



## 1.0 APPLICATION SUMMARY

Applicant: RD Management, LLC

FLU Category: ICMU 35

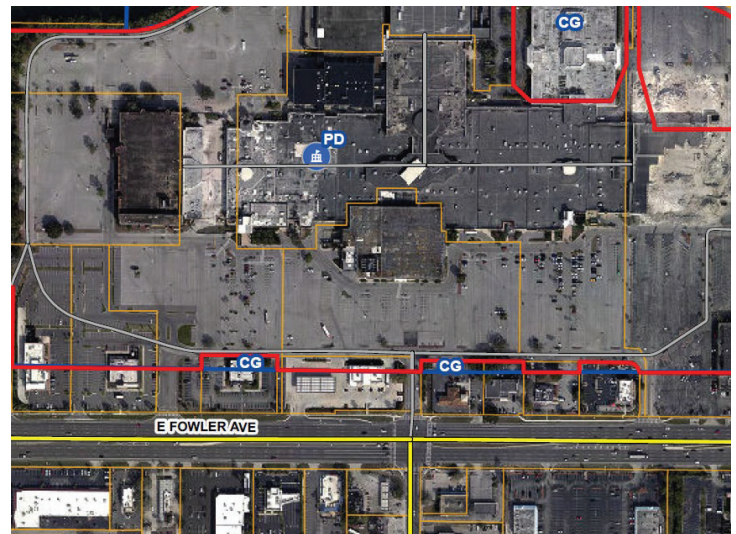
Service Area: Urban

Site Acreage: 28.5

Community Plan Area: University

Overlay: None

Request: Modify Street Type typical sections;  
Add a new Street Type;  
Change block boundaries



### Existing Approvals:

- Commercial General (CG), Residential and Office uses, Urban Mixed Use project with up to 0.5 FAR, or 2.0 FAR, 35 dwelling units per acre and allows use of FAR in lieu of density for residential projects at the developer's option.
- Total of 12 internal Street typologies accommodating vehicular and pedestrian traffic.
- 15 Development Blocks.
- Building Design and Block development standards per Street Type.

### Proposed Modification(s) Summary:

- Additional Street Type: Modify *Type B* street for a planned retail area to *Shopping Center Internal Drive*;
- Reconfigure the boundaries of Blocks 10 and 14;
- Reconfigure the Central District boundary to reflect changes to Block 10;
- Changes to the Type D Street typical section along the east and west boundaries of Block 14;
- Reconfigure and allow existing surface parking lot on Block 14 to serve future retail uses in Block 10;
- Provide a re-development timing mechanism that requires modifications of roadways to conform to current approvals

### Additional Information:

PD Variations	LDC Sec. 6.06.04 Vehicular Use Area buffers
Waiver(s) to the Land Development Code	None
Planning Commission Recommendation	Inconsistent
Development Services Department Recommendation	Supportable, with Conditions

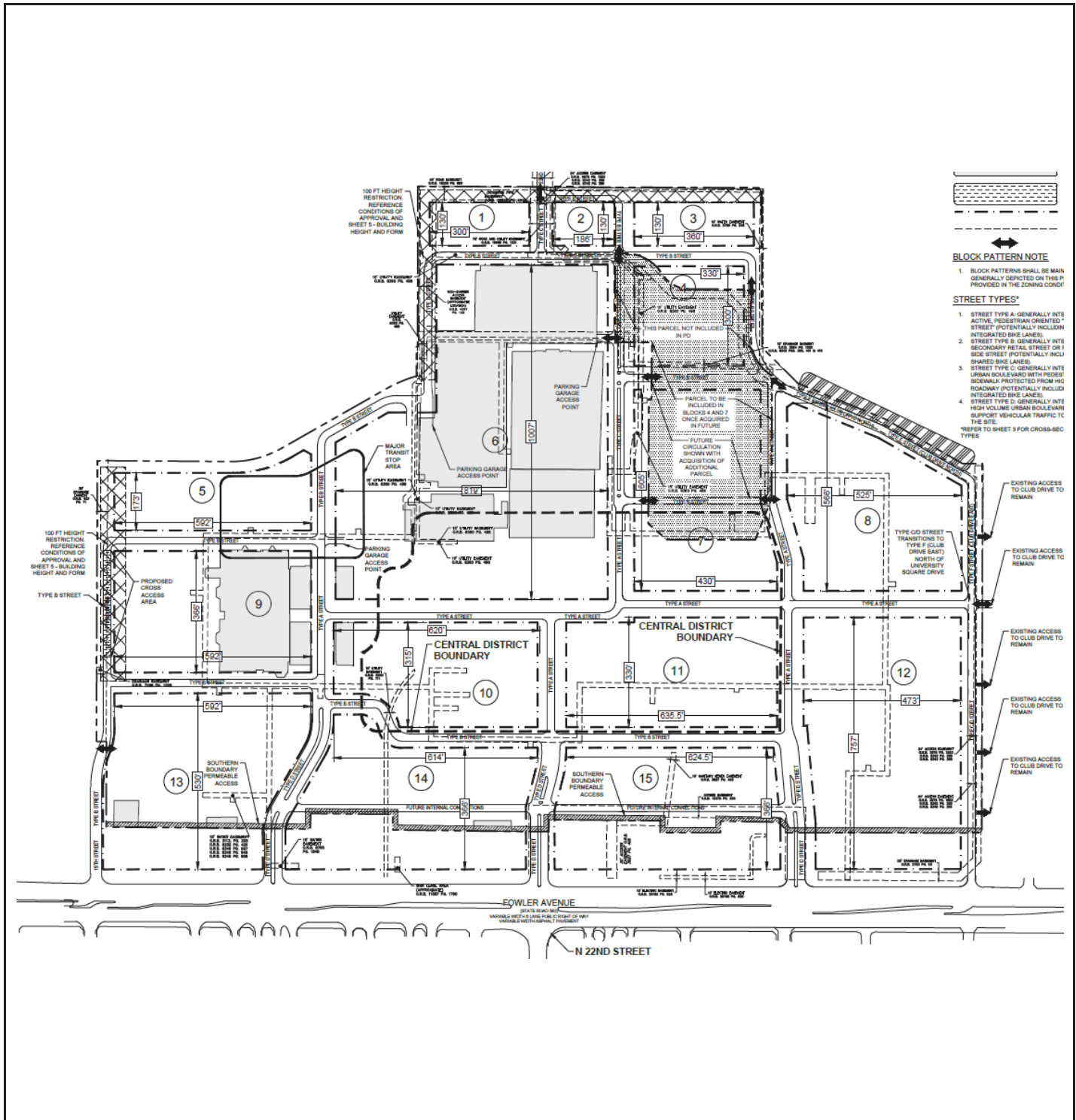






## 2.0 LAND USE MAP SET AND SUMMARY DATA

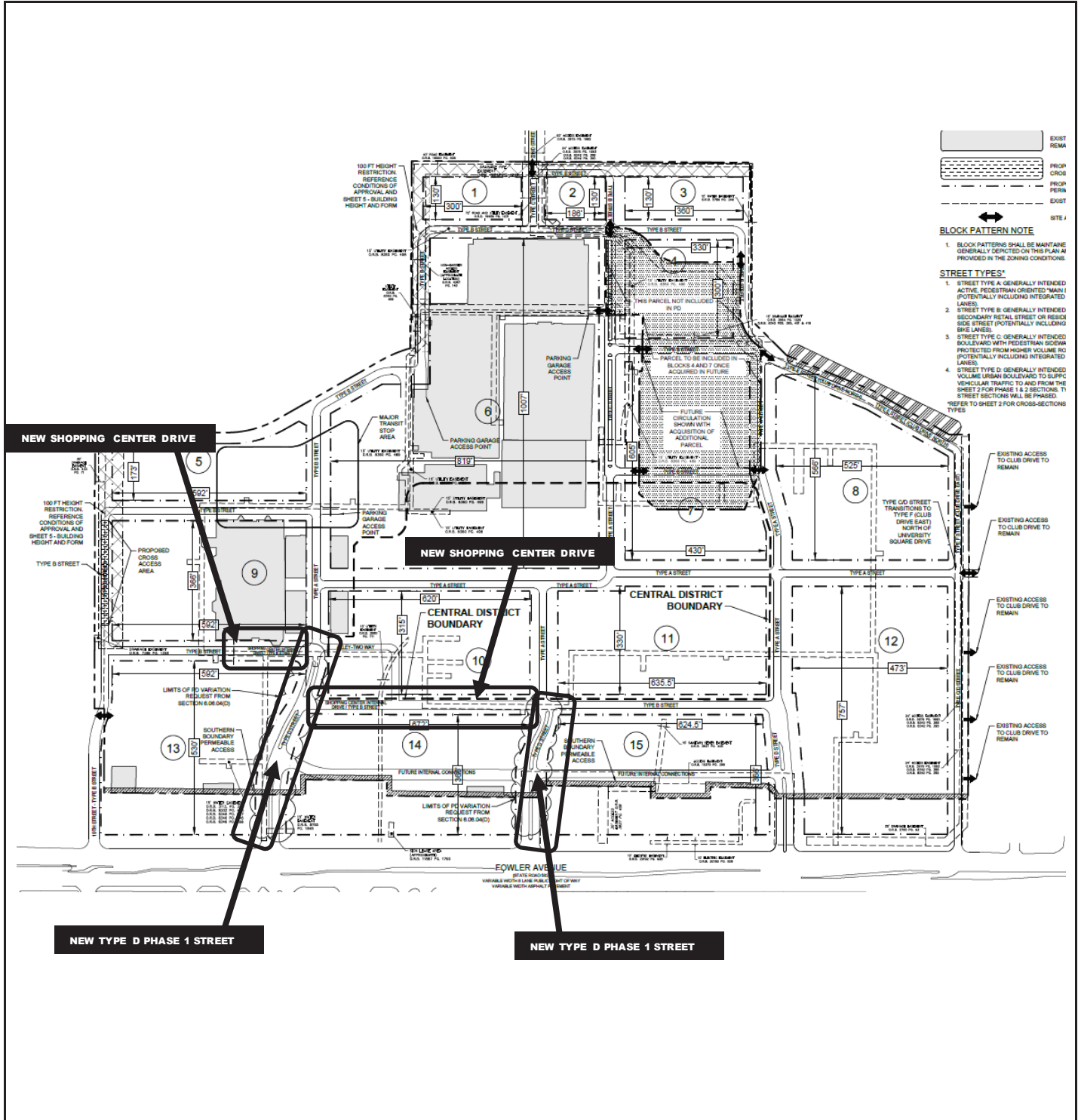
### 2.1 Approved Site Plan – General PD Plan (partial provided below for size and orientation purposes. See Section 8.1 for full site plan)







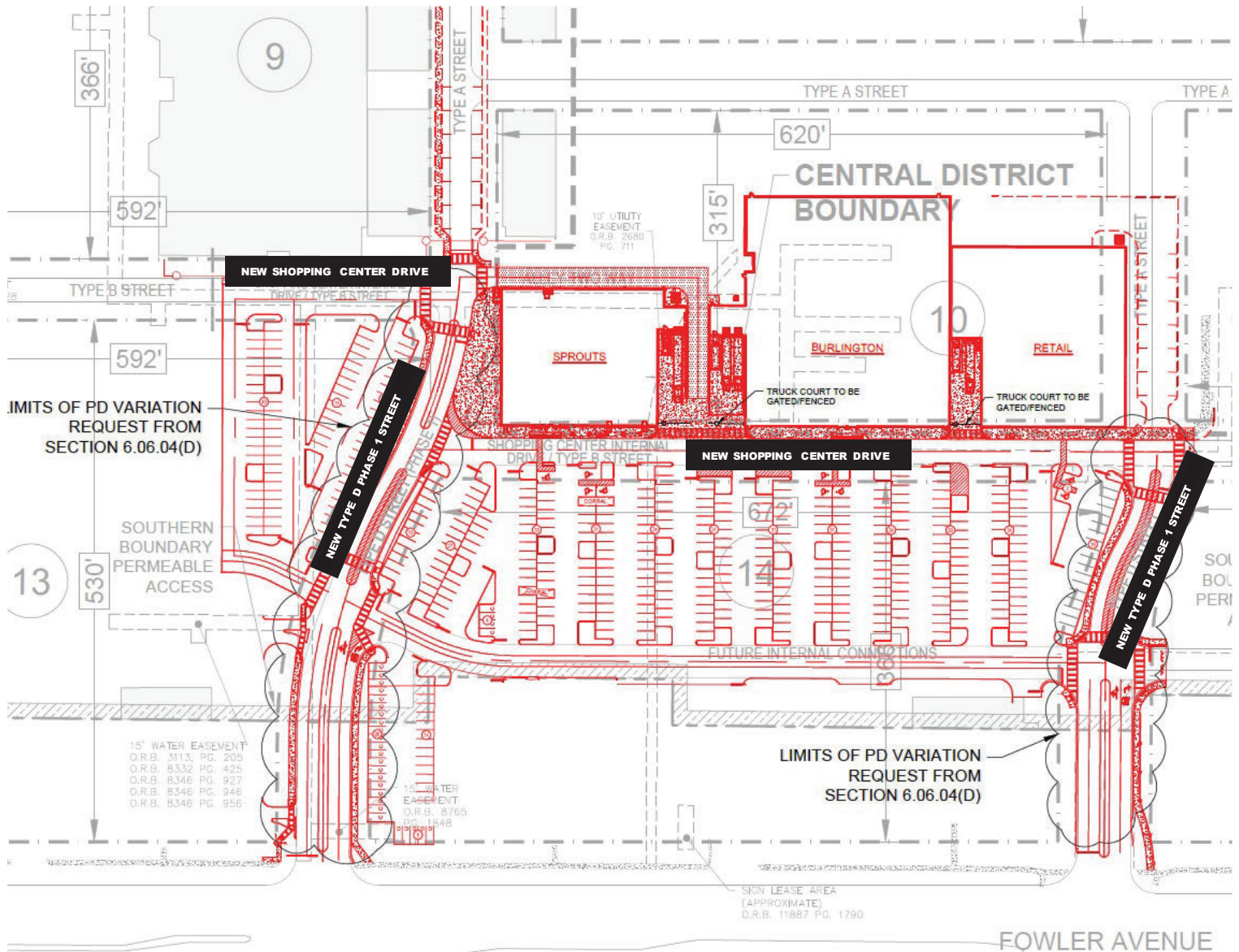
## 2.3 Proposed Site Plan – General Site Plan – (partial provided below for size and orientation purposes. See Section 8.2 for full site plan)





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**2.5 Proposed Site Plan – Future Retail Area** (partial provided below for size and orientation purposes. See Section 8.2 for full site plan)





**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)****Adjoining Roadways (check if applicable)**

Road Name	Classification	Current Conditions	Select Future Improvements
Fowler Ave.	FDOT Principal Arterial - Urban	8 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
22 <sup>nd</sup> St.	County Collector – Urban (North of Project) and City of Tampa Collector (South of Fowler Ave.)	2 Lanes <input checked="" type="checkbox"/> Substandard Road (County Portion) <input checked="" type="checkbox"/> Sufficient ROW Width (County Portion)	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
19 <sup>th</sup> St.	County Local – Urban (Also Partially within City of Tampa)	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other
University Square Dr.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Club Dr.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other

**Project Trip Generation** ☒ Not applicable for this request

	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing			
Proposed			
Difference (+/-)			

\*Trips reported are based on net new external trips unless otherwise noted.

**Connectivity and Cross Access** ☒ Not applicable for this request

Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		Choose an item.	Choose an item.	Choose an item.
South		Choose an item.	Choose an item.	Choose an item.
East		Choose an item.	Choose an item.	Choose an item.
West		Choose an item.	Choose an item.	Choose an item.
Notes:				

**Design Exception/Administrative Variance** ☒ Not applicable for this request

Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

**4.0 Additional Site Information & Agency Comments Summary**

Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested	<input type="checkbox"/> Yes <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes	
<input type="checkbox"/> Off-Site Improvements Provided	<input checked="" type="checkbox"/> No	<input type="checkbox"/> No	

## 4.0 ADDITIONAL SITE INFORMATION &amp; AGENCY COMMENTS SUMMARY

INFORMATION/REVIEWING AGENCY				
Environmental:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Environmental Protection Commission	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Natural Resources	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
Check if Applicable: <input type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area <input type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Other _____ Volume Sensitive				
Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
<b>Transportation</b> <input type="checkbox"/> Design Exc./Adm. Variance Requested <input type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
<b>Service Area/ Water &amp; Wastewater</b> <input type="checkbox"/> Urban <input checked="" type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
<b>Hillsborough County School Board</b> Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
<b>Impact/Mobility Fees</b>				
Comprehensive Plan:	Comments Received	Findings	Conditions Requested	Additional Information/Comments
<b>Planning Commission</b> <input type="checkbox"/> Meets Locational Criteria <input checked="" type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input checked="" type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Inconsistent <input type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	



## 5.0 IMPLEMENTATION RECOMMENDATIONS

### 5.1 Compatibility

The surroundings consist today of a mix of commercial, multi family, office and institutional uses.

#### The University Mall Project

PD 19-1443 is a mixed use district approved for a variety of commercial, office and residential uses. The site today generally consists of a shopping mall housing a variety of retail uses including a movie theater. The Mall is undergoing redevelopment into a new mixed use development. The PD implements the Innovation Corridor Mixed Use (ICMU-35) Future Land Use category.

The Project consist of 15 blocks, as noted in the approved General Development Plan (Section 2.0). The PD district does NOT include the 6.6 acre parcel owned by Burlington Coat Factory and approximately 0.68 acres of outparcels along Fowler Avenue.

Access to the PD is from Fowler Avenue from the south, 19th Street from the west, 22nd Street from the north and Club drive from the east and southeast (existing public access easement provides for motorists to cross from the north to the south along Club Dr.). The site also has cross access to multiple sites to the east. The PD provides cross access to the west for future road expansions. Additional cross access is approved along the northeast, for a future pedestrian connection to the VA hospital.

The PD has multiple street types, in addition to alleys, as shown on sheet 2 of the PD plans. "A" Streets are generally active, pedestrian oriented "main streets" (potentially including integrated bike lanes). "B" Streets are as secondary retail street or residential side streets (potentially including shared bike lanes). "C" Streets are intended as urban boulevards with pedestrian sidewalk protected from higher volume roadway (potentially including integrated bike lanes) and "D" Streets are considered for high volume urban boulevards to support vehicular traffic to and from the site. Types E and F are located on the perimeter to the east, adjacent to open space and retail uses. "A" and "B" Streets cover the majority of the project, which are the streets that promote for a more walkable, pedestrian oriented environment. "A" and "B" street also restrict maximum building setbacks to maintain structures closer to the streets. This allows a proper interaction between the users of the public space and the buildings fronting the streets. Additionally, design standards require windows, entrances and architectural features to be provided along building frontages to enhance the pedestrian experience. Surface parking on "A" streets frontages are required to be placed behind the buildings and are allowed to be placed on the side or behind buildings on the other street type frontages. Sheets 1 and 2 of the PD Plans indicates the general block configuration and street typical sections. The layout provides a grid like interconnected roadway network with links to adjacent sites and roads around the project.

Open spaces consisting of square, plazas or other outdoor areas are permitted throughout the project. A main square or civic space, located within the "Central District", at least 0.5 acres in size, is located central to the project. The square includes green space and place making elements and include hardscapes, seating, pavilion(s) or other amenities available for public use and enjoyment. Plazas or squares throughout the site are also contemplated to accommodate commercial kiosks within open space areas, subject to the typical street section requirements. Place making elements (specialty signage, lighting, landscaping, building architecture, and/or public art) as an enhancing feature of open spaces within the Central District are provided as well.

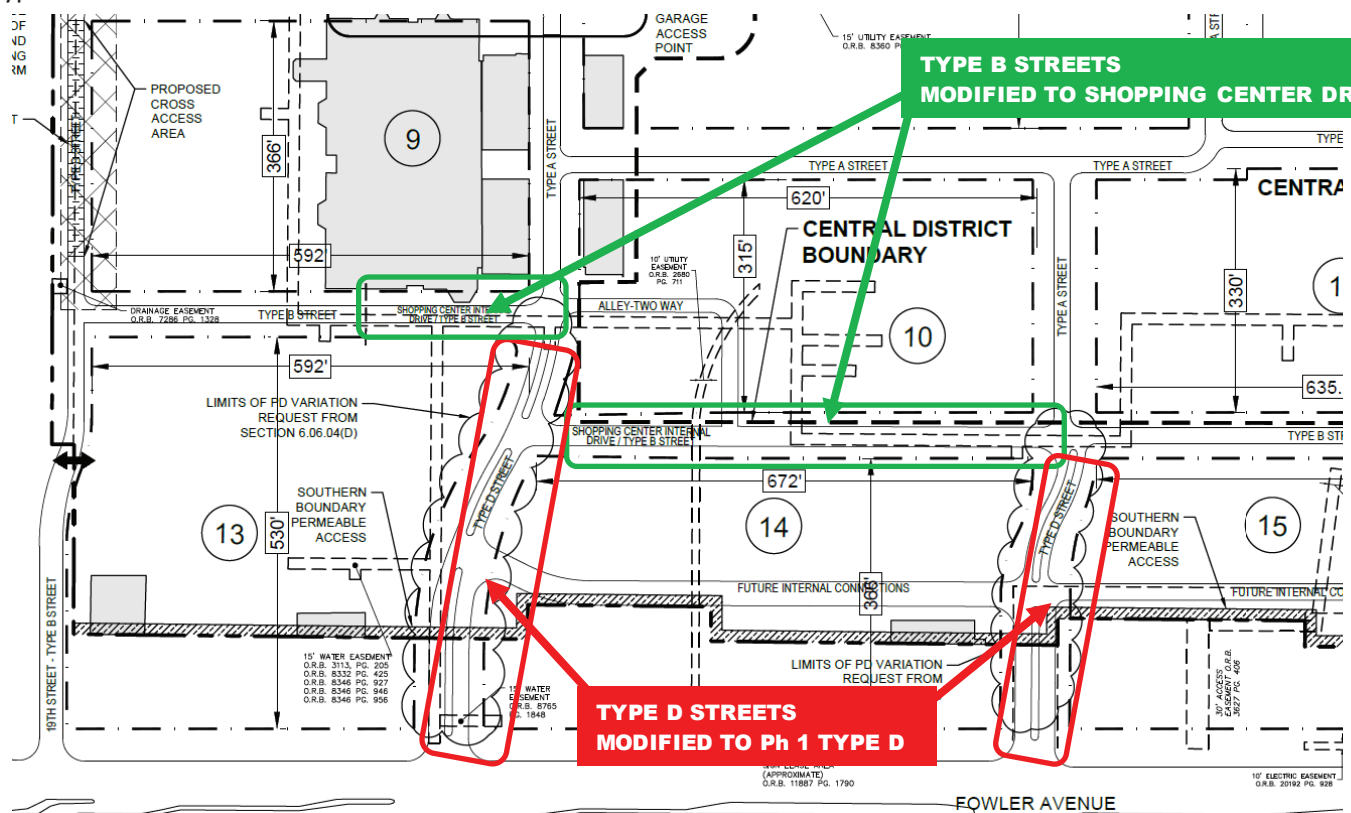
For this Minor Modification, the applicant argues states that, since the time of the original rezoning, the applicant /developer entered into a land swap agreement with the owners of the Burlington Property conditioned upon the Burlington store being relocated to its new location in the proposed new retail area within portions of Block 10 and 14 of the Project ("New Retail Area"). The existing Burlington store area was not included in the original rezoning for PD 19-1443 and is outside the PD limits. No increase of entitlements is being requested in this minor modification.

In the narrative, the applicant states that in order to redevelop the existing Burlington Property that is located in the heart of the area slated for more urban redevelopment in the Approved Zoning and the Urban Design Guidelines, it is imperative that Burlington relocate. Without the relocation of the Burlington store, the vision that the applicant and the County signed off on in the Approved Zoning could not be fully implemented. Therefore, the applicant states that some interim development that allows the Burlington store to move out of the current location must be permitted or the development will fail.

The applicant also affirms that in order to understand why the New Retail Area as proposed needs to be created, it is important to understand the business requirements of national retailers, such as Burlington. Co-tenancy and specific parking requirements are mandated by these retailers. In order to meet the Burlington store relocation requirements (for example: loading/parking/visibility), the New Retail Area has been created which will include a smaller Burlington store, a green grocer and a complimentary retail space facing the existing parking field between the old mall and the City of Tampa parcels along Fowler Avenue. According to the narrative, the Burlington store currently has extensive rights under existing reciprocal easement agreements which give it "veto power" over redevelopment of any areas that may affect the existing Burlington store operations.

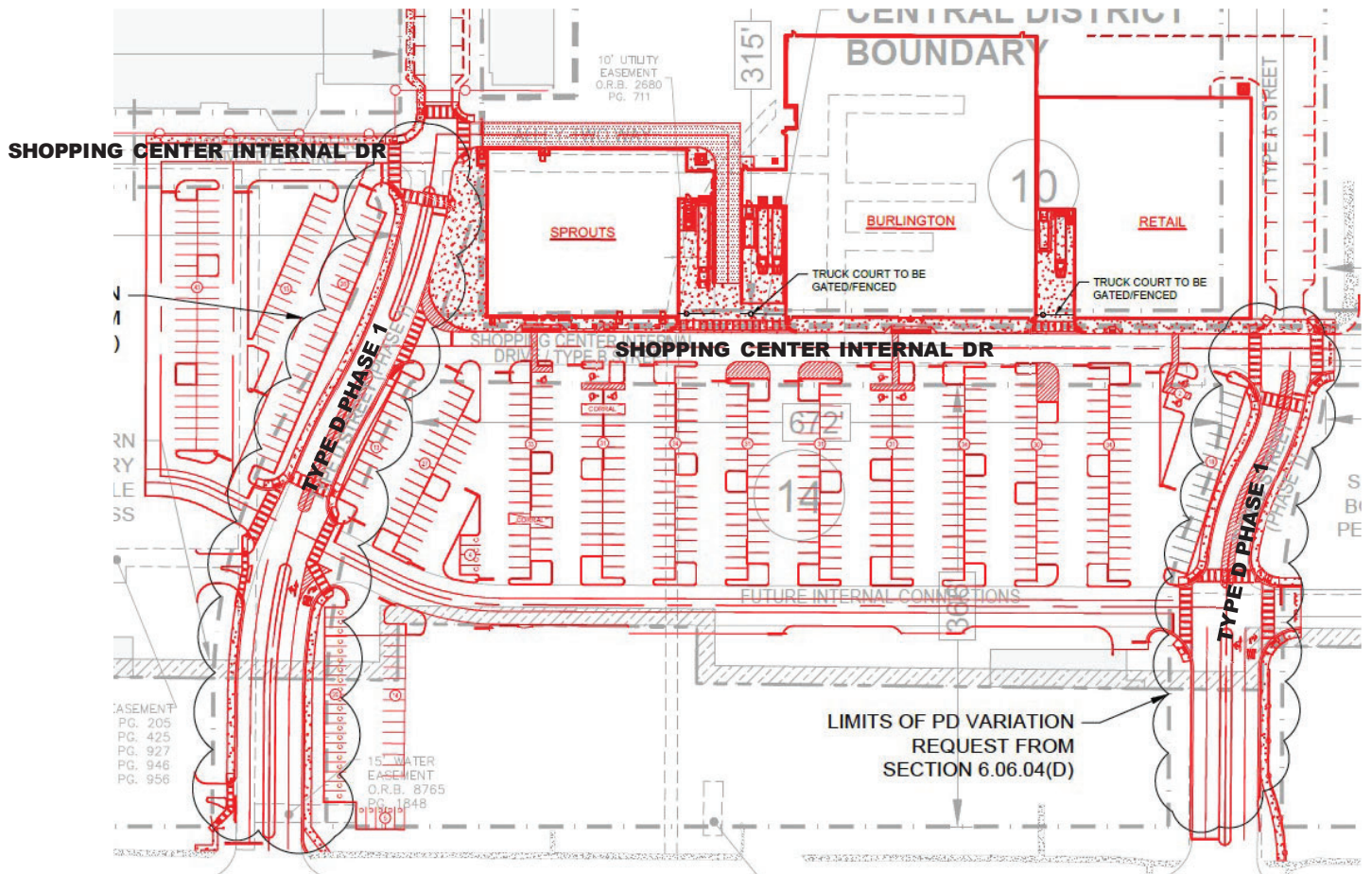
Additionally, the applicant asserts, the Master Plan that was a critical part of the Approved Zoning and always contemplated that a portion of Block 14 would remain surface parking in connection with the redevelopment. These parking areas may redevelop in the future once the rest of the Property is redeveloped. However, this was never contemplated by the applicant or by the Master Plan. Further, there is nothing prohibiting this type of development in the ICMU-35 comprehensive plan provisions. In fact, the ICMU-35 category provides that "retail commercial uses shall be clustered at arterial and collector intersections or integrated as part of a mixed use campus or mall, or combination thereof.

As a result of the request to relocate and accommodate the Burlington store and other retail space in the area shown in Sheet 6 of 6 of the submitted GSP, two types of streets are being re-designed. The streets immediately in front and west of the future retail space (between Blocks 10 and 14; and Blocks 9 and 13) are being modified from a B Type street to a "Shopping Center Drive". Additionally, two Type D streets (west and east of Block 14) are being modified by a "Phase 1 Type D street.





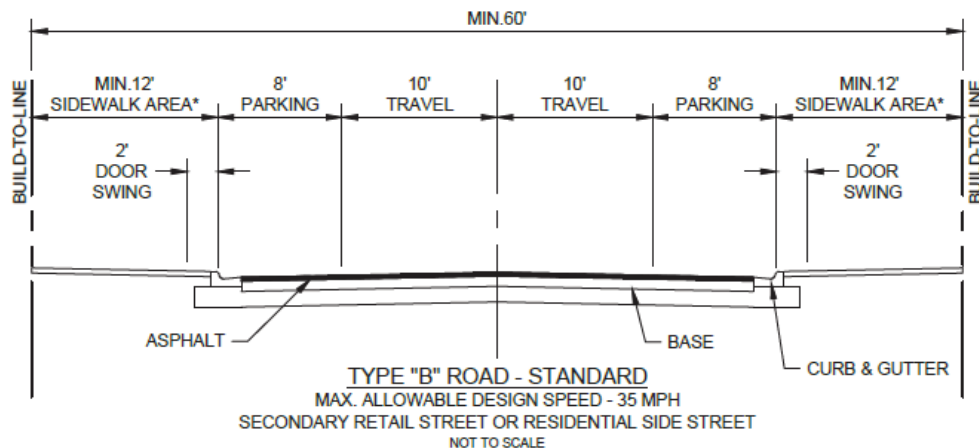
## Proposed "Retail Area"



## Proposed Shopping Center Drive:

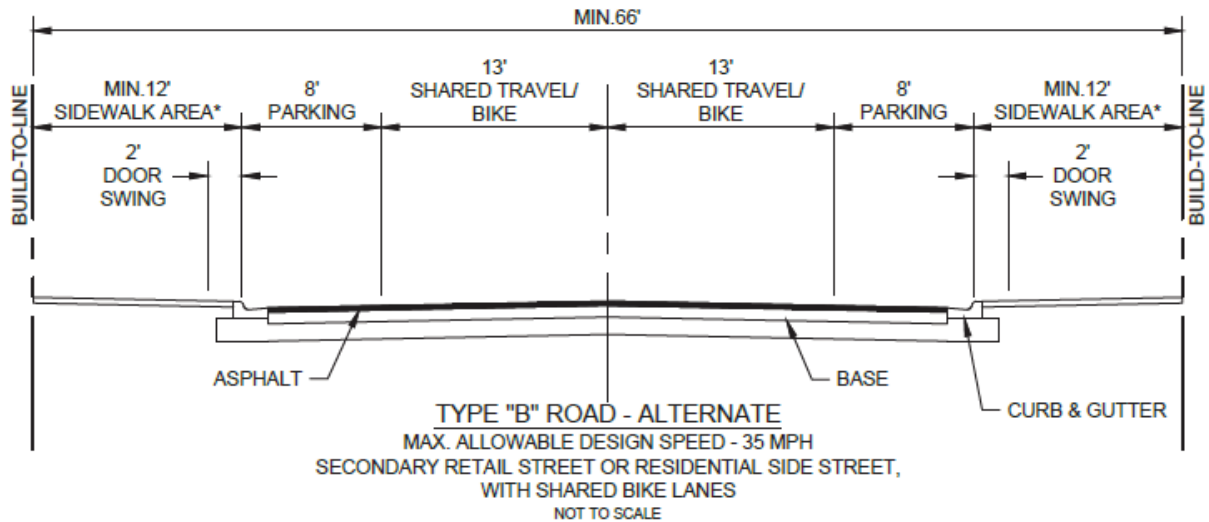
The PD GSP today requires the street between Blocks 10 and 14, as well as 9 and 13, to be designed as a Type B street. There are two Type B typical street sections approved today in the PD. "B" Streets are secondary retail streets or residential side streets (potentially including shared bike lanes).

## Currently approved Type B Streets typical sections (STANDARD):



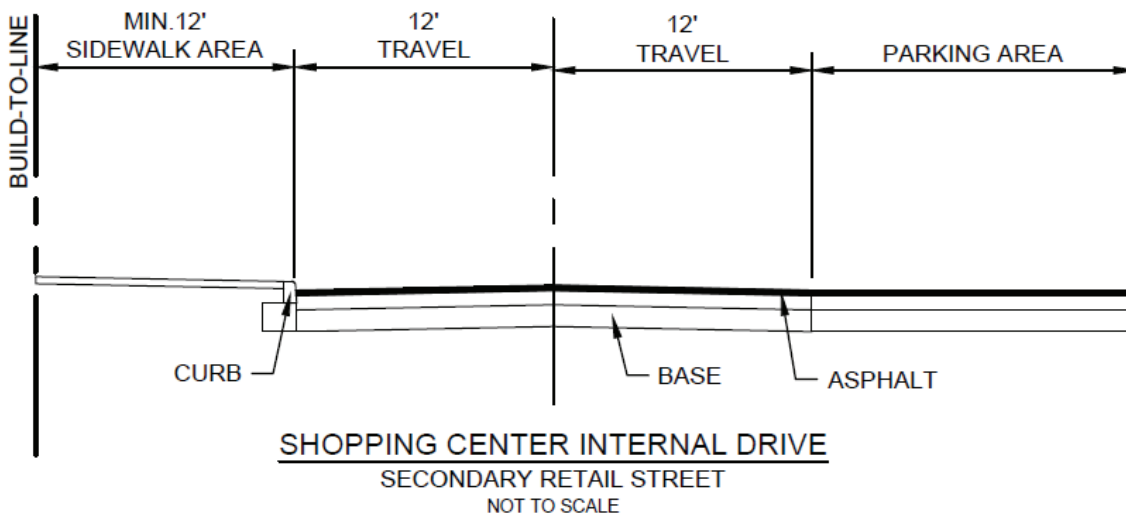
\*SIDEWALK/MULTI-USE PATH AREAS MAY INCLUDE PLANTERS, TREE WELLS, OTHER LANDSCAPED AREAS, AND/OR MAY BE UTILIZED AS SPACE FOR CAFE AND OUTDOOR SEATING AREAS AND STOREFRONT POP-OUTS. NOTWITHSTANDING THE FOREGOING, A CONTINUOUS WALKING PATH NO LESS THAN 6-Feet IN WIDTH SHALL BE MAINTAINED AT ALL TIMES WITHIN SIDEWALK AND MULTI-USE PATH AREAS. ADDITIONALLY, A CONTINUOUS BIKING PATH OF 4-Feet SHALL BE MAINTAINED WITHIN MULTI-USE PATH AREAS. THE 6-FOOT MINIMUM WALKING PATH AND 4-FOOT MINIMUM BIKING PATH ARE NOT REQUIRED TO BE COLLOCATED.

ALLOWABLE 5' REDUCTION IN WIDTH FOR LOADING ZONES AND TRANSITIONS.

**Currently approved Type B Streets typical sections (ALTERNATE):**

\*SIDEWALK/MULTI-USE PATH AREAS MAY INCLUDE PLANTERS, TREE WELLS, OTHER LANDSCAPED AREAS, AND/OR MAY BE UTILIZED AS SPACE FOR CAFE AND OUTDOOR SEATING AREAS AND STOREFRONT POP-OUTS. NOTWITHSTANDING THE FOREGOING, A CONTINUOUS WALKING PATH NO LESS THAN 6-FEET IN WIDTH SHALL BE MAINTAINED AT ALL TIMES WITHIN SIDEWALK AND MULTI-USE PATH AREAS. ADDITIONALLY, A CONTINUOUS BIKING PATH OF 4-FEET SHALL BE MAINTAINED WITHIN MULTI-USE PATH AREAS. THE 6-FOOT MINIMUM WALKING PATH AND 4-FOOT MINIMUM BIKING PATH ARE NOT REQUIRED TO BE COLLOCATED.

ALLOWABLE 5' REDUCTION IN WIDTH FOR LOADING ZONES AND TRANSITIONS.

**Proposed Shopping Center Internal Drive:**

Type B Streets are 60 to 66 feet wide, can accommodate sidewalks on both sides, street parking (parallel) and shared bike lanes. The proposed modification only provides a sidewalk on one side, adjacent to the retail buildings, and parking spaces are located on the opposite side. The total width has been reduced considerably. The design of this drive is typically found in shopping center surface parking lots.

As part of the narrative, the applicant states that at the time PD 19-1443 was approved by the BOCC, the Burlington Property was not owned by the applicant and could not be included in the Approved Zoning. Since that time, the applicant/developer entered into a land swap agreement with the owners of the Burlington Property conditioned upon the Burlington store being relocated to its new location (and subject to a number of conditions) in the proposed new retail area within portions of Blocks 10 and 14 of the Project ("New Retail Area"). By relocating the Burlington store to



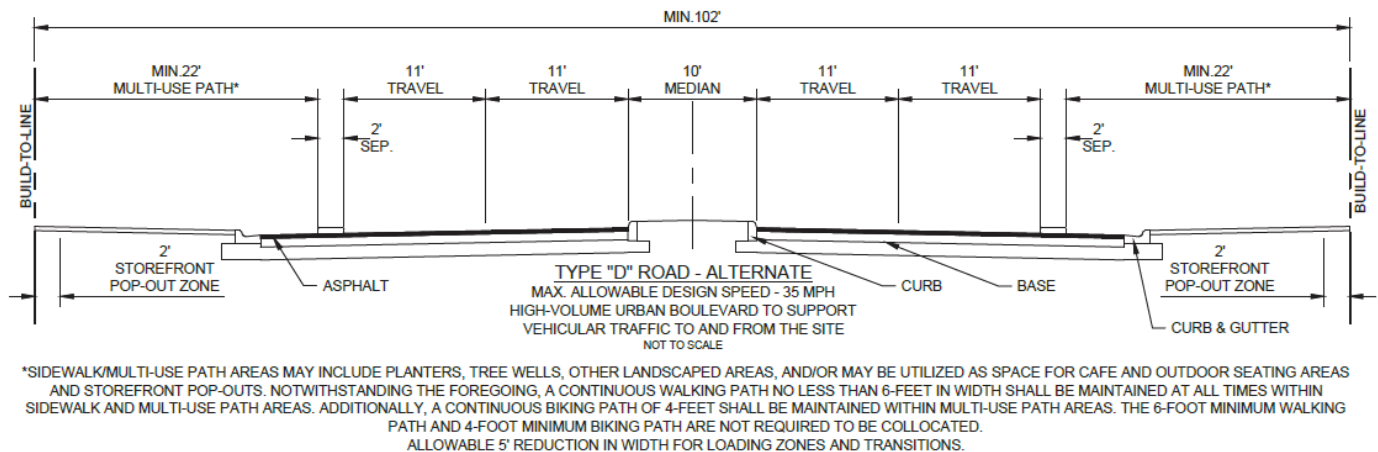
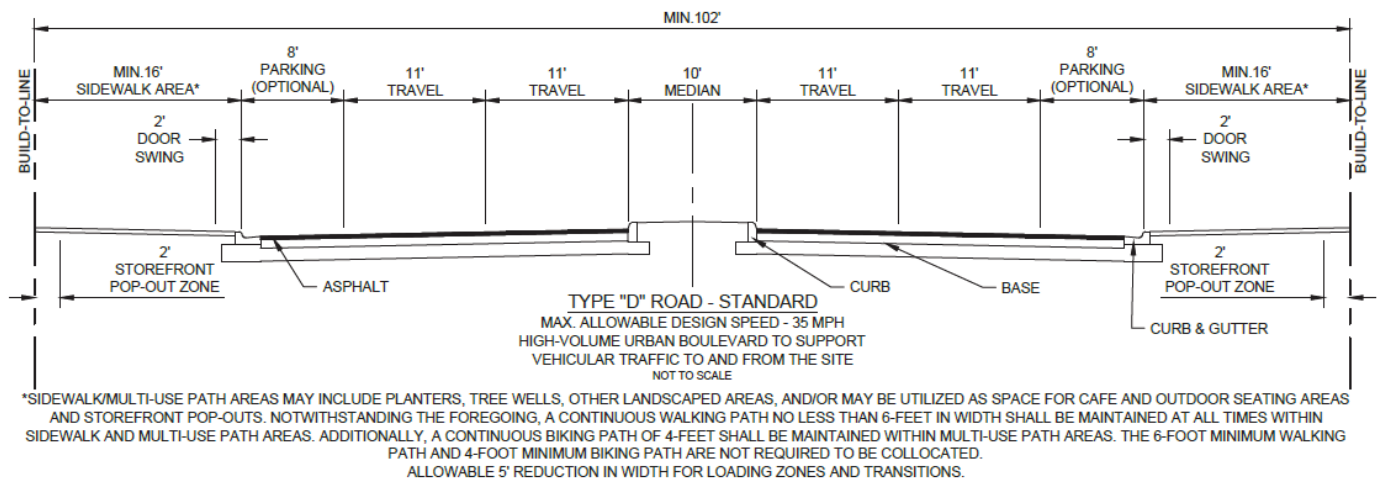
the new area, the existing surface parking lot in front of this future store, would remain to serve this use. The applicant states that these interim changes were not only contemplated but specifically discussed with staff and the Planning Commission during the original rezoning approval process. Based on the files on record for the original rezoning 19-1443, some of the existing parking lots surrounding the University Mall building (including Block 14), were shown containing surface parking lots, but were labeled as "Future Development".

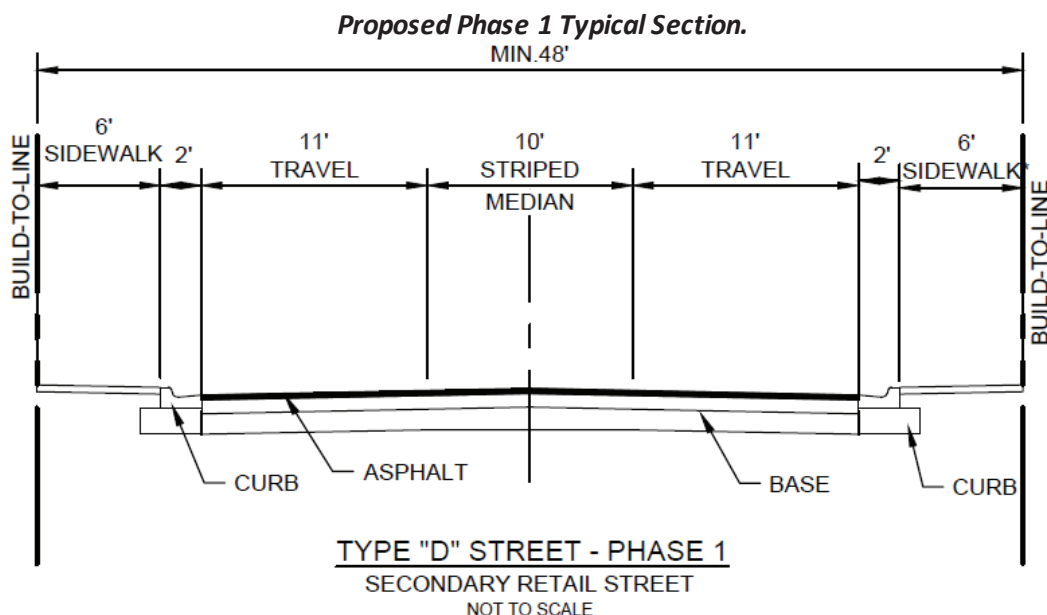
Staff evaluated the modification of the Type B street and the applicant has proposed conditions requiring the timing for when the Shopping Center Drive would be re-built to the originally approved Type B typical sections. The proposed condition would establish two scenarios: *either (i.) portion of adjacent Blocks 9, 13, or 14 is further developed or redeveloped, and/or (ii.) further development or redevelopment of the retail area within Block 10. Additionally, the surface parking lot on Block 14 would be allowed to continue until it is redeveloped in accordance with the conditions and standards stipulated by the PD.*

#### Proposed Phase 1 Type D Street:

Currently, the PD provides for two Type D typical sections that connect three of the Property's existing access points to Fowler Avenue through the Project and through small portions within the City of Tampa's jurisdiction. According to the applicant, these three connections are constrained by existing uses with long-term leases and property owned by third parties. Therefore, development of the approved typical sections to Fowler Avenue are not viable in these locations (at this time) due to these constrained roadway widths.

#### ***Currently approved Type D Typical Sections (STANDARD and ALTERNATE):***





The currently approved Type D typical street sections have a minimum width of 102 feet, can accommodate 16-foot wide sidewalks on both sides, four travel lanes, optional on-street parking and a 10-foot raised median. The proposed "Type D Street – Phase 1" eliminates the two 16-foot sidewalk areas, replacing them with a 6-foot sidewalk on both sides of the street. Additionally, the optional on-street parking has been removed from the cross-section, the 10-foot raised median has been replaced with an at-grade 10-foot striped median, and the number of travel lanes has been reduced from two in each direction to one in each direction. Overall, the road width has been reduced by more than 50%.

According to the narrative, the developer will provide the required typical sections (as originally approved) when the intensity of the Project warrants the improvements as set forth in proposed conditions. In the interim, the applicant proposes a Type D Phase 1 typical section to be constructed concurrent with the development of the New Retail Area. This interim typical section will provide roadways and pedestrian connections to Fowler Avenue to serve the New Retail Area and the remainder of the Project. The New Retail Area will be approximately +/- 94,876 square feet. Therefore, the interim typical sections would provide ingress and egress capacity sufficient for existing and proposed uses into the foreseeable future. To demonstrate sufficient capacity, the applicant has provided a Traffic Monitoring Report prepared by Kimley-Horn dated December 20, 2021 to the County staff as part of this application.

To address staff comments in connection with safety, the applicant has proposed a number of marked crosswalks and speed tables along the Shopping Center Internal Drive (including 3 additional crosswalks in locations requested by staff) to increase pedestrian safety. In addition, sidewalk pedestrian access will be provided from the "New Retail Area" to Fowler Avenue in Phase 1 of the Type "D" Streets.

The triggering mechanisms requiring these two driveways to be re-built in accordance with the Type D Street Typical sections as originally approved, include redevelopment of Blocks adjacent to these streets, or triggered by Level of Service (LOS) as indicated by the Traffic Monitoring Report: *Upon the earlier of (i.) such time as the blocks adjacent to a Type D Phase 1 typical section is further developed or redeveloped with vertical square footage, the Type D Phase 1 typical section shall be replaced with the a Type D Street typical section and constructed in accordance with the Type D Street typical section on sheet 2 of 6, or (ii) such time as the Traffic Monitoring Report indicates that existing plus project traffic is anticipated to exceed the two-way peak-hour Level of Service (LOS) D threshold for a two-lane divided roadway.* As part of the application, the applicant submitted a traffic analysis that provides the scenarios with the amount of development and traffic that would go over the threshold exceeding the two-way peak-hour LOS D.

In order to provide the surface parking fronting the Retail Area, the developer is re-striping and improving the existing parking areas. However, certain Vehicular Use Area buffers are being eliminated. Variations to Sec. 6.06.04D are being



requested by the applicant, which requires an 8' landscape perimeter buffer along parking area boundaries. This buffer area is being eliminated (0'). The request is for the New Retail Area parking area where it abuts the Type "D" Street Phase 1 sections. The applicant states that this is an interim condition and will not be required when the Type "D" Street typical section is required to be constructed. The criteria/justification for the request are listed below:

- 1- The requested variation is an interim condition until re-development of the property triggers replacement of a Type "D" Street - Phase 1 typical section with a Type "D" Street typical section as part of the overall redevelopment of the Property into its ultimate mixed use plan.
- 2- The variation is internal to the Property and the Type "D" Street - Phase 1 typical section has been designed to address pedestrian safety for the interim condition.
- 3- The variation is in harmony with the purpose and intent of the Hillsborough County Land Development Code as it protects the public's health, safety and welfare while providing flexibility to foster catalytic redevelopment.
- 4- The variation is wholly within the PD boundaries and does not abut other property owners and therefore will not injure the rights of other adjacent property owners.

Natural Resources staff has reviewed the justifications provided by the applicant and does not object. Conditions requiring site development review and approvals by Natural Resources are being recommended. As previously noted, these existing parking areas would be permitted to continue to exist until they are redeveloped in accordance with the conditions and standards stipulated by the PD.

Planning Commission staff reviewed the request and provided a Memorandum to the Development Services Department. Per the Memorandum received on January 25, 2022, the Planning Commission found the request inconsistent with the Future of Hillsborough Comprehensive Plan for Unincorporated Hillsborough County. Staff concluded that this request would be in direct conflict with the ICMU-35 Future Land Use category and the University Area Community Plan vision. In their report, staff noted that the proposed street changes drastically different than the original approval by eliminating one side of the sidewalk, eliminating on-street parking, and increasing the travel lane widths. Overall, the proposed changes maintain the large surface parking areas that already accommodate automobile users and then removes amenities for pedestrians and bicyclists, resulting in safety concerns for multimodal users. Staff also cited inconsistencies with the Transportation, Future Land Use and Economically Disadvantage Elements of the Comprehensive Plan, as well as with the Community Design Component of the Plan.

Given that the project was originally conceived and intended to be a destination with a strong urban scale, approved with specific site design standards which accommodates pedestrians with a walkable streetscape, staff expressed concerns with the proposal to re-design or designate the project streets with a different typical section. The applicant made several revisions to the GSP and typical sections in an attempt to design these interim internal roadways as close as possible with the approved Typical street sections. Additionally, the applicant updated the traffic analysis, GSP and request to ensure that future re-configuration of the affected internal roadways as required today by the PD is attainable within a reasonable amount of time. The areas proposed for the re-designed streets are being limited to the New Retail Area, on the south portion of the project. The proposed street designs would not remain in perpetuity since as the project further develops, these internal roadways would be required to be re-built to meet the originally approved typical street sections. Additionally, staff has evaluated the different triggering mechanisms proposed by the applicant and has found these adequate. As noted, by imposing traffic thresholds and conditioning future development or redevelopment of areas within the project, including changes in use in the New Retail Area, the modified typical sections resulting from the Burlington store relocation would be required to be re-built to the standards of the street types as originally approved in the PD.

## 5.2 Recommendation

Based on the above considerations, staff recommends approval, with conditions.

Approval of the request, subject to the conditions listed, is based on the revised general site plan submitted ~~November 24, 2020~~ January 25, 2022.

1. The Project shall be permitted to redevelop with any of the following uses:
  - All CG Uses (except sexually oriented uses, wastewater plants, recyclable material recovery facility).
  - Multi-Family Residential.
  - Community Residential Homes, Type C.
  - Convention Centers, Indoor Entertainment, Sporting Facilities and Amusement/Theme parks.
  - Outdoor events such as farmers market, concerts and temporary fairs/carnivals.
  - Large Scale Retail Development including Big Box Retailers (including Large Scale Building Supply and Do it Yourself Centers). The use shall be subject to the standards found in LDC Section 6.11.106. If the standards in 6.11.106 are in conflict with specific zoning conditions herein, the conditions shall apply.
  - Motion Picture Studio.
  - Research Uses (including biomedical research and labs).
  - Manufacturing, Processing, Assembly, Warehousing and Distribution of Pharmaceuticals, medical devices/equipment and all technology related products.
  - Warehousing, processing and distribution associated with a Food Bank.
2. The Project is located within the ICMU-35 mixed-use land use category that provides for a 2.0 FAR, 35 dwelling units per acre and allows use of FAR in lieu of density for residential projects at the developer's option. The project shall provide a minimum of two uses at all times. Should the development on site meet the following provisions, additional intensity and density is permitted pursuant to Objective 19 and Policies 19.1, 19.2 and 19.3 of the Future Land Use Element (FLUE):
  - If the project includes 3 or more land uses or vertically integrates two land uses, the project is entitled to a FAR Bonus of .50 FAR. If the .50 FAR bonus is granted, the entire site shall be regulated by FAR and not density.
  - Mixed-use projects of 3 or more land uses, can consider different housing types (multi-family, attached single family or detached single family) as different uses.
  - 2.1 The project shall maintain a minimum two uses (office and retail) upon commencement of redevelopment approved in this PD.
  - 2.2 Parking structures shall not count towards the FAR.
  - 2.3 Both FAR and densities and open space shall be calculated on the basis of the entire land area within the PD, including any land dedicated for right-of-way subject to approval of applicable dedication agreement(s) with the County. A tracking data table showing the existing and proposed uses including the intensity/density ratio calculations and open space shall be included on all preliminary site plan and/or construction plan submittals.
  - 2.4 Open space requirements shall be calculated based on the limits of any portion of the PD submitted as a site plan for development or redevelopment. In order to provide opportunities for common open space and/or pedestrian connections throughout the PD, required open space can be provided either within the limits of that site plan or elsewhere within the PD.



- The project shall provide a minimum of ten (10) percent open space within the overall PD area.
3. The following setbacks shall apply to the perimeter boundaries of the project unless otherwise noted on the site plan. No internal setbacks shall be required but development shall meet the building separation requirements of the Florida Building Code.
    - 3.1 The setback from Fowler Avenue is 25 feet, 20 feet from all other property lines.
    - 3.2 The project is not subject to the 2 additional feet of building setback per 1 additional foot of building height over 20 feet found in the Land Development Code (LDC) Part 6.01.01 footnote 8.
    - 3.3 Buffer and screening along the perimeter of the project shall be in accordance with the Land Development Code (LDC) Part 6.06.06. Buffer and screening between uses internal to the project shall not be required.
  4. The existing development is served by two offsite county drainage ponds and existing onsite development is almost completely impervious. Any additional construction within this site and/or required associated off-site improvements are to meet the criteria set forth within the Stormwater Management Technical Manual in effect at the time of the Construction Plan submittal.
  5. To ensure the project develops as a walkable, integrated and connected mixed-use development, the following design principles shall be provided for all new development:
    - 5.1 Location of roadways, street types and blocks shall generally conform to the layout as shown on the Sheet 1 of the PD General Site Plan. At each Preliminary Development Site Plan application for specific projects, the developer shall provide a general site plan depicting the subject site development plan within the context of the entire PD zoning district to demonstrate general conformance with the PD General Site Plan. Any deletion of a roadway, change of street type or reconfiguration of blocks that change the basic form of the blocks will require approval by the Board of County Commissioners in accordance with LDC Section 5.03.07.
    - 5.2 With the exception of Block 6, maximum block perimeters shall be 2,500 linear feet. Blocks shall be bounded on all sides by streets. An alley may be provided between blocks to allow access to an existing parking structure or between a new building and an existing structure.
    - 5.3 All internal streets shall be designated as Type A, B, C, D, E and F. Location and extension of the streets shall be as noted on the PD General Site Plan, Sheet ~~2~~ 1 of 6. Design of the streets shall be as indicated on ~~the Plan PD General Site Plan Sheet 2 of 6 titled Typical Sections and Internal Connections~~. However, the component widths and the overall typical section widths shall be minimums and developer may increase the dimensions of the individual components or the total typical section at Developer's discretion. Where additional squares, civic spaces or hardscaped plazas are provided, on street parking adjacent to the squares, civic spaces or hardscaped plazas may be eliminated.
      - a. Concurrent with the improvement of Club Drive East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive East (Type F), in a location to be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive East (Type F), the developer

shall grant a construction and access easement to the owners of folio 36282.0600, in order for the developer of that property to tie into the crosswalk identified above (at such time as the adjacent property may redevelop in the future).

- b. Concurrent with the improvement of that portion of Club Drive between Fowler Ave. and Club Dr. East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive in a location +/- 475 feet south of University Square Dr. or as otherwise approved by Hillsborough County. Location of the crosswalk shall be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive South of University Square Drive the developer shall grant a construction and access easement to the owners of folio(s) adjacent to such crosswalk, in order for the developer of those properties to tie into the sidewalk system along Club Dr. south of University Square Dr.
  - c. To the west of blocks 10 and 14, and to the east of block 14, where a Type D typical section is provided for on the PD General Site Plan, the typical section may be constructed in phases in accordance with the Type D Phase 1 typical section provided for on PD General Site Plan sheet 2 of 6.
  - d. Notwithstanding the interim improvements envisioned to support the increment of development shown on PD General Site Plan Sheet 6 of 6, or as otherwise provided for herein these conditions, upon the earlier of:
    1. such time as the blocks adjacent to a Type D Phase 1 typical section is further developed or redeveloped) with vertical square footage (to also include a change of use), the Type D Phase 1 typical section shall be replaced with the Type D Street typical section and constructed in accordance with the Type D Street typical section on Sheet 2 of 6; or,
    2. such time as the Traffic Monitoring Report indicates that existing plus project traffic is anticipated to exceed the two-way peak-hour Level of Service (LOS) D threshold for a two-lane divided roadway (per Table 4 of the Florida Department of Transportation Quality/Level of Service Handbook) (for development on any block within the PD), the Type D Phase 1 typical section set forth in Condition 5.3.c shall be replaced with the Type D typical section and constructed in accordance with the Type D Street typical section on PD General Site Plan Sheet 2 of 6. For example, if the typical section between blocks 13 and 14 meets the thresholds set forth in this condition 5.3.d, the developer would only have to replace the typical section in that location and shall have no obligation to replace the typical section in other areas until that section independently meets the thresholds set forth herein.
- 5.4 Maximum building height shall be 300 feet. No building shall exceed 100 feet within 60 feet of the project boundaries along the areas north, west and northwest as indicated in Sheet 5 of the PD General Site Plan.
- The proposed site falls within Zone "B" on the Airport Height Zoning Map. Any structure including construction equipment that exceeds 200 feet Above Ground Level may require an Airport Height Zoning Permit and must be reviewed by the Airport Zoning Director.

- At the time of construction plan submittal, the developer shall provide a letter from the Airport Authority demonstrating compliance with the height restrictions requirements or applicable approvals for any height variances.
- 5.5 All new buildings along 'A' and 'B' Streets greater than 3,500 sq. ft. shall have an average height of 22.5 feet or greater within the setback zone, including parapets and architectural features.
- 5.6 Front setbacks for buildings along A Streets shall be 0 to 15 feet maximum and the entire length of each building façade facing the street shall be placed within the prescribed front yard setback area, up to a minimum average base height of 22.5' including architectural elements. Building facades/street walls above the minimum average base height are not subject to setback/build-to-line restrictions. Where buildings front onto a plaza or square, the setback area shall be measured from the edge of the additional provided open space. The plazas or squares shall be provided as publicly accessible amenities, consistent with condition 5.12.
- 5.7 Front setbacks for buildings along B Streets shall be 0 to 15 feet maximum and at least 50% of the length of each building façade facing the street shall be placed within the prescribed front yard setback area, up to a minimum average base height of 22.5' including architectural elements. Building facades/street walls above a minimum average base height of 22.5' are not subject to the setback/build-to-line restrictions. Where buildings front onto a plaza or square, the setback area shall be measured from the edge of the additional provided open space. The plazas or squares shall be provided as publicly accessible amenities, consistent with condition 5.12.
- 5.8 Minimum front setbacks for buildings along Type C, D, E and F Streets shall be 0 feet. Type C and Type D streets are not subject to maximum setbacks or building façade length percentages.
- 5.9 A building's primary orientation shall be toward the street rather than the parking areas. New buildings shall have at least one entrance which is accessible by pedestrians from a street.
- 5.10 A main square or civic space, located within the "Central District", shall be at least 0.5 acres in size and bounded by at least two 'A' Streets. If streets do not bound the remaining sides of the square, pedestrian thoroughfares shall be provided connecting to the street network. All buildings adjacent to the square shall have frontages facing the square with uses for retail, office, civic, or institutional. All building facing the square shall provide retail, office, civic, institutional or cultural uses on the ground floors. The square shall include green space and place making elements and may include hardscapes, seating, pavilion(s) or other amenities available for public use and enjoyment. The main square may include retail kiosks, provided that the commercial footprint of the building is not counted toward the minimum area required for the main square.
- 5.11 The developer shall provide at least two (2) of the following place making elements (specialty signage, lighting, landscaping, building architecture, and/or public art) as an enhancing feature of open spaces within the Central District.
- 5.12 Additional squares or hardscaped plazas ("Open Spaces") may be provided throughout the site, as publicly accessible amenities. Such Open Spaces may contain landscape elements, site furnishings, commercial kiosks, and/or outdoor seating which may be associated with neighboring eating



establishment uses. Commercial kiosks shall not be subject to the build to line setback requirement of adjacent blocks and may be located anywhere on the Open Space, subject to condition 5.19. On street parking abutting such use may be eliminated per condition 5.3.

- 5.13 Blank walls, defined as walls with no transparency, mural, public art, or architectural enhancements/expressions, shall not occupy more than 50% of a street-facing frontage. Such expressions and enhancements include features such as pilasters, columns, canopies/porticos, arcades or colonnades and shall have a minimum projection or recess of two inches from the wall surface. No more than 40 feet of horizontal distance of wall shall be provided without architectural expressions.
- 5.14 On 'A' Streets, ground floor uses shall contain transparent glass windows or entrances covering a minimum of 50% of a building's linear frontage measured between 3 and 12 feet above sidewalk elevation, and blank walls shall not exceed 30 linear feet without being interrupted by a window, entry, or architectural expression.
- 5.15 On 'A' Streets, loading areas shall be enclosed by architectural screen or operable doors. Loading doors shall be closed when not in operation.
- 5.16 On 'A' Streets, parking lots shall be placed behind buildings. On 'B' Streets, parking lots shall be permitted to be located on the rear or side of the buildings provided that any permanent surface parking areas along 'B' Streets is screened by a 42-inch high wall or a hedge a minimum 42 inches high and a maximum height of 6 feet. Surface parking will comprise no more than 75% of a block once redeveloped. Existing surface parking may continue until the existing surface parking areas are redeveloped in accordance with the conditions and standards stipulated by the PD.
- 5.17 For new parking structures abutting streets, all walls, including the rear wall, shall conform with the façade requirements above, except that wall openings may be provided in lieu of windows. All parking garages that are open to the public shall provide direct pedestrian access from the parking levels of the structure to each street it faces ~~shall be provided~~, except where separated from the street by another building or parking lot.
- 5.18 Tables and/or graphic diagrams demonstrating the facades requirements for the percentages above shall be provided during the site construction permitting for review by Development Services Department staff.
- 5.19 Sidewalk widths shall be a minimum of 5 feet throughout the site, and a minimum of 6 feet on active frontages. Notwithstanding, sidewalk width and design is subject to the Typical Cross Sections noted on Plan Sheet 3 of 5 and condition 5.3.
- 5.20 Gateways shall mark the arrival into the district at key intersections. These are entrance corridors that herald the approach of a new landscape and defines the arrival point as a destination. A minimum of four gateways shall be provided: One (1) to the north, one (1) to the east and two (2) to the south. In order to provide a transition that clearly indicates entrance to the project, each Gateway shall include features or architectural elements such as: change of paving materials, vertical or horizontal signage, arches, obelisks, planters/landscaping, monuments, public art or other similar treatments.

5.21 Notwithstanding any other provision of condition 5 to the contrary, the New Retail Area (PRS 22-0028) depicted on PD General Site Plan Sheet 6 of 6 may be developed with the Shopping Center Internal Drive Typical Section (not a Type B Street) and surface parking lot as shown on PD General Site Plan Sheets 2 of 6 and 6 of 6. Notwithstanding the above, at such time as any of the following occurs the developer shall complete the specified action:

- a. If the portion of adjacent Block 9 and 13 is developed or redeveloped (to include a change of use), the Shopping Center Internal Drive between Block 9 and 13 shall be replaced and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place;
- b. If the New Retail Area within Block 10 as shown on PD General Site Plan Sheet 6 of 6, is further developed or redeveloped (to include a change of use except for the building labeled as retail within the New Retail Area) subsequent to the development authorized via PD Site Plan Sheet 6 of 6, then the developer shall replace the Shopping Center Internal Drive between Block 10 and 14 and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place; and,
- c. If Block 14 is developed or redeveloped (to include a change of use) subsequent to construction of the parking field authorized via PD Site Plan Sheet 6 of 6, then the developer shall replace the Shopping Center Internal Drive between Block 10 and 14 and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place.

5.22 Truck courts/loading areas abutting the Shopping Center Internal Drive shall be gated. The gates shall remain closed at all times, except during loading/truck deliveries. Placement of the gates shall be as shown on Sheet 6 of 6. Notwithstanding the above, truck courts/gates shall not be permitted in locations that interfere with any required alleyway stop signs and/or the required alleyway sidewalk connections. Nothing herein this condition shall be construed to mean that the alleyways cannot also be gated at both ends (regardless of anything shown on the PD site plan to the contrary) provided ungated pedestrian access is maintained).

5.23 Notwithstanding any other provision of condition 5 to the contrary, no sidewalk shall be required along the western side of the that portion of the Type D roadway aligning with 22nd St., between Fowler Ave. and the southern PD boundary, to support the increment of development shown on the PD General Site Plan Sheet 6 of 6.

6. The Applicant will coordinate with Hillsborough County and Florida Department of Transportation (FDOT) staff to conduct a traffic analysis as each block located within the PD as depicted on sheet 1 of 5 (each a "Block") is developed in order to assess the currently proposed and cumulative AM peak- hour and PM peak-hour traffic impact at each project access. Coordination between the Applicant, Hillsborough County, and FDOT is necessary in order for the Applicant to provide an analysis that evaluates the access connections to determine how each connection will function at the time each Block is fully developed, while taking into consideration the future long-range plans for Fowler Avenue.

Construction approvals for each Block will require a Traffic Monitoring Report analyzing the specific

development level (land use and scale). The Traffic Monitoring Report will analyze the operations of the major access locations to Uptown during the AM and PM peak-hours, as indicated during observations of the existing access locations. The Traffic Monitoring Report for each Block approval will include existing data (to identify existing traffic conditions), the projection of anticipated project traffic for the proposed Block (as agreed to with Hillsborough County) at its buildout year, and the future planned improvements along Fowler Avenue that are anticipated to be constructed or implemented at the ~~Block~~ Block's buildout year. Pending the results of the analysis, site access/operational improvements, median modifications or additional signalization may be required based upon review by Hillsborough County and the FDOT.

7. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, alternative intersection designs (i.e. roundabouts) may be used internal to the project. Utilization of such alternative designs shall not require a PD modification.
8. Notwithstanding anything herein these conditions or on the PD Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
9. The developer shall provide the following transit facilities:
  - a. A Major Transit Stop (MTS) within the general area designated for such purpose on the PD site plan. The MTS shall serve as a transit stop for both the Hillsborough Area Regional Transit Authority (HART) and University of South Florida (USF) Bullrunner service. Additionally, the staging area shall accommodate two (2) transit vehicles within dedicated bus bays or within a driveway outside of the path of general vehicular circulation. This transit stop and required amenities shall be provided with the initial increment of redevelopment.
  - b. The developer shall provide, within 500 feet of the intersection of Fowler Ave. and 22nd St., a separate bus stop with loading/unloading within dedicated bus bays or within a driveway outside of the path of general vehicular circulation. This transit stop and required amenities shall be constructed and operating no later than December 31, 2025 or concurrent with the initial increment of (re)development within blocks 10, 11, 14, or 15, whichever occurs earlier.
  - c. Two (2) additional bus stops shall be provided within the project, serving either HART and/or USF Bullrunner. These stops maybe located anywhere within the project, subject to approval of the transit agencies serving that stop. These stops and required amenities shall be constructed and operating no later than December 31, 2027 or concurrent with the first increment of (re)development within the last block (re)developed after approval of this rezoning, whichever occurs earlier.
  - d. If bus bays are provided, each bay shall meet the minimum design standards within the latest edition of the "Accessing Transit, Design Handbook for Florida Bus Passenger Facilities", published by the Florida Department of Transportation, and shall be approved by HART.
  - e. At each of the above required transit facilities the developer shall provide a bus shelter and benches suitable for waiting out of inclement weather. Alternatively, seating may be provided under a building or other architectural feature of the project (which provides equivalent or greater protection from the elements) ~~as long as the seating area is located within 100 feet of the transit~~



stop. Transit accessory pads, trash receptacles and bicycle racks shall be provided at each facility. Location and design of the transit stop and amenities shall be subject to approval of Hillsborough County and the respective transit agencies served by the facility. Prior to approval of the applicable site/construction plan submittal whereby the transit facility within a certain block is required, the developer shall provide written documentation indicating the transit agency or agencies using the facilities have approved their design and location.

10. Upon the re-development of Block 9 or Block 13 (not including reuse of existing buildings or shells) along the western PD boundary, the developer shall provide a stub out within the area located on the site plan as the "Western Cross Access Area" to the N 19th Street right-of-way.
11. Upon complete redevelopment of the Project, the developer shall provide the County with a perpetual easement agreement benefitting the public for the purpose of traversing the Project from the eastern boundary of the Project to the western boundary. The location of the easement area shall be limited to private roadways within the Project (running east to west across the Project but not necessarily in a straight line or linear path) in a location determined solely by the developer and as may be changed by developer from time to time. The easement agreement, among other things, shall provide the following terms and conditions:
  - 11.1 Developer reserves the right to eject any individual(s), groups or entities performing any unlawful act(s) within the easement area;
  - 11.2 Developer may temporarily suspend use of the easement area for special events, festivals or private events that are hosted on the Project;
  - 11.3 Developer may temporarily suspend use of the easement area in order to perform maintenance, repairs or replacements on or to the easement area;
  - 11.4 Developer may relocate the easement area provided the new easement area runs from the eastern edge to the western edge of the Project but not necessarily in a straight line or linear path.

For purposes of this condition, "complete redevelopment" shall mean that (i) the easement area selected by developer running from the eastern boundary of the property to the western boundary of the property shall be completely reconstructed as shown on the adopted Planned Development site plan (as may be amended from time to time) and (ii) all blocks adjacent to the easement area (on both the north and the south) shall be fully constructed and shall have received a certificate of occupancy or final inspections, as applicable (including buildings, internal drives and related improvements) within each block.

12. The Vehicular Use Area (VUA) buffer shall be permitted to be reduced or eliminated in those areas marked as VUA Variation Area on sheet 6 of 6 of the GSP. At such time that Type D Phase 1 typical sections are replaced with the Type D typical sections, the required VUA areas shall be provided in accordance with LDC Sec. 6.06.04.
13. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, no on-site development shall occur inconsistent with any recorded public easements within the site.
14. Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the

Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.

15. The construction and location of any proposed wetland impacts are not approved by this correspondence, but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.
16. Prior to the issuance of any building or land alteration permits or other development, the approved wetland/other surface water (OSW) line must be incorporated into the site plan. The wetland/ OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County Land Development Code (LDC).
17. Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.
18. Approval of this petition by Hillsborough County does not constitute a guarantee that Natural Resources approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to trees, natural plant communities or wildlife habitat, and does not grant any implied or vested right to environmental approvals.
19. The construction and location of any proposed environmental impacts are not approved by this correspondence but shall be reviewed by Natural Resources staff through the site and subdivision development plan process pursuant to the Land Development Code.
20. ~~18.~~ If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
21. ~~19.~~ The Development of the project shall proceed in strict accordance with the terms and conditions contained in the PD General Site Plan, zoning conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
22. ~~20.~~ Within 90 days of rezoning approval of PRS ~~20-1313~~ 22-0028 by the Hillsborough County Board of County Commissioners, the developer shall submit to the County Development Services Department a revised General Development Plan for certification reflecting all the conditions outlined above.


APPLICATION NUMBER: PRS 22-0028

ZHM HEARING DATE: N/A

BOCC LUM MEETING DATE: February 8, 2022

Case Reviewer: Israel Monsanto

**Zoning Administrator Sign Off:**

  
J. Brian Grady  
Mon Jan 31 2022 12:53:42

**SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL**

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.



**APPLICATION NUMBER: PRS 22-0028**

ZHM HEARING DATE: N/A

BOCC LUM MEETING DATE: February 8, 2022

Case Reviewer: Israel Monsanto

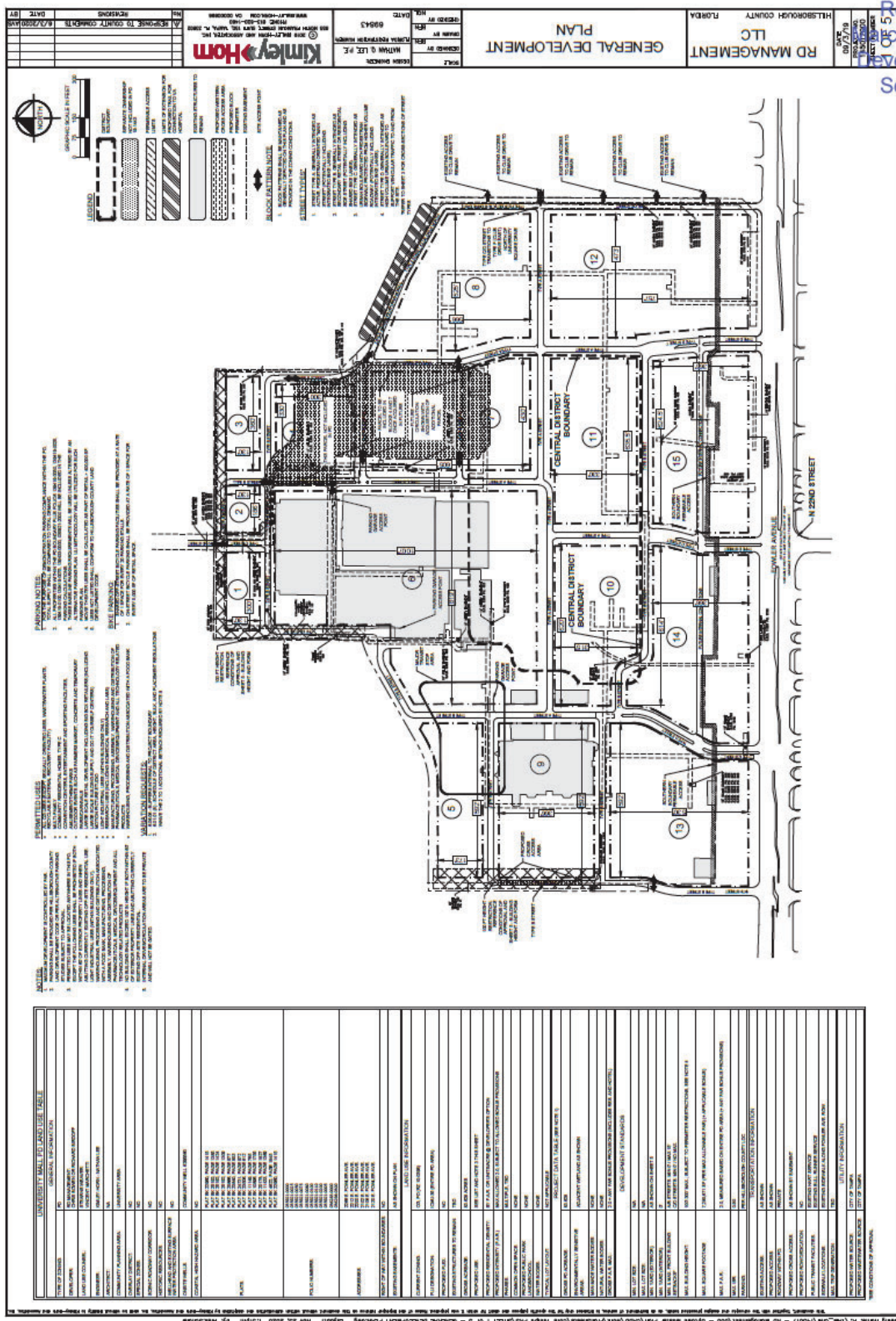
## **7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS**

ZHM HEARING DATE:	N/A
BOCC LUM MEETING DATE:	February 8, 2022

Case Reviewer: Israel Monsanto

## 8.0 SITE PLANS (FULL)

### 8.1 Approved Site Plan (Full)



Received  
March 26, 2022  
Development  
Services

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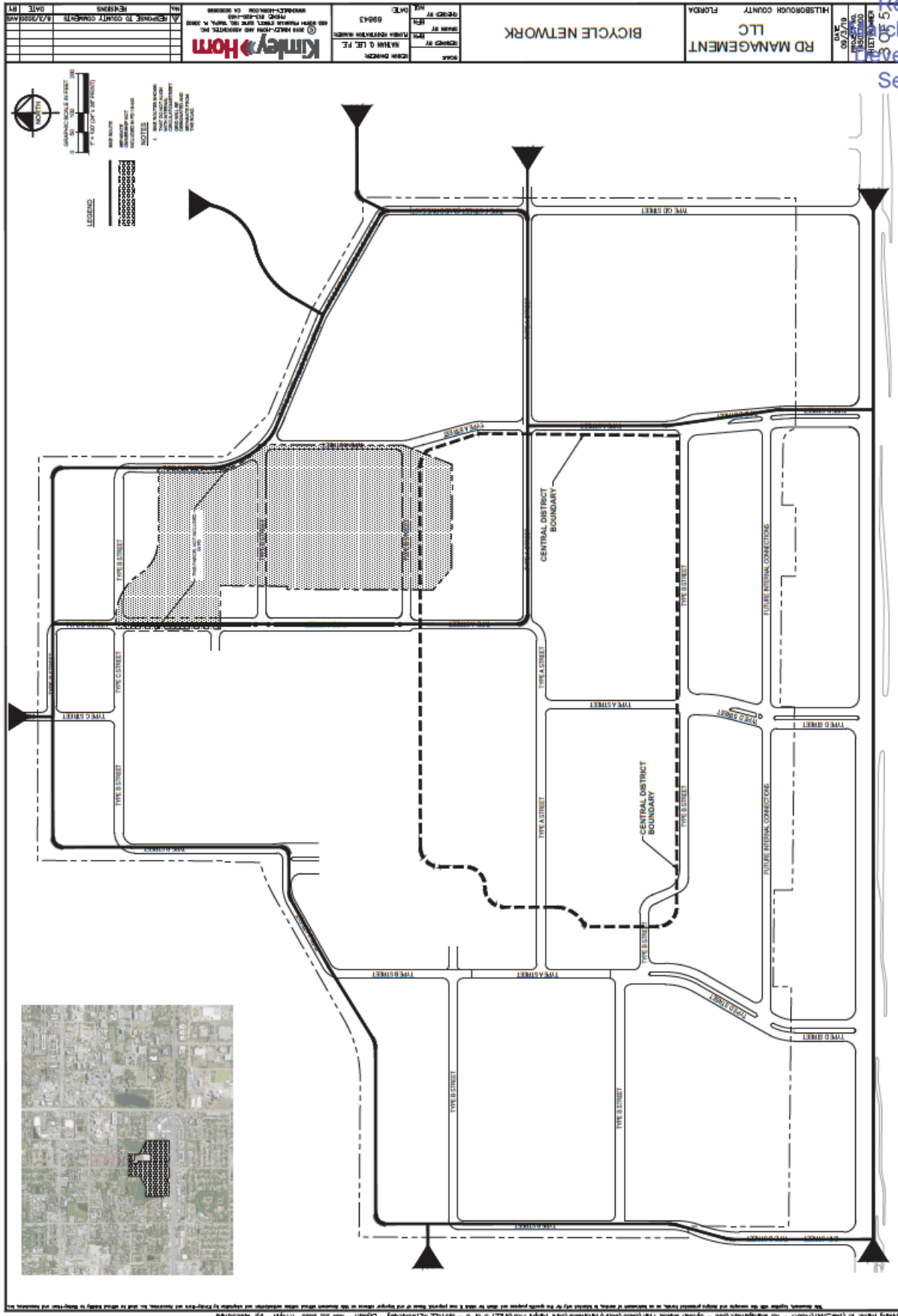
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Clark Nov# 21.0725





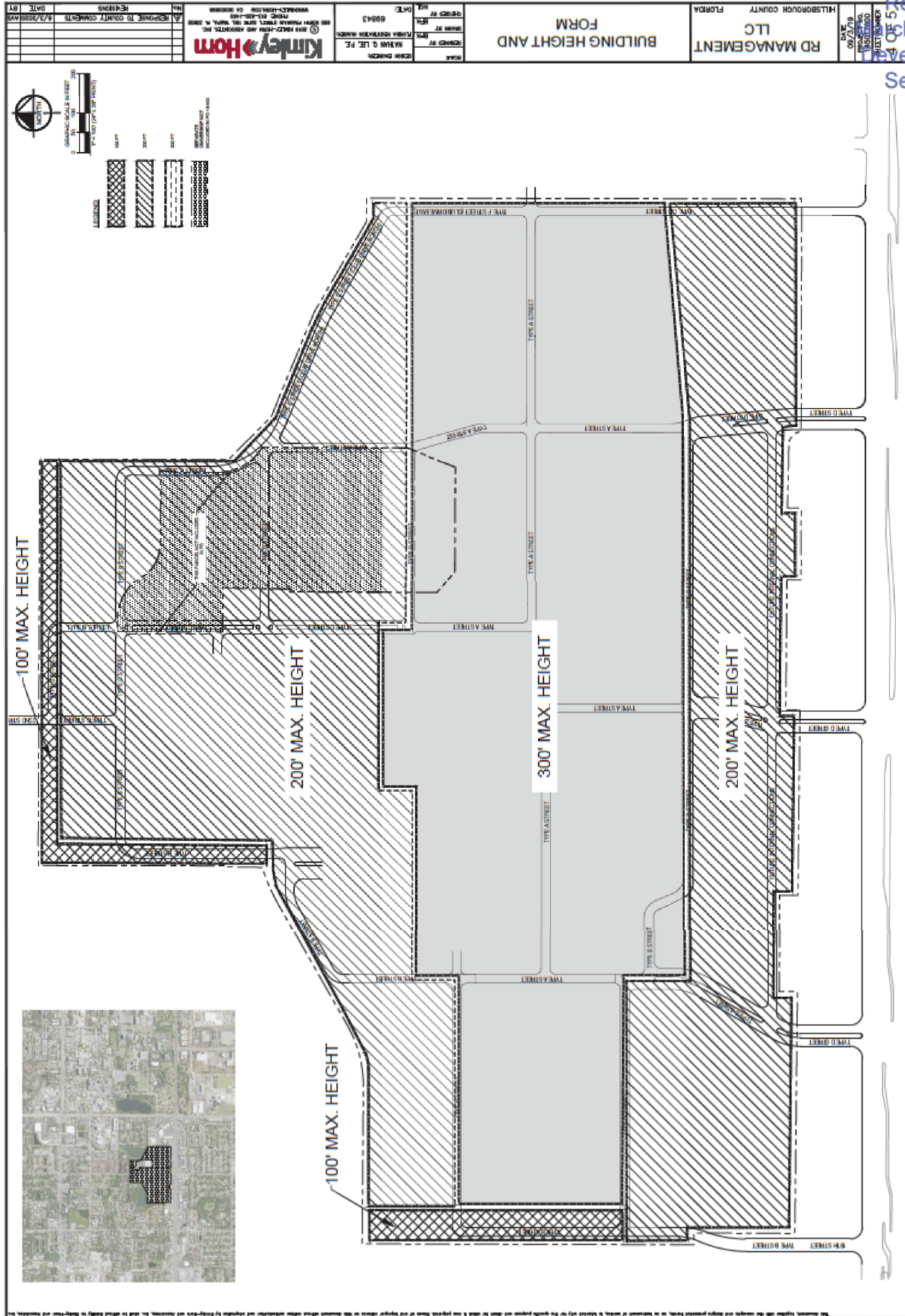
Received  
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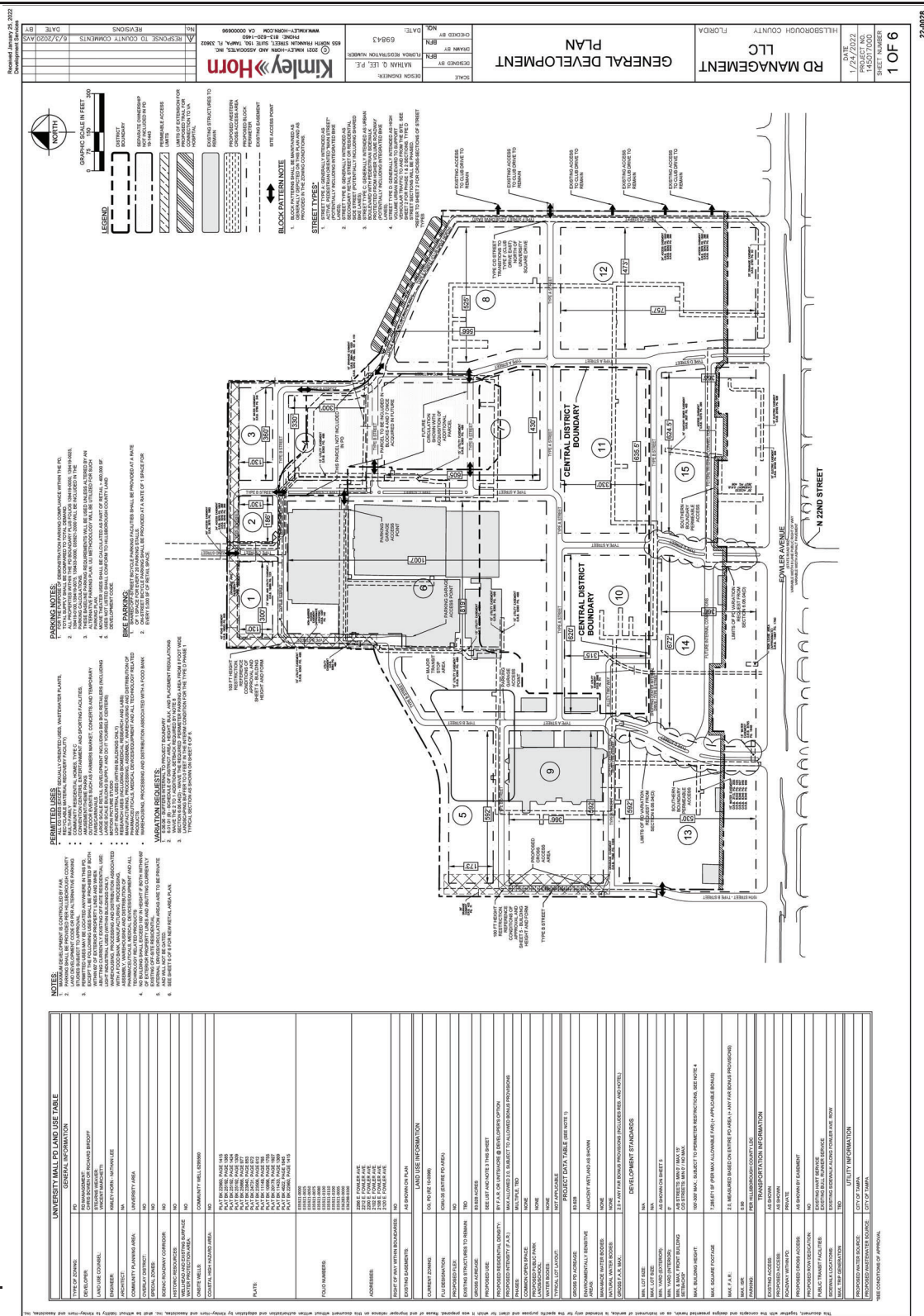






## 8.0 SITE PLANS (FULL)

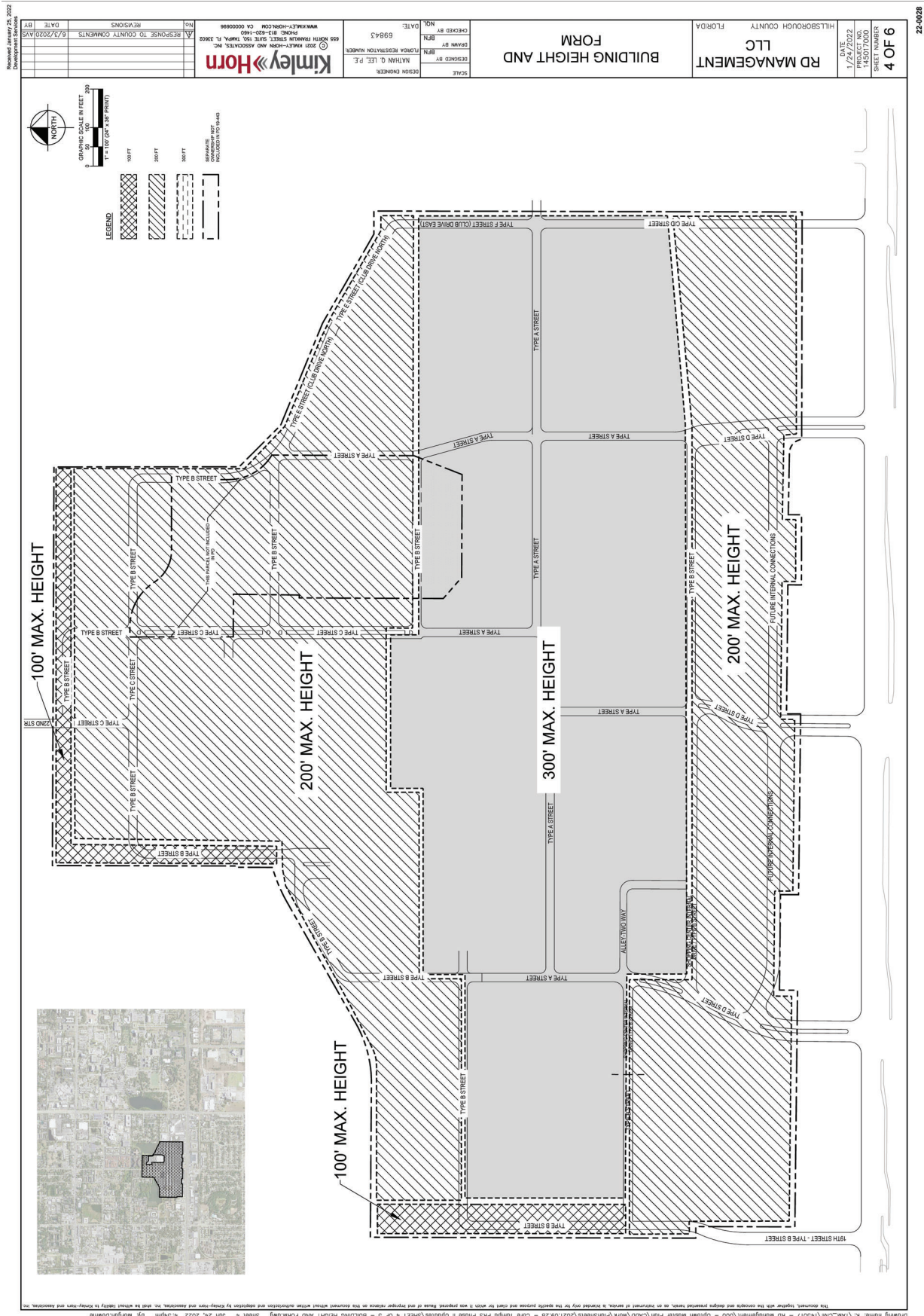
## 8.2 Proposed Site Plan (Full)



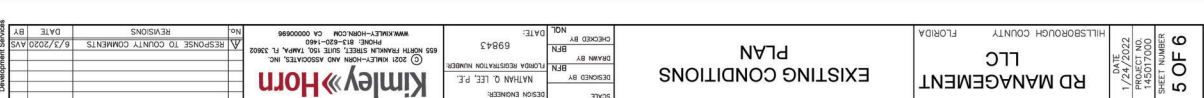
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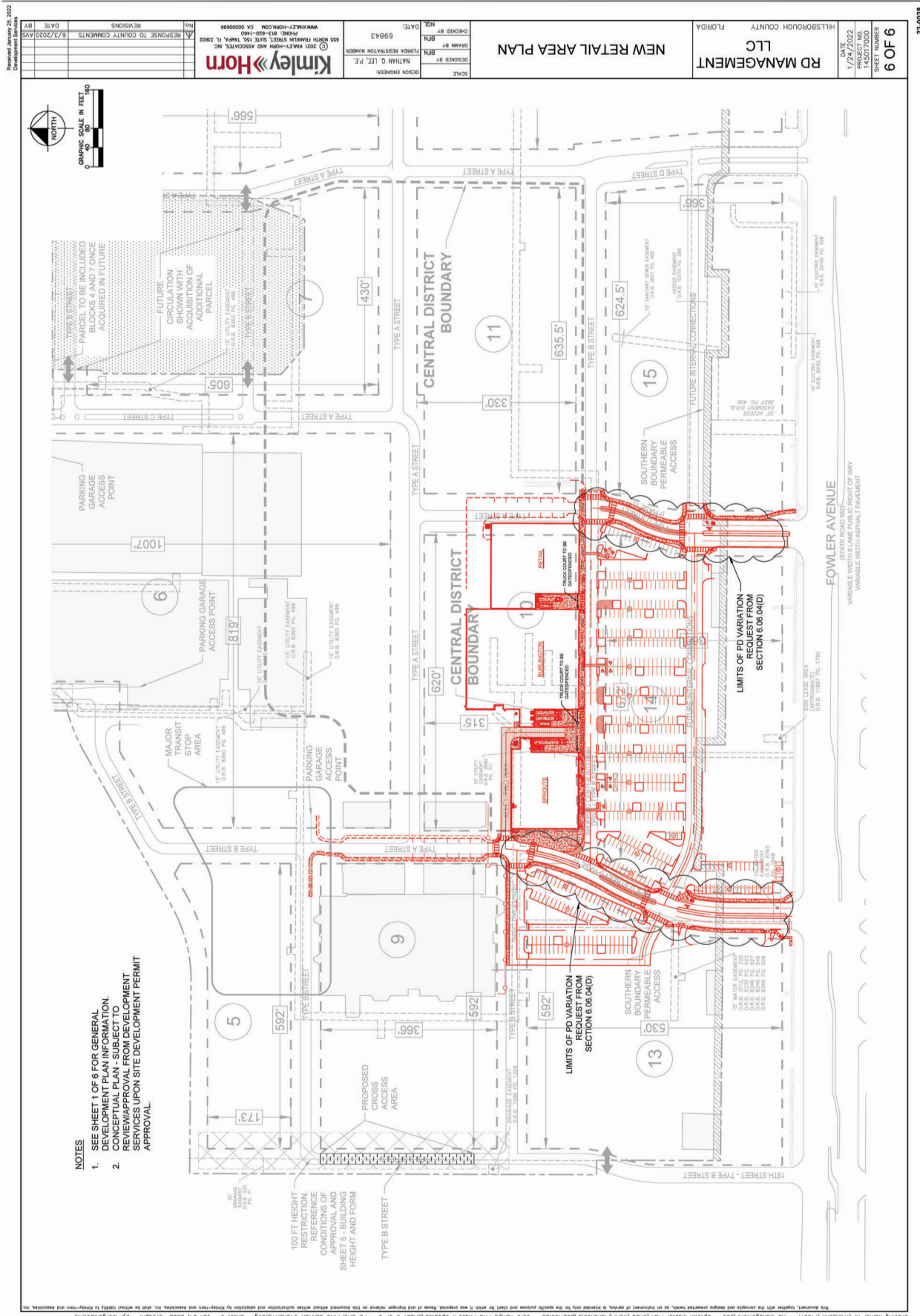












## 9.0 FULL TRANSPORTATION REPORT (see following pages)

## AGENCY REVIEW COMMENT SHEET

TO: Zoning Technician, Development Services Department

DATE: 1/28/2022

REVIEWERS: Richard Perez, AICP, Executive Planner

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: USF/ Northwest

PETITION NO: RZ 20-0028

- ☐ This agency has no comments.
- ☐ This agency has no objection.
- ☒ This agency has no objection, subject to listed or attached conditions.
- ☐ This agency objects, based on the listed or attached conditions.

### NEW AND MODIFIED CONDITIONS OF APPROVAL

#### Modified Conditions

- 5.3 All internal streets shall be designated as Type A, B, C, D, E and F. Location and extension of the streets shall be as noted on the PD General Site Plan, Sheet 21 of 6. Design of the streets shall be as indicated on ~~the Plan-PD General Site Plan, Sheet 2 of 6 titled Typical Sections and Internal Connections.~~ However, the component widths and the overall typical section widths shall be minimums and developer may increase the dimensions of the individual components or the total typical section at Developer's discretion. Where additional squares, civic spaces or hardscaped plazas are provided, on street parking adjacent to the squares, civic spaces or hardscaped plazas may be eliminated.
- a. Concurrent with the improvement of Club Drive East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive East (Type F), in a location to be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive East (Type F), the developer shall grant a construction and access easement to the owners of folio 36282.0600, in order for the developer of that property to tie into the crosswalk identified above (at such time as the adjacent property may redevelop in the future).
- b. Concurrent with the improvement of that portion of Club Drive between Fowler Ave. and Club Dr. East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive in a location +/- 475 feet south of University Square Dr. or as otherwise approved by Hillsborough County. Location of the crosswalk shall be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive South of University Square Drive the developer shall grant a construction and access easement to the owners of folio(s) adjacent to such crosswalk, in order for the developer of those properties to tie into the sidewalk system along Club Dr. south of University Square Dr.
- c. To the west of blocks 10 and 14, and to the east of block 14, where a Type D typical section is provided for on the PD General Site Plan, the typical section may be



constructed in phases in accordance with the Type D Phase 1 typical section provided for on PD General Site Plan sheet 2 of 6.

- d. Notwithstanding the interim improvements envisioned to support the increment of development shown on PD General Site Plan Sheet 6 of 6, or as otherwise provided for herein these conditions, upon the earlier of:

(i.) such time as the blocks adjacent to a Type D Phase 1 typical section is further developed or redeveloped) with vertical square footage (to also include a change of use), the Type D Phase 1 typical section shall be replaced with the Type D Street typical section and constructed in accordance with the Type D Street typical section on Sheet 2 of 6; or,

(ii.) such time as the Traffic Monitoring Report indicates that existing plus project traffic is anticipated to exceed the two-way peak-hour Level of Service (LOS) D threshold for a two-lane divided roadway (per Table 4 of the Florida Department of Transportation Quality/Level of Service Handbook) (for development on any block within the PD), the Type D Phase 1 typical section set forth in Condition 5.3.c shall be replaced with the Type D typical section and constructed in accordance with the Type D Street typical section on PD General Site Plan Sheet 2 of 6. For example, if the typical section between blocks 13 and 14 meets the thresholds set forth in this condition 5.3.d, the developer would only have to replace the typical section in that location and shall have no obligation to replace the typical section in other areas until that section independently meets the thresholds set forth herein.

6. The Applicant will coordinate with Hillsborough County and Florida Department of Transportation (FDOT) staff to conduct a traffic analysis as each block located within the PD as depicted on PD General Site Plan Sheet 1 of 6 (each a "Block") is developed in order to assess the currently proposed and cumulative AM peak-hour and PM peak-hour traffic impact at each project access. Coordination between the Applicant, Hillsborough County, and FDOT is necessary in order for the Applicant to provide an analysis that evaluates the access connections to determine how each connection will function at the time each Block is fully developed, while taking into consideration the future long-range plans for Fowler Avenue.

Construction approvals for each Block will require a Traffic Monitoring Report analyzing the specific development level (land use and scale). The Traffic Monitoring Report will analyze the operations of the major access locations to Uptown during the AM and PM peak-hours, as indicated during observations of the existing access locations. The Traffic Monitoring Report for each Block approval will include existing data (to identify existing traffic conditions), the projection of anticipated project traffic for the proposed Block (as agreed to with Hillsborough County) at its buildout year, and the future planned improvements along Fowler Avenue that are anticipated to be constructed or implemented at the Block's buildout year. Pending the results of the analysis, site access/operational improvements, median modifications or additional signalization may be required based upon review by Hillsborough County and the FDOT.

#### New Conditions

- 5.21 Notwithstanding any other provision of condition 5 to the contrary, the New Retail Area (PRS 22-0028) depicted on PD General Site Plan Sheet 6 of 6 may be developed with the Shopping Center Internal Drive Typical Section (not a Type B Street) and surface parking lot as shown on PD General Site Plan Sheets 2 of 6 and 6 of 6. Notwithstanding the above, at such time as any of the following occurs the developer shall complete the specified action:

- a. If the portion of adjacent Block 9 and 13 is developed or redeveloped (to include a change of use), the Shopping Center Internal Drive between Block 9 and 13 shall be replaced and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place;
- b. If the retail area within Block 10 as shown on PD General Site Plan Sheet 6 of 6, is further developed or redeveloped (to include a change of use) subsequent to the development authorized via PD Site Plan Sheet 6 of 6, then the developer shall replace the Shopping Center Internal Drive between Block 10 and 14 and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place; and,
- c. If Block 14 is developed or redeveloped (to include a change of use) subsequent to construction of the parking field authorized via PD Site Plan Sheet 6 of 6, then the developer shall replace the Shopping Center Internal Drive between Block 10 and 14 and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place.

5.22 Notwithstanding any other provision of condition 5 to the contrary, no sidewalk shall be required along the western side of the that portion of the Type D roadway aligning with 22<sup>nd</sup> St., between Fowler Ave. and the southern PD boundary, to support the increment of development shown on the PD General Site Plan Sheet 6 of 6.

#### **PROJECT SUMMARY AND CONCLUSIONS**

The applicant is requesting a Minor Modification (PRS) to modify a +/- 83.63-acre parcel currently approved as Planned Development (PD) #19-1443, as most recently amended via PRS 20-1313. The site is currently a retail shopping mall with several outparcels and a movie theater. The site was originally developed with 1.4 million square feet of retail uses constructed on the property. The proposed PD excludes a 6.58 ac. section of retail uses (the Burlington Coat Factory).

As currently approved, the developer is permitted to develop at a 2.0 floor area ratio (FAR), or potentially a 2.5 FAR with available bonus, for the following permitted uses.

- All CG uses (except sexually oriented uses, wastewater plants, recyclable material recovery facility);
- Multi-Family Residential;
- Community Residential Homes, Type C;
- Convention Centers, Indoor Entertainment, Sporting Facilities and Amusement/Theme Parks;
- Outdoor Events such as Farmers Market, Concerts and Temporary Fairs/Carnivals;
- Large Scale Retail Development including Big Box Retailers (including Large Scale Building Supply and Do it Yourself Centers);
- Motion Picture Studio;
- Research Uses (including biomedical research and labs);
- Manufacturing, Processing, Assembly, Warehousing and Distribution of Pharmaceuticals, Medical Devices/Equipment and All Technology Related Products; and,
- Warehousing, Processing and Distribution associated with a Food Bank

No changes to approved entitlements or site access are proposed with this PRS. The applicant is proposing the following changes:

1. Elimination of the "Type 'D' Road – Alternate" Typical Section;
2. Addition of the "Shopping Center Internal Drive" Typical Section;
3. Addition of the "Type 'D' Street – Phase 1" Typical Section;
4. Modification of the PD site plan to allow the new "Shopping Center Internal Drive" typical section along that portion of the road along the entire southern boundary of Block 10 (and northern boundary of Block 14), as well as addition to a portion of the northern boundary of Block 13 (southern boundary of Block 9);
5. Addition of Sheet 6 of 6 "New Retail Area Plan", which is intended to be an implementing plan for near term redevelopment within a portion of Block 10, with associated surface parking within Block 14 and a portion of Block 13; and,
6. Various modifications of existing approved zoning conditions in order to facilitate a phased development approach for Type D and Shopping Center Internal Drive/Type B Streets.

### Typical Sections Changes

The applicant is proposing to add a new "Shopping Center Internal Drive" Typical section, eliminate the Type "D" Alternate Typical Section, and is proposing a new Type "D" Phase 1 Typical Section. Staff has prepared a summary of the various features (in table format to aid comparison) for the typical sections referenced in the above described changes.

Type B Related	Developer Action/ Typical Section	Lane Width (ft.) <sup>1</sup>	ROW Width (ft.)	Sidewalk Width (ft.)	Accommodation for On Street Parking	Accommodation for Bicycle Traffic	Accommodation for Café /Retail Pop Outs/Landscaping <sup>4</sup>
	<b>Maintained Typical Sections</b>						
	Type "B" - Standard	10	60	12 (both sides)	Yes (both sides)	No	Yes
	Type "B" - Alternate	13 <sup>2</sup>	66	12 (both sides)	Yes (both sides)	Yes	Yes
	<b>New Typical Sections</b>						
	Shopping Center Internal Drive	12	36 <sup>3</sup>	12 (one side)	No	No	No

Type D Related	Developer Action/ Typical Section	Lane Width (ft.) <sup>1</sup>	ROW Width (ft.)	Sidewalk Width (ft.)	Accommodation for On Street Parking	Accommodation for Bicycle Traffic	Accommodation for Café /Retail Pop Outs/Landscaping <sup>4</sup>
	<b>Maintained Typical Sections</b>						
	Type "D" - Standard	11	102	16 (both sides)	Yes (both sides)	No	Yes
	<b>New Typical Sections</b>						
	Type "D" - Phase 1	11	48	6 (both sides) <sup>5</sup>	No	No	No
	<b>Eliminated Typical Section</b>						
	Type "D" - Alternate	11	102	22 (both sides) <sup>6</sup>	No	Yes <sup>6</sup>	Yes

**General Note:** Only Typical Sections which are proposed for modification (or related to a proposed modification) are listed.

#### Footnotes:

<sup>1</sup> Can be widened at any time at developer's option pursuant to approved zoning conditions

<sup>2</sup> Travel lanes include additional width for shared bicycle travel

<sup>3</sup> Right-of-way width is not specified on the proposed Typical Section. Value represents an approximate envelope of improvements.

<sup>4</sup> In sidewalk or multi-purpose path areas

<sup>5</sup> Does not include areas not within PD boundaries (i.e. between Fowler Ave. and project site).

<sup>6</sup> Multi-purpose path

#### **On-site Type “B” Phasing**

The applicant has proposed new and modified zoning conditions relative to those portions of the Type “B” streets also identified as “Shopping Center Internal Drives” within the PD, which will allow the applicant to:

- a) Construct a potentially interim typical section (the “Shopping Center Internal Drives” section) to support the increment of development shown on Sheet 6 of 6 (i.e. within a portion of Blocks 10, 13, and 14); and,
- b) Phase construction of the full typical sections (the Type “B” Typical Section) within those areas until such time as a certain portion of the adjacent block is redeveloped.

#### **Offsite / On-site Type “D” / Phase 1 Typical Sections**

During the course of discussions with the County, the applicant has stated that there are constraints which may prevent them from constructing the Typical Sections at the three (3) middle project roadway connections to Fowler Ave. Specifically, the applicant has stated that while they own many but not all of these parcels, there are lease agreements which prevent them from making modifications to the parcels. Staff also notes that the City of Tampa has jurisdiction of these lands, which is why they are not included in the PD.

The applicant has proposed new and modified zoning conditions relative to the portions of Type “D” streets within the PD, which will allow the applicant to:

- a) Construct a potentially interim Typical Section (i.e. the “Type ‘D’ Street Phase 1” Typical Section) to support the increment of development shown on Sheet 6 of 6 (i.e. within a portion of Block 10); and,
- b) Phase construction of the full Type “D” Roadway Typical Sections for portion of Type “D” roads adjacent to Blocks 10, 13, 14 and 15, within the PD project until such time as a portion of the adjacent block is developed or the certain Level of Service (LOS) triggers are met (see below).

The proposed conditions also allow the developer to only construct two (2) of the ultimate four (4) Type “D” lanes within the PD project until such time as a Level of Service (LOS) analysis indicates that the additional lanes are warranted. Enhanced pedestrian improvements within the site are also tied to these LOS triggers.

#### **TRANSPORTATION ANALYSIS**

The applicant conducted a transportation analysis which examined existing background traffic, plus additional anticipated traffic generated with the addition of the proposed uses shown on Sheet 6 of 6. According to the analysis, the existing access connections are anticipated to accommodate this proposed increment of development.

Given the proposed changes to typical sections which reduced segments of the internal roadways from 4 lanes to 2 lanes (at least in the interim), County staff also asked the applicant to examine when it was anticipated that future development might trigger such improvements. According to the analysis, it was anticipated that the 22<sup>nd</sup> St. intersection would be the first connection to trigger the four-lane improvement threshold. The applicant’s analysis indicated that there is an additional 360 peak hour trip capacity remaining, which could potentially equate to an additional 200,000 s.f. of Shopping Center uses, 600,000 s.f. of General Office Uses, or 1,000 hotel rooms, according to the applicant.

It should be noted that project traffic will be dispersed among all project driveways (with distribution between them being a factor of type of use being developed and location within the site, among others). Additionally, staff notes that traffic capacity may be utilized by already approved/constructed but underutilized space within the project (although it is not possible to estimate such impacts at this time). Staff notes that each driveway will be reviewed with each increment of development, to determine when thresholds have been met.



#### **EXISTING AND PROPOSED TRANSPORTATION INFRASTRUCTURE SERVING THE SITE**

Fowler Ave. is an 8-lane principal arterial, owned and maintained by the Florida Department of Transportation (FDOT), and is characterized by +/- 12-foot lanes. There are marked bicycle facilities on both sides. There are +/- 5-foot sidewalks on both sides of Fowler Avenue in the vicinity of the project. Fowler Avenue is a designated truck route. Local transit service (Route 12) is provided adjacent to the site. The segment of Fowler Avenue from B. B. Downs to Nebraska Avenue is currently operating at LOS F.

22<sup>nd</sup> St. is a 2-lane collector roadway characterized by +/- 10-foot lanes. There are marked bicycle facilities on both sides. There are +/- 5-foot sidewalks along on both sides of 22<sup>nd</sup> Street in the vicinity of the project. Local transit service (Route 12) is provided adjacent to the site. The segment of 22<sup>nd</sup> Avenue from the mall site to Fletcher Avenue is currently operating at LOS B.

19<sup>th</sup> Street is a 2-lane local roadway characterized by +/- 10-foot lanes. There are no marked bicycle facilities on either side of the road. There is a sidewalk along the west side of 19<sup>th</sup> Street. LOS information is not available for segment of the roadway.

University Square Dr. is a 2-lane, undivided, private street with +/- 12-foot lanes. There is a sidewalk along the south side of University Square Dr. There are no marked bicycle facilities. There are unsheltered USF Bull Runner transit stop (Route D) located on both the north side and south side of University Square Drive in the vicinity of the project.

Club Drive is a 2-lane, undivided, private road characterized by +/- 12-foot lanes. There are no marked bicycle facilities on either side of the road. There is approximately 590 feet of sidewalk on Club Drive adjacent to the site. Club Drive functions as a primary ring road that provides access along the north, west and east property boundaries.

#### **ROADWAY LEVEL OF SERVICE (LOS) INFORMATION**

Roadway	From	To	LOS Standard	LOS
Fowler Avenue	Florida Avenue	30 <sup>th</sup> Street	D	C
22 <sup>nd</sup> Street	Club Drive	Bearss Avenue	E	D

Sources: City of Tampa Level of Service Report, 2015.  
Hillsborough County Level of Service Report, 2020.

**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)**

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Fowler Ave.	FDOT Principal Arterial - Urban	8 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
22 <sup>nd</sup> St.	County Collector – Urban (North of Project) and City of Tampa Collector (South of Fowler Ave.)	2 Lanes <input checked="" type="checkbox"/> Substandard Road (County Portion) <input checked="" type="checkbox"/> Sufficient ROW Width (County Portion)	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
19 <sup>th</sup> St.	County Local – Urban (Also Partially within City of Tampa)	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other
University Square Dr.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Club Dr.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other

Project Trip Generation <input checked="" type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing			
Proposed			
Difference (+/-)			

\*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input checked="" type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		Choose an item.	Choose an item.	Choose an item.
South		Choose an item.	Choose an item.	Choose an item.
East		Choose an item.	Choose an item.	Choose an item.
West		Choose an item.	Choose an item.	Choose an item.
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

**CURRENTLY  
APPROVED**



Approval of the request, subject to the conditions listed, is based on the revised general site plan submitted November 24, 2020.

1. The Project shall be permitted to redevelop with any of the following uses:
  - All CG Uses (except sexually oriented uses, wastewater plants, recyclable material recovery facility).
  - Multi-Family Residential.
  - Community Residential Homes, Type C.
  - Convention Centers, Indoor Entertainment, Sporting Facilities and Amusement/Theme parks.
  - Outdoor events such as farmers market, concerts and temporary fairs/carnivals.
  - Large Scale Retail Development including Big Box Retailers (including Large Scale Building Supply and Do it Yourself Centers). The use shall be subject to the standards found in LDC Section 6.11.106. If the standards in 6.11.106 are in conflict with specific zoning conditions herein, the conditions shall apply.
  - Motion Picture Studio.
  - Research Uses (including biomedical research and labs).
  - Manufacturing, Processing, Assembly, Warehousing and Distribution of Pharmaceuticals, medical devices/equipment and all technology related products.
  - Warehousing, processing and distribution associated with a Food Bank.
2. The Project is located within the ICMU-35 mixed-use land use category that provides for a 2.0 FAR, 35 dwelling units per acre and allows use of FAR in lieu of density for residential projects at the developer's option. The project shall provide a minimum of two uses at all times. Should the development on site meet the following provisions, additional intensity and density is permitted pursuant to Objective 19 and Policies 19.1, 19.2 and 19.3 of the Future Land Use Element (FLUE):
  - If the project includes 3 or more land uses or vertically integrates two land uses, the project is entitled to a FAR Bonus of .50 FAR. If the .50 FAR bonus is granted, the entire site shall be regulated by FAR and not density.
  - Mixed-use projects of 3 or more land uses, can consider different housing types (multi-family, attached single family or detaches single family) as different uses.
- 2.1 The project shall maintain a minimum two uses (office and retail) upon commencement of redevelopment approved in this PD.
- 2.2 Parking structures shall not count towards the FAR.
- 2.3 Both FAR and densities and open space shall be calculated on the basis of the entire land area within the PD, including any land dedicated for right-of-way subject to approval of applicable dedication agreement(s) with the County. A tracking data table showing the existing and proposed uses including the intensity/density ratio calculations and open space shall be included on all preliminary site plan and/or construction plan submittals.
- 2.4 Open space requirements shall be calculated based on the limits of any portion of the PD submitted as a site plan for development or redevelopment. In order to provide opportunities for common open space and/or pedestrian connections throughout the PD, required open space can be provided either within the limits of that site plan or elsewhere within the PD.

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- The project shall provide a minimum of ten (10) percent open space within the overall PD area.
3. The following setbacks shall apply to the perimeter boundaries of the project unless otherwise noted on the site plan. No internal setbacks shall be required but development shall meet the building separation requirements of the Florida Building Code.
- 3.1 The setback from Fowler Avenue is 25 feet, 20 feet from all other property lines.
  - 3.2 The project is not subject to the 2 additional feet of building setback per 1 additional foot of building height over 20 feet found in the Land Development Code (LDC) Part 6.01.01 footnote 8.
  - 3.3 Buffer and screening along the perimeter of the project shall be in accordance with the Land Development Code (LDC) Part 6.06.06. Buffer and screening between uses internal to the project shall not be required.
4. The existing development is served by two offsite county drainage ponds and existing onsite development is almost completely impervious. Any additional construction within this site and/or required associated off-site improvements are to meet the criteria set forth within the Stormwater Management Technical Manual in effect at the time of the Construction Plan submittal.
5. To ensure the project develops as a walkable, integrated and connected mixed-use development, the following design principles shall be provided for all new development:
- 5.1 Location of roadways, street types and blocks shall generally conform to the layout as shown on the Sheet 1 of the PD General Site Plan. At each Preliminary Development Site Plan application for specific projects, the developer shall provide a general site plan depicting the subject site development plan within the context of the entire PD zoning district to demonstrate general conformance with the PD General Site Plan. Any deletion of a roadway, change of street type or reconfiguration of blocks that change the basic form of the blocks will require approval by the Board of County Commissioners in accordance with LDC Section 5.03.07.
  - 5.2 With the exception of Block 6, maximum block perimeters shall be 2,500 linear feet. Blocks shall be bounded on all sides by streets. An alley may be provided between blocks to allow access to an existing parking structure or between a new building and an existing structure.
  - 5.3 All internal streets shall be designated as Type A, B, C, D, E and F. Location and extension of the streets shall be as noted on the General Site Plan, Sheet 2. Design of the streets shall be as indicated on the Plan Sheet titled Typical Sections and Internal Connections. However, the component widths and the overall typical section widths shall be minimums and developer may increase the dimensions of the individual components or the total typical section at Developer's discretion. Where additional squares, civic spaces or hardscaped plazas are provided, on street parking adjacent to the squares, civic spaces or hardscaped plazas may be eliminated.
    - a. Concurrent with the improvement of Club Drive East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive East (Type F), in a location to be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive East (Type F), the developer shall grant a construction and access easement to the owners of folio 36282.0600, in order

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for the developer of that property to tie into the crosswalk identified above (at such time as the adjacent property may redevelop in the future).

- b. Concurrent with the improvement of that portion of Club Drive between Fowler Ave. and Club Dr. East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive in a location +/- 475 feet south of University Square Dr. or as otherwise approved by Hillsborough County. Location of the crosswalk shall be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive South of University Square Drive the developer shall grant a construction and access easement to the owners of folio(s) adjacent to such crosswalk, in order for the developer of those properties to tie into the sidewalk system along Club Dr. south of University Square Dr.
- 5.4 Maximum building height shall be 300 feet. No building shall exceed 100 feet within 60 feet of the project boundaries along the areas north, west and northwest as indicated in Sheet 5 of the General Site Plan.
- The proposed site falls within Zone "B" on the Airport Height Zoning Map. Any structure including construction equipment that exceeds 200 feet Above Ground Level may require an Airport Height Zoning Permit and must be reviewed by the Airport Zoning Director.
  - At the time of construction plan submittal, the developer shall provide a letter from the Airport Authority demonstrating compliance with the height restrictions requirements or applicable approvals for any height variances.
- 5.5 All new buildings along 'A' and 'B' Streets greater than 3,500 sq. ft. shall have an average height of 22.5 feet or greater within the setback zone, including parapets and architectural features.
- 5.6 Front setbacks for buildings along A Streets shall be 0 to 15 feet maximum and the entire length of each building façade facing the street shall be placed within the prescribed front yard setback area, up to a minimum average base height of 22.5' including architectural elements. Building facades/street walls above the minimum average base height are not subject to setback/build-to-line restrictions. Where buildings front onto a plaza or square, the setback area shall be measured from the edge of the additional provided open space. The plazas or squares shall be provided as publicly accessible amenities, consistent with condition 5.12.
- 5.7 Front setbacks for buildings along B Streets shall be 0 to 15 feet maximum and at least 50% of the length of each building façade facing the street shall be placed within the prescribed front yard setback area, up to a minimum average base height of 22.5' including architectural elements. Building facades/street walls above a minimum average base height of 22.5' are not subject to the setback/build-to-line restrictions. Where buildings front onto a plaza or square, the setback area shall be measured from the edge of the additional provided open space. The plazas or squares shall be provided as publicly accessible amenities, consistent with condition 5.12.
- 5.8 Minimum front setbacks for buildings along Type C, D, E and F Streets shall be 0 feet. Type C and Type D streets are not subject to maximum setbacks or building façade length percentages.

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- 5.9 A building's primary orientation shall be toward the street rather than the parking areas. New buildings shall have at least one entrance which is accessible by pedestrians from a street.
- 5.10 A main square or civic space, located within the "Central District", shall be at least 0.5 acres in size and bounded by at least two 'A' Streets. If streets do not bound the remaining sides of the square, pedestrian thoroughfares shall be provided connecting to the street network. All buildings adjacent to the square shall have frontages facing the square with uses for retail, office, civic, or institutional. All building facing the square shall provide retail, office, civic, institutional or cultural uses on the ground floors. The square shall include green space and place making elements and may include hardscapes, seating, pavilion(s) or other amenities available for public use and enjoyment. The main square may include retail kiosks, provided that the commercial footprint of the building is not counted toward the minimum area required for the main square.
- 5.11 The developer shall provide at least two (2) of the following place making elements (specialty signage, lighting, landscaping, building architecture, and/or public art) as an enhancing feature of open spaces within the Central District.
- 5.12 Additional squares or hardscaped plazas ("Open Spaces") may be provided throughout the site, as publicly accessible amenities. Such Open Spaces may contain landscape elements, site furnishings, commercial kiosks, and/or outdoor seating which may be associated with neighboring eating establishment uses. Commercial kiosks shall not be subject to the build to line setback requirement of adjacent blocks and may be located anywhere on the Open Space, subject to condition 5.19. On street parking abutting such use may be eliminated per condition 5.3.
- 5.13 Blank walls, defined as walls with no transparency, mural, public art, or architectural enhancements/expressions, shall not occupy more than 50% of a street-facing frontage. Such expressions and enhancements include features such as pilasters, columns, canopies/porticos, arcades or colonnades and shall have a minimum projection or recess of two inches from the wall surface. No more than 40 feet of horizontal distance of wall shall be provided without architectural expressions.
- 5.14 On 'A' Streets, ground floor uses shall contain transparent glass windows or entrances covering a minimum of 50% of a building's linear frontage measured between 3 and 12 feet above sidewalk elevation, and blank walls shall not exceed 30 linear feet without being interrupted by a window, entry, or architectural expression.
- 5.15 On 'A' Streets, loading areas shall be enclosed by architectural screen or operable doors. Loading doors shall be closed when not in operation.
- 5.16 On 'A' Streets, parking lots shall be placed behind buildings. On 'B' Streets, parking lots shall be permitted to be located on the rear or side of the buildings provided that any permanent surface parking areas along 'B' Streets is screened by a 42-inch high wall or a hedge a minimum 42 inches high and a maximum height of 6 feet. Surface parking will comprise no more than 75% of a block once redeveloped.
- 5.17 For new parking structures abutting streets, all walls, including the rear wall, shall conform with the façade requirements above, except that wall openings may be provided in lieu of windows. All parking garages that are open to the public shall provide direct pedestrian access from the parking



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levels of the structure to each street it faces shall be provided, except where separated from the street by another building or parking lot.

- 5.18 Tables and/or graphic diagrams demonstrating the facades requirements for the percentages above shall be provided during the site construction permitting for review by Development Services Department staff.
- 5.19 Sidewalk widths shall be a minimum of 5 feet throughout the site, and a minimum of 6 feet on active frontages. Notwithstanding, sidewalk width and design is subject to the Typical Cross Sections noted on Plan Sheet 3 of 5 and condition 5.3.
- 5.20 Gateways shall mark the arrival into the district at key intersections. These are entrance corridors that herald the approach of a new landscape and defines the arrival point as a destination. A minimum of four gateways shall be provided: One (1) to the north, one (1) to the east and two (2) to the south. In order to provide a transition that clearly indicates entrance to the project, each Gateway shall include features or architectural elements such as: change of paving materials, vertical or horizontal signage, arches, obelisks, planters/landscaping, monuments, public art or other similar treatments.
6. The Applicant will coordinate with Hillsborough County and Florida Department of Transportation (FDOT) staff to conduct a traffic analysis as each block located within the PD as depicted on sheet 1 of 5 (each a "Block") is developed in order to assess the currently proposed and cumulative AM peak- hour and PM peak-hour traffic impact at each project access. Coordination between the Applicant, Hillsborough County, and FDOT is necessary in order for the Applicant to provide an analysis that evaluates the access connections to determine how each connection will function at the time each Block is fully developed, while taking into consideration the future long-range plans for Fowler Avenue.
- Construction approvals for each Block will require a Traffic Monitoring Report analyzing the specific development level (land use and scale). The Traffic Monitoring Report will analyze the operations of the major access locations to Uptown during the AM and PM peak-hours, as indicated during observations of the existing access locations. The Traffic Monitoring Report for each Block approval will include existing data (to identify existing traffic conditions), the projection of anticipated project traffic for the proposed Block (as agreed to with Hillsborough County) at its buildout year, and the future planned improvements along Fowler Avenue that are anticipated to be constructed or implemented at the Block's buildout year. Pending the results of the analysis, site access/operational improvements, median modifications or additional signalization may be required based upon review by Hillsborough County and the FDOT.
7. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, alternative intersection designs (i.e. roundabouts) may be used internal to the project. Utilization of such alternative designs shall not require a PD modification.
8. Notwithstanding anything herein these conditions or on the PD Plan to the contrary, bicycle and pedestrian access may be permitted anywhere along the PD boundaries.
9. The developer shall provide the following transit facilities:

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- a. A Major Transit Stop (MTS) within the general area designated for such purpose on the PD site plan. The MTS shall serve as a transit stop for both the Hillsborough Area Regional Transit Authority (HART) and University of South Florida (USF) Bullrunner service. Additionally, the staging area shall accommodate two (2) transit vehicles within dedicated bus bays or within a driveway outside of the path of general vehicular circulation. This transit stop and required amenities shall be provided with the initial increment of redevelopment.
  - b. The developer shall provide, within 500 feet of the intersection of Fowler Ave. and 22nd St., a separate bus stop with loading/unloading within dedicated bus bays or within a driveway outside of the path of general vehicular circulation. This transit stop and required amenities shall be constructed and operating no later than December 31, 2025 or concurrent with the initial increment of (re)development within blocks 10, 11, 14, or 15, whichever occurs earlier.
  - c. Two (2) additional bus stops shall be provided within the project, serving either HART and/or USF Bullrunner. These stops may be located anywhere within the project, subject to approval of the transit agencies serving that stop. These stops and required amenities shall be constructed and operating no later than December 31, 2027 or concurrent with the first increment of (re)development within the last block (re)developed after approval of this rezoning, whichever occurs earlier.
  - d. If bus bays are provided, each bay shall meet the minimum design standards within the latest edition of the "Accessing Transit, Design Handbook for Florida Bus Passenger Facilities", published by the Florida Department of Transportation, and shall be approved by HART.
  - e. At each of the above required transit facilities the developer shall provide a bus shelter and benches suitable for waiting out of inclement weather. Alternatively, seating may be provided under a building or other architectural feature of the project (which provides equivalent or greater protection from the elements), as long as the seating area is located within 100 feet of the transit stop. Transit accessory pads, trash receptacles and bicycle racks shall be provided at each facility. Location and design of the transit stop and amenities shall be subject to approval of Hillsborough County and the respective transit agencies served by the facility. Prior to approval of the applicable site/construction plan submittal whereby the transit facility within a certain block is required, the developer shall provide written documentation indicating the transit agency or agencies using the facilities have approved their design and location.
10. Upon the re-development of Block 9 or Block 13 (not including reuse of existing buildings or shells) along the western PD boundary, the developer shall provide a stub out within the area located on the site plan as the "Western Cross Access Area" to the N 19th Street right-of-way.
  11. Upon complete redevelopment of the Project, the developer shall provide the County with a perpetual easement agreement benefitting the public for the purpose of traversing the Project from the eastern boundary of the Project to the western boundary. The location of the easement area shall be limited to private roadways within the Project (running east to west across the Project but not necessarily in a straight line or linear path) in a location determined solely by the developer and as may be changed by developer from time to time. The easement agreement, among other things, shall provide the following terms and conditions:

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- 11.1 Developer reserves the right to eject any individual(s), groups or entities performing any unlawful act(s) within the easement area;
  - 11.2 Developer may temporarily suspend use of the easement area for special events, festivals or private events that are hosted on the Project;
  - 11.3 Developer may temporarily suspend use of the easement area in order to perform maintenance, repairs or replacements on or to the easement area;
  - 11.4 Developer may relocate the easement area provided the new easement area runs from the eastern edge to the western edge of the Project but not necessarily in a straight line or linear path.

For purposes of this condition, “complete redevelopment” shall mean that (i) the easement area selected by developer running from the eastern boundary of the property to the western boundary of the property shall be completely reconstructed as shown on the adopted Planned Development site plan (as may be amended from time to time) and (ii) all blocks adjacent to the easement area (on both the north and the south) shall be fully constructed and shall have received a certificate of occupancy or final inspections, as applicable (including buildings, internal drives and related improvements) within each block.

- 12. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, no on-site development shall occur inconsistent with any recorded public easements within the site.
- 13. Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
- 14. The construction and location of any proposed wetland impacts are not approved by this correspondence, but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.
- 15. Prior to the issuance of any building or land alteration permits or other development, the approved wetland/other surface water (OSW) line must be incorporated into the site plan. The wetland/ OSW line must appear on all site plans, labeled as “EPC Wetland Line”, and the wetland must be labeled as “Wetland Conservation Area” pursuant to the Hillsborough County Land Development Code (LDC).
- 16. Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.
- 17. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.

FINAL CONDITIONS  
OF APPROVAL

PETITION NUMBER: PRS 20-1313  
MEETING DATE: December 8, 2020  
DATE TYPED: December 21, 2020

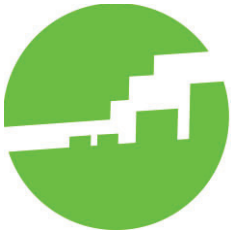
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18. The Development of the project shall proceed in strict accordance with the terms and conditions contained in the General Site Plan, zoning conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
19. Within 90 days of rezoning approval of PRS 20-1313 by the Hillsborough County Board of County Commissioners, the developer shall submit to the County Development Services Department a revised General Development Plan for certification reflecting all the conditions outlined above.





# **AGENCY COMMENTS**



**Hillsborough County  
City-County  
Planning Commission**

Memorandum

January 25, 2022

To: Brian Grady, Hillsborough County Development Services Department

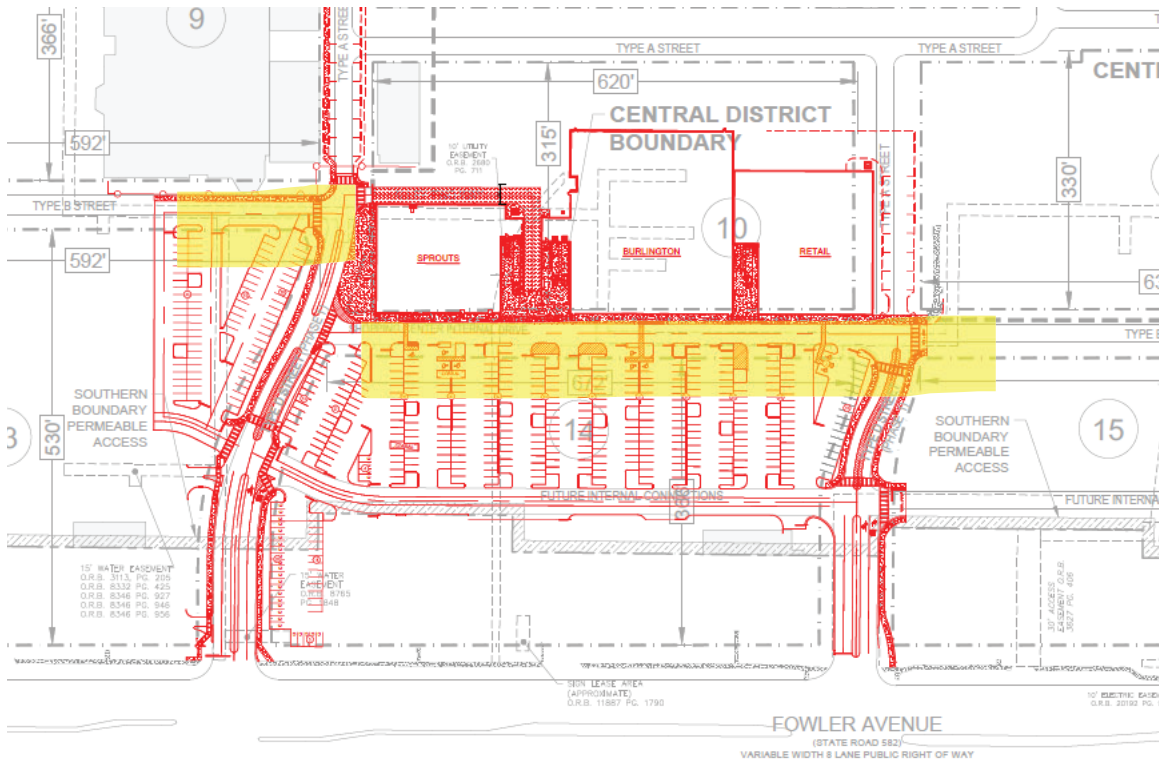
From: Melissa Lienhard, Hillsborough County City-County Planning Commission

RE: PRS 22-0028

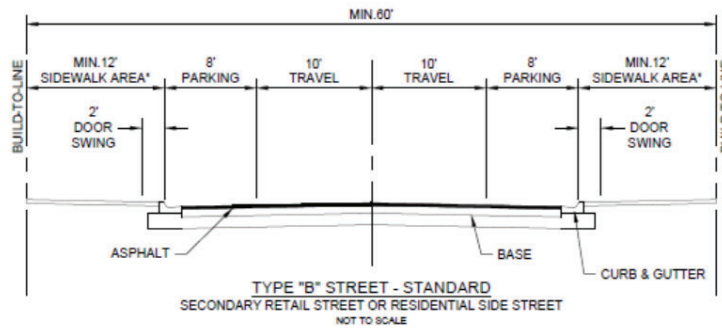
The applicant is requesting a Personal Appearance/Minor Modification (PRS) to PD 19-1443. The request, based upon the Site Plan dated 01-25-2022 in Optix, is as follows:

- Add a Type D Phase 1 typical section;
- Modify the street type in front of a planned retail area to “Shopping Center Internal Drive” and add a “Shopping Center Internal Drive” typical section;
- Elimination of the “Type D Road – Alternate” typical section;
- Modify the site plan to allow surface parking as shown on sheet 6 of 6 until such time Block 14 is redeveloped in accordance with the PD standards;
- Revise the Conditions of Approval to identify when a “Type D Street – Phase 1” typical section is required to convert to a “Type D Street”;
- Revise the Conditions of Approval to identify when “Shopping Center Retail Drive” is required to convert to a “Type B Street”;
- Add a “Type D Street – Phase 1” typical section along the east and west boundaries of Block 14;
- Modify the boundaries of Blocks 10 and 14;
- Modify the Central District boundary to reflect the boundaries of the planned retail area;
- Remove the label on 19th Street indicating Type B Street; and
- Revise Sheet 2 of 6 to reflect the PD variation requested (waiving the parking area landscape buffer from 8 feet to 0 feet along the Type D Phase 1 driveways until such time as the Type D typical section is required).

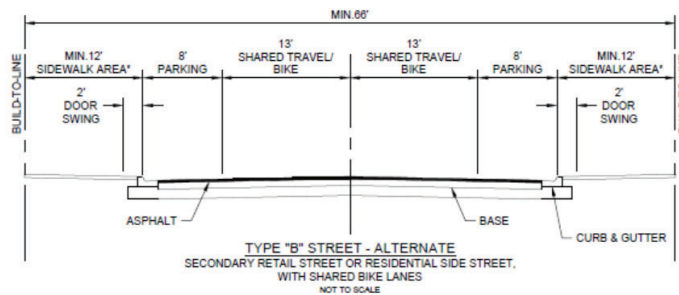
The request for PRS 22-0028 includes proposed modifications to the street type on the south side of Block 10 in front of the proposed Sprouts, Burlington, and Retail buildings, as shown here:



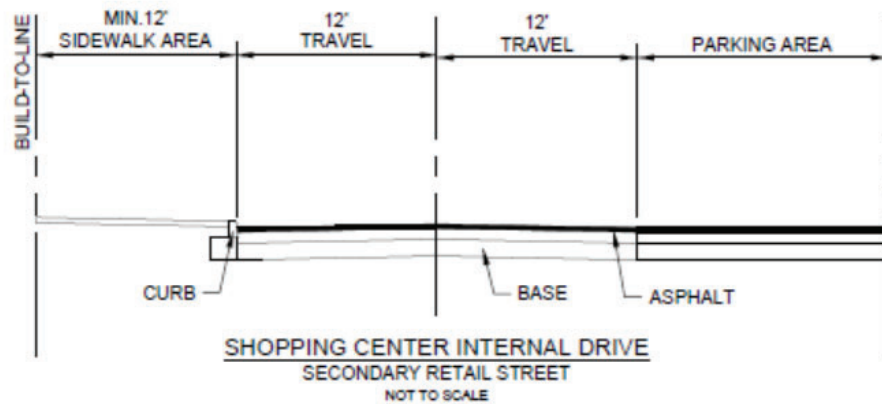
The original approval (PD 19-1443) showed this area as a “Type B Street.” The approved “Type B Street – Standard” cross-section is as follows:



The approved “Type B Street – Alternate” is as follows:

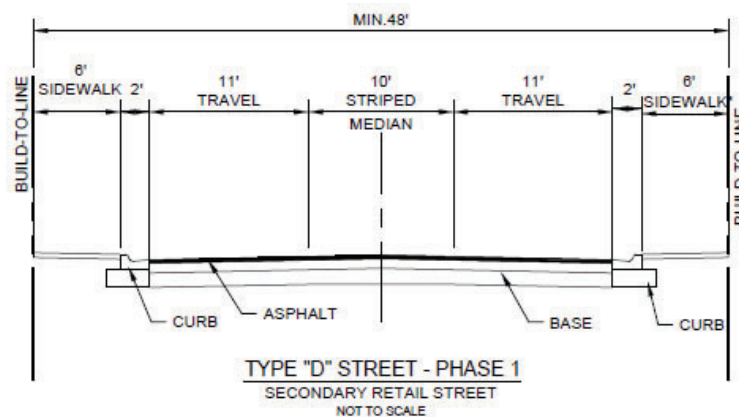


The applicant is proposing to change the “Type B Street” in front of this retail area to a “Shopping Center Internal Drive,” which includes the following proposed cross-section:



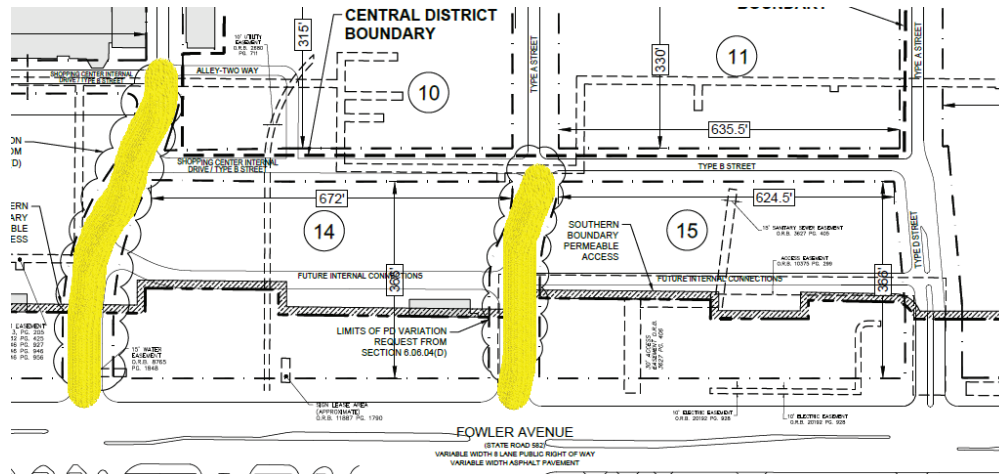
While the “Shopping Center Internal Drive” includes a minimum of 12 feet as a sidewalk area in front of the building, the cross-section shows the removal of the reciprocal 12-foot sidewalk on the other side of the cross-section. “Shopping Center Internal Drive” also eliminates the 8-foot on-street parking lanes shown on both sides of the travel lanes in both the “Type B Street – Standard” and the “Type B Street – Alternate” approved cross-sections. The proposed “Shopping Center Internal Drive” cross-section increases the two travel lanes from 10 feet to 12 feet each. The proposed changes are drastically different than the original approval by eliminating one side of the sidewalk, eliminating on-street parking, and increasing the travel lane widths. Overall, the proposed changes maintain the large surface parking areas that already accommodate automobile users and then removes amenities for pedestrians and bicyclists, resulting in safety concerns for multimodal users.

The applicant is also proposing to add a “Type D Street – Phase 1” cross-section, which is shown as follows:

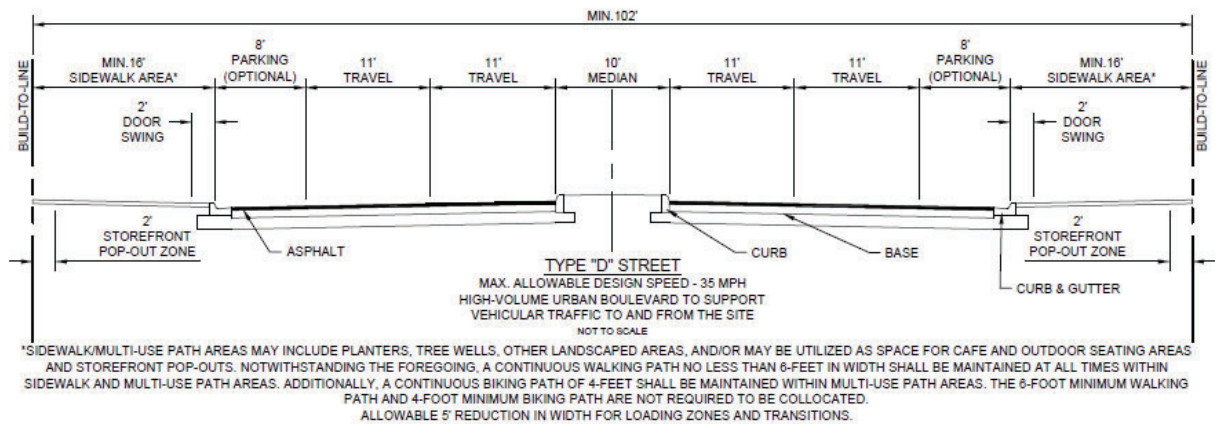


The “Type D Street – Phase 1” cross-section is proposed for two of the entrances to the site, as shown below.





The approved “Type D Street” is as follows:



As shown above, the new proposed “Type D Street – Phase 1” proposes to eliminate the two 16-foot sidewalk areas, replacing them with a 6-foot sidewalk on both sides of the street. In addition, the optional on-street parking has been removed from the cross-section, the 10-foot raised median has been replaced with an at-grade 10-foot striped median, and the number of travel lanes has been reduced from two in each direction to one in each direction.

Planning Commission staff notes that the proposed changes to the street types mentioned above are moving away from the intent of the Innovation Corridor Mixed Use-35 (ICMU-35) Future Land Use category. The ICMU-35 category was created through two privately initiated Comprehensive Plan Amendments (HC/CPA 18-21 and HC/CPA 18-22) that were approved in April of 2019. Per the ICMU-35 category description included in Appendix A of the Future Land Use Element, developments in the ICMU-35 category should reflect elements of mixed-use design such as pedestrian, bicycle, and roadway connectivity, be pedestrian-oriented and walkable and have sensitivity to the roadway corridor presence. Design changes such as the elimination of sidewalks, reduction of sidewalk widths, elimination of on-street parking, and the widening of drive aisles are reflective of more of standard strip commercial, which is prohibited in this category, and do not meet the intent of the land use category as described.

Furthermore, the demographic analysis recently conducted as part of Plan Hillsborough's Nondiscrimination and Equity Plan notes that the University Area has a high concentration of racial and low income populations and is one of the areas with the highest percentage of zero vehicle households. Walking, cycling and transit are lifelines for the community's mobility needs. Historically, land use and transportation decisions have been based on automobile level of service, which is one of the reasons why the State of Florida and Hillsborough County lead the country in traffic related fatalities. A 20-35 miles per hour speed is preferable, as speed is the critical factor in a person surviving a crash. The Transportation Planning Organization (TPO) Speed Management Action Plan was consulted as part of our best practices research. Transportation Element Policy 1.3.2 includes language about improving high vehicular traffic areas by better physical design for bicycle and pedestrian safety and significantly reducing the number of crashes.

In addition to speed, connectivity of walking and cycling facilities makes using those modes and transit comfortable and safe. The proposed "Type D Street – Phase 1" cross-section will not provide a place for cycling and requires a pedestrian to utilize a 6-foot sidewalk adjacent to an 11-foot travel lane with only a 2-foot raised curb to separate a pedestrian from the vehicle travel lane. The applicant has not demonstrated how the proposed changes create a unified and safe pedestrian network, which is inconsistent with policy direction in the Comprehensive Plan regarding pedestrian safety (Community Design Component Objective 15-2 and Transportation Element Policy 3.1.3).

Fowler Avenue currently has bike lanes and sidewalks. The Florida Department of Transportation (FDOT) is currently completing a multimodal plan for Fowler Avenue in recognition of the many residents using transit and foot travel in this corridor. If approved, "Type D Street – Phase 1" would be the connection from Fowler Avenue for customers, residents, and employees traveling to the University Mall property for the foreseeable future. The originally approved "Type D Street," which includes generous sidewalk areas on both sides of the street, optional on-street parking and two travel lanes in each direction with a 10-foot raised median, takes into account the safety and connectivity originally envisioned by the ICMU-35 Future Land Use category.

"Type D Street – Phase 1" and "Shopping Center Internal Drive" are not multimodal nor transit oriented in design and therefore are inconsistent with the intent of the ICMU-35 Future Land Use category. The proposed timing mechanism for "Type D Street – Phase 1", as outlined in proposed conditions in 5.3d, is the earlier of the two following options: (1) when new vertical square footage is added to any block adjacent to a "Type D – Phase 1" street; or (2) when the Traffic Monitoring Report indicates that the existing traffic plus new project traffic is anticipated to exceed the two-way peak-hour Level of Service (LOS) D threshold for a two-lane divided roadway. While the applicant has submitted supplemental information entitled "Type D Phase I Cross Section to Type D Cross Section Monitoring" dated 01-24-2022 that attempts to describe the proposed condition in more detail while also providing some examples of development that would render the "Type D Street" in effect, the proposed timing mechanism does not alleviate the concern that the "Type D Street- Phase 1" design could remain for an extended period of time. The timing mechanism does not give Planning Commission staff a comfort level that would render the proposed minor modifications consistent with the ICMU-35 Future Land Use category and policy direction cited below regarding pedestrian and bicycle connectivity and foster a pedestrian-oriented and walkable urban form.

According to the proposed condition 5.21, “Shopping Center Retail Drive” would only be re-constructed as a “Type B Street” when either Block 14 (the block directly south of the proposed Burlington and Sprouts area) develops or when the southern face of Block 10 (the Burlington and Sprouts block) is redeveloped with new vertical square footage. Again, the timing of either of these options is uncertain and unpredictable. East-west pedestrian connectivity is lost by replacing the originally approved “Type B Street” with the new “Shopping Center Retail Drive” typical section. While this typical section does include a 12-foot sidewalk area directly in front of the proposed storefronts, this area will accommodate customers coming in and out of stores in addition to truck traffic, as shown on Sheet 6 of 6 in the most recent submittal. Not having a dedicated area for pedestrians, as shown in the “Type B Street – Standard” typical section, or a decided area for pedestrians and bicyclists, as shown in “Type B Street – Alternate” typical section, would significantly decrease pedestrian and bicyclist safety as well as connectivity, which is inconsistent with Comprehensive Plan policy direction as cited in Transportation Element Policies 1.3.2, 3.1.3 and 3.2.1.

The proposed range of uses and the relocation of the uses onsite meet the intent of the ICMU-35 category by providing for a mix of uses. The basis for the inconsistency finding is founded on the proposed changes in cross sections and the conditions related to pedestrian safety and connectivity. The proposed modifications regarding site design are not consistent with the ICMU’s intent to provide for pedestrian and bicycle connectivity, pedestrian-oriented development or roadway corridor sensitivity and presence. The proposed changes align with a traditional suburban development and do not foster opportunities to live, work and play as the ICMU-35 envisions.

The request has been found **INCONSISTENT** with the *Future of Hillsborough Comprehensive Plan for Unincorporated Hillsborough County*. This request would be in direct conflict with the ICMU-35 Future Land Use category and the University Area Community Plan vision. The following Goals, Objectives and Policies of the *Future of Hillsborough Comprehensive Plan for Unincorporated Hillsborough County* apply to this Personal Appearance/Minor Modification request (PRS 22-0028) and are used as a basis for an inconsistency finding:

## **TRANSPORTATION ELEMENT**

***Policy 1.3.2:*** Continue to program improvements for high motor vehicle, bicycle and pedestrian crash locations where better physical design or improved traffic controls can be expected to cause a significant reduction in the number of crashes.

***Policy 3.1.3:*** Enforce regulations requiring private developers to include bicycle facilities/trails and sidewalks in private developments to promote bicycle/pedestrian connections, in accordance with adopted policies and standards.

***Policy 3.2.1:*** Continue to apply existing standards to include bicycle lanes and sidewalks as part of the typical section for roadway designs, where appropriate.

## **FUTURE LAND USE ELEMENT**

***Policy 19.2:*** In the mixed use land use categories, when two or more uses are required on the same project, then the development shall be implemented through a zoning district that demonstrates street connectivity, description of land uses, and site placement, access locations and internal connections at a minimum.

## **COMMUNITY DESIGN COMPONENT**

### **6.0 ROADWAY LEVEL DESIGN**

#### **6.2 PEDESTRIAN NETWORK**

**OBJECTIVE 15-1:** *Provide for pedestrian needs in the design of new and existing roadways.*

**15-1.1:** *Design pedestrian facilities for designated roadways in urban and suburban areas to include the following considerations:*

- *Continuous sidewalks, free of obstruction, buffered from traffic by landscape or setbacks, and shaded by awnings or trees where appropriate.*

**15-1.3:** *Locate sidewalks along both sides of all public streets and in all residential areas, particularly near schools and adjacent to parks or recreational facilities, office buildings, local businesses and other similar locations that attract high amounts of pedestrian activity.*

#### **6.3 PEDESTRIAN CROSSINGS**

**OBJECTIVE 15-2:** *Provide safe and convenient roadway crossings that are easily identifiable by pedestrians and motorists.*

#### **6.4 PEDESTRIAN AMENITIES**

**OBJECTIVE 15-3:** *Provide pedestrian amenities along roadways, sidewalks, and bike paths to increase the comfort, safety, and security of users.*

### **Appendix A: Innovation Corridor Mixed Use-35 (ICMU-35)**

*The ICMU-35 category shall form a regional activity center which incorporates internal road systems, building clustering and mixing of uses. The category intends to foster opportunities for live, work and play developments. This category shall be made available for consideration along segments of those major corridors within 1.5 miles of the University of South Florida main campus, with the intent of providing increased development opportunities. The ICMU-35 category should be surrounded by other urban level plan categories and be located along fixed-guideway and major transit lines.*

*Developments should reflect elements of mixed-use design such as:*

- *Pedestrian, bicycle and roadway connectivity*
- *Horizontal and/or vertical integration of uses*
- *Pedestrian oriented/walkable*
- *Roadway corridor sensitivity and presence*

### **LIVABLE COMMUNITIES ELEMENT: University Area Community Plan**

#### **Goal 1: Build Community Infrastructure**

*The community supports:*



- *implementing traffic calming techniques including traffic islands and roundabouts, where appropriate,*
- *creating a network of boulevards, main streets, and pedestrian links (sidewalks/trails) to facilitate mobility and non-motorized travel within the University Area Community;*

### **Goal 2: Eliminate Obsolete Land Uses**

- *maintaining a variety of land-use options and promotion mixed-use developments in the area;*
- *implementing the University Area Community Main Street and Neighborhood Office zoning districts to create stable, pedestrian friendly neighborhoods with compatible mixed uses, a variety of housing types, and tree lined main streets and boulevards;*
- *providing greater flexibility in land-use and density for future development while ensuring that neighborhood buffers and urban amenities are enhanced;*
- *creating unified pedestrian links between land uses by means of community Main Streets and sidewalks;*
- *encouraging the redevelopment of sub-standard and deteriorated housing focused along main streets through mixed land uses and higher densities; and*

### **Goal 3: Create Community Identity**

*The community supports:*

- *developing physical improvements such as landscaping, gateways, sidewalks, signage, and street trees to define community identity;*
- *using landscape buffers and urban design features/techniques to reduce conflicts between land uses, to promote high quality development, and to create an identifiable community character;*

## **ECONOMICALLY DISADVANTAGED GROUPS ELEMENT**

*Without the provision of an efficient transportation system, those who do not have access to private transportation cannot fully participate in society-employment, recreation, education. (descriptive text, page 4)*

*Access to transportation is another aspect that needs to be addressed when analyzing the needs of the economically disadvantaged. (descriptive text, page 14)*

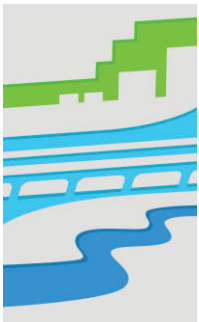
### **Transportation System Provision**

***Policy 5.7:*** *The County, understanding that mass transit serves the dual purpose of relieving overburdened road systems and providing access to the various opportunities of the community to the economically disadvantaged, shall coordinate with HART to integrate, where appropriate and economically feasible, mass transit amenities into future capital improvement projects. The amenities shall include, but not be limited to, roads and sidewalks, and shall be integrated in facilities such as schools, public offices, public housing, parks and recreation facilities.*

If you have any questions regarding these comments or need further information, please contact me at (813) 547-4364.

Sincerely,

Melissa Lienhard, AICP  
Executive Planner



**Plan Hillsborough**  
[planhillsborough.org](http://planhillsborough.org)  
[planner@plancom.org](mailto:planner@plancom.org)  
813- 272-5940  
601 E Kennedy Blvd  
18<sup>th</sup> Floor  
Tampa, FL, 33602

## AGENCY REVIEW COMMENT SHEET

TO: Zoning Technician, Development Services Department

DATE: 1/28/2022

REVIEWERS: Richard Perez, AICP, Executive Planner

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: USF/ Northwest

PETITION NO: RZ 20-0028

- ☐ This agency has no comments.
- ☐ This agency has no objection.
- ☒ This agency has no objection, subject to listed or attached conditions.
- ☐ This agency objects, based on the listed or attached conditions.

### NEW AND MODIFIED CONDITIONS OF APPROVAL

#### Modified Conditions

- 5.3 All internal streets shall be designated as Type A, B, C, D, E and F. Location and extension of the streets shall be as noted on the PD General Site Plan, Sheet 21 of 6. Design of the streets shall be as indicated on ~~the Plan-PD General Site Plan, Sheet 2 of 6 titled Typical Sections and Internal Connections.~~ However, the component widths and the overall typical section widths shall be minimums and developer may increase the dimensions of the individual components or the total typical section at Developer's discretion. Where additional squares, civic spaces or hardscaped plazas are provided, on street parking adjacent to the squares, civic spaces or hardscaped plazas may be eliminated.
- a. Concurrent with the improvement of Club Drive East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive East (Type F), in a location to be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive East (Type F), the developer shall grant a construction and access easement to the owners of folio 36282.0600, in order for the developer of that property to tie into the crosswalk identified above (at such time as the adjacent property may redevelop in the future).
- b. Concurrent with the improvement of that portion of Club Drive between Fowler Ave. and Club Dr. East (Type F), the developer shall construct a mid-block pedestrian crossing across Club Drive in a location +/- 475 feet south of University Square Dr. or as otherwise approved by Hillsborough County. Location of the crosswalk shall be reviewed and approved at the time of plat/site/construction plan review. Prior to or concurrent with construction plan approval for Club Drive South of University Square Drive the developer shall grant a construction and access easement to the owners of folio(s) adjacent to such crosswalk, in order for the developer of those properties to tie into the sidewalk system along Club Dr. south of University Square Dr.
- c. To the west of blocks 10 and 14, and to the east of block 14, where a Type D typical section is provided for on the PD General Site Plan, the typical section may be

constructed in phases in accordance with the Type D Phase 1 typical section provided for on PD General Site Plan sheet 2 of 6.

- d. Notwithstanding the interim improvements envisioned to support the increment of development shown on PD General Site Plan Sheet 6 of 6, or as otherwise provided for herein these conditions, upon the earlier of:

(i.) such time as the blocks adjacent to a Type D Phase 1 typical section is further developed or redeveloped) with vertical square footage (to also include a change of use), the Type D Phase 1 typical section shall be replaced with the Type D Street typical section and constructed in accordance with the Type D Street typical section on Sheet 2 of 6; or,

(ii.) such time as the Traffic Monitoring Report indicates that existing plus project traffic is anticipated to exceed the two-way peak-hour Level of Service (LOS) D threshold for a two-lane divided roadway (per Table 4 of the Florida Department of Transportation Quality/Level of Service Handbook) (for development on any block within the PD), the Type D Phase 1 typical section set forth in Condition 5.3.c shall be replaced with the Type D typical section and constructed in accordance with the Type D Street typical section on PD General Site Plan Sheet 2 of 6. For example, if the typical section between blocks 13 and 14 meets the thresholds set forth in this condition 5.3.d, the developer would only have to replace the typical section in that location and shall have no obligation to replace the typical section in other areas until that section independently meets the thresholds set forth herein.

6. The Applicant will coordinate with Hillsborough County and Florida Department of Transportation (FDOT) staff to conduct a traffic analysis as each block located within the PD as depicted on PD General Site Plan Sheet 1 of 6 (each a "Block") is developed in order to assess the currently proposed and cumulative AM peak-hour and PM peak-hour traffic impact at each project access. Coordination between the Applicant, Hillsborough County, and FDOT is necessary in order for the Applicant to provide an analysis that evaluates the access connections to determine how each connection will function at the time each Block is fully developed, while taking into consideration the future long-range plans for Fowler Avenue.

Construction approvals for each Block will require a Traffic Monitoring Report analyzing the specific development level (land use and scale). The Traffic Monitoring Report will analyze the operations of the major access locations to Uptown during the AM and PM peak-hours, as indicated during observations of the existing access locations. The Traffic Monitoring Report for each Block approval will include existing data (to identify existing traffic conditions), the projection of anticipated project traffic for the proposed Block (as agreed to with Hillsborough County) at its buildout year, and the future planned improvements along Fowler Avenue that are anticipated to be constructed or implemented at the Block's buildout year. Pending the results of the analysis, site access/operational improvements, median modifications or additional signalization may be required based upon review by Hillsborough County and the FDOT.

#### New Conditions

- 5.21 Notwithstanding any other provision of condition 5 to the contrary, the New Retail Area (PRS 22-0028) depicted on PD General Site Plan Sheet 6 of 6 may be developed with the Shopping Center Internal Drive Typical Section (not a Type B Street) and surface parking lot as shown on PD General Site Plan Sheets 2 of 6 and 6 of 6. Notwithstanding the above, at such time as any of the following occurs the developer shall complete the specified action:



- a. If the portion of adjacent Block 9 and 13 is developed or redeveloped (to include a change of use), the Shopping Center Internal Drive between Block 9 and 13 shall be replaced and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place;
- b. If the retail area within Block 10 as shown on PD General Site Plan Sheet 6 of 6, is further developed or redeveloped (to include a change of use) subsequent to the development authorized via PD Site Plan Sheet 6 of 6, then the developer shall replace the Shopping Center Internal Drive between Block 10 and 14 and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place; and,
- c. If Block 14 is developed or redeveloped (to include a change of use) subsequent to construction of the parking field authorized via PD Site Plan Sheet 6 of 6, then the developer shall replace the Shopping Center Internal Drive between Block 10 and 14 and the full Type B Street -Standard Typical Section or Type B Street – Alternate Typical Section shall be constructed in its place.

5.22 Notwithstanding any other provision of condition 5 to the contrary, no sidewalk shall be required along the western side of the that portion of the Type D roadway aligning with 22<sup>nd</sup> St., between Fowler Ave. and the southern PD boundary, to support the increment of development shown on the PD General Site Plan Sheet 6 of 6.

#### **PROJECT SUMMARY AND CONCLUSIONS**

The applicant is requesting a Minor Modification (PRS) to modify a +/- 83.63-acre parcel currently approved as Planned Development (PD) #19-1443, as most recently amended via PRS 20-1313. The site is currently a retail shopping mall with several outparcels and a movie theater. The site was originally developed with 1.4 million square feet of retail uses constructed on the property. The proposed PD excludes a 6.58 ac. section of retail uses (the Burlington Coat Factory).

As currently approved, the developer is permitted to develop at a 2.0 floor area ratio (FAR), or potentially a 2.5 FAR with available bonus, for the following permitted uses.

- All CG uses (except sexually oriented uses, wastewater plants, recyclable material recovery facility);
- Multi-Family Residential;
- Community Residential Homes, Type C;
- Convention Centers, Indoor Entertainment, Sporting Facilities and Amusement/Theme Parks;
- Outdoor Events such as Farmers Market, Concerts and Temporary Fairs/Carnivals;
- Large Scale Retail Development including Big Box Retailers (including Large Scale Building Supply and Do it Yourself Centers);
- Motion Picture Studio;
- Research Uses (including biomedical research and labs);
- Manufacturing, Processing, Assembly, Warehousing and Distribution of Pharmaceuticals, Medical Devices/Equipment and All Technology Related Products; and,
- Warehousing, Processing and Distribution associated with a Food Bank

No changes to approved entitlements or site access are proposed with this PRS. The applicant is proposing the following changes:

1. Elimination of the "Type 'D' Road – Alternate" Typical Section;
2. Addition of the "Shopping Center Internal Drive" Typical Section;
3. Addition of the "Type 'D' Street – Phase 1" Typical Section;
4. Modification of the PD site plan to allow the new "Shopping Center Internal Drive" typical section along that portion of the road along the entire southern boundary of Block 10 (and northern boundary of Block 14), as well as addition to a portion of the northern boundary of Block 13 (southern boundary of Block 9);
5. Addition of Sheet 6 of 6 "New Retail Area Plan", which is intended to be an implementing plan for near term redevelopment within a portion of Block 10, with associated surface parking within Block 14 and a portion of Block 13; and,
6. Various modifications of existing approved zoning conditions in order to facilitate a phased development approach for Type D and Shopping Center Internal Drive/Type B Streets.

### Typical Sections Changes

The applicant is proposing to add a new "Shopping Center Internal Drive" Typical section, eliminate the Type "D" Alternate Typical Section, and is proposing a new Type "D" Phase 1 Typical Section. Staff has prepared a summary of the various features (in table format to aid comparison) for the typical sections referenced in the above described changes.

Type B Related	Developer Action/ Typical Section	Lane Width (ft.) <sup>1</sup>	ROW Width (ft.)	Sidewalk Width (ft.)	Accommodation for On Street Parking	Accommodation for Bicycle Traffic	Accommodation for Café /Retail Pop Outs/Landscaping <sup>4</sup>
	<b>Maintained Typical Sections</b>						
	Type "B" - Standard	10	60	12 (both sides)	Yes (both sides)	No	Yes
	Type "B" - Alternate	13 <sup>2</sup>	66	12 (both sides)	Yes (both sides)	Yes	Yes
	<b>New Typical Sections</b>						
	Shopping Center Internal Drive	12	36 <sup>3</sup>	12 (one side)	No	No	No

Type D Related	Developer Action/ Typical Section	Lane Width (ft.) <sup>1</sup>	ROW Width (ft.)	Sidewalk Width (ft.)	Accommodation for On Street Parking	Accommodation for Bicycle Traffic	Accommodation for Café /Retail Pop Outs/Landscaping <sup>4</sup>
	<b>Maintained Typical Sections</b>						
	Type "D" - Standard	11	102	16 (both sides)	Yes (both sides)	No	Yes
	<b>New Typical Sections</b>						
	Type "D" - Phase 1	11	48	6 (both sides) <sup>5</sup>	No	No	No
	<b>Eliminated Typical Section</b>						
	Type "D" - Alternate	11	102	22 (both sides) <sup>6</sup>	No	Yes <sup>6</sup>	Yes

**General Note:** Only Typical Sections which are proposed for modification (or related to a proposed modification) are listed.

#### Footnotes:

<sup>1</sup> Can be widened at any time at developer's option pursuant to approved zoning conditions

<sup>2</sup> Travel lanes include additional width for shared bicycle travel

<sup>3</sup> Right-of-way width is not specified on the proposed Typical Section. Value represents an approximate envelope of improvements.

<sup>4</sup> In sidewalk or multi-purpose path areas

<sup>5</sup> Does not include areas not within PD boundaries (i.e. between Fowler Ave. and project site).

<sup>6</sup> Multi-purpose path

#### **On-site Type “B” Phasing**

The applicant has proposed new and modified zoning conditions relative to those portions of the Type “B” streets also identified as “Shopping Center Internal Drives” within the PD, which will allow the applicant to:

- a) Construct a potentially interim typical section (the “Shopping Center Internal Drives” section) to support the increment of development shown on Sheet 6 of 6 (i.e. within a portion of Blocks 10, 13, and 14); and,
- b) Phase construction of the full typical sections (the Type “B” Typical Section) within those areas until such time as a certain portion of the adjacent block is redeveloped.

#### **Offsite / On-site Type “D” / Phase 1 Typical Sections**

During the course of discussions with the County, the applicant has stated that there are constraints which may prevent them from constructing the Typical Sections at the three (3) middle project roadway connections to Fowler Ave. Specifically, the applicant has stated that while they own many but not all of these parcels, there are lease agreements which prevent them from making modifications to the parcels. Staff also notes that the City of Tampa has jurisdiction of these lands, which is why they are not included in the PD.

The applicant has proposed new and modified zoning conditions relative to the portions of Type “D” streets within the PD, which will allow the applicant to:

- a) Construct a potentially interim Typical Section (i.e. the “Type ‘D’ Street Phase 1” Typical Section) to support the increment of development shown on Sheet 6 of 6 (i.e. within a portion of Block 10); and,
- b) Phase construction of the full Type “D” Roadway Typical Sections for portion of Type “D” roads adjacent to Blocks 10, 13, 14 and 15, within the PD project until such time as a portion of the adjacent block is developed or the certain Level of Service (LOS) triggers are met (see below).

The proposed conditions also allow the developer to only construct two (2) of the ultimate four (4) Type “D” lanes within the PD project until such time as a Level of Service (LOS) analysis indicates that the additional lanes are warranted. Enhanced pedestrian improvements within the site are also tied to these LOS triggers.

#### **TRANSPORTATION ANALYSIS**

The applicant conducted a transportation analysis which examined existing background traffic, plus additional anticipated traffic generated with the addition of the proposed uses shown on Sheet 6 of 6. According to the analysis, the existing access connections are anticipated to accommodate this proposed increment of development.

Given the proposed changes to typical sections which reduced segments of the internal roadways from 4 lanes to 2 lanes (at least in the interim), County staff also asked the applicant to examine when it was anticipated that future development might trigger such improvements. According to the analysis, it was anticipated that the 22<sup>nd</sup> St. intersection would be the first connection to trigger the four-lane improvement threshold. The applicant’s analysis indicated that there is an additional 360 peak hour trip capacity remaining, which could potentially equate to an additional 200,000 s.f. of Shopping Center uses, 600,000 s.f. of General Office Uses, or 1,000 hotel rooms, according to the applicant.

It should be noted that project traffic will be dispersed among all project driveways (with distribution between them being a factor of type of use being developed and location within the site, among others). Additionally, staff notes that traffic capacity may be utilized by already approved/constructed but underutilized space within the project (although it is not possible to estimate such impacts at this time). Staff notes that each driveway will be reviewed with each increment of development, to determine when thresholds have been met.

#### **EXISTING AND PROPOSED TRANSPORTATION INFRASTRUCTURE SERVING THE SITE**

Fowler Ave. is an 8-lane principal arterial, owned and maintained by the Florida Department of Transportation (FDOT), and is characterized by +/- 12-foot lanes. There are marked bicycle facilities on both sides. There are +/- 5-foot sidewalks on both sides of Fowler Avenue in the vicinity of the project. Fowler Avenue is a designated truck route. Local transit service (Route 12) is provided adjacent to the site. The segment of Fowler Avenue from B. B. Downs to Nebraska Avenue is currently operating at LOS F.

22<sup>nd</sup> St. is a 2-lane collector roadway characterized by +/- 10-foot lanes. There are marked bicycle facilities on both sides. There are +/- 5-foot sidewalks along on both sides of 22<sup>nd</sup> Street in the vicinity of the project. Local transit service (Route 12) is provided adjacent to the site. The segment of 22<sup>nd</sup> Avenue from the mall site to Fletcher Avenue is currently operating at LOS B.

19<sup>th</sup> Street is a 2-lane local roadway characterized by +/- 10-foot lanes. There are no marked bicycle facilities on either side of the road. There is a sidewalk along the west side of 19<sup>th</sup> Street. LOS information is not available for segment of the roadway.

University Square Dr. is a 2-lane, undivided, private street with +/- 12-foot lanes. There is a sidewalk along the south side of University Square Dr. There are no marked bicycle facilities. There are unsheltered USF Bull Runner transit stop (Route D) located on both the north side and south side of University Square Drive in the vicinity of the project.

Club Drive is a 2-lane, undivided, private road characterized by +/- 12-foot lanes. There are no marked bicycle facilities on either side of the road. There is approximately 590 feet of sidewalk on Club Drive adjacent to the site. Club Drive functions as a primary ring road that provides access along the north, west and east property boundaries.

#### **ROADWAY LEVEL OF SERVICE (LOS) INFORMATION**

Roadway	From	To	LOS Standard	LOS
Fowler Avenue	Florida Avenue	30 <sup>th</sup> Street	D	C
22 <sup>nd</sup> Street	Club Drive	Bearss Avenue	E	D

Sources: City of Tampa Level of Service Report, 2015.  
Hillsborough County Level of Service Report, 2020.



**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)**

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Fowler Ave.	FDOT Principal Arterial - Urban	8 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
22 <sup>nd</sup> St.	County Collector – Urban (North of Project) and City of Tampa Collector (South of Fowler Ave.)	2 Lanes <input checked="" type="checkbox"/> Substandard Road (County Portion) <input checked="" type="checkbox"/> Sufficient ROW Width (County Portion)	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
19 <sup>th</sup> St.	County Local – Urban (Also Partially within City of Tampa)	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other
University Square Dr.	Private	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
Club Dr.	Private	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other

Project Trip Generation <input checked="" type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing			
Proposed			
Difference (+/-)			

\*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input checked="" type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		Choose an item.	Choose an item.	Choose an item.
South		Choose an item.	Choose an item.	Choose an item.
East		Choose an item.	Choose an item.	Choose an item.
West		Choose an item.	Choose an item.	Choose an item.
Notes:				

Design Exception/Administrative Variance <input checked="" type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
	Choose an item.	Choose an item.
	Choose an item.	Choose an item.
Notes:		

Transportation Comment Sheet

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

**From:** [Gwynn, David](#)  
**To:** [Gormly, Adam](#)  
**Cc:** [Williams, Michael](#); [mark@tampainnovation.com](mailto:mark@tampainnovation.com); [Hall, Justin](#); [Chin, Ronald](#)  
**Subject:** RE: Fowler Ave  
**Date:** Thursday, January 20, 2022 5:38:03 PM  
**Attachments:** [image003.png](#)

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**External email:** Use caution when clicking on links and attachments from outside sources.

Adam

We do not have an objection. Please excuse the confusion regarding the communications. The PD&E was recently programmed and apparently the coordination between our two groups did not catch this conflict. Please disregard the memo from Mr. Provenzano.

David W. Gwynn, P.E.  
District Seven Secretary  
Florida Department of Transportation  
11201 N. McKinley Drive  
Tampa, FL 33612  
813-975-6039



*"Make us to choose the harder right instead  
of the easier wrong, and never to be content  
with a half truth when the whole can be won"*

- *West Point Cadet Prayer*

---

**From:** Gormly, Adam <Gormlya@HillsboroughCounty.ORG>  
**Sent:** Thursday, January 20, 2022 5:19 PM  
**To:** Gwynn, David <David.Gwynn@dot.state.fl.us>  
**Cc:** Williams, Michael <williamsm@hillsboroughcounty.org>; [mark@tampainnovation.com](mailto:mark@tampainnovation.com); Hall, Justin <Justin.Hall@dot.state.fl.us>  
**Subject:** RE: Fowler Ave

**EXTERNAL SENDER:** Use caution with links and attachments.

David,

Thank you for including me on this email. We were provided the attached memorandum from Joel

Provenzano that appears to be objecting to the PRS for the mall site, PRS 22-0028.

Could you please advise if FDOT does have an objection to this application?

Thank you. Adam

**Adam Gormly**

**Director**

Development Services Department

---

P: (813) 276-8422

E: [GormlyA@HCFLGov.net](mailto:GormlyA@HCFLGov.net)

W: [HCFLGov.net](http://HCFLGov.net)

**Hillsborough County**

601 E. Kennedy Blvd., Tampa, FL 33602

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**From:** Gwynn, David <[David.Gwynn@dot.state.fl.us](mailto:David.Gwynn@dot.state.fl.us)>

**Sent:** Wednesday, January 19, 2022 3:23 PM

**To:** [mark@tampainnovation.com](mailto:mark@tampainnovation.com); Gormly, Adam <[Gormlya@HillsboroughCounty.ORG](mailto:Gormlya@HillsboroughCounty.ORG)>; Hall, Justin <[Justin.Hall@dot.state.fl.us](mailto:Justin.Hall@dot.state.fl.us)>

**Subject:** Fowler Ave

**External email:** Use caution when clicking on links and attachments from outside sources.

Mark

Thank you for meeting with us regarding Fowler Avenue. As discussed we are preparing to begin our PD&E study on Fowler. Over the next six months or so we will be collecting existing traffic data and analyzing projected intersection traffic. At that point we will be able to better discuss intersection configurations and control.

Adam if you have any questions please feel free to call either me or Justin Hall to discuss.

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*Florida Department of Transportation*

RON DESANTIS  
GOVERNOR

11201 N. McKinley Drive  
Tampa, FL 33612

KEVIN J. THIBAUT, P.E.  
SECRETARY

**MEMORANDUM**

DATE: November 23, 2021

TO: James Ratliff, Hillsborough County

FROM: Joel Provenzano, FDOT

COPIES: Richard Perez, Hillsborough County  
Melanie Calloway, City of Tampa  
Daniel Santos, FDOT  
Lindsey Mineer, FDOT  
Elise Batsel, Stearns Weaver

SUBJECT: PRS 22-0028, University Mall

FDOT objects to the proposed accesses for the University Mall redevelopment.

FDOT met with the developer on 11/5/19 and 4/15/20 and expressed concerns over the proposed accesses. Of particular concern is Club Drive and 24th Street not being able to support the volumes or provide safe ingress and egress for vehicles trying to enter or leave the site. FDOT would be willing to support a new main full connection with a signal at 24<sup>th</sup> Street, which would need to be relocated to the east. Club Drive potentially would be made a right-in/right-out only. FDOT has not seen any proposal to solve this concern. If one is not presented, then the alternative may be to have the 24th Street median opening closed, making it a right-in/right-out only.

FDOT is willing to consider additional signals to support bike, ped and traffic safety. With an understanding between FDOT and the developer that if 19th Street becomes signalized, the full median opening at 20th Street would need to be closed or directionalized. FDOT believes this signalization is necessary due to the increase in traffic of the proposed redevelopment.

Type B streets show parking on them. 19<sup>th</sup> Street should not be a Type B street.

END OF MEMO



## COMMISSION

Mariella Smith CHAIR  
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Harry Cohen  
Ken Hagan  
Gwendolyn "Gwen" W. Myers  
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Reginald Sanford, MPH AIR DIVISION  
Steffanie L. Wickham WASTE DIVISION  
Sterlin Woodard, P.E. WETLANDS DIVISION

### AGENCY COMMENT SHEET

REZONING	
<b>HEARING DATE:</b> January 18, 2022	<b>COMMENT DATE:</b> December 9, 2021
<b>PETITION NO.:</b> 22-0028	<b>PROPERTY ADDRESS:</b> 2102, 2108, 2120, 2200, 2212, 2266 E. Fowler Ave; 2201 University Square Mall; 12270 University Mall Court; and 12313 Club Drive
<b>EPC REVIEWER:</b> Abbie Weeks	<b>FOLIO #:</b> 0359210000, 0359210025, 0359210050, 0359210075, 0359210080, 0359210110, 0359210005, 0359210100, 0359210200, 0362980300, 0362830000
<b>CONTACT INFORMATION:</b> (813)627-2600 X1101	<b>STR:</b> 07/08-28S-19E
<b>EMAIL:</b> <a href="mailto:weeksa@epchc.org">weeksa@epchc.org</a>	
<b>REQUESTED ZONING:</b> Minor Modification to PD	
FINDINGS	
<b>WETLANDS PRESENT</b>	YES
<b>SITE INSPECTION DATE</b>	07/13/2017
<b>WETLAND LINE VALIDITY</b>	Valid to February 2023
<b>WETLANDS VERIFICATION (AERIAL PHOTO, SOILS SURVEY, EPC FILES)</b>	Two ponds located in the northeast and northwest property corners and an OSW Ditch along the west property boundary
<p>The EPC Wetlands Division has reviewed the proposed rezoning. In the site plan's current configuration, a resubmittal is not necessary. If the zoning proposal changes and/or the site plans are altered, EPC staff will need to review the zoning again. This project as submitted is conceptually justified to move forward through the zoning review process as long as the following conditions are included:</p> <ul style="list-style-type: none"><li>• Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.</li><li>• The construction and location of any proposed wetland impacts are not approved by this correspondence, but shall be reviewed by EPC staff under separate application pursuant to the</li></ul>	

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EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.

- Prior to the issuance of any building or land alteration permits or other development, the approved wetland / other surface water (OSW) line must be incorporated into the site plan. The wetland/ OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County Land Development Code (LDC).
- Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.

**INFORMATIONAL COMMENTS:**

The following specific comments are made for informational purposes only and to provide guidance as to the EPC review process. However, future EPC staff review is not limited to the following, regardless of the obviousness of the concern as raised by the general site plan and EPC staff may identify other legitimate concerns at any time prior to final project approval.

- The site plan depicts wetland impacts that have not been authorized by the Executive Director of the EPC. The wetland impacts are indicated for a VA Connection and a newly proposed cross access area on the western site boundary. Chapter 1-11, prohibits wetland impacts unless they are necessary for reasonable use of the property. Staff of the EPC recommends that this requirement be taken into account during the earliest stages of site design so that wetland impacts are avoided or minimized to the greatest extent possible. The size, location, and configuration of the wetlands may result in requirements to reduce or reconfigure the improvements depicted on the plan. If you choose to proceed with the wetland impacts depicted on the plan, a separate wetland impact/mitigation proposal and appropriate fees must be submitted to this agency for review.
- The Hillsborough County Land Development Code (LDC) defines wetlands and other surface waters as Environmentally Sensitive Areas. Pursuant to the LDC, wetlands and other surface waters are further defined as Conservation Areas or Preservation Areas and these areas must be designated as such on all development plans and plats. A minimum setback must be maintained around the Conservation/Preservation Area and the setback line must also be shown on all future plan submittals.
- Any activity interfering with the integrity of wetland(s) or other surface water(s), such as clearing, excavating, draining or filling, without written authorization from the Executive Director of the EPC or authorized agent, pursuant to Section 1-11.07, would be a violation of Section 17 of the Environmental Protection Act of Hillsborough County, Chapter 84-446, and of Chapter 1-11.

aow/mst

cc: [ebatsel@stearnsweaver.com](mailto:ebatsel@stearnsweaver.com)  
[swalker@stearnsweaver.com](mailto:swalker@stearnsweaver.com)

## AGENCY COMMENT SHEET

TO: **Zoning/Code Administration, Development Services Department**

FROM: **Reviewer:** Carla Shelton Knight **Date:** January 26, 2022

**Agency:** Natural Resources **Petition #:** 22-0028

☐ This agency has **no comment**

☐ This agency has **no objections**

☒ This agency has **no objections, subject to listed or attached conditions**

☐ This agency **objects, based on the listed or attached issues.**

1. Natural Resources has no objection to elimination of the landscape buffers between the parking areas and entrance drives in order to provide safe pedestrian access.
2. Approval of this petition by Hillsborough County does not constitute a guarantee that Natural Resources approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to trees, natural plant communities or wildlife habitat, and does not grant any implied or vested right to environmental approvals.
3. The construction and location of any proposed environmental impacts are not approved by this correspondence, but shall be reviewed by Natural Resources staff through the site and subdivision development plan process pursuant to the Land Development Code.
4. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.



## AVIATION AUTHORITY LAND USE REVIEW

### Hillsborough County - OPTIX

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**DATE:** December 30, 2021

**PROPOSED USE INFORMATION:**

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Case No.: 22-0028 Reviewer: Tony Mantegna

Location: University Mall

Folio: 35921.0000

Current use of Land: Commercial

Zoning: CG

REQUEST: PD

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**COMMENTS:**

The proposed site falls within Zone "B" on the Airport Height Zoning Map. Any structure including construction equipment that exceeds 200' feet Above Ground Level may require an Airport Height Zoning Permit and must be reviewed by the Airport Zoning Director.

☐ Compatible without conditions (see comments above) - \_\_\_\_\_

☐ Not compatible (comments) - \_\_\_\_\_

☒ Compatible with conditions (see comments above) – Any height greater than 200' above ground level will require a height zoning permit or letter of compliance to be issued.

---

cc: Aviation Authority Zoning Director/Legal/Records Management/Central Records

**WATER RESOURCE SERVICES  
REZONING REVIEW COMMENT SHEET: WATER & WASTEWATER**

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**PETITION NO.:** PRS22-0028    **REVIEWED BY:** Randy Rochelle    **DATE:** 11/5/2021

**FOLIO NO.:** 35921.0000 Plus Multiple More

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**WATER**

- ☒ The property lies within the City of Tampa Water Service Area. The applicant should contact the provider to determine the availability of water service.
- ☐ A \_\_\_ inch water main exists ☐ (adjacent to the site), ☐ (approximately \_\_\_ feet from the site) \_\_\_\_\_. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.
- ☐ Water distribution system improvements will need to be completed prior to connection to the County's water system. The improvements include \_\_\_\_\_ and will need to be completed by the \_\_\_\_\_ prior to issuance of any building permits that will create additional demand on the system.

**WASTEWATER**

- ☒ The property lies within the City of Tampa Wastewater Service Area. The applicant should contact the provider to determine the availability of wastewater service.
- ☐ A \_\_\_ inch wastewater gravity main exists ☐ (adjacent to the site), ☐ (approximately \_\_\_ feet from the site) \_\_\_\_\_. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.
- ☐ Wastewater collection system improvements will need to be completed prior to connection to the County's wastewater system. The improvements include \_\_\_\_\_ and will need to be completed by the \_\_\_\_\_ prior to issuance of any building permits that will create additional demand on the system.

**COMMENTS:** \_\_\_\_\_.