions:

Comprehensive Plan Policy 7.3 permits a Future Land Use category line be relocated a maximum of 500 feet from the existing land use boundary. Major Modifications that change the intensity, density, or range of uses requires previous flex requests be re-evaluated for consistency. In some situations, a new flex request may be required. Comprehensive Plan Policy 7.4 sets forth the criteria that must be fulfilled in order to be granted a flex.

Policy 7.4: The criteria for consideration of a flex request are as follows:

The availability and adequacy of public facilities to serve the proposed development accommodated by the flex; the compatibility with surrounding land uses and their density

and intensity; the utilization of the flex furthers other goals, objectives, and policies of the Future Land Use Element.

The Property is located within the Urban Service Area where adequate public facilities exist. The Existing PD, as approved, already accounts for a 125 room hotel. The total number of rooms will remain the same in this request, thus there is no change to the previous finding that adequate public facilities exist. As discussed in the Policy 1.4 evaluation above, the hotel is compatible with existing, surrounding land uses, their densities, and their intensities. Additionally, the overall Existing PD will continue to develop with future neighborhood commercial uses. Future development for those uses will positively impact the compatibility and consistency of the Existing PD as a mixed-use center. Utilizing the flex permits development that is more consistent with the surrounding area, furthering the goals, objectives, and policies of the Future Land Use Element.

VIII. Policy 19.3 Mixed-Use Bonus Provisions:

Comprehensive Plan Policy 19.3 states the following:

“The following incentives are available to encourage mixed use and vertically integrated mixed use projects within the Urban Service Area:

Parking structures shall not count towards the FAR for projects that include 3 or more land uses or vertically integrate two land uses.

Projects that either include 3 or more land uses or vertically integrate two land uses may utilize a density bonus to the next higher land use category or the following FAR bonus:

Property with a Future Land Use Category of 35 units per acre and/or 1.00 FAR and higher and within the USA – may increase up to 50 units and/or an additional 0.50 FAR

Property with a Future Land Use Category of 9 units per acre and/or .5 FAR and higher within the USA – Increase in FAR by 0.25

Property with a Future Land Use Category of 4 units per acre and/or 0.25 FAR and higher and within the USA – Increase in FAR by .10

When considering mixed use projects of 3 or more land uses, a different housing type (multi-family, attached single family or detached single family) may be considered as one of the uses.”

The Existing PD involves three (3) or more land uses including multi-family, the proposed hotel, neighborhood commercial, and offices. Because the Existing PD is comprised of two (2) different Future Land Use categories, each area will receive a different intensity bonus. The CMU-12 area will be granted an additional 0.25 FAR which is the equivalent of 48,133 square feet. The SMU-6 area will be granted an additional 0.10 FAR which is the equivalent of 5,096 square feet.

IX. Policy 22.2 Commercial Locational Criteria Waiver

Objective 22 describes the purpose of the CLC is “[t]o avoid strip commercial development . . . for neighborhood serving commercial uses.” The goal is to ensure “new commercial development [is] consistent with the character of the areas[,] availability of public facilities and the market.” CLC is not needed to achieve the purposes and goals of Objective 22 for this Project. Thus, a waiver to the CLC requirement of Policy 22.2 is requested.

The Project is consistent with the character of the area. It is located at the interchange between Interstate – 75 and Gibsonton Drive, and is less than one mile from the intersection of Gibsonton Drive and Highway 301, three of the largest roadways in the area. The area includes a mix of various types of commercial uses and various types of residential uses. Further, the subject PD includes a mix of residential and commercial uses, as well as an internal roadway network. The Project is within the Urban Service Area with access to adequate public facilities. The Project, as requested, will render the PD to near buildout, as driven by market demand, which demonstrates a need for commercial (and renders commercial more compatible than residential in the limited, undeveloped areas of the PD). The commercial nature of the existing PD, and general area, renders the CLC unnecessary to ensure commercial development is consistent with the character of the area because the area is already established as a commercial and mixed-use area.

Additionally, the Project only minimally changes the existing zoning, which included the waiver to the CLC of Policy 22.2. The Project’s use is already approved, and the minimal square footage increase for the hotel, with no additional guest rooms, will not change the transportation impact of the approved uses. The Project will simply permit the development of an extended stay hotel, which requires more floor area than a traditional hotel.

X. Transportation Analysis Waiver Request:

The Major Mod does not increase the number of hotel rooms. Trip generation for hotels is based on the number of rooms; therefore, there is no change in the cumulative peak hour trips. In lieu of providing a Transportation Analysis, the Applicant requests an analysis waiver under Sec. 6.2.1.C.8.i based on the finding that the transportation analysis does not change under this Major Modification. In support of this request, the Applicant included a letter, prepared by Lincks & Associates, that confirms the trip generation is unchanged.

XI. Transportation Infrastructure Serving Site

The entire PD accesses Gibsonton Drive, which meets the standards for the Hillsborough County Transportation Technical Manual (“TTM”) TS-7 roadway. The Property will access Gibsonton Drive via Alafia Preserve Avenue and Fern Hill Drive.

Alafia Preserve Avenue is a two-lane asphalt roadway with 12’ lanes, and type F curbing. Bike lanes and sidewalks vary along Alafia Preserve Avenue, with some portions having a 4’ bike lane and 5’ sidewalk. Fern Hill Drive is a two-lane asphalt roadway with 15’ lanes and a type F curb. Fern Hill Drive has a 6’ sidewalk along the west side and portions of the east side. Alafia Preserve

Avenue and Fern Hill drive should comply with the TS-3 standard, but fail in some portions due to right-of-way width and Clear Zone.

XII. Developer Commitments

The developer is in the process of completing a dedication application for Fern Hill Drive and Alafia Preserve Avenue. The improvements for Fern Hill Drive and Alafia Preserve Avenue have been completed, inspected, and placed into operation. Pavement core analysis and video inspection of the stormwater system are complete. Condition 7 has been updated to re-affirm the commitment to dedicate the right-of-way and improvements for Fern Hill Drive and Alafia Preserve Avenue and the future right-of-way to provide access to the development to the west. New timing considerations are proposed to ensure the County that dedication will take place, but permit the developer time to further confirm the location of the future right-of-way and its location relative to pending development. In accordance with Condition 7.1, a design exception for the existing roadway improvements to be dedicated will accompany the dedication application to codify the existing “design, approval, and construction” of those improvements.

XIII. Waivers:

No waiver changes are proposed, though a new waiver is listed on the site plan to correct a prior oversight. PRS 19-0778 included and approved waivers to Sections 6.11.35.B.1 and 6.11.35.B.2, as shown again on the site plan. Additionally, while the site plan never showed a waiver to the 2:1 additional setback for buildings over 20 feet in height, each of the prior approvals included a waiver to that requirement for the western and northern PD boundaries in the conditions (as shown below). As such, a waiver to Section 6.01.01 to eliminate the 2:1 setback requirement at the western and northern PR boundaries is shown on the site plan.

- RZ 06-0848

APPLICATION: RZ 06-0848 RV	
ZHM HEARING DATE: June 20, 2006	
BOCC MEETING DATE: August 8, 2006	CASE REVIEWER: D. Lyons
RECOMMENDED CONDITIONS	
RECOMMENDED ZONING CONDITIONS ARE BASED ON SITE PLAN RECEIVED: May 31, 2006	
1.	The multi-use development shall be limited as follows:
1.1.	The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.
Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary
	40 feet from ELAPP property
	50 feet from Alafia River
Maximum height:	60 feet (5 stories)
	The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
1.2.	The project shall be permitted a maximum of 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein.

- MM 07-1378

RECOMMENDED CONDITIONS

RECOMMENDED ZONING CONDITIONS ARE BASED ON SITE PLAN RECEIVED: AUGUST 6, 2007

1. The multi-use development shall be limited as follows:
 - 1.1. The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary
	40 feet from ELAPP property
	50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2. The project shall be permitted a maximum of 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.

- PRS 08-0816

RECOMMENDED CONDITIONS

RECOMMENDED ZONING CONDITIONS ARE BASED ON SITE PLAN RECEIVED APRIL 16, 2008

1. The multi-use development shall be limited as follows:
 - 1.1. The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary
	40 feet from ELAPP property
	50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2. The project shall be permitted a maximum of ~~60,980~~ 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.

- PRS 19-0778

FINAL CONDITIONS OF APPROVAL	PETITION NUMBER: PRS 19-0778 RV MEETING DATE: October 8, 2019 DATE TYPED: October 8, 2019
Approval of the request, subject to the conditions listed, is based on the revised general site plan submitted September 20, 2019.	
1. The multi-use development shall be limited as follows:	
1.1 The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.	
Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)
The 2:1 additional setback for buildings over 20 feet in height shall not apply to the western and northern property boundaries.	

XIV. Variations:

This Major Mod requests a variation to the required parking spaces for the hotel use. As applied, Section 6.05.02 requires 1.1 spaces per room for a total requirement of 138 parking spaces. The applicant seeks a reduction to 125 parking spaces. The attached “Variations Criteria Review Form” discuss the request in detail.

If you have any questions, please do not hesitate to contact me.

Elise

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 Alhadeff & Sitterson, P.A.
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www.stearnsweaver.com

STEARNS WEAVER MILLER

Land Use Application Summary Report

Application Number: RZ 06-0848 RV

Existing Zoning: ASC-1, PD

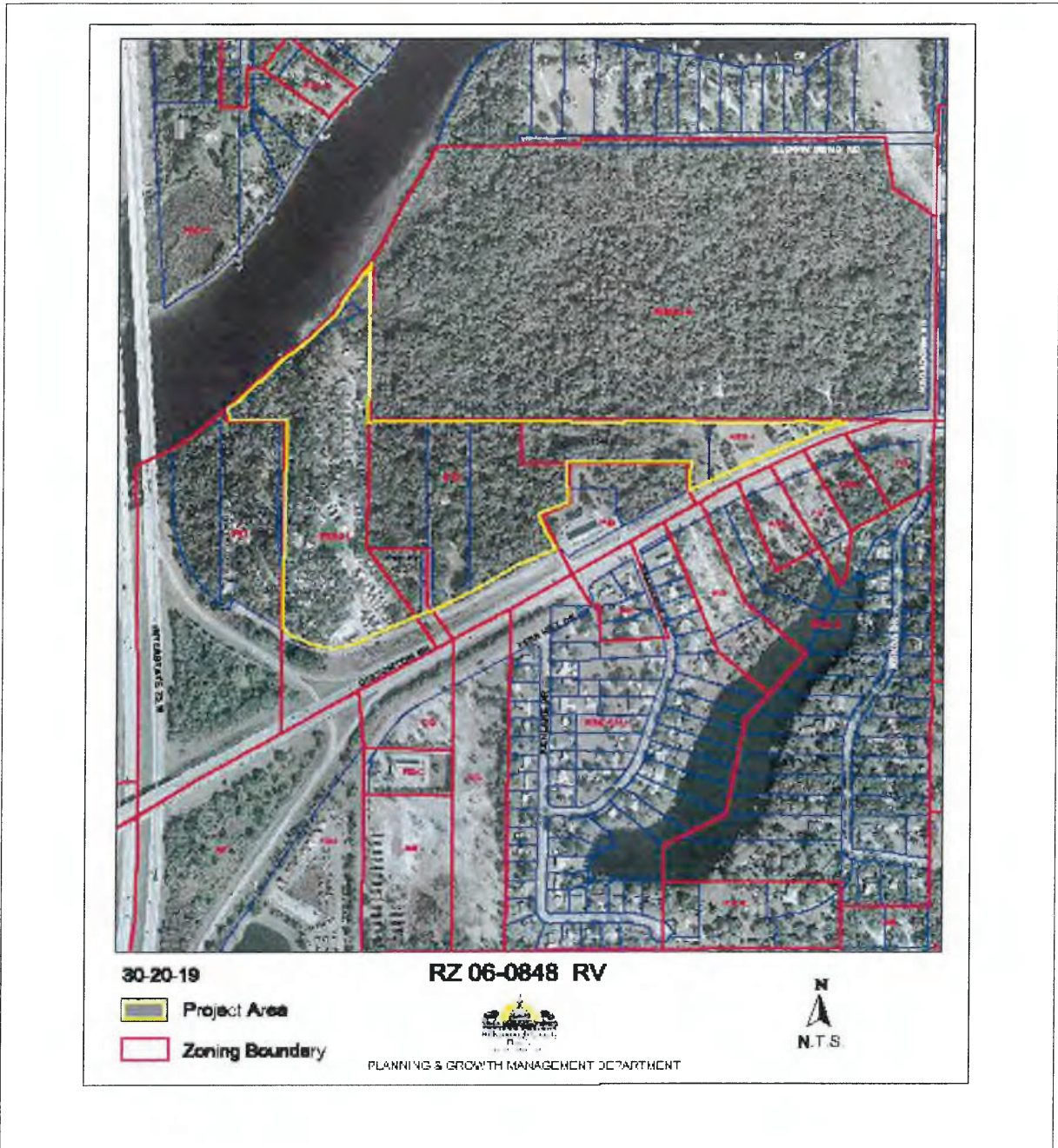
Requested Zoning: PD

Comp Plan: CMU-12, SMU-6

Service Area: UDA

Adjacent Zoning and Land Uses:

North:	ASC-1, Alafia River; RMC-6, ELAPP Property
East:	PD, Commercial, Single-family Residential; ASC-1, Single-family Residential; RSC-6MH, Single-family Residential (MH)
South:	CG, Commercial
West:	PD, Vacant




Request Details:

The request is to rezone an approximate 39.96 acre parcel from Agricultural Single-family Conventional (ASC-1) and Planned Development (PD) to Planned Development. The purpose of this rezoning is to allow a multi-use development including 351 multi-family dwelling units, 60,980 square feet of Commercial Neighborhood (CN), 10,890 square feet of Business Professional Office (BP-O) and a 32,670 square feet/150 room hotel.

Staff Findings:

- The subject property includes two PD zoning districts.
 - Folio 76324.0000 (RZ 91-0098) is approved for 143 single family mobile home spaces and 12 recreational vehicle units per acre on 19 acres.
 - Folios 76369.0500, 76367.0000 and 76365.0000 (RZ 04-1475) are approved for 117,785 square feet of commercial uses including: 60,984 square feet of BP-O uses, 50,442 square feet of CN uses, 24,306 square feet of Commercial General (CG) uses.
- Folio 76324.0000 is zoned for mobile home units and appears to be operating as a mobile home park. The applicant states the park does not meet the requirements of Florida Statutes, Chapter 723. Per Florida Statutes, Chapter 723 (F.S.A. 723.083. Governmental action affecting the removal of mobile home owners), local governments shall not approve a rezoning application which will result in the removal or relocation of mobile home owners residing in a mobile home park without first determining that adequate mobile home parks or other suitable facilities exist for the relocation of the residents. The applicant has been advised that information needs to be submitted into the ZHM record verifying that the subject mobile park is either not subject to the requirement (per F.S. A. 723.002. Application of Chapter) or adequate mobile home parks or other suitable facilities exist for the relocation of the residents. The County Attorney's office has advised that the Zoning Hearing Master needs to include in his recommendation a finding of whether adequate mobile home parks or other suitable facilities exist for the relocation of the mobile home owners in cases where this requirement is applicable.
- Folio 76326.0000 (RZ 99-0735) to the west is approved for a two phase Planned Development consisting of Phase I; 159 space dry boat storage, 100 space recreational vehicle storage, 1,300 square feet of residence/office/retail store and Phase II a 35,000 square foot (70 room) hotel. The property is developed as a boat/RV storage yard.
- An ELAPP property, the Alafia Scrub Nature Preserve, is to the north.
- The Waste Water Department notes that oversizing of the offsite forcemain by the developer may be required.
- The Public Works department notes that the northern portion of parcel 76324.0000 is located within the 100 year Flood Plain. Any site modification on this area must adhere to floodplain compensation criterion and all other requirements of the Stormwater Technical Manual.
- Staff finds the request to be compatible with the emerging pattern of development in the area

Staff's Recommendation:	Approvable, with Conditions
	
Zoning Administrator Sign-off:	Brian Grady Fri Jun 09 16:20:45 2006

REVIEWING AGENCY COMMENTS

AGENCY	NO OBJECTION	CONDITIONS REQUESTED	AGENCY	NO OBJECTION	CONDITIONS REQUESTED
Code Enforcement	<input type="checkbox"/>	<input type="checkbox"/>	City of Plant City	<input type="checkbox"/>	<input type="checkbox"/>
Environmental Protection Commission	<input type="checkbox"/>	<input type="checkbox"/>	City of Tampa	<input type="checkbox"/>	<input type="checkbox"/>
Florida DOT	<input type="checkbox"/>	<input type="checkbox"/>	City of Temple Terrace	<input type="checkbox"/>	<input type="checkbox"/>
Fire Rescue	<input type="checkbox"/>	<input type="checkbox"/>	Tampa Bay Water	<input type="checkbox"/>	<input type="checkbox"/>
HART Line	<input type="checkbox"/>	<input checked="" type="checkbox"/>	Tampa Electric Company	<input type="checkbox"/>	<input type="checkbox"/>
Natural Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Parks and Recreation	<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
School Board	<input type="checkbox"/>	<input checked="" type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Sheriff	<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Transportation	<input type="checkbox"/>	<input checked="" type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Reclaimed Water	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Storm Water	<input type="checkbox"/>	<input checked="" type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Water Utilities	<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Wastewater	<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>

RECOMMENDED CONDITIONS

RECOMMENDED ZONING CONDITIONS ARE BASED ON SITE PLAN RECEIVED: May 31, 2006

1. The multi-use development shall be limited as follows:
 - 1.1. The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back: 25 feet
Minimum Set-back: 20 feet along western property boundary
40 feet from ELAPP property
50 feet from Alafia River

Maximum height: 60 feet (5 stories)
The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2. The project shall be permitted a maximum of 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein.
 - 1.3. The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-O) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.
 - 1.4. Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
2. The project may be permitted a maximum of 351 multi-family dwelling units, 60,980 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.
3. Buffering and screening shall be consistent with the requirements in the Land Development Code (LDC) unless otherwise stated herein. Scenic corridor standards shall apply along the southern property boundary along Gibsonton Drive and the DOT/Gibsonton Drive right-of-way.
4. Where applicable, the general design, number and location of the access point(s) shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (LDC Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and Growth Management Department. Final design, if approved by the Hillsborough County Planning and Growth Management Department may include, but is not limited to: left turn lanes, acceleration lane(s), and deceleration lane(s). Access points may be restricted in movements. Internal access points shown the General Site Plan are conceptual regarding the number and location of access points and may be adjusted or relocated subject to the approval of the Hillsborough County Planning and Growth Management Department.

5. Prior to Concurrency approval, the developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic. The turn lanes shall be constructed to FDOT and/or Hillsborough County Standards. For existing turn lanes, if the required turn lane storage, as identified in the transportation analysis, is greater than the length of the existing turn lane, then the developer shall extend the turn lanes by the necessary queue storage length, while maintaining the proper taper and braking distance lengths. The only exceptions to access related roadway improvements shall be based on documented safety or environmental concerns. All exceptions shall be approved by the Planning and Growth Management and Public Works Department. The developer, consistent with requirements of the Land Development Code, shall construct the following turn lanes:
 - A. If warranted, EB dual left turn lanes on Gibsonton Dr @ Fernhill, with receiving lane on Fernhill, at a minimum, additional EB left turn lane storage is required,
 - B. WB right turn lane on Gibsonton Dr @ Fernhill,
 - C. Exclusive NB left turn and a NB through-right turn lane on Fernhill @ Gibsonton,
 - D. Exclusive SB left turn, SB through, and SB right turn lane on Fernhill @ Gibsonton,
 - E. WB right turn lane on Gibsonton @ Fernhill,
6. Based on the projected trip generation to the site, access onto the public road would be via "Type III" Minor Roadway Connection (more than 1500 trip ends per day). The Land Development Code requires that all internal access (the "throat") to the driveways must be a minimum of 100 feet from the edge of pavement of the public roadway, and shall remain free of internal connections or parking spaces which might interfere with the movement of vehicles into or out of the site.
7. Prior to Concurrency Approval the Developer shall conduct a detailed traffic analysis on Gibsonton Drive that accounts for vested traffic on the surrounding roadways. If warranted to meet Concurrency, the Developer shall widen Gibsonton Drive to a six (6) lane divided roadway. The length of the improvements shall be determined at Concurrency review. The Developer has the option to wait to develop until such time as the roadway is improved. The Developer shall be responsible for all the cost associated with the design for Gibsonton Drive and shall be responsible for the cost associated with the construction of the Roadway, including all required drainage facilities and right-of-way. The final design and construction plans shall be approved by Planning & Growth Management & FDOT. Gibsonton Drive is identified in the 2025 MPO LRTP as a six (6) lane divided roadway.
8. Prior to General Site Plan certification, the Developer shall revise the ROW shown on the Site Plan to provide for a 50' ROW as required in the Transportation Technical Manual for an urban non-residential local roadway. The roadway shall be designed to the standards of the Tech Manual for an urban non-residential local roadway. Per the Tech Manual, the Developer shall also be required to improve Ida Street along the property boundary to the standards for an urban non-residential local roadway.
9. The Developer shall install a traffic signal, when warranted and approved by FDOT & Hillsborough County, at the intersection of Gibsonton Dr @ Fernhill Drive. The Public Works Department has conducted a signal warrant analysis and has determined that a signal is warranted. The developer has the option of either installing the signal or making payment to the County for the cost of the signal in lieu of installation. The installation or payment option shall be determined by Planning & Growth Management Staff in conjunction with the Public Works Department. If the County allows the Developer to choose the payment option, direct payment in the form of a Letter of Credit is acceptable or a Certified Check paid to the County to hold in Escrow. Payment shall be made to the County prior to preliminary site plan approval. If the Developer chooses to install the signal the developer shall indicate on the preliminary site plan that a traffic signal will be installed. Prior to Construction Plan Approval, the developer shall submit 60% signal design plans. The Developer shall be responsible for the cost of design and installation of the traffic signal

and appropriate interconnect with adjacent signals. The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department and/or FDOT.

10. Sidewalks of a minimum width of five (5) feet shall be constructed within the right-of-way along all roadways adjacent to the property boundaries and along both sides of all internal roadways. The Developer shall also provide sidewalk connections between sidewalks on external roadways and internal developments. The Developer shall also provide a sidewalk on the north side of Fernhill Dr that extends to the sidewalk on Gibsonton Dr.
11. The developer shall design and construct a major internal roadway, as shown on the general site plan, separating the commercial area from the residential area, including the following considerations: continuous sidewalks, free of obstruction and buffered from traffic by landscaping or setbacks; marked crosswalks at intersections, with handicapped provisions. The roadway shall be constructed to Hillsborough County standards in regard to pavement width, sidewalks and pavement section, unless otherwise approved by Hillsborough County. The roadways shall incorporate pedestrian and vehicular cross access, pedestrian interconnectivity and crosswalk safety features.
12. Cross access shall be provided between all outparcels and the major internal roadway. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.
13. The applicant shall show the ability to provide cross access to the western property as shown on the site plan. If any of the adjacent properties are developed by the same developer/owner, then cross access must be provided.
14. Dedicate and convey a total of 48' from center line of the road right-of-way along the frontage on Old Gibsonton Drive as specified in the current Hillsborough County Transportation Technical Manual.
15. A temporary access easement shall remain over vacated portions of Ida Street until such time as a permanent easement is approved by Hillsborough County. A permanent paved easement approved by Hillsborough County shall provide access from the site to the western portion of the ELAPP preserve.
16. The developer will provide a bus bay, transit accessory pad, shelter for existing transit stop on Gibsonton Drive.
17. The type, location, size and number of signs permitted shall be as set forth in Part 7.03.00 of the Land Development Code with the following exception(s):
 - 1) Ground Signs shall be limited to Monument Signs.
 - 2) Billboards, pennants and banners shall be prohibited.
18. The stormwater management system shall be designed and constructed in such a manner so as to not adversely impact off-site surface and groundwater elevations.
19. Approval of this application does not ensure that water will be available at the time when the applicant seeks permits to develop.
20. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
21. Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations

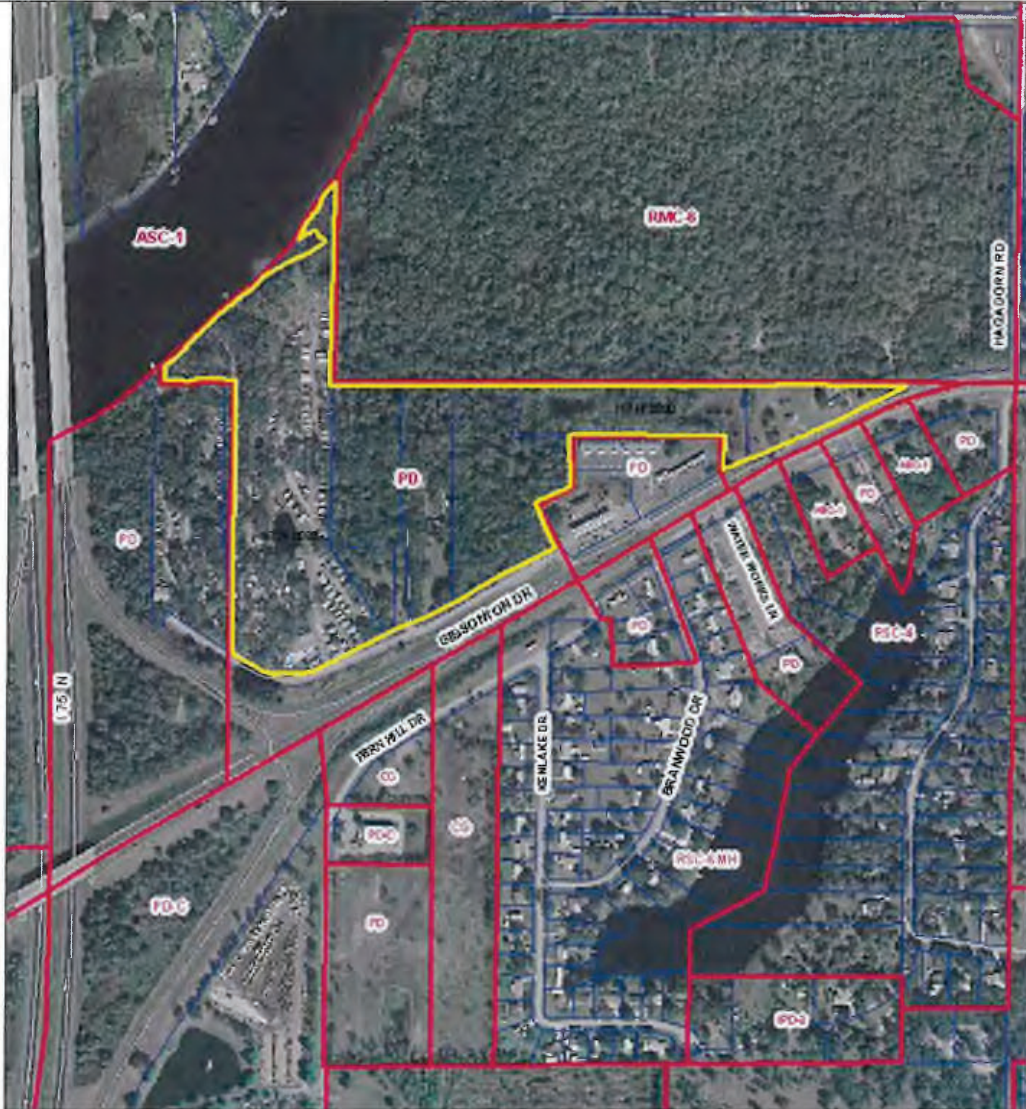
of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.

22. A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to Construction Site Development Review approval.
23. Policy C-36.6 of the Future Land Use Element of the Comprehensive Plan provides that the timing of new development should be coordinated with adequate school capacity as determined by the School District of Hillsborough County. Approval of the final Construction Plans for any portion of the residential development shall not occur until documentation is provided from the School District of Hillsborough County indicating that either:
 - a) Adequate capacity exists to accommodate the future residents of the project, as identified/determined by the School District of Hillsborough County;
or
 - b) Adequate school capacity is planned and funded to accommodate the future residents of the project, as identified/determined by the School District of Hillsborough County;
or
 - c) The applicant has provided adequate mitigation to offset inadequacies in school capacity, as identified/determined by the School District of Hillsborough County.The aforementioned documentation shall include a time period during which the School District of Hillsborough County determination shall be valid.
24. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
25. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
26. Within 90 days of approval by the Hillsborough County Board of County Commissioners, the applicant shall submit to the Planning and Growth Management Department a revised General Development Plan for certification which conforms to the notes and graphic of the plan to the conditions outlined above and the Land Development Code (LDC). Subsequent to certification of the plan, if it is determined the certified plan does not accurately reflect the conditions of approval or requirements of the LDC, said plan will be deemed invalid and certification of the revised plan will be required. The revised General Development Plan submitted for certification shall include corrected zoning, and land use for adjacent properties within 150 feet exterior of the project boundaries.
27. Effective as of February 1, 1990, this development order/permit shall meet the concurrency requirements of Florida Statutes Chapter 163, Part II. Approval of this development order/permit does not constitute a guarantee that there will be public facilities at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.

Land Use Application Summary Report

Application Number: PRS 08-0816 RV
 Existing Zoning: PD 06-0848
 Requested Zoning: Minor Modification
 Comp Plan: SMU-6, CMU-12
 Service Area: Urban

Adjacent Zoning and Land Uses:
 North: PD 06-848 Undeveloped/Mobile Homes
 South: CG/RSC-6MH Commercial, Residential
 East: PD 01-0045 Commercial
 West: PD 99-0735 Undeveloped (Approved for Commercial)



30-20-19

PRS 08-0816 RV

- Project Area
- Zoning Boundary



Planning & Growth Management Department



NTS


Request Details:

The request is for a Minor Modification to Planned Development (PD) 06-0848 for the following changes

- To eliminate Condition 5 that states road improvements originally required by Transportation Review staff;
- To increase the hotel square footage from 65,340 square feet to 68,880 square feet; and
- To increase the hotel height from 60 feet (5 stories) to 74 feet (6 stories).

Staff Findings:

- In 2006, the Board of County Commissioners (BOCC) approved the approximate 40-acre Planned Development (PD) 06-0848 for 351 multi-family residential units, 60,980 square feet of Commercial, Neighborhood (CN) uses, 10,890 square feet of Business, Professional Office (BPO) uses, and for a 32,670-square-foot, 150 room hotel.
- On October 23, 2007, the BOCC approved a Major Modification (MM) 07-1378 to increase the hotel square footage to 65,340 square feet, permit the Neighborhood, Commercial (CN) buildings to have 2-stories, and increase the CN impervious area to 75 percent.
- According to the application, the elimination of the transportation improvements required by Condition 5 is requested because it is no longer appropriate "due to the relocation of the traffic signal to the main entrance instead of Fernhill Drive." Transportation review staff does not object to the elimination of the condition with the modification of another condition to clarify the requirements for installation of the traffic signal.
- According to the application, the increase in height to six stories and the 3,540-square-foot increase in square footage are necessary to redesign the hotel to preserve two large oak trees on the site. The modification has reduced the number of rooms from 150 to 125 rooms, even though the square footage has increased due to additional common areas. The Commercial, Neighborhood (CN) square footage for the PD has been slightly reduced to comply with the 5 percent square-foot increase allowed by the Land Development Code (LDC) for a minor modification.
- Staff finds the request comparable and compatible with the existing and proposed development pattern along this portion of Gibsonton Drive, and finds the justifications provided by the applicant and verified by reviewing agencies sufficient to support the request. In addition to the requested condition changes, conditions that restate LDC regulations have been removed.

Staff's Recommendation:	Approvable, Subject to Conditions
Zoning Administrator Sign-off:	 J. Brian Grady Thu May 29 10:04:19 2008

NEIGHBORHOOD MEETING

Per LDC Section 5.03.06.B.2 the applicant is required to conduct a neighborhood meeting if requested by noticed residents.

- According to the applicant, noticed residents did not request a neighborhood meeting.
- The applicant conducted a neighborhood meeting. A summary of the meeting and the sign-in sheet are attached pursuant to the requirements of Sec. 5.03.06.B.2.

VARIANCE(S) TO NON-DISTRICT REGULATIONS

- No variances to non-district regulations have been requested.
- The applicant has requested a variance to . Staff finds the applicant's justifications (attached) for the variance(s), as provided in accordance with LDC Section 5.03.03, are sufficient / insufficient to warrant approval of the requested variance(s).

REVIEWING AGENCY COMMENTS

AGENCY	NO OBJECTION	CONDITIONS REQUESTED	AGENCY	NO OBJECTION	CONDITIONS REQUESTED
Community Improvement	<input type="checkbox"/>	<input type="checkbox"/>	Reclaimed Water	<input type="checkbox"/>	<input type="checkbox"/>
Environmental Protection Commission	<input type="checkbox"/>	<input type="checkbox"/>	Storm Water	<input type="checkbox"/>	<input type="checkbox"/>
Florida DOT	<input type="checkbox"/>	<input type="checkbox"/>	Water Utilities	<input type="checkbox"/>	<input type="checkbox"/>
Fire Rescue	<input type="checkbox"/>	<input type="checkbox"/>	Wastewater	<input type="checkbox"/>	<input type="checkbox"/>
HART Line	<input type="checkbox"/>	<input type="checkbox"/>	City of Plant City	<input type="checkbox"/>	<input type="checkbox"/>
Natural Resources	<input type="checkbox"/>	<input type="checkbox"/>	City of Tampa	<input type="checkbox"/>	<input type="checkbox"/>
Parks, Recreation, and Conservation	<input type="checkbox"/>	<input type="checkbox"/>	City of Temple Terrace	<input type="checkbox"/>	<input type="checkbox"/>
Public Works - Engineering	<input type="checkbox"/>	<input type="checkbox"/>	FDOT	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Public Works - Roadway	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Public Works - Traffic	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
School Board	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Sheriff	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Transportation	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>

REQUIREMENTS FOR CERTIFICATION

The following issues must be addressed on the site plan prior to General Site Plan Certification:

1. Page 1 site plan data shall be revised to correspond with the conditions and additional information submitted on May 20, 2008
2. Page 2 of the PD that includes the building elevations shall be submitted for certification, the notes shall be updated to correspond to the conditions and the additional information submitted on May 20, 2008.

RECOMMENDED CONDITIONS

RECOMMENDED ZONING CONDITIONS ARE BASED ON SITE PLAN RECEIVED APRIL 16, 2008

1. The multi-use development shall be limited as follows:
 - 1.1. The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2. The project shall be permitted a maximum of ~~60,980~~ 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.
 - 1.3. The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-O) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.
 - 1.4. The project shall be permitted a maximum ~~65,340~~ 68,880 square foot hotel containing a maximum of ~~150~~ 125 rooms on 3.0 acres. The project will be subject to the Commercial General (CG) zoning district development standards, except as referenced herein. Maximum building height shall be ~~sixty (60) feet (5 stories)~~ 74 feet (6 stories).
 - 1.5. Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
2. The project may be permitted a maximum of 351 multi-family dwelling units, a ~~65,340~~ 68,880 square foot hotel, ~~60,980~~ 60,707 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.

3. ~~Buffering and screening shall be consistent with the requirements in the Land Development Code (LDC) unless otherwise stated herein. Scenic corridor standards shall apply along the southern property boundary along Gibsonton Drive and the DOT/Gibsonton Drive right-of-way.~~
4. ~~Where applicable, the general design, number and location of the access point(s) shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (LDC Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and Growth Management Department. Final design, if approved by the Hillsborough County Planning and Growth Management Department may include, but is not limited to: left turn lanes, acceleration lane(s), and deceleration lane(s). Access points may be restricted in movements. Internal access points shown the General Site Plan are conceptual regarding the number and location of access points and may be adjusted or relocated subject to the approval of the Hillsborough County Planning and Growth Management Department.~~
5. ~~Prior to Concurrency approval, the developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic. The turn lanes shall be constructed to FDOT and/or Hillsborough County Standards. For existing turn lanes, if the required turn lane storage, as identified in the transportation analysis, is greater than the length of the existing turn lane, then the developer shall extend the turn lanes by the necessary queue storage length, while maintaining the proper taper and braking distance lengths. The only exceptions to access related roadway improvements shall be based on documented safety or environmental concerns. All exceptions shall be approved by the Planning and Growth Management and Public Works Department. The developer, consistent with requirements of the Land Development Code, shall construct the following turn lanes:~~
 - A. ~~If warranted, EB dual left turn lanes on Gibsonton Dr @ Fernhill, with receiving lane on Fernhill, at a minimum, additional EB left turn lane storage is required,~~
 - B. ~~WB right turn lane on Gibsonton Dr @ Fernhill,~~
 - C. ~~Exclusive NB left turn and a NB through right turn lane on Fernhill @ Gibsonton,~~
 - D. ~~Exclusive SB left turn, SB through, and SB right turn lane on Fernhill @ Gibsonton,~~
 - E. ~~WB right turn lane on Gibsonton @ Fernhill,~~
6. ~~Based on the projected trip generation to the site, access onto the public road would be via "Type III" Minor Roadway Connection (more than 1500 trip ends per day). The Land Development Code requires that all internal access (the "throat") to the driveways must be a minimum of 100 feet from the edge of pavement of the public roadway, and shall remain free of internal connections or parking spaces which might interfere with the movement of vehicles into or out of the site.~~
7. ~~Prior to Concurrency Approval the Developer shall conduct a detailed traffic analysis on Gibsonton Drive that accounts for vested traffic on the surrounding roadways. If warranted to meet Concurrency, the Developer shall widen Gibsonton Drive to a six (6) lane divided roadway. The length of the improvements shall be determined at Concurrency review. The Developer has the option to wait to develop until such time as the roadway is improved. The Developer shall be responsible for all the cost associated with the design for Gibsonton Drive and shall be responsible for the cost associated with the construction of the Roadway, including all required drainage facilities and right-of-way. The final design and construction plans shall be approved by Planning & Growth Management & FDOT. Gibsonton Drive is identified in the 2025 MPO LRTP as a six (6) lane divided roadway.~~
8. ~~Prior to General Site Plan certification, the Developer shall revise the ROW shown on the Site Plan to provide for a 50' ROW as required in the Transportation Technical Manual for an urban non-residential local roadway. The roadway shall be designed to the standards of the Tech Manual for an urban non-residential local roadway. Per the Tech Manual, the Developer shall also be required to improve Ida Street along the property boundary to the standards for an urban non-residential local roadway.~~
- 9.3. The Developer shall install a traffic signal, when warranted and approved by FDOT & Hillsborough County, at the intersection of Gibsonton Dr @ the project's main entrance Fernhill Drive. The Public Works Department has conducted a signal warrant analysis and has determined that a signal is warranted. ~~The developer has the option of either installing the signal or making payment to the County for the cost of the signal in lieu of installation. The installation or payment option shall be determined by Planning & Growth Management Staff in conjunction with the~~

Public Works Department. If the County allows the Developer to choose the payment option, direct payment in the form of a Letter of Credit is acceptable or a Certified Check paid to the County to hold in Escrow. Payment shall be made to the County prior to preliminary site plan approval. If the Developer chooses to install the signal the developer shall indicate on the preliminary site plan that a traffic signal will be installed. Prior to Construction Plan Approval, the developer shall submit 60% signal design plans. The Developer shall be responsible for the cost of design and installation of the traffic signal and appropriate interconnect with adjacent signals. The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department and/or FDOT.

10. ~~Sidewalks of a minimum width of five (5) feet shall be constructed within the right of way along all roadways adjacent to the property boundaries and along both sides of all internal roadways. The Developer shall also provide sidewalk connections between sidewalks on external roadways and internal developments. The Developer shall also provide a sidewalk on the north side of Fernhill Dr that extends to the sidewalk on Gibsonton Dr.~~
11. ~~The developer shall design and construct a major internal roadway, as shown on the general site plan, separating the commercial area from the residential area, including the following considerations: continuous sidewalks free of obstruction and buffered from traffic by landscaping or setbacks; marked crosswalks at intersections, with handicapped provisions. The roadway shall be constructed to Hillsborough County standards in regard to pavement width, sidewalks and pavement section, unless otherwise approved by Hillsborough County. The roadways shall incorporate pedestrian and vehicular cross access, pedestrian interconnectivity and crosswalk safety features.~~
- 12.4. Cross access shall be provided between all outparcels and the major internal roadway. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.
- 13.5. The applicant shall show the ability to provide cross access to the western property as shown on the site plan. If any of the adjacent properties are developed by the same developer/owner, then cross access must be provided.
14. ~~Dedicate and convey a total of 48' from center line of the road right of way along the frontage on Old Gibsonton Drive as specified in the current Hillsborough County Transportation Technical Manual.~~
- 15.6. A temporary access easement shall remain over vacated portions of Ida Street until such time as a permanent easement is approved by Hillsborough County. A permanent paved easement approved by Hillsborough County shall provide access from the site to the western portion of the ELAPP preserve.
- 16.7. The following conditions are required by the Florida Department of Transportation (FDOT):
 - 16.7.1 The developer shall modify the median opening at Fern Hills Drive to allow east and west inbound left turns only.
 - 16.7.2 The next median opening to the east (second from I-75) shall be closed.
 - 16.7.3 The next median opening to the east (third from I-75) shall remain as a full opening and have left turn lanes in both directions installed. These turn lanes shall be FDOT Index 301 requirements.
 - 16.7.4 The next median opening to the east (fourth from I-75) shall be closed.
 - 16.7.5 The driveway aligning with Fern Hill Drive shall be reconfigured to serve as a right in/out and left in. This driveway shall be 3 lanes. There shall be no crossing movements on this driveway depth (throat) for a minimum of 150 feet from Gibsonton Drive. This can be accomplished by installation of a traffic separator.
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 - 16.7.7 FDOT will not allow a traffic signal at Fern Hills Drive. This median opening is too close to the ramp terminus of I-75. A signal at this location has the potential to back traffic onto I-75.
- 17.8. The developer will provide a bus bay, transit accessory pad, shelter for existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.
18. ~~The type, location, size and number of signs permitted shall be as set forth in Part 7.03.00 of the Land Development Code with the following exception(s):~~

- ~~18.1~~ — Ground Signs shall be limited to Monument Signs.
- ~~18.2~~ — Billboards, pennants and banners shall be prohibited.
- ~~19.~~ — The stormwater management system shall be designed and constructed in such a manner so as to not adversely impact off-site surface and groundwater elevations.
- ~~20.~~ — Approval of this application does not ensure that water will be available at the time when the applicant seeks permits to develop.
- ~~21.9.~~ Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
- ~~22.10.~~ Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.
- ~~23.11.~~ A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to Construction Site Development Review approval.
- ~~24.~~ — Policy C 36.6 of the Future Land Use Element of the Comprehensive Plan provides that the timing of new development should be coordinated with adequate school capacity as determined by the School District of Hillsborough County. Approval of the final Construction Plans for any portion of the residential development shall not occur until documentation is provided from the School District of Hillsborough County indicating that either:
- ~~a)~~ — Adequate capacity exists to accommodate the future residents of the project, as identified/determined by the School District of Hillsborough County;
 - ~~b)~~ — Adequate school capacity is planned and funded to accommodate the future residents of the project, as identified/determined by the School District of Hillsborough County;
 - ~~e)~~ — The applicant has provided adequate mitigation to offset inadequacies in school capacity, as identified/determined by the School District of Hillsborough County.
- ~~The aforementioned documentation shall include a time period during which the School District of Hillsborough County determination shall be valid.~~
- ~~25.12.~~ If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
- ~~26.13.~~ The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- ~~27.14.~~ Effective as of February 1, 1990, this development order/permit shall meet the concurrency requirements of Florida Statutes Chapter 163, Part II. Approval of this development order/permit does not constitute a guarantee that there will be public facilities at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.

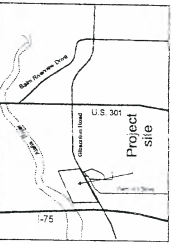
GIBSONTON DRIVE PARCEL PD
 CERTIFIED GENERAL DEVELOPMENT PLAN
 MAJOR MODIFICATION #MM 07-1378

RECEIVED
 APR 16 2008
 HILLSBOROUGH
 PLANNING DEPARTMENT
 08-0816

DATE: July 16, 2007
 DRAWN BY: J. J. [unreadable]
 CHECKED BY: [unreadable]
 SCALE: [unreadable]
 SHEET: 1 of 2

PROJECT DATA FOR TOTAL PD

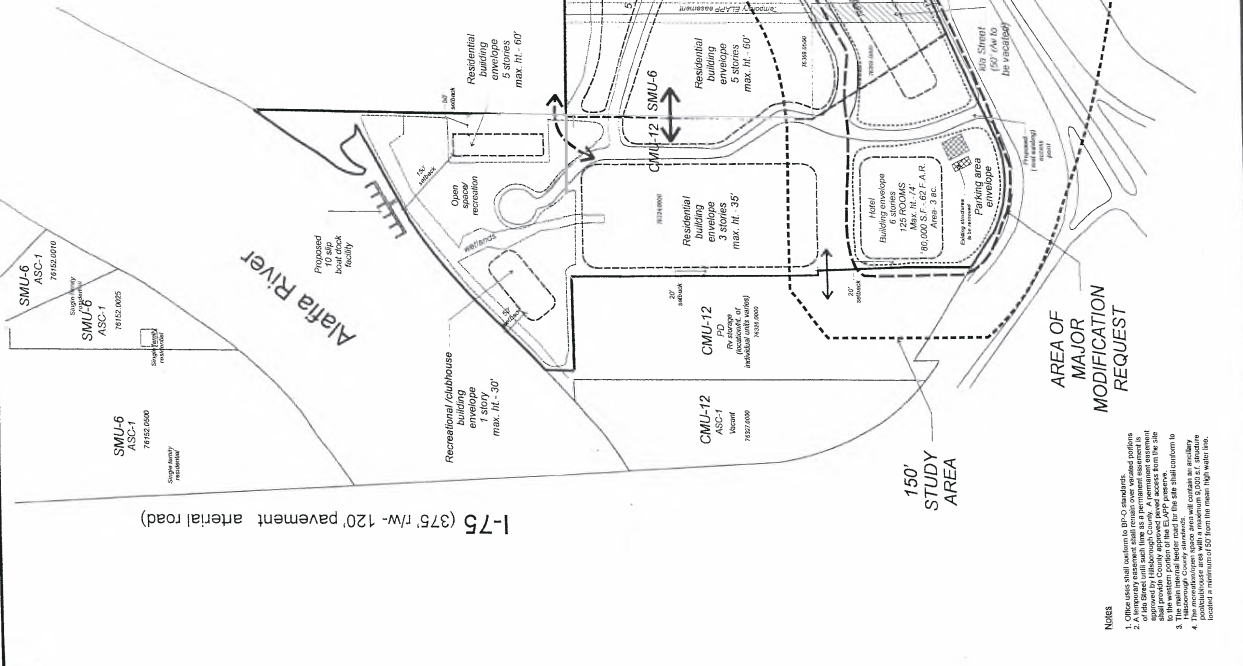
OWNER/DEVELOPER	ALPHA RIVER PROPERTY GROUP, L.L.P. 1900 S. GIBSON DRIVE CORPSEY, FL 33620
PLANNER	WILSON MILLER 1107 CHANNELSIDE DRIVE TAMPA, FL 33602
ATTORNEY	JAMES M. [unreadable] JR. HOLLAND & KINGSIT 100 N. TAMPA ST. TAMPA, FL 33602-3644
TOTAL AC.	39.38 AC.
ZONING	CMU 12, SMU 6
FUTURE LAND USE	PD
PROPOSED USES	RESIDENTIAL RECREATION/CLUBHOUSE OFFICE HOTEL UTILITIES
PROPOSED USES	RESIDENTIAL RECREATION/CLUBHOUSE OFFICE HOTEL UTILITIES
PROPOSED USES	RESIDENTIAL RECREATION/CLUBHOUSE OFFICE HOTEL UTILITIES



Location Map

Flex calculations (major modification area not affected)

Residential area Ac. x H.F.R.	29.2 ac. (17.9 ac. existing) 4.12 ratio: 1.58:1
CMU 12 flex	9.6 ac. x 6 H.F.R.: 17.96
SMU 6	2.6 ac. x 6 H.F.R.: 3.11 units
Total permitted	104,544 S.F.



LEGEND

SMU-6	FUTURE LAND USE
PD	ZONING
[Symbol]	FOLIO NUMBER
[Symbol]	EXISTING LAND USE
[Symbol]	EXISTING SIDEWALK
[Symbol]	EXISTING STRUCTURES
[Symbol]	APPROX. METLAND LIMITS
[Symbol]	PORTION OF DA ST RW TO BE WIDENED
[Symbol]	TEMPORARY ELLIPSE ASSESSMENT TO BE RELOCATED WITH FUTURE PERMANENT EASEMENT
[Symbol]	POTENTIAL ELLIPSE ACCESS
[Symbol]	POTENTIAL CROSS ACCESS
[Symbol]	ROADWAY TO BE BUILT TO COUNTY STANDARDS

REVISED: APRIL 16, 2008

RZ 06-0848 RV

- NOTES**
1. The site plan shall conform to all applicable codes and standards.
 2. A preliminary plat shall be filed with the Hillsborough County Board of County Commissioners for their review and approval.
 3. The western portion of this site plan is reserved for future development.
 4. The site plan shall be subject to the approval of the Hillsborough County Board of County Commissioners.
 5. The site plan shall be subject to the approval of the Hillsborough County Board of County Commissioners.

Land Use Application Summary Report

Application Number: MM 07-1378 RV
 Existing Zoning: PD 06-0848
 Requested Zoning: Major Modification
 Comp Plan: SMU-6 & CMU-12
 Service Area: Urban

Adjacent Zoning and Land Uses:

North: PD 06-0848; Undeveloped / Mobile homes
 South: CG/RSC-6MH; Gibsonton Dr./Commercial / Residential
 East: PD 01-0045; Commercial
 West: PD 99-0735; Undeveloped



30-20-19

MM 07-1378 RV



Project Area



Zoning Boundary



PLANNING & GROWTH MANAGEMENT DEPARTMENT




Request Details:

The request is for a Major Modification to Planned Development 06-0848 located on the north side of Frontage Road, approximately 100 feet southwest of Fern Hill Drive. The purpose of the modification is to permit the following changes:

- Increase the permitted Floor Area Ratio (FAR) for the hotel use to allow a 65,340 square foot building. No increase in the number of hotel rooms (150) is proposed as part of the modification.
- Permit the neighborhood commercial building to have a maximum of 2 stories. No additional building height is proposed.
- Increase the maximum impervious surface for the neighborhood commercial parcel to 75%.

Staff Findings:

- The 3-acre hotel parcel is currently permitted to be developed with a 32,670 square foot hotel containing 150 rooms with a maximum height of 60 feet. The applicant is requesting to increase the square footage of the hotel to 65,340 square feet in order to develop larger rooms within the hotel. No additional rooms or building height is requested as part of this modification.
- The requested 65,340 square feet equates to a 0.50 FAR for the 3-acre parcel. The site is located in the CMU-12 Future Land Use category which permits consideration of up to a maximum intensity of 0.50. The Planning Commission has found the request to be consistent with the Comprehensive Plan.
- The current conditions of approval permit the commercial parcel to be developed consistent with CN Zoning District standards which includes a maximum building height of 35 feet. However, the Certified Site Plan for the project included a note that the building will be limited to a single-story. Staff has reviewed the prior record and has no objection to permitting a 2-story commercial building, with a maximum height of 35 feet, to be developed within this project.
- The CN Zoning District permits a maximum impervious surface of 60% and the applicant has requested to increase the impervious surface for the commercial development parcel to 75%. However, the applicant has stated that the overall impervious surface for the planned development will be 60%. Staff does not object to this increase in impervious surface area.
- The applicant has submitted a detailed concept plan for the hotel use and a detailed concept plan and building elevation for the commercial neighborhood portion of the project. These detailed plans indicate that the commercial neighborhood uses will be distributed among several buildings with parking or landscaped areas interspersed between the buildings. This will reduce the appearance of strip commercial along Gibsonton Drive. Additionally, the building elevation shows a design that includes pitched roof lines, window treatments, and architectural treatments. Staff has included a condition requiring these drawings to be incorporated onto the site plan prior to certification.
- The site is located east of the I-75 Gibsonton Drive interchange and is surrounded by the Alafia Scrub Preserve ELAPP site to the north, a PD (99-0735) approved for dry boat storage and a hotel to the west, and a PD (01-0045) approved for commercial uses to the east. Additionally there is CG zoning to the south.
- Staff finds that the proposed modifications are in keeping with the intent of the original Planned Development zoning of the property and are consistent with the emerging development pattern of this segment of Gibsonton Drive.

Staff's Recommendation:	Approvable, Subject to Conditions
Zoning Administrator Sign-off:	 J. Brian Grady, AICP Thu Aug 23 13:26:19 2007

NEIGHBORHOOD MEETING

Per LDC Section 5.03.06.B.2 the applicant is required to conduct a neighborhood meeting if requested by noticed residents.

- According to the applicant, noticed residents did not request a neighborhood meeting.
- The applicant conducted a neighborhood meeting. A summary of the meeting and the sign-in sheet are attached pursuant to the requirements of Sec. 5.03.06.B.2.

VARIANCE(S) TO NON-DISTRICT REGULATIONS

- No variances to non-district regulations have been requested.
- The applicant has requested a variance to . Staff finds the applicant's justifications (attached) for the variance(s), as provided in accordance with LDC Section 5.03.03, are sufficient / insufficient to warrant approval of the requested variance(s).

REVIEWING AGENCY COMMENTS

AGENCY	NO OBJECTION	CONDITIONS REQUESTED	AGENCY	NO OBJECTION	CONDITIONS REQUESTED
Community Improvement	<input type="checkbox"/>	<input type="checkbox"/>	Reclaimed Water	<input type="checkbox"/>	<input type="checkbox"/>
Environmental Protection Commission	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Storm Water	<input type="checkbox"/>	<input type="checkbox"/>
Florida DOT	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Water Utilities	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Fire Rescue	<input type="checkbox"/>	<input type="checkbox"/>	Wastewater	<input type="checkbox"/>	<input type="checkbox"/>
HART Line	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	City of Plant City	<input type="checkbox"/>	<input type="checkbox"/>
Natural Resources	<input checked="" type="checkbox"/>	<input type="checkbox"/>	City of Tampa	<input type="checkbox"/>	<input type="checkbox"/>
Parks, Recreation, and Conservation	<input type="checkbox"/>	<input type="checkbox"/>	City of Temple Terrace	<input type="checkbox"/>	<input type="checkbox"/>
Public Works – Engineering	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Public Works – Roadway	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Public Works – Traffic	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
School Board	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Sheriff	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Transportation	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>

REQUIREMENTS FOR CERTIFICATION

The following issues must be addressed on the site plan prior to Application Approval and Site Plan Certification by the Board of County Commissioners:

1. Remove the reference to the proposed pad/transit shelter, shown on the out parcel PD 01-0045, from the site plan.
2. Include the concept plan and building elevation, submitted on August 17, 2007, on sheet 2 of the general site plan.

RECOMMENDED CONDITIONS

RECOMMENDED ZONING CONDITIONS ARE BASED ON SITE PLAN RECEIVED: AUGUST 6, 2007

1. The multi-use development shall be limited as follows:
 - 1.1. The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12) zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.
 - 1.2. The project shall be permitted a maximum of 60,980 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.
 - 1.3. The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-O) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.
 - 1.4. The project shall be permitted a maximum ~~32,670~~ 65,340 square foot ~~(150 rooms)~~ hotel containing a maximum of 150 rooms on 3.0 acres. The project will be subject to the Commercial General (CG) zoning district development standards, except as referenced herein. Maximum building height shall be sixty (60) feet (5 stories).
 - 1.5. Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
2. The project may be permitted a maximum of 351 multi-family dwelling units, a 65,340 square foot hotel, 60,980 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.

3. Buffering and screening shall be consistent with the requirements in the Land Development Code (LDC) unless otherwise stated herein. Scenic corridor standards shall apply along the southern property boundary along Gibsonton Drive and the DOT/Gibsonton Drive right-of-way.
4. Where applicable, the general design, number and location of the access point(s) shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (LDC Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and Growth Management Department. Final design, if approved by the Hillsborough County Planning and Growth Management Department may include, but is not limited to: left turn lanes, acceleration lane(s), and deceleration lane(s). Access points may be restricted in movements. Internal access points shown the General Site Plan are conceptual regarding the number and location of access points and may be adjusted or relocated subject to the approval of the Hillsborough County Planning and Growth Management Department.
5. Prior to Concurrency approval, the developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic. The turn lanes shall be constructed to FDOT and/or Hillsborough County Standards. For existing turn lanes, if the required turn lane storage, as identified in the transportation analysis, is greater than the length of the existing turn lane, then the developer shall extend the turn lanes by the necessary queue storage length, while maintaining the proper taper and braking distance lengths. The only exceptions to access related roadway improvements shall be based on documented safety or environmental concerns. All exceptions shall be approved by the Planning and Growth Management and Public Works Department. The developer, consistent with requirements of the Land Development Code, shall construct the following turn lanes:
 - A. If warranted, EB dual left turn lanes on Gibsonton Dr @ Fernhill, with receiving lane on Fernhill, at a minimum, additional EB left turn lane storage is required,
 - B. WB right turn lane on Gibsonton Dr @ Fernhill,
 - C. Exclusive NB left turn and a NB through-right turn lane on Fernhill @ Gibsonton,
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9. The Developer shall install a traffic signal, when warranted and approved by FDOT & Hillsborough County, at the intersection of Gibsonton Dr @ Fernhill Drive. The Public Works Department has conducted a signal warrant analysis and has determined that a signal is warranted. The developer has the option of either installing the signal or making payment to the County for the cost of the signal in lieu of installation. The installation or payment option shall be

determined by Planning & Growth Management Staff in conjunction with the Public Works Department. If the County allows the Developer to choose the payment option, direct payment in the form of a Letter of Credit is acceptable or a Certified Check paid to the County to hold in Escrow. Payment shall be made to the County prior to preliminary site plan approval. If the Developer chooses to install the signal the developer shall indicate on the preliminary site plan that a traffic signal will be installed. Prior to Construction Plan Approval, the developer shall submit 60% signal design plans. The Developer shall be responsible for the cost of design and installation of the traffic signal and appropriate interconnect with adjacent signals. The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department and/or FDOT.

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- ~~19.20.~~ Approval of this application does not ensure that water will be available at the time when the applicant seeks permits to develop.
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- ~~21.22.~~ Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.
- ~~22.23.~~ A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to Construction Site Development Review approval.
- ~~23.24.~~ Policy C-36.6 of the Future Land Use Element of the Comprehensive Plan provides that the timing of new development should be coordinated with adequate school capacity as determined by the School District of Hillsborough County. Approval of the final Construction Plans for any portion of the residential development shall not occur until documentation is provided from the School District of Hillsborough County indicating that either:
- a) Adequate capacity exists to accommodate the future residents of the project, as identified/determined by the School District of Hillsborough County;
 - or
 - b) Adequate school capacity is planned and funded to accommodate the future residents of the project, as identified/determined by the School District of Hillsborough County;
 - or
 - c) The applicant has provided adequate mitigation to offset inadequacies in school capacity, as identified/determined by the School District of Hillsborough County.
- The aforementioned documentation shall include a time period during which the School District of Hillsborough County determination shall be valid.
- ~~24.25.~~ If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
- ~~25.26.~~ The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- ~~26.~~ Within 90 days of approval by the Hillsborough County Board of County Commissioners, the applicant shall submit to the Planning and Growth Management Department a revised General Development Plan for certification which conforms to the notes and graphic of the plan to the conditions outlined above and the Land Development Code (LDC). Subsequent to certification of the plan, if it is determined the certified plan does not accurately reflect the conditions of approval or requirements of the LDC, said plan will be deemed invalid and certification of the revised plan will be required. The revised General Development Plan submitted for certification shall include corrected zoning, and land use for adjacent properties within 150 feet exterior of the project boundaries.

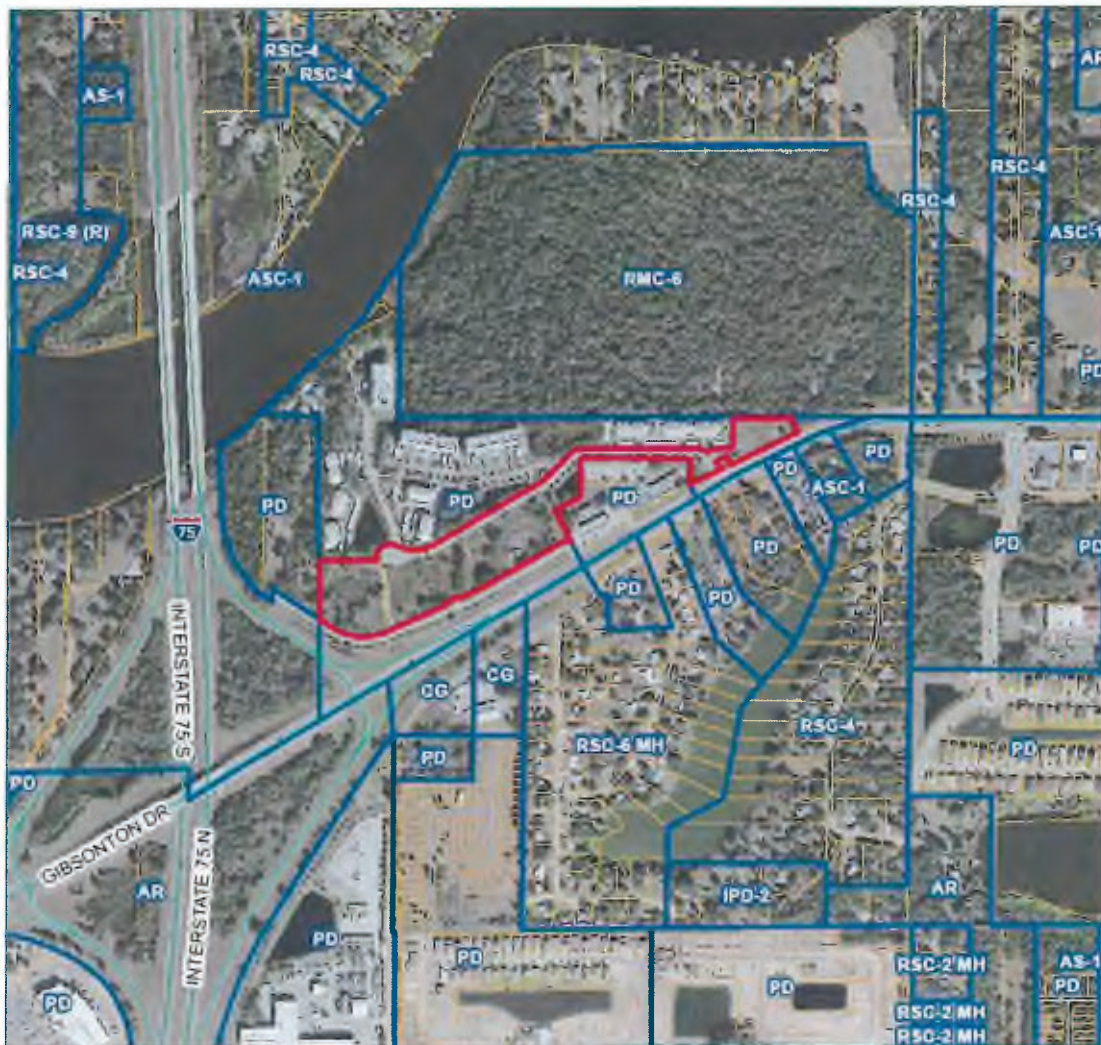
27. Prior to approval by the Hillsborough County Board of County Commissioners at the scheduled Land Use Meeting, the applicant shall submit to the Planning and Growth Management Department a revised General Development Plan for certification which conforms the notes and graphic of the plan to the conditions outlined above, the Land Development Code (LDC) and/or the recommendations of the Land Use Hearing Officer. The revised general site plan shall be certified upon approval of the petition at the scheduled Board of County Commissioners Land Use Meeting. Prior to petition approval and certification, if it is determined the certified plan does not accurately reflect the conditions of approval, the requirements of the LDC and/or the recommendations of the Land Use Hearing Officer, as may be modified at the Board of County Commissioners Land Use Meeting, said plan will be deemed invalid and a revised General Development Plan shall be submitted for certification. Certification of the revised plan (and approval of the petition) shall occur at the next available Board of County Commissioners Land Use Meeting. The submittal deadline for the revised General Development Plan under all of the above scenarios shall be the deadline for the filing of Oral Argument.
- 27.28. Effective as of February 1, 1990, this development order/permit shall meet the concurrency requirements of Florida Statutes Chapter 163, Part II. Approval of this development order/permit does not constitute a guarantee that there will be public facilities at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.



Hillsborough County Florida

STAFF REPORT

SUBJECT:	PRS 19-0778 RV/S	PLANNING AREA:	Riverview
REQUEST:	Minor Modification to a Planned Development	SECTOR:	South
APPLICANT:	Alafia Crossing Acquisition, LLC (Ronald Scaglione)		
Existing Zoning : PD 06-0848		Comp Plan Category: SMU-6, CMU-12	



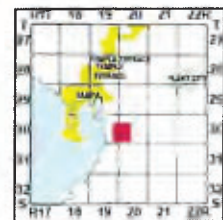
PRS 19-0778

Folio:
76324.0010,
76343.1000

- Application Site
- Zoning Boundary
- Parcels



STR: 19-30-20



Map of Hillsborough County Florida has been made to ensure the accuracy of the map information. The user assumes any liability arising from use of the map. The map is provided as a service and is not intended to be used for any other purpose. The user assumes any liability arising from use of the map.

This map has been prepared for the inventory of new property found within Hillsborough County and is complete with recorded deeds, state and other public records. The user assumes any liability arising from use of the map.

Users of this data are hereby notified that the information is not intended to be used for any other purpose. The user assumes any liability arising from use of the map.

Application Review Summary and Recommendation

1.0 Summary

1.1 Project Narrative

The applicant seeks to modify a Planned Development (PD) 06-0848 located in the NE corner of I-75 and Gibsonton Dr. The PD is approved for Multifamily and Commercial Neighborhood Uses today. The applicant proposes the following:

1. To waive the minimum distance and screening requirements for order boxes for Eating Establishments with Drive-Through Service adjacent to residential property. The Land Development Code (LDC) Sec. 6.11.35 requires a minimum of 200 feet of distance between an order box and residentially zoned properties and a fence separating the drive through facility from abutting residential parcels;
2. To eliminate condition #3 which pertains to the installation of a traffic signal, when warranted and approved by FDOT & Hillsborough County, at the intersection of Gibsonton Dr. @ the project's main entrance;
3. Amend Transportation conditions for road improvements and turn lanes.



Figure 1 –Subject PD

No changes to entitlements or other standards are being requested.

APPLICATION: PRS 19-0778 RV/S
 ZHM HEARING DATE: N/A
 BOCC MEETING DATE: October 8, 2019

CASE REVIEWER: Israel Monsanto

Surrounding zoning and uses are:

LOCATION	ZONING	USE / APPROVED FOR
North	PD 06-0848	Multi-family Apartment
South	Commercial General (CG); Residential Sing-family Conventional- Mobile Home (RSC-6 MH); PD 18-0946, 02-0791	Mobile Homes, single-family homes, Commercial, Office.
East	PD 06-0848	Open Space
West	PD 99-0735	Vacant (Hotel and RV/Boat Storage)

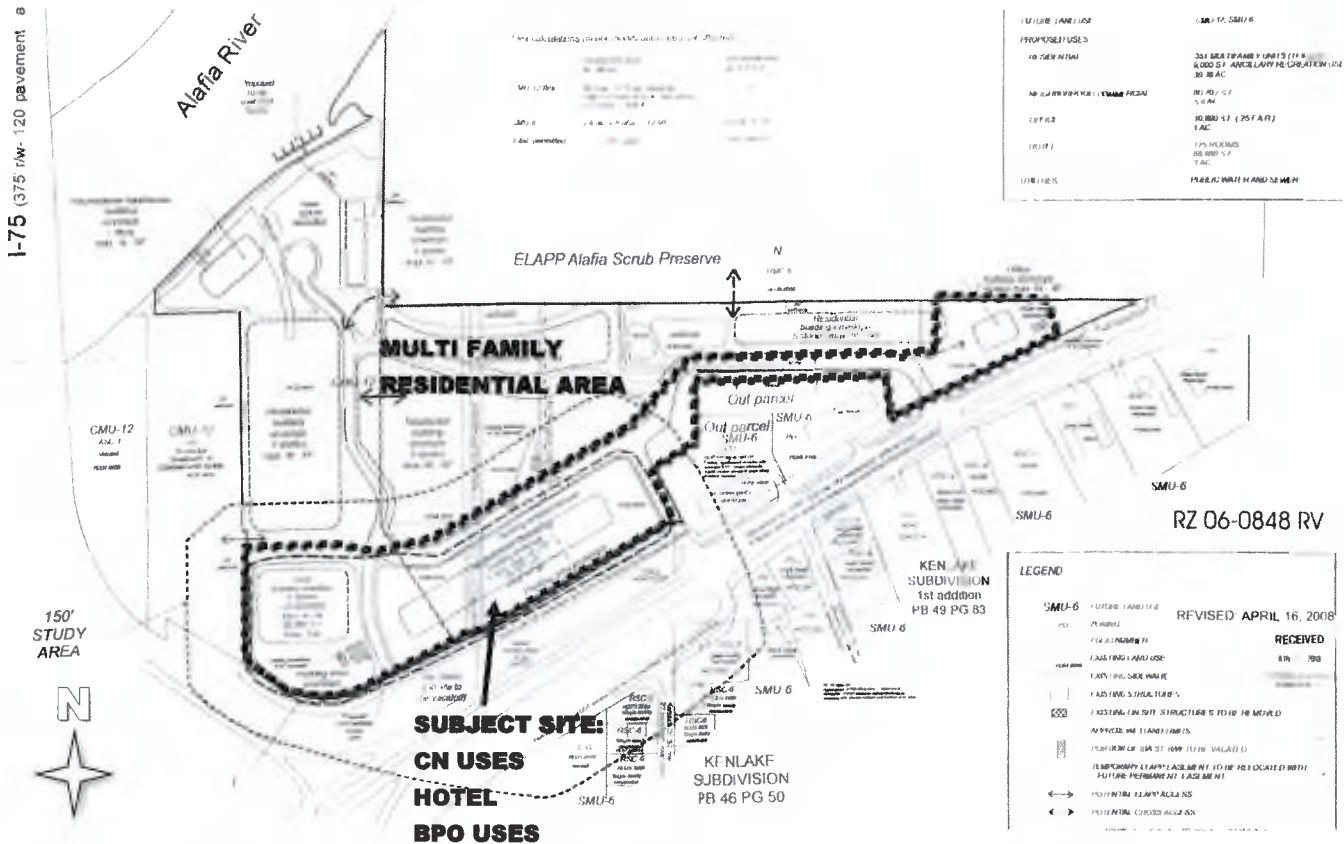


Figure 2 -Current PD Plan

PD 06-0848 was modified in 2008 (08-0816) to change commercial entitlements and to reduce the number of beds for a hotel. The PD is currently approved for 351 multi-family residential units and a maximum of 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses. Additionally, the project is permitted a maximum of 10,890 square feet of Business Professional Office (BP-O) zoning district uses and a maximum 68,880 square foot hotel containing a maximum of 125 rooms. Today, only the multi-family residential portion of the mixed use project is built.

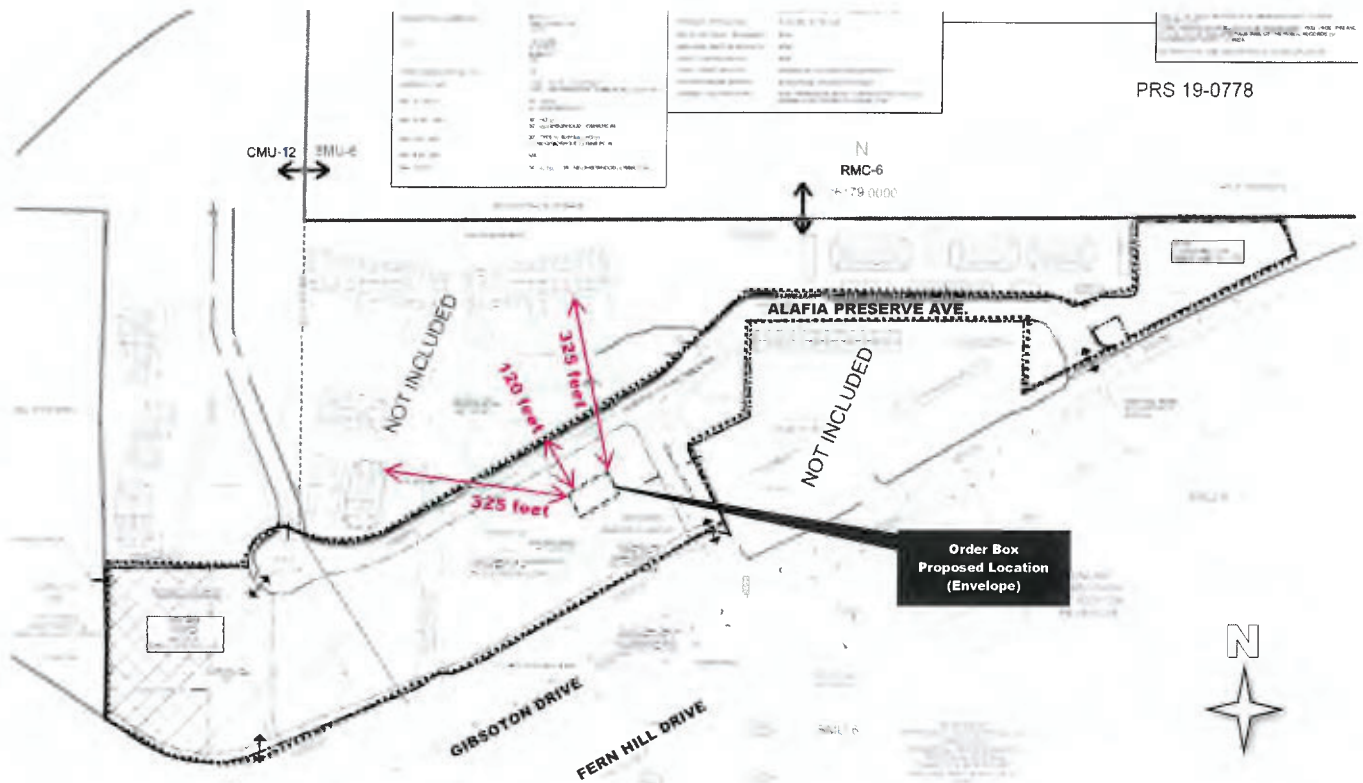


Figure 3 –Proposed PD Plan Changes

1.2 Compliance Overview with Land Development Code and Technical Manuals

The applicant has not requested variations from the general site development requirements found in Parts 6.05.00, Parking and Loading; 6.06.00, Landscaping, Irrigation and Buffering Requirements; or 6.07.00, Fences and Walls of the Land Development Code.

However, as part of this Minor Modification, the applicant has requested two waivers from the standards found in the LDC Section 6.11.35, Eating Establishments:

1. LDC Section 6.11.35.B.1 states that *no order box used in the ordering of food or beverages from a drive-through window shall be located within 200 feet of any property zoned residential; and*
2. LDC Section 6.11.35.B.2 *requires a solid screening fence or wall, a minimum of six feet in height, to be placed between any property used for a drive-through facility and any abutting property zoned residential. The intent of this solid screening is to screen vehicular headlight glare from adjacent residential property.*

The applicant is requesting to reduce the order box distance separation and to eliminate the solid fence requirement.

APPLICATION: PRS 19-0778 RV/S

ZHM HEARING DATE: N/A

BOCC MEETING DATE: October 8, 2019

CASE REVIEWER: Israel Monsanto

Per the applicant's narrative and justifications, the project was approved as a mixed-use development as encouraged by the Comprehensive Plan and the developable areas of the commercial parcel (which includes the Order Box Envelope) are separated from the multi-family areas of the Planned Development by Alafia Preserve Avenue – a planned future public right-of-way. While property line for the apartments is 120 feet from the anticipated order box, the 5-story apartment buildings are approximately 325 feet away from the proposed location of the order box. There is also a retention pond, a frontage road, multiple landscaping areas and parking rows between the commercial area and the apartments. The distance between the building and Code-required parking lot landscaping will adequately screen against headlight intrusion. Collectively, these existing conditions provide similar or greater protection from headlight intrusion sought to be addressed by a strict application of Section 6.11.35 (B)(2) of the LDC. The applicant also notes that the developable portions of the subject commercial site and multi-family parcels within the Planned Development are separated at their common boundary by a private roadway know as Alafia Preserve Avenue constructed within an approximately 50 foot right-of-way. While Alafia Preserve Avenue is currently located within the commercial parcel, the proposed conditions for this PRS require its dedication upon completion of the Fern Hill Extension – after which the commercial and multi-family parcels will no longer be “abutting” properties and the screening would not be required.

Staff has reviewed the justifications for the PD variations and offers no objection. The site is an approved mixed use Planned Development with a Mixed Use Future Land Use classification, SMU-6 and CMU-12. Location of the uses in the PD contemplates a project with an integrated mixed of residential and non-residential uses. While the distance from the proposed location of the order box to the residential parcel to the north (The Preserve at Alafia apartment complex) is less than 200 feet, the current placement of the multi-family buildings (325 feet) exceeds the minimum 200 feet requirement by more than 100 feet. Additionally, the apartment complex site contains existing natural vegetation, landscaped areas, a rod iron fence and parking areas placed between the apartment buildings and the proposed order box location. Alafia Preserve Avenue will be dedicated to the County to be a public right of way, at least 50-feet in width, thus the screening requirement would not apply per the LDC. The proposed drive-through facility site will also be subject to landscaping and buffering requirements per the Land Development Code, which would add screening to the existing vegetation in the Multi-family site. All of the above would provide ample screening between the uses in the mixed use project.



Figure 4 – Driveway separating subject site and M-F Apartments

APPLICATION: PRS 19-0778 RV/S

ZHM HEARING DATE: N/A

BOCC MEETING DATE: October 8, 2019

CASE REVIEWER: Israel Monsanto

1.3 Analysis of Recommended Conditions

New Condition 1.6

This condition is being added to specify a minimum of 120 feet of separation between the order box for a drive thru facility and the residential multi-family property to the north. A solid fence would not be required; however, buffering and screening within the drive thru facility site will be subject to the LDC.

Condition 3

This condition requiring the installation of a traffic signal, when warranted and approved by FDOT & Hillsborough County, at the intersection of Gibsonton Dr. @ the project's main entrance is being removed. Hillsborough County and FDOT have determined that a signal located at Gibsonton Drive and Fern Hill Road is a better alternative give the high volumes of heavy trucks at this location.

Old Condition 7

This condition is being amended per transportation comments. Many of the required improvements have been completed by the developer. New road improvements have been added.

New Condition 7

This condition includes Right of Way (ROW) dedication language and timing. The public ROW would provide access for adjacent parcels to the west.

Condition 12

This condition has been satisfied by the previous applicant. The GSP was provided for certification with the required revisions.

1.4 Evaluation of Existing and Planned Public Facilities

No public facilities issues are posed by the requested modification.

1.5 Natural Resources/Environmental

N/A

1.6 Comprehensive Plan Consistency

The Planning Commission staff finds the proposed re-zoning **consistent** with the *Future of Hillsborough* Comprehensive Plan.

1.7 Compatibility

The proposed changes requested by this Minor Modification would not affect the integrity of the Planned Development. Transportation staff has reviewed the proposed changes to the transportation conditions and does not object, as the proposed modification will have no impact on the transportation network. Cross access to the west is being maintained and current and future connectivity to adjacent commercial parcels west of the subject site is being provided.

As noted before, staff does not object to the reduction of the setback for the order box location. Although the minimum required 200 feet of minimum distance is being reduced to 120 between the proposed order box location and the Multi-family parcel line, existing driveways, retention pond, parking drive aisles and spaces are placed between the proposed order box and the nearest residential multifamily building. Required landscaping from the subject site in addition to existing trees along the southern portion of the

multifamily project would assist with the screening in lieu of a solid fence (Figure 5). The approximate distance between the proposed order box location and the closest MF apartment building is over 300 feet.

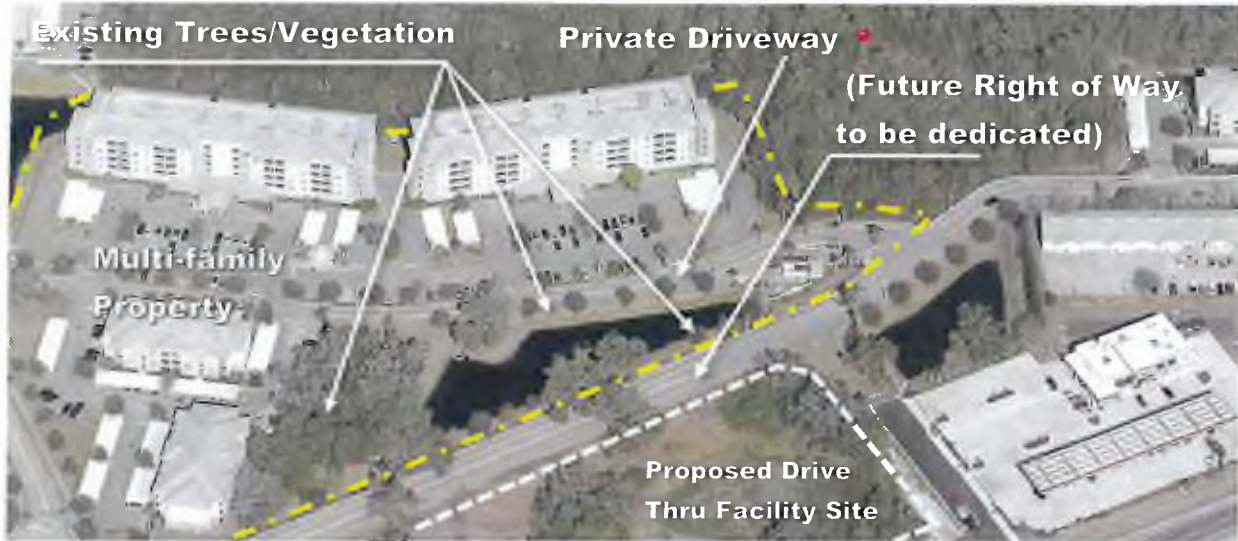


Figure 5 – MF Apartments, parking areas, driveways and landscaping

The entitlements in the PD are being maintained and no increase of commercial square footage is being requested. Transportation staff has proposed conditions requiring right of way dedication and cross access to the west for future connectivity. Based on all of the above, staff finds the request approvable, with conditions.

1.7 Agency Comments

No objections were received from reviewing agencies.

1.8 Exhibits

Exhibit 1: Aerial Map

Exhibit 2: Existing Certified Site Plan PD 06-0848 (08-0816)

Exhibit 3: Proposed PD Plan (PRS 19-0778)

2.0 Recommendation

Staff recommends approval, subject to the following conditions.

2.1 Recommended Conditions of Approval

APPROVAL - Approval, subject to the conditions listed below, is based on site plan received April 16, 2008 ~~September 20, 2019~~

1. The multi-use development shall be limited as follows:
 - 1.1. The project shall be permitted a maximum of 351 multi-family residential units on 30.36 acres; and shall be subject to the Residential Multi-family Conventional-12 (RMC-12)

zoning district development standards, except as referenced herein.

Minimum Front Yard Set-back:	25 feet
Minimum Set-back:	20 feet along western property boundary 40 feet from ELAPP property 50 feet from Alafia River
Maximum height:	60 feet (5 stories)

The 2:1 additional set back for buildings over 20 feet in height shall not apply to the western and northern property boundaries.

- 1.2. The project shall be permitted a maximum of 60,707 square feet of building area for Commercial Neighborhood (CN) zoning district uses on 5.6 acres. The project will be subject to the Commercial Neighborhood (CN) zoning district development standards, except as referenced herein. Maximum impervious surface for the commercial neighborhood portion of the project shall be 75% and said area shall be developed in accordance with the concept plan and building elevation on Sheet 2 of the Certified Site Plan.
 - 1.3. The project shall be permitted a maximum of 10,890 square feet of Business Professional Office (BP-O) zoning district uses on 1.0 acres. The project will be subject to the Business Professional Office zoning district development standards, except as referenced herein. The maximum height shall be 40 feet. Building(s) shall be architecturally finished on all sides and all development shall be residential in appearance including a pitched roof.
 - 1.4. The project shall be permitted a maximum 68,880 square foot hotel containing a maximum of 125 rooms on 3.0 acres. The project will be subject to the Commercial General (CG) zoning district development standards, except as referenced herein. Maximum building height shall be 74 feet (6 stories).
 - 1.5. Building envelopes shall be as generally located on the site plan. No residential dwelling buildings shall be placed parallel with the Alafia River frontage.
 - 1.6 Location of order boxes for the drive-through facility shall be a minimum of 120 feet from the Residential Multi-family property line internal to the mixed use project as shown on the General Site Plan. Additionally, a solid fence or wall shall not be required to be placed between the site used for a drive-through facility and the Multi-family property. Buffering and Screening requirements for the vehicular use areas shall be in accordance with LDC Section 6.06.00.
2. The project may be permitted a maximum of 351 multi-family dwelling units, a 68,880 square foot hotel, 60,707 square feet of Commercial Neighborhood and 10,890 square feet of Business Professional Office uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum density/intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum number of units and/or maximum square footage for the project as permitted herein exceeds the maximum density/intensity (FAR) permitted by the

APPLICATION: PRS 19-0778 RV/S

ZHM HEARING DATE: N/A

BOCC MEETING DATE: October 8, 2019

CASE REVIEWER: Israel Monsanto

Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the number of dwelling units and/or square footage allowed in the project shall decrease as necessary to conform to the Comprehensive Plan.

- ~~3. The Developer shall install a traffic signal, when warranted and approved by FDOT & Hillsborough County, at the intersection of Gibsonton Dr. @ the project's main entrance Fernhill Drive. The Public Works Department has conducted a signal warrant analysis and has determined that a signal is warranted.~~
34. Cross access shall be provided between all outparcels and the major internal roadway. Direct access from any outparcel onto Gibsonton Drive shall be prohibited.
45. The applicant shall show the ability to provide cross access to the western property as shown on the site plan. If any of the adjacent properties are developed by the same developer/owner, then cross access must be provided.
56. A temporary access easement shall remain over vacated portions of Ida Street until such time as a permanent easement is approved by Hillsborough County. A permanent paved easement approved by Hillsborough County shall provide access from the site to the western portion of the ELAPP preserve.
67. The following conditions are required by the Florida Department of Transportation (FDOT) access improvements to Gibsonton Drive:
 - ~~7.1 The developer shall modify the median opening at Fern Hills Drive to allow east and west inbound left turns only.~~
 - ~~7.2 The next median opening to the east (second from I-75) shall be closed.~~
 - ~~7.3 The next median opening to the east (third from I-75) shall remain as a full opening and have left turn lanes in both directions installed. These turn lanes shall be FDOT Index 301 requirements.~~
 - ~~7.4 The next median opening to the east (fourth from I-75) shall be closed.~~
 - ~~7.5 The driveway aligning with Fern Hill Drive shall be reconfigured to serve as a right in/out and left in. This driveway shall be 3 lanes. There shall be no crossing movements on this driveway depth (throat) for a minimum of 150 feet from Gibsonton Drive. This can be accomplished by installation of a traffic separator.~~
 - ~~7.6 Affected property owners must be contacted as part of this permit process. The developer shall provide names and addresses of any property owners these modifications will affect.~~
 - ~~7.7 FDOT will not allow a traffic signal at Fern Hills Drive. This median opening is too close to the ramp terminus of I-75. A signal at this location has the potential to back traffic onto I-75.~~

APPLICATION: PRS 19-0778 RV/S

ZHM HEARING DATE: N/A

BOCC MEETING DATE: October 8, 2019

CASE REVIEWER: Israel Monsanto

- 6.1 The Gibsonton Drive and Fern Hill Drive is currently signalized. The applicant shall be required to lengthen the existing eastbound left turn lane. The total distance is subject to FDOT approval.
- 6.2 The project driveway at Gibsonton Drive Alafia Preserve Avenue will be modified to left-in/right-in/right-out.
- 6.3 The site currently has access to the FDOT frontage road that runs along Gibsonton Drive. FDOT has indicated that the frontage road access may be restricted/modified in the future.
7. Gibsonton Drive and Fern Hill Drive has been signalized. As a result, the connection from Old Gibsonton Drive to Fern Hill Drive due to its proximity to the intersection will be closed in the future. Old Gibsonton Drive currently provides primary access to parcels located west of the subject property referred to as Folio Numbers 076326-0000 and 076327-0000, which are jointly zoned as Planned Development 99-0735 (the "Adjacent Properties"). To accommodate future access from the Adjacent Properties to Gibsonton Drive via a public right-of-way, the Developer shall make the following dedications:
- 7.1 Upon completion of the Fern Hill Drive Extension to Alafia Preserve Avenue, Developer shall dedicate Fern Hill Drive and Alafia Preserve Avenue to Hillsborough County, as such roadways have been designed, approved, and constructed, together with necessary drainage easements. The dedication of Fern Hill Drive extension and Alafia Preserve Avenue shall occur within 90 days of the completion of the Fern Hill Drive Extension. The Developer shall be allowed future driveway connections to such roadways consistent with Hillsborough County Access Management regulations; and
- 7.2 To provide the alternative connection for Old Gibsonton Dr. to the Fern Hill Drive Extensions via Alafia Preserve Avenue, the Developer shall dedicate a fifty-four (54) foot right-of-way within the parcel labeled "Last Parcel to Develop" as depicted on the Site Plan, to Hillsborough County connecting Alafia Preserve Avenue to Old Gibsonton Drive.

Dedication shall occur upon the earlier of the following:

- (a) prior to final approval of the Construction Plan of the "Last Parcel to Develop" as depicted on the Site Plan, or
- (b) within ninety (90) days from Developer's receipt of written notice from Hillsborough County that a site construction plan application has been submitted to Hillsborough County requesting approval to develop either of the Adjacent Properties.

The dedicated right-of-way shall be located within the area labeled "Proposed Location of Future ROW" on the Site Plan, at the discretion of Developer consistent with applicable transportation technical standards. Developer shall have no obligation to construct any roadway improvements within the dedicated right-of-way but shall be allowed driveway connections consistent with Hillsborough County Access Management regulations.

8. The developer will provide a bus bay, transit accessory pad, shelter for existing transit stop on Gibsonton Drive. The location of these improvements shall be subject to review and approval by Hillsborough Area Regional Transit Authority.
9. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
10. Prior to submittal of the preliminary plan for the Site Development Review process, the significant mature trees on the properties shall be identified and the developer shall consult with the Natural Resources Team for design input that would minimize the impact and removal of the significant mature trees. The locations of the significant mature trees and the design efforts to avoid the removal of these trees (i.e., building locations, vehicular use areas, and stormwater pond adjustments) are to be displayed on the submitted preliminary plan.
11. A compatibility plan, as required by the County's Upland Wildlife Habitat Ordinance, addressing issues related to how the development will ensure compatibility with the Alafia Scrub Nature Preserve, such as access, prescribed fire, and landscaping shall be submitted prior to Construction Site Development Review approval.
12. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.

The following issues must be addressed on the site plan prior to General Site Plan Certification:

- ~~1. Page 1 site plan data shall be revised to correspond with the conditions and additional information submitted on May 20, 2008.~~
- ~~2. Page 2 of the PD that includes the building elevations shall be submitted for certification, the notes shall be updated to correspond to the conditions and the additional information submitted on May 20, 2008.~~
13. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
14. Effective as of February 1, 1990, this development order/permit shall meet the concurrency requirements of Florida Statutes Chapter 163, Part II. Approval of this development order/permit does not constitute a guarantee that there will be public facilities at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits. Within 90 days of approval by the Hillsborough County Board of County Commissioners, the applicant shall submit to the Development Services Department a revised General

APPLICATION: PRS 19-0778 RV/S

ZHM HEARING DATE: N/A

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CASE REVIEWER: Israel Monsanto

Development Plan for certification which conforms the notes and graphic of the plan to the conditions outlined above and the Land Development Code (LDC). Subsequent to certification of the plan, if it is determined the certified plan does not accurately reflect the conditions of approval or requirements of the LDC, said plan will be deemed invalid and certification of the revised plan will be required.

Staff's Recommendation: Supportable, Subject to Conditions

Zoning
Administrator



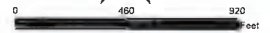
Brian Grady

Sign-off: Fri Sep 20 2019 14:42:18

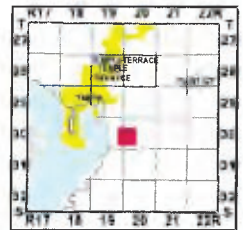
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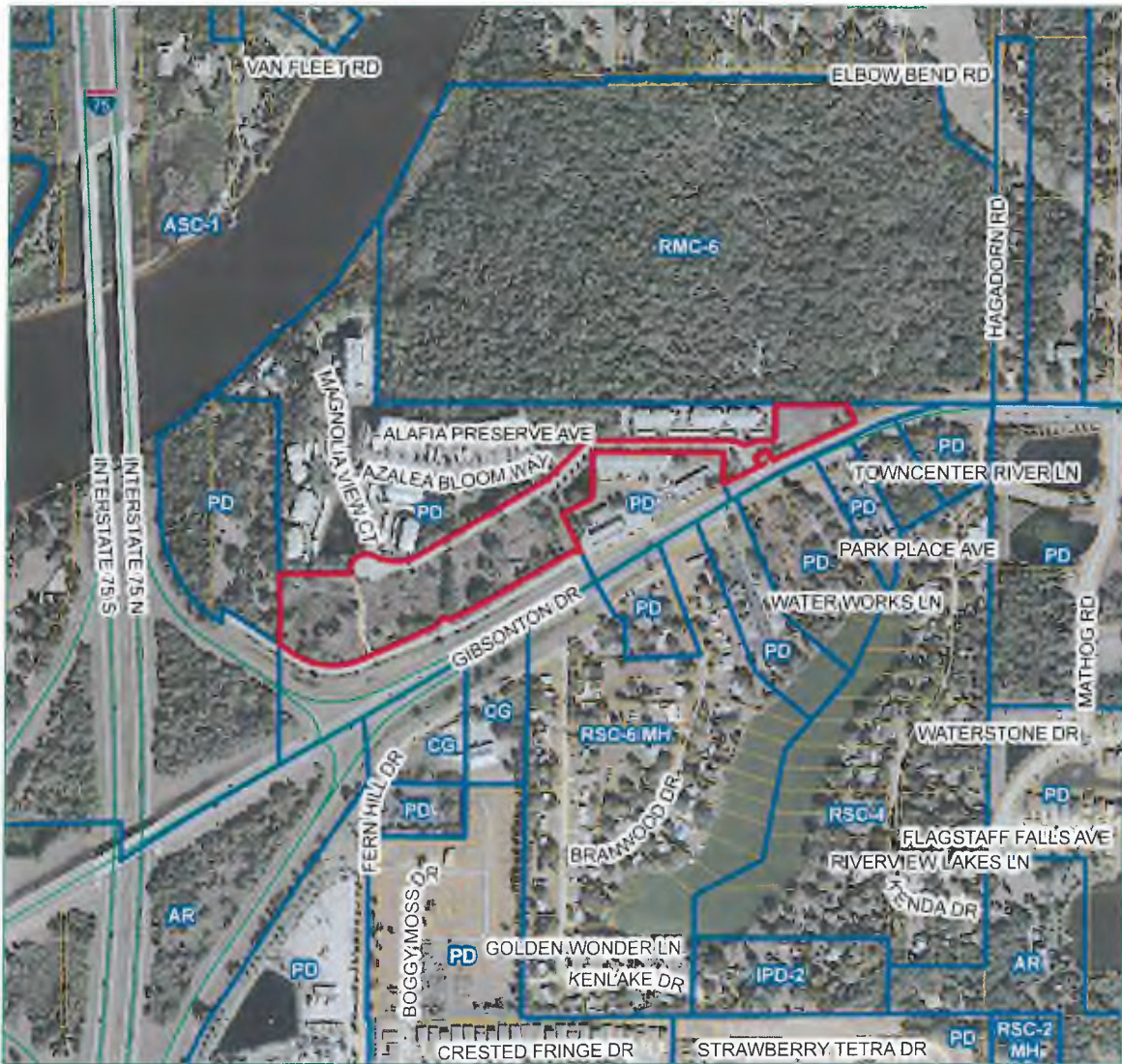
- Application Site
- Zoning Boundary
- Parcels



STR: 19-30-20



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I-75 (375) r/w- 120' pavement arterial road

150' STUDY AREA

AREA OF MAJOR MODIFICATION REQUEST



Location Map

Flow capabilities (major modification areas not illustrated)

SMU 12 Rev	78.2 ac (11.9 ac existing)	0
SMU 6	24.4 ac (8.0 ac existing)	19.50
Total available		19.50

ELAPP Alafia Scrub Preserve

RZ 06-0848 RV

KENNI SUBDIVISION
1st addition
PB 49 PG 83

KENLAKE SUBDIVISION
PB 46 PG 50

PROJECT DATA FOR TOTAL PD	
OWNER/APPLICANT	ALAFIA RIVER PRIORITY GROUP (LLP) 411 WINDWARD OUTLEAVE OSPREY, FL 34229
FINANCER	
ADVISORY	REVISED & 100% MARK MARK 71.7
TOTAL AC.	30.96 AC
ZONING	PD
FUTURE LAND USE	CMU 12, SMU 6
PROPOSED USES	MULTIFAMILY UNITS (118 UNITS) 300 SF AUXILIARY RECREATION USES 30.96 AC
APPROXIMATE COVERAGE	60.00 %
OFFICE	100,000 S.F. (25-FAR) TAC
INDUS.	175,000 S.F. 60,000 S.F. TAC
UTILITIES	PUBLIC WATER AND SEWER

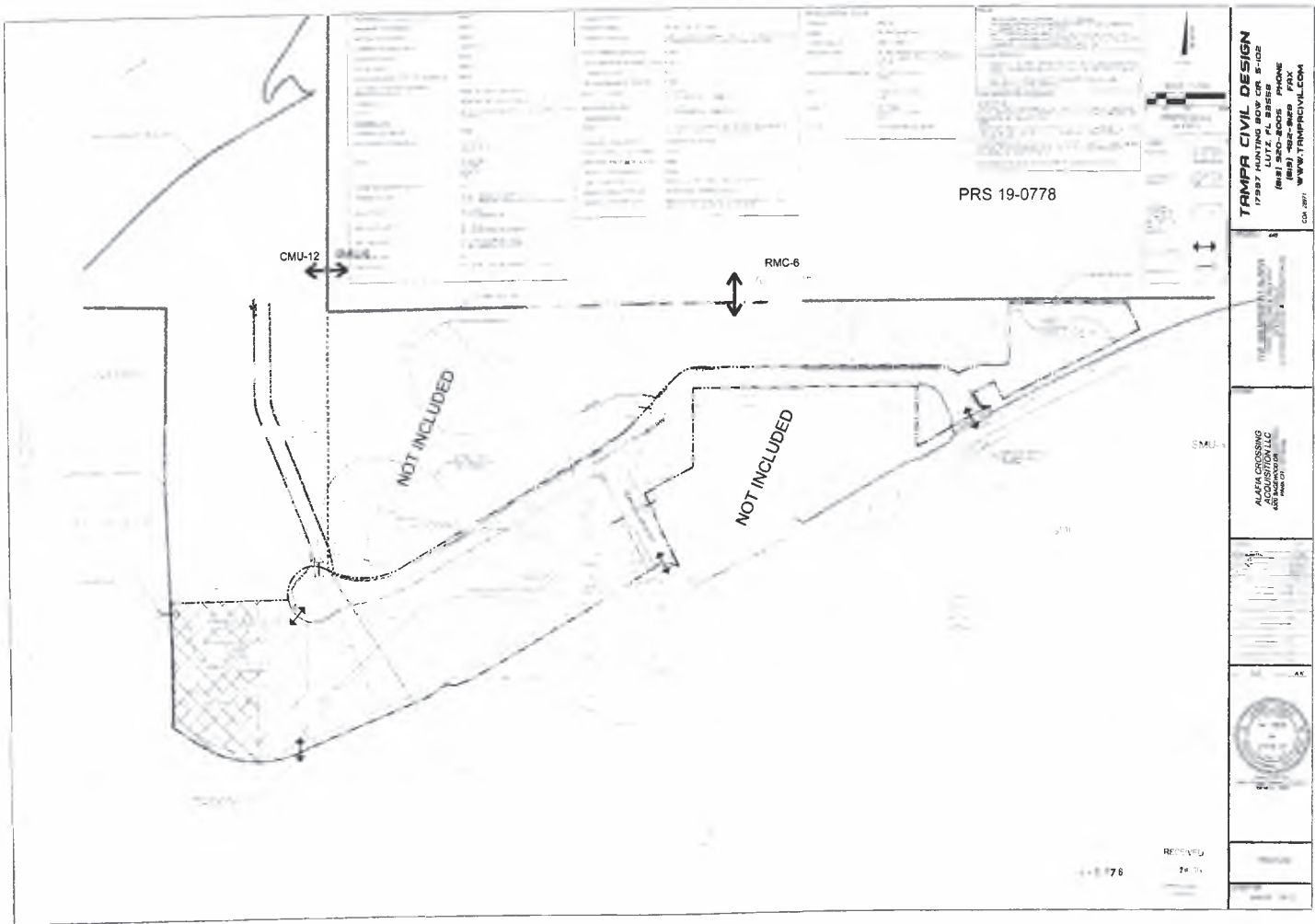
LEGEND	
SMU-6	FUTURE LAND USE
PD	ZONING
REVISION	FIELD MARK
EXISTING SIDE WALK	EXISTING SIDE WALK
EXISTING STRUCTURES	EXISTING ON SITE STRUCTURES TO BE REMOVED
APPROXIMATE LAND LIMITS	APPROXIMATE LAND LIMITS
TEMPORARY EAPPLACEMENT TO BE RELOCATED WITH FUTURE PERMANENT LAND MARK	TEMPORARY EAPPLACEMENT TO BE RELOCATED WITH FUTURE PERMANENT LAND MARK
EXISTING ACCESS	EXISTING ACCESS
FUTURE LAND ACQUISITION	FUTURE LAND ACQUISITION
REMARKS TO BE BUILT TO COUNTY STANDARDS	REMARKS TO BE BUILT TO COUNTY STANDARDS

REVISED: APRIL 16, 2008

RECEIVED

Wilson Miller

GIBSONTON DRIVE PARCEL PD
CERTIFIED GENERAL DEVELOPMENT PLAN
MAJOR MODIFICATION #MM 07-1378



PRS 19-0778

TAMPA CIVIL DESIGN
 17227 HUNTING BOW CR S-102
 LUTZ, FL 33558
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 WWW.TRANCIVIL.COM

ALABAMA CROSSING
 SOLUTIONS LLC
 10000 W. ALABAMA
 MOBILE, AL 36688

ALABAMA CROSSING
 SOLUTIONS LLC
 10000 W. ALABAMA
 MOBILE, AL 36688



REVISIONS
 19-0778

FINAL CONDITIONS OF APPROVAL
 PROJECT: PRS 19-0778 RV (RD 06-0648)
 DEVELOPER: [Redacted]
 APPROVED BY: [Signature]
 CHAIRMAN BOARD OF COMMISSIONERS
 CLERK DEPUTY CLERK: PATRICIA [Redacted]
 CLERK OF THE CIRCUIT COURT: [Redacted]

FINAL CONDITIONS OF APPROVAL
 PROJECT: PRS 19-0778 RV (RD 06-0648)
 DEVELOPER: [Redacted]

- The minimum development shall be located as follows:
 - The project shall be permitted a maximum of [Redacted] and shall be subject to the Residential Sub-development Ordinance except as referenced Minimum Front Yard Setback Minimum Setback Minimum height The 21 additional setback for the corner units 70 feet in height shall not apply to the western and northern perimeter boundaries. [Redacted] shall be permitted a maximum of 60,703 square feet of building area [Redacted] of the corner units. The project will be subject to the Minimum Setback Ordinance except as referenced [Redacted] area shall be developed in accordance with the [Redacted] Site Plan
 - The project shall be permitted a maximum of 10,000 square feet of business professional office (BPO) zoning district. The project will be subject to the Business Professional Office (BPO) zoning district except as referenced herein [Redacted] shall be permitted a maximum of 1.5 acres of (C-1) zoning district [Redacted] shall be 74 feet or more.
- Building envelopes shall be as generally located on the site plan. No residential dwellings buildings shall be placed parallel with the Alafia River Frontage.
 - Location of order lines for the drive-through facility. Residential Multi-family property line external to the site. Additionally, a well (cove) or well shall be used for a drive-through facility and for such lots located for the vehicle service area shall be [Redacted]

The project shall be permitted a maximum of 213 multi-family units of 703 square feet of commercial neighborhood and 10,000 square feet of residential [Redacted] of on-site construction area.

FINAL CONDITIONS OF APPROVAL
 PROJECT: PRS 19-0778 RV (RD 06-0648)
 DEVELOPER: [Redacted]

The applicant shall provide [Redacted] with the Environmental Impact Statement and all other information required by the Commission.

The applicant shall provide [Redacted] with the following information:

- The applicant shall show the ability to provide access to the site from the adjacent properties to be developed.
- Access to the site shall be provided from the adjacent properties to be developed.
- The following conditions are required access improvement to the site:
 - The site access to the site shall be provided from the adjacent properties to be developed.
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FINAL CONDITIONS OF APPROVAL
 PROJECT: PRS 19-0778 RV (RD 06-0648)
 DEVELOPER: [Redacted]

The applicant shall provide [Redacted] with the following information:

- The applicant shall show the ability to provide access to the site from the adjacent properties to be developed.
- Access to the site shall be provided from the adjacent properties to be developed.
- The following conditions are required access improvement to the site:
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FINAL CONDITIONS OF APPROVAL
 PROJECT: PRS 19-0778 RV (RD 06-0648)
 DEVELOPER: [Redacted]

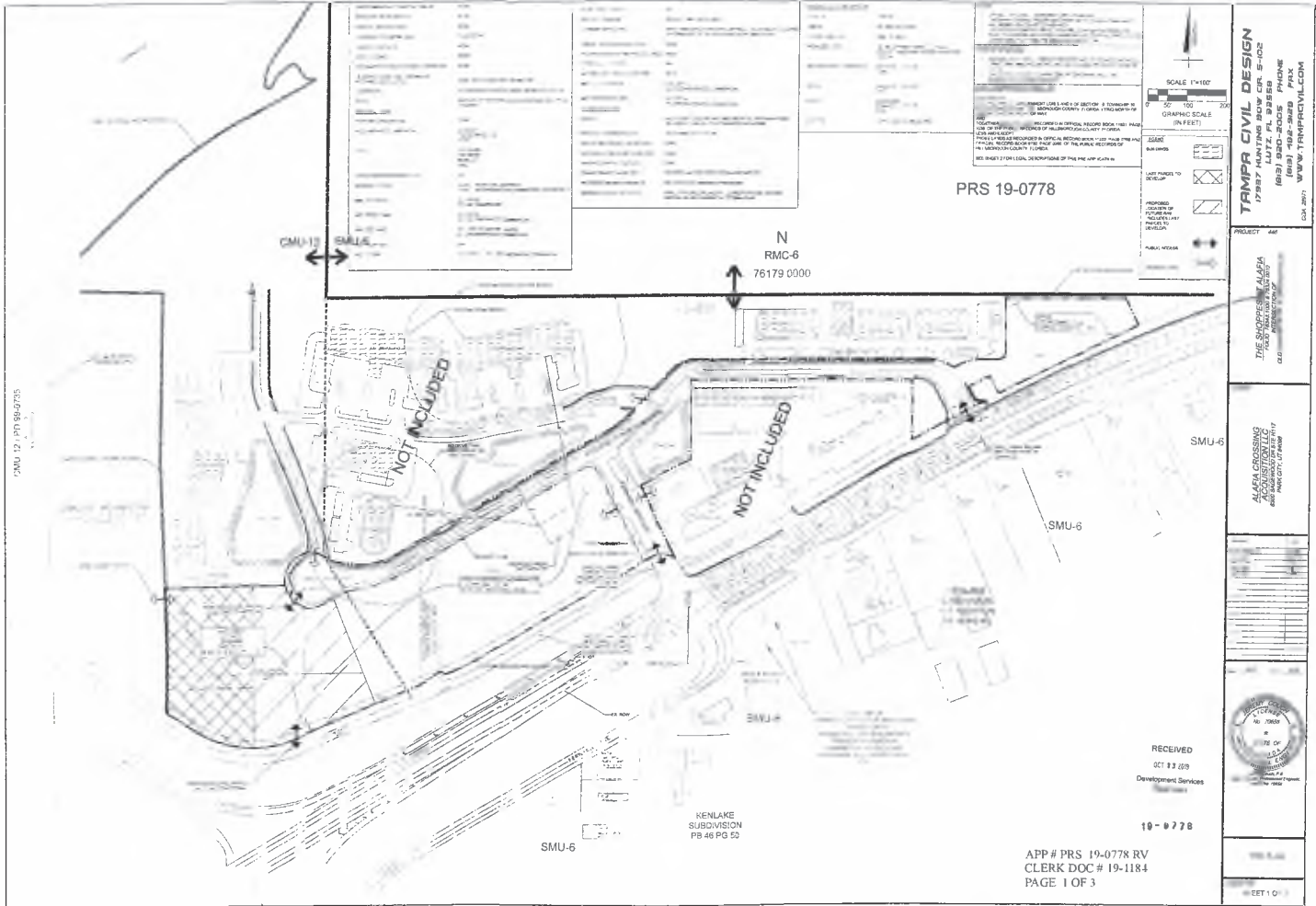
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FINAL CONDITIONS OF APPROVAL
 PROJECT: PRS 19-0778 RV (RD 06-0648)
 DEVELOPER: [Redacted]

The applicant shall provide [Redacted] with the following information:

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CMU 12 - PT 99-0735

CMU-12 SMU-6

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NOT INCLUDED

NOT INCLUDED

SMU-6

SMU-6

SMU-6

KENLAKE
SUBDIVISION
PB 46 PG 52

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OCT 13 2019
Development Services

19-0778

APP # PRS 19-0778 RV
CLERK DOC # 19-1184
PAGE 1 OF 3

TAMPA CIVIL DESIGN
17887 HUNTING BOW CIR S-402
LUTZ, FL 33558
PH: (813) 830-2005
CELL: (813) 830-2005
WWW.TAMPACIVIL.COM

PROJECT # 448

THE SHOPPING ALAPPA
LOCAL GOVERNMENT

ALAPPA CROSSING
LOCAL GOVERNMENT

STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION
FLORIDA TURNPIKE AUTHORITY

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19-0778

APP # PRS 19-0778 RV
CLERK DOC # 19-1184
PAGE 1 OF 3

ET 1

PRS 19-0778

SCALE 1"=100'
GRAPHIC SCALE
(IN FEET)

GRAPHIC SCALE
(IN FEET)

LEGEND
PROPERTY TO BE DEVELOPED
EXISTING IMPROVEMENTS
EXISTING UTILITIES
EXISTING EASEMENTS
EXISTING RIGHT-OF-WAY

PROPERTY OWNER'S REVIEW AND SIGNATURE IS REQUIRED TO VERIFY THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN. THE PROPERTY OWNER'S REVIEW AND SIGNATURE IS REQUIRED TO VERIFY THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN. THE PROPERTY OWNER'S REVIEW AND SIGNATURE IS REQUIRED TO VERIFY THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN.



**PARTY OF
RECORD**

NONE