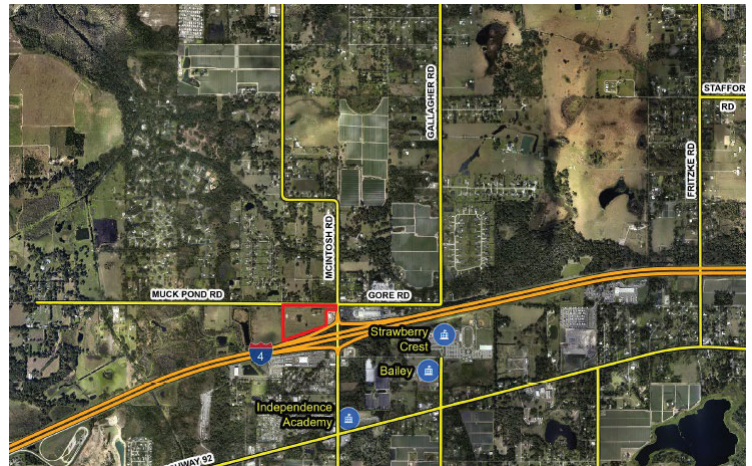


PD Modification Application: PRS 22-0844
Zoning Hearing Master Date: N/A
BOCC Land Use Meeting Date: August 25, 2022

1.0 APPLICATION SUMMARY

Applicant: Kami Corbett, Hill Ward Henderson
FLU Category: RES-1
Service Area: Rural
Site Acreage: 19.04 +/-
Community Plan Area: Thonotosassa
Overlay: None



Introduction Summary:

The subject site is zoned PD 98-0823, as most recently modified by MM 07-0529. The PD is approved for two parcels of development consisting of the display, sales and pre-sale maintenance and service of RVs in Parcel A and 154,000 sf of enclosed RV storage and an associated clubhouse/office in Parcel B. The site plan provides for one access point on Muck Pond Road and one access point on McIntosh Road. Additionally, a shared access point to property to the immediate west is provided along the PD's western boundary.

| Existing Approval(s): | Proposed Modification(s): |
|---|---|
| One access point on McIntosh Road | Remove the McIntosh Road access point |
| Clubhouse/office location along the northern boundary of the PD | Relocate the clubhouse/office to Building C |
| One access point on Muck Pond Road | Add a second access point on Muck Pond Road |
| No pedestrian cross access points approved | Add a pedestrian cross access point in Parcel A to the property to the immediate east |

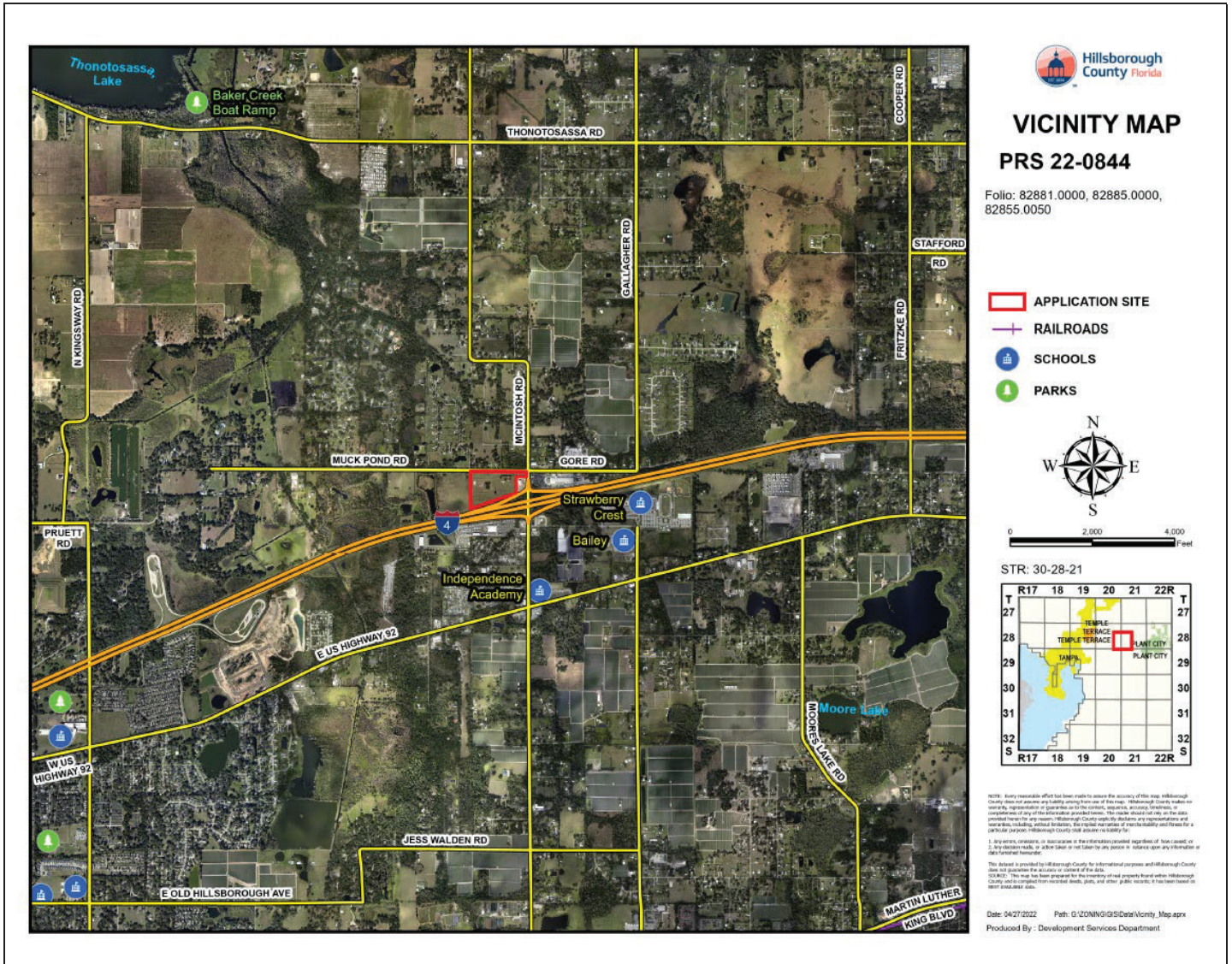
Additional Information:

| | |
|---|--|
| PD Variation(s): | None Requested as part of this application |
| Waiver(s) to the Land Development Code: | None Requested as part of this application |

| | |
|---|---|
| Planning Commission Recommendation: N/A | Development Services Recommendation: Approvable, subject to proposed conditions |
|---|---|

2.0 LAND USE MAP SET AND SUMMARY DATA

2.1 Vicinity Map

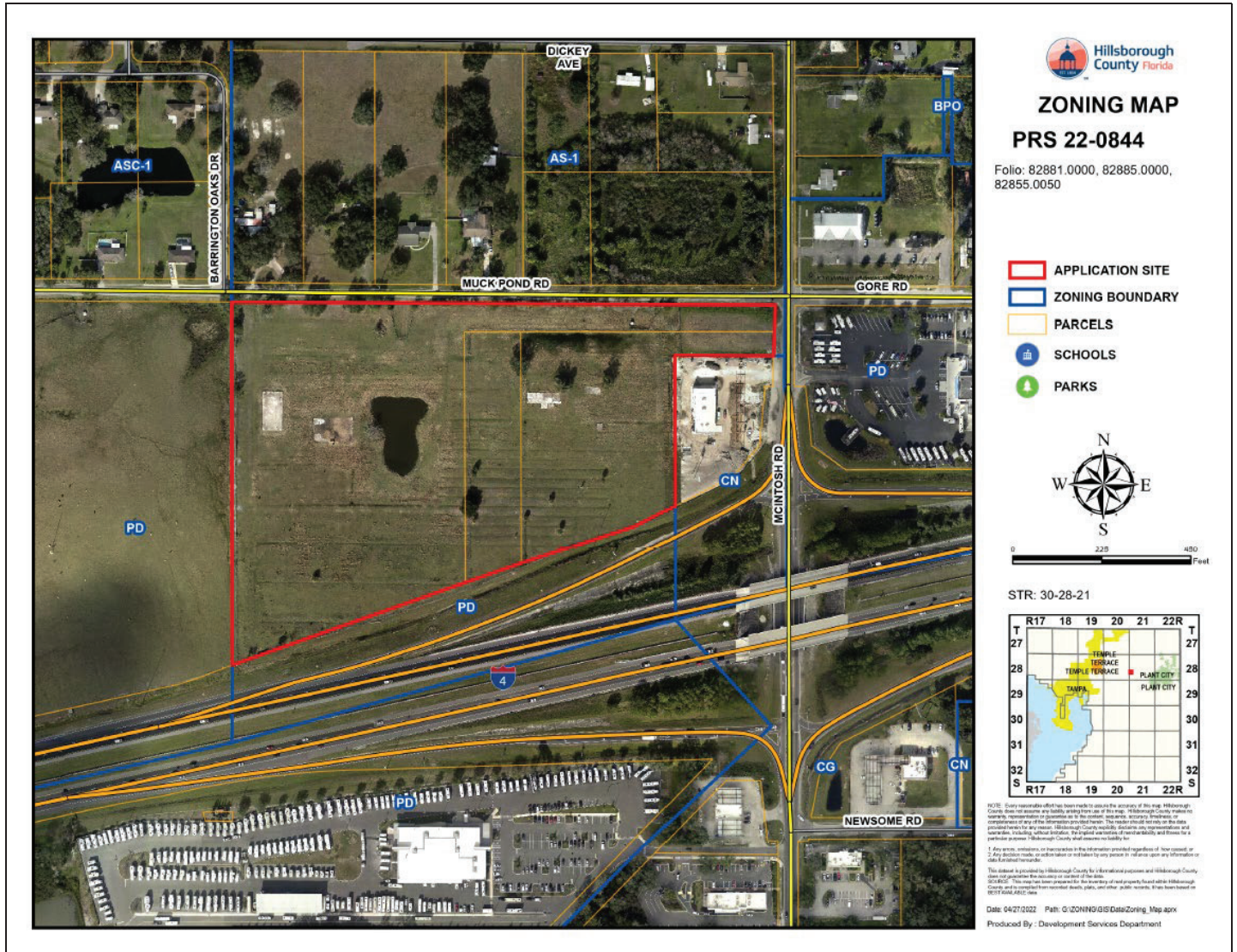


Context of Surrounding Area:

The site is located at the southwest corner of Muck Pond Road and McIntosh Road, north Interstate 4, in the Thonotosassa community. The I-4 and McIntosh Road intersection is developed with commercial uses (such as a convenience store with gas sales, RV sales and fast food) on both the north and south sides of Interstate 4. Low density residential is present outside of this activity area within the community.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.2 Immediate Area Map

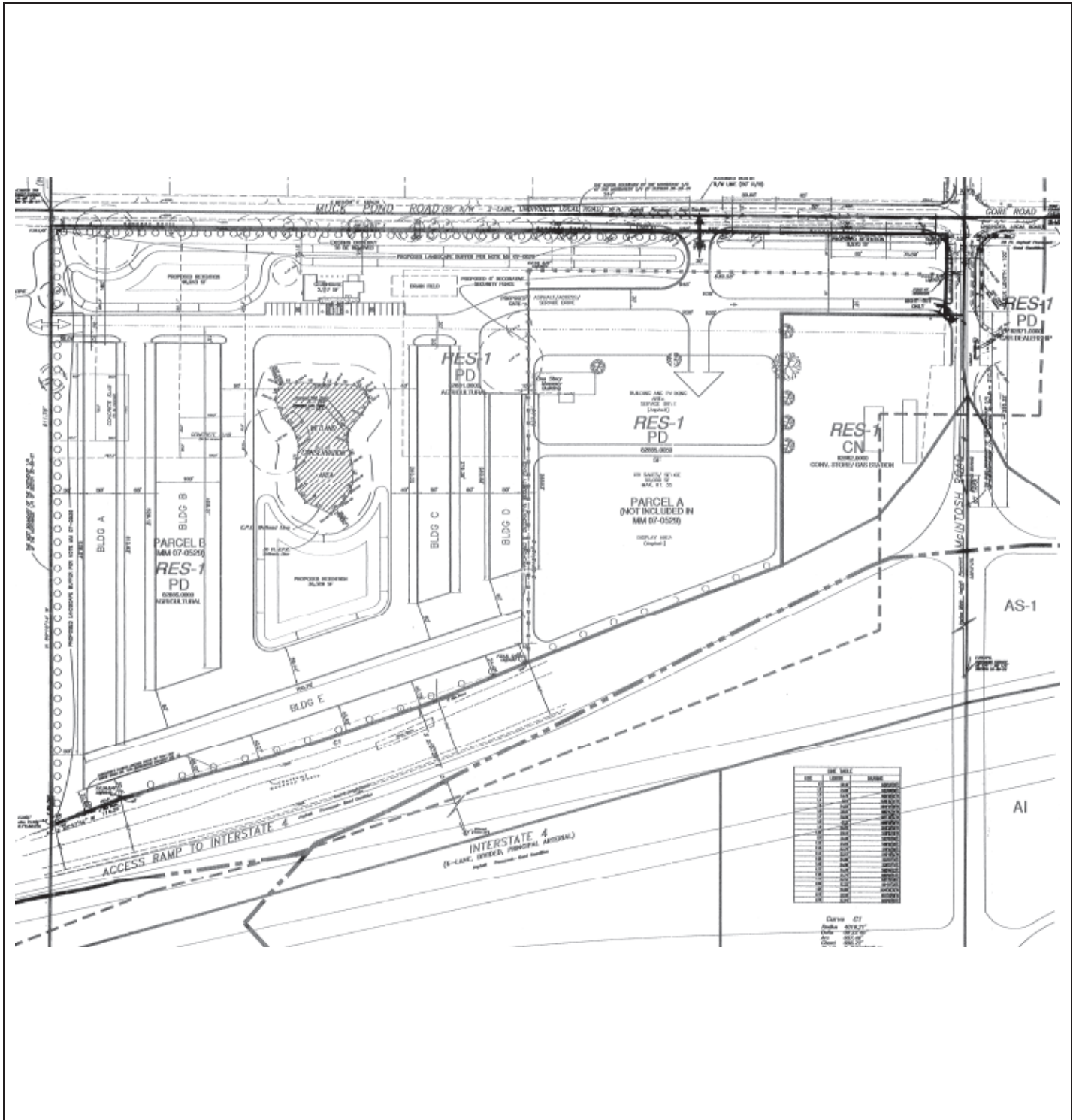


Adjacent Zonings and Uses

| Location: | Zoning: | Maximum Density/F.A.R. Permitted by Zoning District: | Allowable Use: | Existing Use: |
|-----------|---------|--|--|--|
| North | AS-1 | 1 unit per acre | Single-Family Residential/Agricultural | Single-Family Residential and Vacant |
| South | N/A | N/A | N/A | Interstate |
| East | CN & PD | 0.25 FAR | CN: Commercial PD: RV Sales and Service | CN: Convenience Store with Gas PD: RV Sales and Service (Camping World) |
| West | PD | 0.25 FAR | Daycare and limited BPO uses | Vacant |

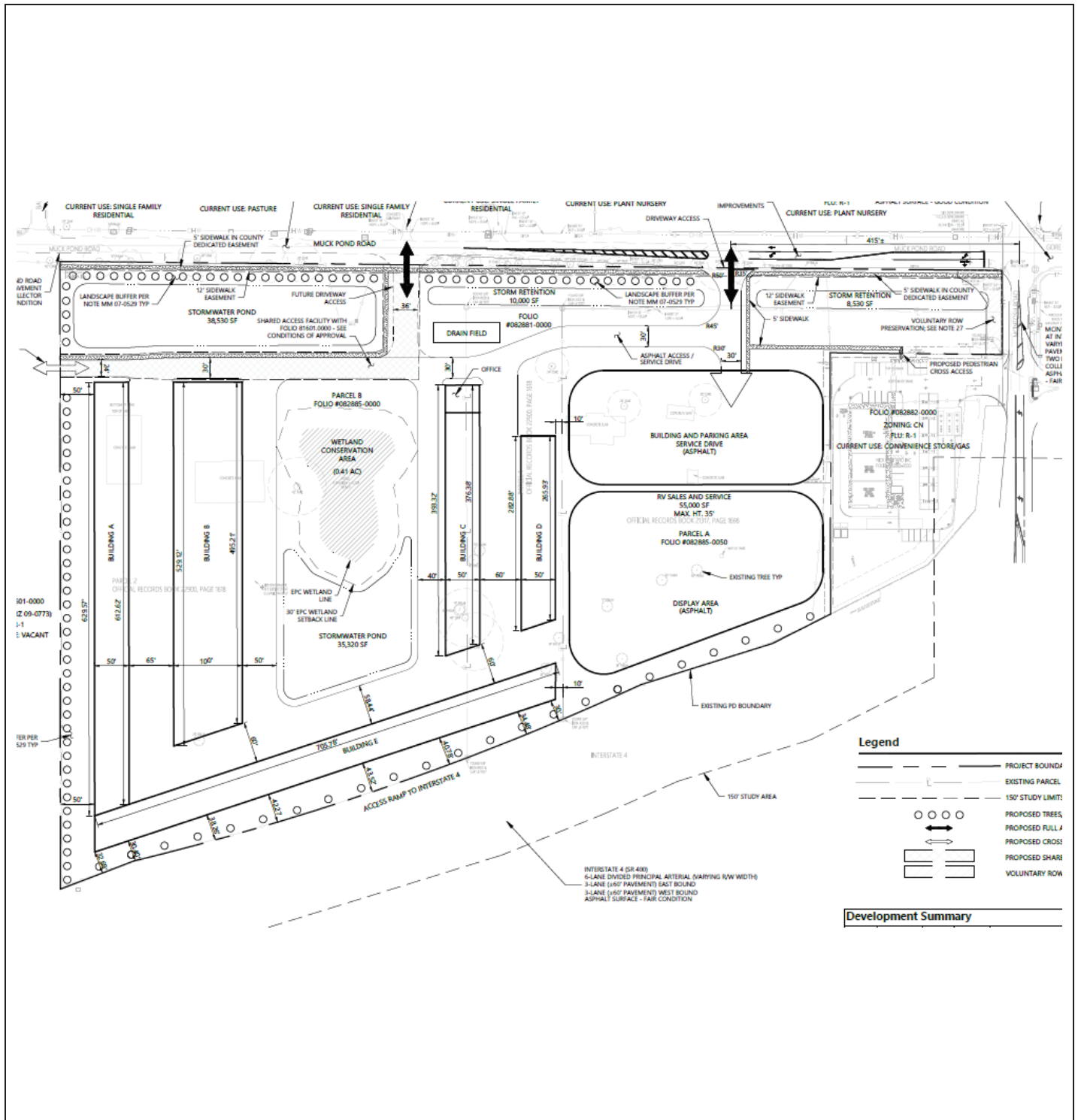
2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Approved Site Plan (partial provided below for size and orientation purposes. See Section 8.1 for full site plan)



2.0 LAND USE MAP SET AND SUMMARY DATA

2.4 Proposed Site Plan (partial provided below for size and orientation purposes. See Section 8.2 for full site plan)



3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

| Adjoining Roadways (check if applicable) | | | |
|--|--------------------------|---|--|
| Road Name | Classification | Current Conditions | Select Future Improvements |
| Muck Pond Rd. | County Collector - Rural | 2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input checked="" type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |
| McIntosh Rd. | County Collector - Rural | 2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other – ROW Preservation |
| | Choose an item. | Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |
| | Choose an item. | Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |

| Project Trip Generation <input type="checkbox"/> Not applicable for this request | | | |
|--|----------------------------|----------------------|----------------------|
| | Average Annual Daily Trips | A.M. Peak Hour Trips | P.M. Peak Hour Trips |
| Existing | 498 | 39 | 65 |
| Proposed | 498 | 39 | 65 |
| Difference (+/-) | No Change | No Change | No Change |

*Trips reported are based on net new external trips unless otherwise noted.

| Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request | | | | |
|--|----------------|--------------------------------|------------------------|-----------|
| Project Boundary | Primary Access | Additional Connectivity/Access | Cross Access | Finding |
| North | X | Vehicular & Pedestrian | None | Meets LDC |
| South | | None | None | Meets LDC |
| East | | Pedestrian | None | Meets LDC |
| West | | Vehicular & Pedestrian | Vehicular & Pedestrian | Meets LDC |
| Notes: | | | | |

| Design Exception/Administrative Variance <input type="checkbox"/> Not applicable for this request | | |
|---|----------------------------|----------------------------|
| Road Name/Nature of Request | Type | Finding |
| Muck Pond Rd. – Substandard Road | Design Exception Requested | Approvable with Conditions |
| | Choose an item. | Choose an item. |
| Notes: | | |

4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY

| INFORMATION/REVIEWING AGENCY | | | | |
|---|---|---|--|---------------------------------|
| Environmental: | Comments Received | Objections | Conditions Requested | Additional Information/Comments |
| Environmental Protection Commission | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | |
| Natural Resources | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input type="checkbox"/> Yes <input type="checkbox"/> No | <input type="checkbox"/> Yes <input type="checkbox"/> No | |
| Conservation & Environ. Lands Mgmt. | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | |
| Check if Applicable: <input checked="" type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area | <input checked="" type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Other _____ | | | |
| Public Facilities: | Comments Received | Objections | Conditions Requested | Additional Information/Comments |
| Transportation <input checked="" type="checkbox"/> Design Exc./Adm. Variance Requested <input checked="" type="checkbox"/> Off-site Improvements Provided | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | |
| Service Area/ Water & Wastewater <input type="checkbox"/> Urban <input type="checkbox"/> City of Tampa (to the west) <input checked="" type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | |
| Hillsborough County School Board Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input type="checkbox"/> Yes <input type="checkbox"/> No | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | |
| Impact/Mobility Fees N/A | | | | |
| Comprehensive Plan: | Comments Received | Findings | Conditions Requested | Additional Information/Comments |
| Planning Commission <input type="checkbox"/> Meets Locational Criteria <input checked="" type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input checked="" type="checkbox"/> N/A | <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No | <input type="checkbox"/> Inconsistent <input type="checkbox"/> Consistent <input checked="" type="checkbox"/> N/A | <input type="checkbox"/> Yes <input type="checkbox"/> No | |

5.0 IMPLEMENTATION RECOMMENDATIONS

5.1 Compatibility

The proposed changes are access related or internal to the project. All use restrictions, hours of operation, building setbacks, building heights, project square footage and perimeter buffers and landscaping will remain and are not proposed for modification.

The existing McIntosh Road access point location, as discussed in the Transportation staff report, needs to be changed due to changes to technical design standards, changes to the transportation mitigation requirements and changes in the development pattern along McIntosh Road that have occurred since the original approval. Additionally, it has been found that the subject site, and the property to the immediate west which is accessed solely through this project, cannot be supported with one access point. Therefore, a second access point is needed and proposed. The area available for a second access is along Muck Pond Road. To provide spacing with the existing Muck Pond Road access point, the second access point will be located across from folio 81635.6006. This property is 2.42 acres in size and developed with a single-family home. The home is located approximately 96 feet from the property line and separated from the site by Muck Pond Road (a 2-lane roadway). The northern PD boundary is provided with a 4-foot high landscaped berm consisting of hedges and trees. Additionally, a 6-foot high decorative fence is to be provided along the boundary. The access is depicted to align with the driveway located on folio 81635.6006. No use changes are proposed, and the access will serve the RV storage portion of the site (Parcel B), as well as the western PD approved for daycare and office uses.

The addition of the second Muck Pond Road access point requires relocation of the RV storage use's clubhouse/office. It will be moved further into the site and away from adjacent residential.

5.2 Recommendation

Staff recommends approval, subject to proposed conditions.

6.0 PROPOSED CONDITIONS

Requirements for Certification:

1. Prior to PD site plan certification, the developer shall revise the PD site plan to:
 - a. Revise the labels reading "12' Sidewalk Easement" to state "15' Public Access, Utility and Maintenance Easement". Staff notes such revisions are necessary for consistency with the site plan shown in the requested Design Exception.
 - b. Revise the labels reading "5' Sidewalk In County Dedicated Easement" to state "10' Multi-Purpose Path In County Dedicated Easement". Staff notes such revisions are necessary for consistency with the site plan shown in the requested Design Exception.
 - c. Revise the label reading "Voluntary ROW Preservation; See Note 27" to read, "Voluntary Right-of-way Preservation – See PD Note 27 and Zoning Condition 26".
 - d. Update the PD site plan for adjacent folio 82882.0000 to show the site improvements constructed within the adjacent project. Staff notes that the pedestrian infrastructure and cross access within that site are not as currently shown.
 - e. Revise the location of the proposed pedestrian cross access and internal pedestrian infrastructure such that the sidewalk connection occurs along the adjacent folio's western project boundary, in the location such stubout was actually constructed (see Transportation staff report).
 - f. Revise Note 21 to change the words "is provided" to read "exists".
 - g. Revise Note 23 to add to the end of the sentence "and/or as required by the condition of approval."
 - h. Replace Note 28 with a note reading something similar to "Internal fencing and gating may be provided. Reference zoning conditions, including conditions 12.6 and 25 for restrictions and other pertinent information."
2. Prior to PD site plan certification, if the zoning modification is approved, the Developer shall provide a signed and sealed Design Exception (the revised date of the letter shall be unchanged). Staff notes that the while the first two versions of the letter were signed, the August 8, 2022 letter was sealed, but appears to have been inadvertently unsigned.

Approval - Approval of the request, subject to the conditions listed below, is based on the general site plan submitted July 20, 2022.

1. Parcel A shall be limited to sales and display of recreational vehicles with pre-sale maintenance and preparation services only. Said maintenance and services shall be limited to minor motor vehicle repair as defined by the Land Development Code. After-sale services, including warranty work and routine maintenance, and all general repair and maintenance services shall be prohibited unless the warranty work is for non-motorized cosmetic repair items only. Cosmetic repairs shall not include exterior body repair, grinding, sanding, sandblasting or painting. Additionally, the sale, leasing or warehousing of parts for off-site use or distribution, and the in-ground storage of fuel or sale of fuel to the public, shall be prohibited.
 - 1.1 The facility shall be permitted a maximum of 55,000 square feet of floor area. At least 30 percent of the total project area shall remain open space.
2. Parcel B shall be limited to a maximum of 154,000 square feet of floor area for a condominium recreational vehicle (RV) storage facility that includes a 2,117-square-foot and associated clubhouse/office. Individual buildings shall be developed in accordance with the building dimensions depicted on the site plan. An RV wash center and septic dump station is permitted within the interior of the storage facility. Fifty percent of the project shall be open space. The recreational vehicle storage facility buildings shall have solid walls exterior to the project.

3. Maximum building height shall be 35 feet. Minimum building setbacks shall be 200 feet from the north project boundary, except in Parcel B which shall be 180 feet for the vehicle storage buildings and ~~70 feet for the clubhouse/office building~~, 50 feet from the west PD boundary, 30 feet from the south boundary and 20 feet (10 feet for Parcel B from Parcel A) from the east boundary. Existing buildings shall be brought into conformance with these setbacks prior to any commercial use of the site. All buildings shall be architecturally finished on all sides and the use of metal exterior siding shall be prohibited. Paint shall not constitute an architectural finish. Existing buildings that do not conform with this requirement shall be finished with stucco on all exterior walls prior to any commercial use of the site.
4. All vehicle parking, display, storage and service areas shall be located a minimum of 200 feet from the north boundary of the PD site, except as referenced above. ~~The project's internal access drive shall be located a minimum of 100 feet from the north boundary of the PD site.~~
5. For Parcel A, all service bay doors shall face southward and/or eastward. Existing buildings shall be brought into conformance with this requirement prior to any commercial use of the site. All service activities, excluding vehicle detailing and minor washing by hand for dust removal, shall be performed indoors only. Automated vehicle washing facilities shall include a water recycling system and runoff shall not drain into project stormwater retention ponds.
6. No outdoor loudspeakers shall be permitted in the project.
7. Overhead pole lights shall be prohibited within 200 feet of the north boundary of the project. Notwithstanding, pole lights shall be permitted along the project's internal access drive subject to the following requirements: 1) the lights shall be placed only on the north side of the drive; 2) the lights shall have a maximum height of 15 feet; and, 3) the lights shall be directed southward and utilize full cutoff fixtures to completely shield the bulbs or other sources of illumination from passersby on Muck Pond Road.
8. Project signs shall conform with Article VII of the Land Development Code, subject to the following additional restrictions:
 - 8.1. The project shall be limited to a total of two pole signs. One pole sign shall be permitted on Parcel A and one pole sign shall be permitted on Parcel B. The pole signs shall be located within 50 feet of the south boundary of the project next to Interstate-4.
 - 8.2. Wall signs shall be permitted only on the southern and eastern faces of the principal buildings.
 - 8.3. Signs at the project access points shall be monument signs with a maximum height of four feet. Illumination shall be limited to external floodlights; no internal illumination shall be permitted.
 - 8.4. No other ground signs, except traffic advisory and directional signs, shall be permitted in the project.
 - 8.5. Billboards, pennants and banners shall be prohibited.
9. If septic tanks are utilized, the drain fields for Parcel A shall be located a minimum of 200 feet from the north property boundary. The Parcel B drain field may be located as shown on the site plan. Existing septic tank locations and drainfields are permitted to remain if certified for commercial use by the Health Department.
10. All vehicle drives, parking, display, storage and service areas shall be paved.

11. Parcel A hours of operation shall be limited to the following: Monday through Saturday, 7:00 a.m. to 7:00 p.m.; and Sunday, 9:00 a.m. to 5:00 p.m. Parcel B hours of operation shall be 24 hours for the gated, owner-entry only condominium storage units; hours of operation for the clubhouse/office shall be 7:00 a.m. to 10:00 p.m. Monday through Saturdays and Sunday 9:00 a.m. to 5:00 p.m.
12. A buffer area with a minimum width of 50 feet shall be provided along the entire west boundary. The westernmost 955 feet of the northern boundary shall have a 100-foot buffer with the exception of the clubhouse, which will have a 70-foot-wide buffer. Within the buffer area, the developer shall install screening as follows:
 - 12.1. Along the north boundary and extending southward along the west boundary, there shall be a landscaped berm of undulating height. The berm shall have a minimum height of four feet above grade and shall be crowned with evergreen shrubs and trees. The shrubs shall have a minimum height of two feet above berm height and minimum opacity of 75 percent at time of planting. The trees shall have a minimum height of 10 feet above berm height and minimum caliper of two inches at time of planting, and shall be placed no more than 20 feet apart on centers. The berm shall be placed along the exterior edge of the buffer area.
 - 12.2. Along the remainder of the west boundary, there shall be an evergreen hedge with a minimum height of six feet and minimum opacity of 75 percent at time of planting.
 - 12.3. The balance of the buffer area shall be grassed. Existing encroachments in the buffer area, such as building foundations, shall be removed prior to any commercial use of the PD site. Prior to certification, notations on the general site plan shall be revised to conform with this condition.
 - 12.4. All required screening shall be installed prior to any commercial use of the PD site.
 - 12.5. No fences shall be allowed within the buffer area, unless otherwise specified herein. Existing fences shall be relocated accordingly prior to any commercial use of the PD site. Fences shall conform with Section 6.07.00.C.3 of the Land Development Code, except that for Parcel A chain link fences with a maximum height of 7 feet and topped with barbed wire shall be permitted if placed at least 100 feet from the Muck Pond Road and McIntosh Road rights-of-way.

For Parcel B, a six foot opaque decorative fence shall be permitted to be located as shown on the general site plan. The decorative fence shall be wrought iron or similar material but cannot be chain link.
 - 12.6. Notwithstanding anything herein these conditions or on the PD site plan to the contrary, buffering, screening, and fencing shall not be permitted in areas requiring to facilitate proposed vehicular and pedestrian access or cross access.
13. Along the Interstate-4 frontage, the developer shall plant trees, a minimum of 10 feet in height at the time of planting, and shall be placed no more than 50 feet apart on centers.
14. The stormwater management system shall be designed and constructed in such a manner as to not adversely impact off-site surface and groundwater elevations. To the greatest extent possible, stormwater ponds shall be located in the buffer areas along the north boundaries of the project.
15. Advisory and directional signs shall be posted within the project to direct vehicles exiting the site to McIntosh

Road.

16. ~~Subject to FDOT approval, the site will be restricted to one right-out only access on McIntosh Road. The general design and location of the access point on Muck Pond Road shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (Land Development Code Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and Growth Management Department. Final design, if approved by Hillsborough County Planning and Growth Management Department may include, but is not limited to: left turn lanes, acceleration lane(s) and deceleration lane(s). Access points may be restricted in movements. The project shall be served by two (2) full access connections to Muck Pond Rd. Vehicular access to McIntosh Road shall not be permitted. Additionally:~~

16.1 The developer shall construct the easternmost access concurrent with the initial increment of development; and,

16.2 The developer shall have the option of constructing the westernmost access, i.e. the portion between the east/west running drive aisle (construction of which shall be the responsibility of the developer of this project) and Muck Pond Rd., together with the initial increment of development. Additionally:

16.2.1 If the developer decides not to construct such access, the developer shall record, an ingress/egress, construction and any other easements necessary to permit the owners of folio 81601.0000 (which is subject to a PD zoning approval that requires the majority of its development to take access through the subject PD) to construct and utilize the westernmost project access without further consultation of the owners of the lands within the subject PD.

16.2.2 If the developer decides to construct such access, the developer shall record an ingress and egress easement over the area, as required pursuant to Condition 18. Additionally, the developer shall be responsible for substandard road improvements on Muck Pond Rd. between the westernmost project access and the improvements described in the Design Exception, described hereinbelow.

17. ~~Prior to Concurrency approval, the Developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic at the project driveway(s). This analysis shall include a passenger car to heavy vehicle conversion ratio to determine the increased length in turn lane being required. The turn lane(s) shall be constructed to FDOT and/or Hillsborough County standards using FDOT standard Index 301 & 526 and an asphalt overlay shall be applied over the entire portion of roadway where a left turn lane is provided. The Developer shall construct a westbound to southbound left turn lane on Muck Pond into the easternmost project driveway. The turn lane shall be constructed in accordance with the Design Exception described in condition 22. Construction of a westbound to southbound left turn lane into the westernmost driveway shall be the responsibility of the developer of the adjacent PD utilizing such access, if warranted pursuant to Sec. 6.04.04.D. of the LDC. the following turn lanes at his expense:~~

~~17.1 Provide NB to WB left turn lane at the intersection of McIntosh Road @ Muck Pond Road~~

~~17.2 Provide a WB to SB left turn lane on Muck Pond Road into the site~~

~~If it is determined by the results of the analysis submitted by the developer, that adequate right-of-way does not exist to construct any needed improvements (i.e. turn lanes), then the developer shall re-submit an analysis showing the maximum development that could occur to still enable the needed improvements to be constructed. The development will be limited to the size as shown in the submitted analysis.~~

18. ~~Based on the projected trip generation to the site, access onto the public road would be via "Type II" Minor Roadway Connection (50 – 1,500 trip ends per day), LDC 6.04.01.E. The Land Development Code requires that all internal access (the "throat") to the driveways must be a minimum of 50 feet from the edge of pavement of the public roadway, and shall remain free of internal connections or parking spaces that might interfere with the movement of vehicles into or out of the site. The applicant has the option of submitting an analysis showing that for his particular site, a throat of less than 50 feet is appropriate and will result in no adverse impact to the public roadway system. Because of the length of the vehicles using the site, additional throat depth may be required. If the Driveway is to be gated, then the throat depth may exceed this length to accommodate stacking vehicles. As shown on the PD site plan, certain internal driveways, drive aisles and sidewalks shall be considered Shared Access Facilities with folio 81601.0000 (and such facilities shall be treated as the sole legal means of vehicular access to certain development within the adjacent folio, as specified within the PD regulating such adjacent property)". Together with the initial increment of development within the subject PD, the property owner shall record in the Official Records of Hillsborough County a non-exclusive ingress and egress easement and any other easements necessary to permit the developer of folio 81601.0000 to take access through the areas designed as a Shared Access Facility within subject PD, without further consultation of the property owner of the subject PD. The design and locations of all connections shall be subject to the review and approval of Hillsborough County.~~
19. Prior to Construction Plan approval, the Developer may be required to dedicate one-half of the right-of-way on Muck Pond Road to meet Hillsborough County standards. The right-of-way shall be dedicated to bring the substandard right-of-way up to Transportation Technical Manual Standards for a two lane collector roadway. Right-of-way shall be measured from the centerline of the roadway.
20. ~~As is noted in the Land Development Code, one of the major reasons for diminished capacity of public roads is an increase in access points along roadways which increases the potential conflict points. Because of this, the applicant shall show the ability to provide cross access to adjacent parcels of like land uses. If any of the adjacent properties are developed under the same developer/owner, then cross access must be provided. All cross access shall be in accordance with the Hillsborough County Land Development Code Section 6.04.03 Q. Prior to or concurrent with the initial increment of development, the developer shall construct an vehicular and pedestrian access/cross-access stubout to the western project boundary. This access shall consist of a minimum 24-foot-wide drive aisle between the stubout along the project's western boundary and the westernmost Muck Pond Rd. access connection. The drive aisle shall be constructed with a minimum 6-foot wide sidewalk along the north side, at the back of a raised vertical curb. If Miami curbing is utilized, then a minimum 5-foot wide sidewalk may be constructed; however, such sidewalk shall be separated from the drive aisle with a minimum 5-foot wide green strip. Nothing in this condition shall be constructed to require the developer of the subject PD to construct the north/south portion of the vehicular drive aisle within the Shared Access Facility unless the developer chooses to do so in accordance with the options specified in condition 16.2.~~
21. ~~The applicant shall be required to pave any portion of the access drive which lies within the existing right-of-way (LDC 6.04.05). Notwithstanding anything shown on the PD site plan to the contrary, the developer shall construct internal and external sidewalks in accordance with Sec. 6.03.02. of the LDC.~~
22. ~~The Developer shall widen that portion of Muck Pond Road and McIntosh Road adjacent to the site to accommodate two-way RV traffic. The roadway shall be widened to Hillsborough County Transportation Technical Manual standards. The existing roadway shall be overlaid with a new layer of asphalt per Hillsborough County Standards. The Developer shall provide documentation at Preliminary Site Plan submittal regarding the existing pavement width and right of way along Muck Pond Road and McIntosh Road. The Preliminary Site Plan shall provide notation that the roadway is to be widened to County standards. The Developer shall include the roadway improvements on the submitted Construction Plans. If the Developer can provide signed and sealed~~

documentation that the current pavement width on Muck Pond Road and McIntosh Road meets the minimum standards for two lane collector roadways, then the Developer may not be required to widen the roadway. If PRS 22-0844 is approved, the County Engineer will approve a Design Exception (dated May 31, 2022 and last revised August 8, 2022) which was found approvable with conditions by the County Engineer (on August 9, 2022) for the Muck Pond Rd. substandard road improvements. As Muck Pond Rd. is a substandard collector roadway, the developer will be required to make certain improvements to Muck Pond Rd. consistent with the Design Exception. Specifically, between McIntosh Rd. and the easternmost project driveway, the developer shall:

22.1 Widen the existing travel lanes such that they are 11-feet in width; and,

22.2 Install Type-F curbing along the south side of the roadway.

Additionally, along the entirety of the project's frontage, the developer shall:

22.3 Dedicate and convey to Hillsborough County, a +/- 15-foot wide easement for public access, utilities, and maintenance purposes, within which a minimum 10-foot wide multi-purpose path shall be constructed. Alternatively, at the developer's sole option, the developer may dedicate and convey the underlying fee to Hillsborough County (in lieu of an easement). All such conveyances shall be subject to the review and approval of Hillsborough County.

23. Where applicable and as determined by County Staff the Developer shall construct sidewalks within the right-of-way along all roadways adjacent to the property boundaries and along both sides of all internal roadways. The sidewalks shall be a minimum width of five (5) feet. Pedestrian interconnectivity shall be provided between uses and adjacent parcels. All internal sidewalks shall meet the accessibility requirements of the Americans with Disabilities Act and/or Florida Accessibility Code.
24. The applicant shall provide internal access to any existing or future out parcels on the site (LDC 5.03.05 H). The developer shall construct a minimum 5-foot wide pedestrian cross access connection between the internal sidewalk network and the existing cross access stubout along the project eastern boundary (i.e. to the stubout that was constructed within and along the western boundary of folio 82882.0000). Such connection may be gated; however, if gated the access shall be available for the daily use of project employees, customers, and visitors.
25. If as a result of this developments impact, a signal should be warranted at the intersection of Muck Pond Road and McIntosh Road, the developer shall install, with the concurrence of the County, a signal and appropriate interconnect adjacent signals. The development shall be responsible for the pro-rata share of their impacts. The developer shall submit the 60% roadway construction plans for all collector or arterial roadways to Public Works (Traffic Section) for inclusion of possible signal interconnect communications conduit and pull boxes. The route, number and size of conduit will be stipulated by this review process. If signals are not immediately warranted, the cable will not be required for installation. (This is intended to prevent the need to retrofit new roadways with conduit once signals are warranted). The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department. No vehicular or pedestrian gates shall be permitted within the portion of the site plan designated as a Shared Access Facility, nor any other area of the site plan where such gate will cause traffic to queue within the Shared Access Facility. Additionally, no parking spaces or loading zones shall be permitted within the project which are located within or back out into the Shared Access Facility.
26. According to FDOT, there currently is an insufficient right turn lane from McIntosh Road to westbound I-4. The design vehicle (motor homes, RVs) will require installation of an adequate turn lane to serve these large, slow moving vehicles. FDOT requires that a right turn lane be installed on McIntosh Road for entrance onto the I-4

Interstate ramp. As proffered by the property owner and as shown on the PD site plan, the property owner shall preserve right-of-way along the project's McIntosh Rd. frontage and at the intersection of McIntosh Rd. and Muck Pond Rd. for future potential acquisition and use by the County or others for construction of a roundabout, traffic signal, and/or other intersection improvements. Nothing within this condition shall be construed as prohibiting the developer from locating the required McIntosh Rd. sidewalk within this area and providing the County with an easement for public access and maintenance purposes (or dedication of the underlying fee), if required pursuant to Sec. 6.03.02.D. of the LDC.

~~26.1 — The size of the design vehicle should be considered regarding the width of McIntosh Rd.~~

~~26.2 — Though the driveway onto McIntosh is proposed as a right out only, there must be a physical barrier to keep traffic from making left turns into the driveway.~~

~~26.3 — A possible conflict exists with the auto dealership driveway on the east side of McIntosh Rd.~~

27. The project may be permitted a maximum of 209,000 square feet of commercial uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum square footage for the project as permitted herein exceeds the maximum intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the square footage of the project shall decrease as necessary to conform to the Comprehensive Plan.
28. The stormwater management system shall be designed and constructed in such a manner so as to not adversely impact off-site surface and groundwater elevations.
29. Approval of this application does not ensure that water will be available at the time when the applicant seeks permits to actually develop.
30. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
31. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
32. Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations and ordinances of Hillsborough County.
33. Effective as of February 1, 1990, this development order/permit shall meet the concurrency requirements of Chapter 163, Part II, Florida Statutes. Approval of this development order/permit does not constitute a guarantee that there will be public facilities in place at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.

Zoning Administrator Sign Off:



J. Brian Grady
Tue Aug 9 2022 15:59:11

SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.

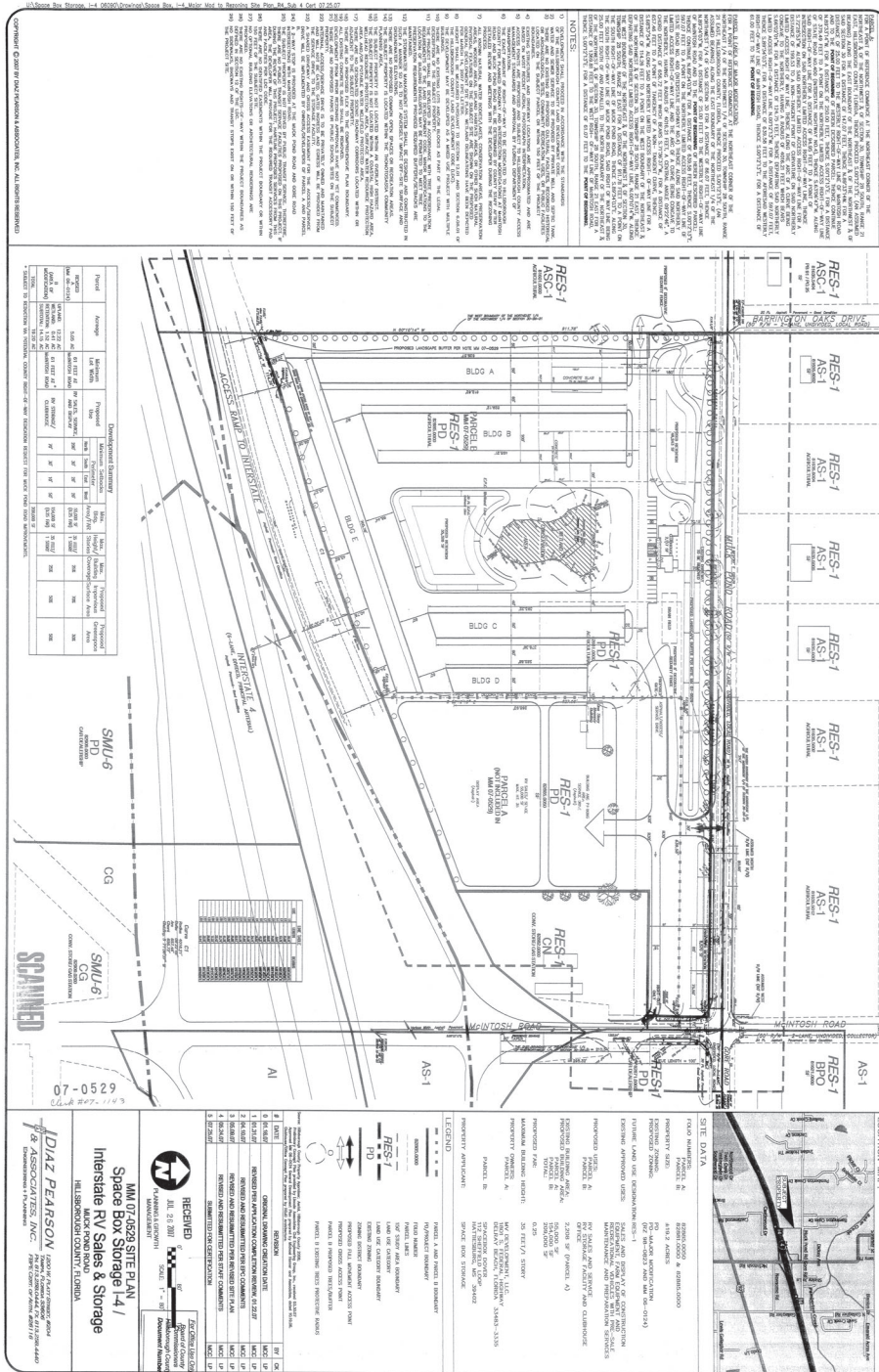
Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS

None.

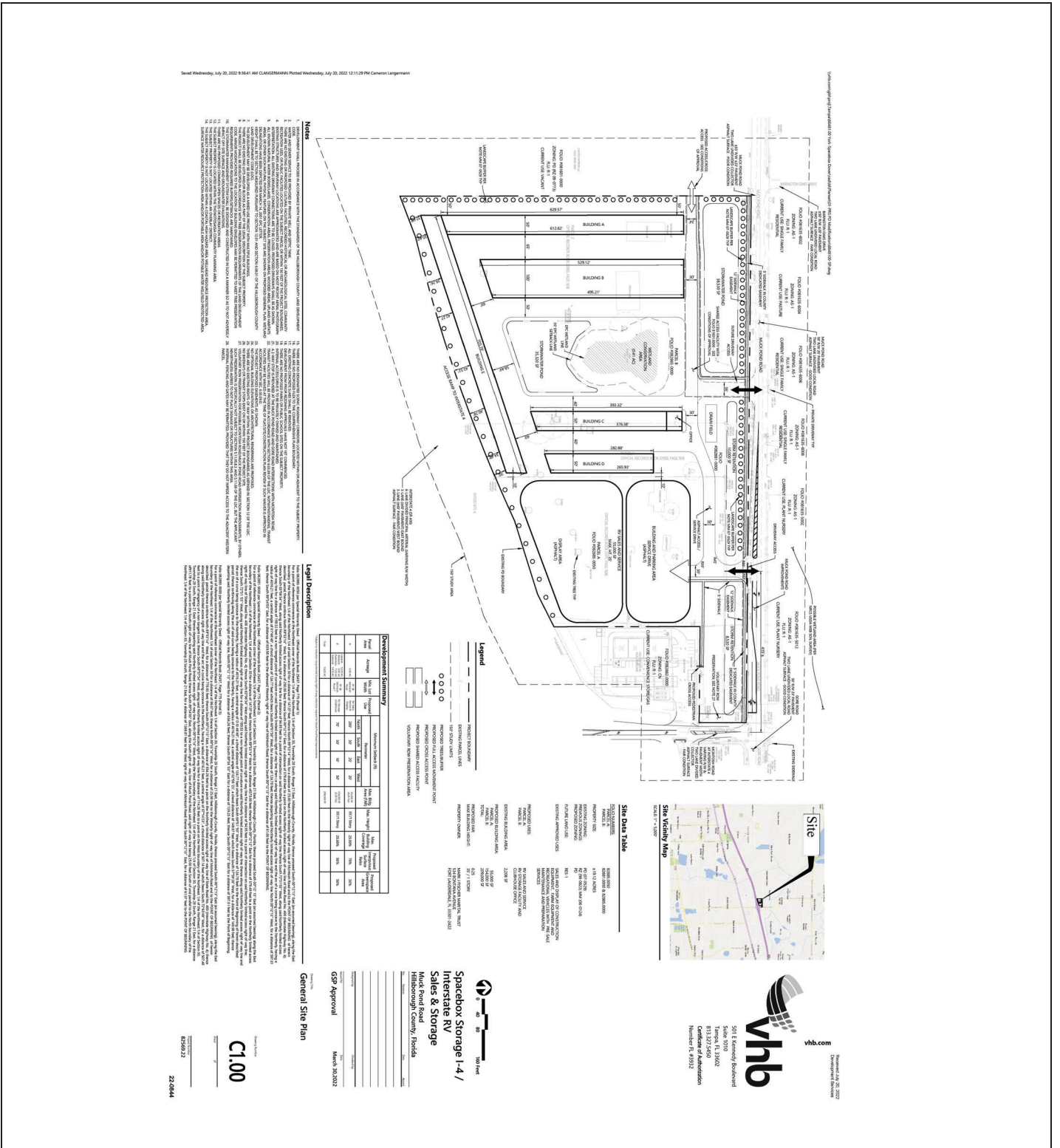
8.0 SITE PLANS (FULL)

8.1 Approved Site Plan (Full)



8.0 SITE PLANS (FULL)

8.2 Proposed Site Plan (Full)



9.0 FULL TRANSPORTATION REPORT (see following pages)

AGENCY REVIEW COMMENT SHEET

TO: Zoning Technician, Development Services Department

DATE: 07/13/2022

Revised: 8/9/2022

REVIEWER: James Ratliff, AICP, PTP, Principal Planner

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: TH/ Central

PETITION NO: PRS 22-0844

- This agency has no comment.
- This agency has no objection.
- This agency has no objection, subject to listed or attached conditions.
- This agency objects for the reasons outlined below.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Conditions

4. All vehicle parking, display, storage and service areas shall be located a minimum of 200 feet from the north boundary of the PD site, except as referenced above. ~~The project's internal access drive shall be located a minimum of 100 feet from the north boundary of the PD site.~~

[Staff is recommending modification of this condition to comport with current practice. Staff notes that minimum throat depth is governed by Sec. 6.04.03.G. and Sec. 6.04.04.A. of the LDC. The currently proposed site plan appears to include a +/- 90-foot throat depth for the easternmost access which, based upon current standards, exceeds minimum requirements. The westernmost access includes a throat depth in excess of 100 feet, which staff believes is appropriate and satisfies the minimum requirements of the LDC. Staff notes that compliance with LDC throat depth requirements and the PD site plan will be required, regardless of whether there is a specific implementing regulation to that effect.]

12. A buffer area with a minimum width of 50 feet shall be provided along the entire west boundary. The westernmost 955 feet of the northern boundary shall have a 100-foot buffer with the exception of the clubhouse, which will have a 70-foot-wide buffer. Within the buffer area, the developer shall install screening as follows:
- 12.1. Along the north boundary and extending southward along the west boundary, there shall be a landscaped berm of undulating height. The berm shall have a minimum height of four feet above grade and shall be crowned with evergreen shrubs and trees. The shrubs shall have a minimum height of two feet above berm height and minimum opacity of 75 percent at time of planting. The trees shall have a minimum height of 10 feet above berm height and minimum caliper of two inches at time of planting, and shall be placed no more than 20 feet apart on centers. The berm shall be placed along the exterior edge of the buffer area.
- 12.2. Along the remainder of the west boundary, there shall be an evergreen hedge with a minimum height of six feet and minimum opacity of 75 percent at time of planting.
- 12.3. The balance of the buffer area shall be grassed. Existing encroachments in the buffer area, such as building foundations, shall be removed prior to any commercial use of the PD site. Prior to certification, notations on the general site plan shall be revised to conform with this condition.
- 12.4. All required screening shall be installed prior to any commercial use of the PD site.
- 12.5. No fences shall be allowed within the buffer area, unless otherwise specified herein. Existing fences shall be relocated accordingly prior to any commercial use of the PD site. Fences shall conform with Section 6.07.00.C.3 of the Land Development Code., except that for Parcel A chain

link fences with a maximum height of 7 feet and topped with barbed wire shall be permitted if placed at least 100 feet from the Muck Pond Road and McIntosh Road rights-of-way. For Parcel B, a six foot opaque decorative fence shall be permitted to be located as shown on the general site plan. The decorative fence shall be wrought iron or similar material but cannot be chain link.

12.6 Notwithstanding anything herein these conditions or on the PD site plan to the contrary, buffering, screening, and fencing shall not be permitted in areas required to facilitate proposed vehicular and pedestrian access or cross access.

[Staff is recommending modification of this condition to clarify where buffering will be required. Staff notes that the developer is not constructing the westernmost access to Muck Pond Rd. with the initial increment of development, rather such connection will be constructed by the developer of the project to west. Staff notes the majority of that project's entitlements/area is required to take access through the subject PD.]

16. ~~Subject to FDOT approval, the site will be restricted to one right-out only access on McIntosh Road. The general design and location of the access point on Muck Pond Road shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (Land Development Code Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and Growth Management Department. Final design, if approved by Hillsborough County Planning and Growth Management Department may include, but is not limited too: left turn lanes, acceleration lane(s) and deceleration lane(s). Access points may be restricted in movements. The project shall be served by two (2) full access connections to Muck Pond Rd. Vehicular access to McIntosh Rd. shall not be permitted. Additionally:~~

16.1 The developer shall construct the easternmost access concurrent with the initial increment of development; and,

16.2 The developer shall have the option of constructing the westernmost access, i.e. the portion between the east/west running drive aisle (construction of which shall be the responsibility of the developer of this project) and Muck Pond Rd., together with the initial increment of development. Additionally:

16.2.1 If the developer decides not to construct such access, the developer shall record, an ingress/egress, construction and any other easements necessary to permit the owners of folio 81601.0000 (which is subject to a PD zoning approval that requires the majority of its development to take access through the subject PD) to construct and utilize the westernmost project access without further consultation of the owners of the lands within the subject PD.

16.2.2 If the developer decides to construct such access, the developer shall record an ingress and egress easement over the area, as required pursuant to Condition 18. Additionally, the developer shall be responsible for substandard road improvements on Muck Pond Rd. between the westernmost project access and the improvements described in the Design Exception, described hereinbelow.

[Staff is recommending modification of this condition to comport with the current access plan. Also, as discussed in the body of the report, the lands within the adjacent PD and subject PD were previously owned by the same owner. That owner then chose to restrict an adjacent PD such that the majority its project must take access through this PD; however, no easements were apparently necessary or formalized, due to the shared ownership at the time. Now that ownership interests have diverged, it is important that these arrangements be formalized, and site plan and zoning condition notations be brought into conformance with the current practice and wording for such facilities/concepts as contained within the current LDC.]

17. ~~Prior to Concurrency approval, the Developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic at the project driveway(s). This analysis shall include a passenger car to heavy vehicle conversion ratio to determine the increased length in turn lane being required. The turn lane(s) shall be constructed to FDOT and/or Hillsborough County standards using FDOT standard Index 301 & 526 and an asphalt overlay shall be applied over the entire portion of roadway where a left turn lane is provided. The Developer shall construct a westbound to southbound left turn lane on Muck Pond into the easternmost project driveway. The turn lane shall be constructed in accordance with the Design Exception described in condition 22. Construction of a westbound to southbound left turn lane into the westernmost driveway shall be the responsibility of the developer of the adjacent PD utilizing such access, if warranted pursuant to Sec. 6.04.04.D. of the LDC, the following turn lanes at his expense:~~

~~17.1 Provide NB to WB left turn lane at the intersection of McIntosh Road @ Muck Pond Road~~

~~17.2 Provide a WB to SB left turn lane on Muck Pond Road into the site~~

~~If it is determined by the results of the analysis submitted by the developer, that adequate right of way does not exist to construct any needed improvements (i.e. turn lanes), then the developer shall re-submit an analysis showing the maximum development that could occur to still enable the needed improvements to be constructed. The development will be limited to the size as shown in the submitted analysis.~~

[Staff is recommending modification of this condition, as it was based upon Hillsborough County's former system for the evaluation and mitigation of transportation impacts, i.e. transportation concurrency. Given HB 7207 (2011) and the County's adoption of a mobility fee ordinance, this condition is no longer enforceable. Staff notes future development will be required to pay mobility fees unless grandfathered in accordance with the ordinance. Staff has repurposed this condition to describe required site access improvements, as well as discuss the potential 2nd westbound left turn lane required at the westernmost access, the presence of which is contingent upon the type and amount of uses ultimately developed within the adjacent PD to the west of this site.]

18. ~~Based on the projected trip generation to the site, access onto the public road would be via "Type II" Minor Roadway Connection (50 — 1,500 trip ends per day), LDC 6.04.01.E. The Land Development Code requires that all internal access (the "throat") to the driveways must be a minimum of 50 feet from the edge of pavement of the public roadway, and shall remain free of internal connections or parking spaces that might interfere with the movement of vehicles into or out of the site. The applicant has the option of submitting an analysis showing that for his particular site, a throat of less than 50 feet is appropriate and will result in no adverse impact to the public roadway system. Because of the length of the vehicles using the site, additional throat depth may be required. If the Driveway is to be gated, then the throat depth may exceed this length to accommodate stacking vehicles. As shown on the PD site plan, certain internal driveways, drive aisles and sidewalks shall be considered Shared Access Facilities with folio 81601.0000 (and such facilities shall be treated as the sole legal means of vehicular access to certain development within the adjacent folio, as specified within the PD regulating such adjacent property)". Together with the initial increment of development within the subject PD, the property owner shall record in the Official Records of Hillsborough County a non-exclusive ingress and egress easement and any other easements necessary to permit the developer of folio 81601.0000 to take access through the areas designed as a Shared Access Facility within subject PD, without further consultation of the property owner of the subject PD. The design and locations of all connections shall be subject to the review and approval of Hillsborough County.~~

[Staff is recommending modification of this condition to repurpose it order to describe Shared Access Facility requirements. As currently approved, this condition speaks to minimum throat depth requirements, which is duplicative of the existing requirements within existing condition 4. Those existing conditions within condition 4 were themselves not needed, for the reasons explained in the staff analysis provided underneath that modified condition.]

20. ~~As is noted in the Land Development Code, one of the major reasons for diminished capacity of public roads is an increase in access points along roadways which increases the potential conflict points. Because of this, the applicant shall show the ability to provide cross access to adjacent parcels of like land uses. If any of the adjacent properties are developed under the same developer/owner, then cross access must be provided. All cross access shall be in accordance with the Hillsborough County Land Development Code Section 6.04.03~~ Prior to or concurrent with the initial increment of development, the developer shall construct an vehicular and pedestrian access/cross-access stubout to the western project boundary. This access shall consist of a minimum 24-foot-wide drive aisle between the stubout along the project's western boundary and the westernmost Muck Pond Rd. access connection. The drive aisle shall be constructed with a minimum 6-foot wide sidewalk along the north side, at the back of a raised vertical curb. If Miami curbing is utilized, then a minimum 5-foot wide sidewalk may be constructed; however, such sidewalk shall be separated from the drive aisle with a minimum 5-foot wide green strip. Nothing in this condition shall be constructed to require the developer of the subject PD to construct the north/south portion of the vehicular drive aisle within the Shared Access Facility unless the developer chooses to do so in accordance with the options specified in condition 16.2

[Staff is recommending modification of this condition to clarify the wording, conform with current practice, and also expand the condition to address certain design considerations which are needed due to the potential for a school use on the adjacent PD (which would take its sole access through the subject PD).]

21. ~~The applicant shall be required to pave any portion of the access drive which lies within the existing right of way (LDC 6.04.05).~~ Notwithstanding anything shown on the PD site plan to the contrary, the developer shall construct internal and external sidewalks in accordance with Sec. 6.03.02. of the LDC.

[Staff is recommending modification of this condition to repurpose it order include a condition which makes it clear that while some sidewalk connections are shown on the PD site plan, others may be needed into order to comply with Sec. 6.03.02. of the LDC and/or ADA and Florida Accessibility Code requirements. A detailed review of sidewalk routing and sidewalk design will not be possible until the site/construction plan review stage.]

22. If PRS 22-0844 is approved, the County Engineer will approve a Design Exception (dated May 31, 2022 and last revised August 8, 2022) which was found approvable with conditions by the County Engineer (on August 9, 2022) for the Muck Pond Rd. substandard road improvements. As Muck Pond Rd. is a substandard collector roadway, the developer will be required to make certain improvements to Muck Pond Rd. consistent with the Design Exception. Specifically, between McIntosh Rd. and the easternmost project driveway, the developer shall:

22.1 Widen the existing travel lanes such that they are 11-feet in width; and,

22.2 Install Type-F curbing along the south side of the roadway.

Additionally, along the entirety of the project's frontage, the developer shall:

22.3 Dedicate and convey to Hillsborough County, a +/- 15-foot wide easement for public access, utilities, and maintenance purposes, within which a minimum 10-foot wide multi-purpose path shall be constructed. Alternatively, at the developer's sole option, the developer may dedicate and convey the underlying fee to Hillsborough County (in lieu of an easement). All such conveyances shall be subject to the review and approval of Hillsborough County.

~~The Developer shall widen that portion of Muck Pond Road and McIntosh Road adjacent to the site to accommodate two way RV traffic. The roadway shall be widened to Hillsborough County Transportation Technical Manual standards. The existing roadway shall be overlaid with a new layer of asphalt per Hillsborough County Standards. The Developer shall provide documentation at Preliminary Site Plan~~

~~submittal regarding the existing pavement width and right of way along Muck Pond Road and McIntosh Road. The Preliminary Site Plan shall provide notation that the roadway is to be widened to County standards. The Developer shall include the roadway improvements on the submitted Construction Plans. If the Developer can provide signed and sealed documentation that the current pavement width on Muck Pond Road and McIntosh Road meets the minimum standards for two lane collector roadways, then the Developer may not be required to widen the roadway.~~

[Staff is recommending modification of this condition to comport with current practice. Given the changes to proposed access, the applicant was required to address the Muck Pond Rd. substandard road issues concurrent with this zoning modification request. This condition reflects the Design Exception requested by the applicant, and found approvable with conditions by the County Engineer.]

24. ~~The applicant shall provide internal access to any existing or future out parcels on the site (LDC 5.03.05 H).~~The developer shall construct a minimum 5-foot wide pedestrian cross access connection between the internal sidewalk network and the existing cross access stubout along the project eastern boundary (i.e. to the stubout that was constructed within and along the western boundary of folio 82882.0000). Such connection may be gated; however, if gated the access shall be available for the daily use of project employees, customers, and visitors.

[Staff is recommending modification of this condition to repurpose it for use in addressing the pedestrian cross access along the eastern project boundary. The existing condition language speaks to vehicular interconnectivity between internal uses and outparcels. This condition is not necessary. The PD site plan, together with the definition of the purpose of intent of PD districts within Sec. 5.03.04.A. and Sec. 5.03.06.C.6.a. of the LDC, make it clear that a unified PD is required with the intent of permitting creative, innovative and/or mixed-use development.]

25. ~~If as a result of this developments impact, a signal should be warranted at the intersection of Muck Pond Road and McIntosh Road, the developer shall install, with the concurrence of the County, a signal and appropriate interconnect adjacent signals. The development shall be responsible for the pro rata share of their impacts. The developer shall submit the 60 % roadway construction plans for all collector or arterial roadways to Public Works (Traffic Section) for inclusion of possible signal interconnect communications conduit and pull boxes. The route, number and size of conduit will be stipulated by this review process. If signals are not immediately warranted, the cable will not be required for installation. (This is intended to prevent the need to retrofit new roadways with conduit once signals are warranted). The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department. No vehicular or pedestrian gates shall be permitted within the portion of the site plan designated as a Shared Access Facility, nor any other area of the site plan where such gate will cause traffic to queue within the Shared Access Facility. Additionally, no parking spaces or loading zones shall be permitted within the project which are located within or back out into the Shared Access Facility.~~

[Staff is recommending repurposing of this condition for use in addressing certain internal project gates as well as drive aisle design within the Shared Access Facility. Staff is recommending removal of the existing condition, which was based upon Hillsborough County's former system for the evaluation and mitigation of transportation impacts, i.e. transportation concurrency. Given HB 7207 and the County's adoption of a mobility fee ordinance, this condition is no longer enforceable. Additionally, the applicant has proffered a condition, included herein below, which requires the applicant to preserve right-of-way to accommodate a future potential roundabout or signalization infrastructure at the intersection of McIntosh Rd. and Muck Pond Rd.]

26. As proffered by the property owner and as shown on the PD site plan, the property owner shall preserve right-of-way along the project's McIntosh Rd. frontage and at the intersection of McIntosh Rd. and Muck Pond Rd. for future potential acquisition and use by the County or others for construction of a roundabout, traffic signal, and/or other intersection improvements. Nothing within this condition shall be construed as prohibiting the developer from locating the required McIntosh Rd. sidewalk within this area and providing

~~the County with an easement for public access and maintenance purposes (or dedication of the underlying fee), if required pursuant to Sec. 6.03.02.D. of the LDC. According to FDOT, there currently is an insufficient right turn lane from McIntosh Road to westbound I 4. The design vehicle (motor homes, RVs) will require installation of an adequate turn lane to serve these large, slow moving vehicles. FDOT requires that a right turn lane be installed on McIntosh Road for entrance onto the I 4 Interstate ramp.~~

~~26.1 — The size of the design vehicle should be considered regarding the width of McIntosh Rd.~~

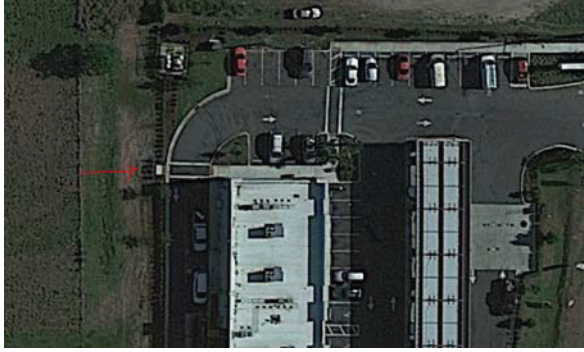
~~26.2 — Though the driveway onto McIntosh is proposed as a right out only, there must be a physical barrier to keep traffic from making left turns into the driveway.~~

~~26.3 — A possible conflict exists with the auto dealership driveway on the east side of McIntosh Rd.~~

[Staff is recommending repurposing of this condition to address the proffered right-of-way preservation at the intersection of McIntosh Rd. and Muck Pond Rd. Staff is recommending removal of the existing condition language. Portions of this condition relating to improvements at the ramp at I-4 and McIntosh Rd. were based upon Hillsborough County's former system for the evaluation and mitigation of transportation impacts, i.e. transportation concurrency. Given HB 7207 and the County's adoption of a mobility fee ordinance, this condition is no longer enforceable. Additionally, staff notes that the applicant is proposing to remove the exit only driveway onto McIntosh Rd., and that the "possible conflict" with the auto dealership driveway on the east side of McIntosh Rd. appears to have been mitigated by restriction of that access to right-in/right-out turning movements only.]

Other Conditions

- Prior to PD site plan certification, the developer shall revise the PD site plan to:
 - Revise the labels reading "12' Sidewalk Easement" to state "15' Public Access, Utility and Maintenance Easement". Staff notes such revisions are necessary for consistency with the site plan shown in the requested Design Exception.
 - Revise the labels reading "5' Sidewalk In County Dedicated Easement" to state "10' Multi-Purpose Path In County Dedicated Easement". Staff notes such revisions are necessary for consistency with the site plan shown in the requested Design Exception.
 - Revise the label reading "Voluntary ROW Preservation; See Note 27" to read, "Voluntary Right-of-way Preservation – See PD Note 27 and Zoning Condition 26".
 - Update the PD site plan for adjacent folio 82882.0000 to show the site improvements constructed within the adjacent project. Staff notes that the pedestrian infrastructure and cross access within that site are not as currently shown.
 - Revise the location of the proposed pedestrian cross access and internal pedestrian infrastructure such that the sidewalk connection occurs along the adjacent folio's western project boundary, in the location such stubout was actually constructed (see below aerial).
 - Revise Note 21 to change the words "is provided" to read "exists".
 - Revise Note 23 to add to the end of the sentence "and/or as required by the condition of approval."
 - Replace Note 28 with a note reading something similar to "Internal fencing and gating may be provided. Reference zoning conditions, including conditions 12.6 and 25 for restrictions and other pertinent information."
- Prior to PD site plan certification, if the zoning modification is approved, the Developer shall provide a signed and sealed Design Exception (the revised date of the letter shall be unchanged). Staff notes that while the first two versions of the letter were signed, the August 8, 2022 letter was sealed, but appears to have been inadvertently unsigned.



PROJECT SUMMARY AND TRIP GENERATION

The applicant is requesting a Major Modification (MM) to previously approved Planned Development (PD) #98-0823, as most recently modified via MM 07-0529. The MM consists of three parcels totaling +/- 19.07 ac., and is currently approved for 55,000 s.f. Recreational Vehicle (RV) sales/display and (pre-sale) maintenance center uses, 154,000 s.f. of RV storage facility uses, and a 2,117 s.f. clubhouse. The applicant is proposing to add an additional access driveway to Muck Pond Rd., eliminate the exit-only driveway on McIntosh Rd., and add a pedestrian cross access connection to the east. Several other changes to the site plan were made to accommodate redesign of the site based on these and/or other issues, including redesign of the stormwater ponds, and considerations for multi-purpose path placement within an easement along Muck Pond Rd.

Although the project is not modifying the project entitlements and is therefore not increasing or decreasing the maximum trip generation potential of the subject property, the project is modifying its project access (to eliminate the exit only driveway) which will impact the amount of eastbound traffic on Muck Pond Rd. As such, the developer to submit a trip generation and site access analysis consistent with the Development Review Procedures Manual (DRPM). Staff did not require the applicant to submit a trip generation and site access analysis which examined access impacts created by the adjacent PD to the west, which takes its access through the subject PD, since those impacts were previously analyzed as a part of the rezoning of that property (as further described herein below).

Staff has prepared a comparison of the potential number of peak hour trips generated under the existing and proposed zoning designations utilizing a generalized worst-case scenario. Information show below is based upon data from the 11th Edition of the Institute of Transportation Engineer’s Trip Generation Manual.

Existing Zoning:

| Land Use/Size | 24 Hour Two-Way Volume | Total Peak Hour Trips | |
|--|------------------------|-----------------------|----|
| | | AM | PM |
| PD, 55,000 s.f. RV Sales (ITE Code 842) | 275 | 25 | 42 |
| PD, 154,000 s.f. RV Storage (ITE Code 151) | 223 | 14 | 23 |
| Subtotal: | 498 | 39 | 65 |

Proposed Zoning:

| Land Use/Size | 24 Hour Two-Way Volume | Total Peak Hour Trips | |
|---|------------------------|-----------------------|----|
| | | AM | PM |
| PD, 55,000 s.f. RV Sales (ITE Code 842) | 275 | 25 | 42 |
| PD, 154,000 s.f. RV Storage | 223 | 14 | 23 |

| | | | |
|----------------|-----|----|----|
| (ITE Code 151) | | | |
| Subtotal: | 498 | 39 | 65 |

Trip Generation Difference:

| Land Use/Size | 24 Hour Two-Way Volume | Total Peak Hour Trips | |
|-------------------|------------------------|-----------------------|------------------|
| | | AM | PM |
| Difference | No Change | No Change | No Change |

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Muck Pond Rd. a 2-lane, undivided, substandard, collector roadway characterized by +/- 19 feet of pavement in average condition. The roadway lines within a +/- 50-foot wide right-of-way along the project's frontage. There are no sidewalks or bicycle facilities along Muck Pond Rd. in the vicinity of the proposed project.

SITE ACCESS

The applicant is proposing two access driveways to Muck Pond Rd. The applicant is also adding a pedestrian connection to the convenience/gas station to the east of the proposed project, which itself constructed a pedestrian stubout to the subject property. Although the maximum trip generation potential of the proposed by itself does not warrant two access driveways per Section 6.04.03.I. of the LDC, the addition of project trips from the adjacent PD #09-0773 (as more fully discussed below), would warrant the additional access driveway. Given this, staff believes the second access driveway is necessary and appropriate.

Adjacent PD

Subsequent to approval of a modification to this PD in 2007 (via case #07-0529), the parcel to west was rezoned to PD via case #09-0773. The adjacent PD is approved for up to 206,365 s.f. of a variety of uses; however, there was a restriction placed on the project such that 7,500 s.f. of the uses had to be developed as a daycare, and that particular use was the only one permitted to take direct access to Muck Pond Rd. The remaining 198,865 s.f. of entitlements were required to access Muck Pond Rd. via a cross access connection through the PD that is the subject of this rezoning petition.

Given the above, Transportation Review Section staff estimates that up to 7,185 Average Daily Trips, 475 a.m. peak hour trips, and 710 p.m. peak hour trips could take access through the subject PD. These calculations were arrived at assuming all 198,865 s.f. were developed as Medical Office uses (ITE Code 720). It should be noted that there is the potential for significantly higher amounts of traffic to utilize the shared access driveways within the subject PD, given the list of allowable uses includes public and private K-12 schools. For example, a 1,500-student private school could generate 1,215 a.m. peak hour trips.

The adjacent PD has the potential to generate significantly more traffic that the PD which is the subject of this zoning action. Together both projects would require three (3) or more access driveways per the LDC; however, given proposed site access improvements and other considerations, staff believes the combined projects' traffic can be safely accommodated with two access driveways on Muck Pond Rd.

Additional Background on Shared Access

The Zoning Hearing Master's October 9, 2009 Recommendation for adjacent PD #09-0773, which was subsequently approved by the Hillsborough County BOCC, states the following as a Finding of Fact, "The project traffic, with the exception of the proposed day care center, will be required to circulate through the adjacent approved planned development in order to gain access to Muck Pond Road and McIntosh Road. Cross-access was approved for the adjacent PD and cross-access is required of the subject PD."

The Development Services Department's Staff Recommendation Report for adjacent PD #09-0773, dated September 28, 2009, stated, "The applicant has agreed to only the daycare use having access onto Muck Pond Road. All other uses will need to obtain access via cross access with the PD to the east, which is partially owned by the same property owner."

Shared Driveway

Given the above approvals for the adjacent PD described above, the applicant has the area to be utilized by the adjacent development to be designed as a Shared Access Facility, in accordance with current Land Development Code requirements and staff procedures.

Staff has proposed a condition requiring the Shared Access Facility be designed such that there are no parking spaces with direct access to/from the facility, nor any gates which will impede the safe and efficient functioning of this facility. This will ensure that there are no disruptions to the safe and efficient flow of traffic, both within this PD and at the project driveway(s) onto Muck Pond Rd. Section 6.04.04.G. of the LDC states, "The driveway should be constructed so that all entering and exiting movements can be accomplished with minimum disruption to traffic flow on the intersection roadway." Section 6.04.03.N. states "...all buildings and other structures, land preparation, and landscaping shall be so located and arranged as to provide safe and convenient access for emergency purposes, fire protection, serving, and off-street parking and loading..." Finally, Section 6.04.04.A. states "Driveways provide the physical transition between a site and the abutting roadway. Driveways should be located and designed to minimize impacts on traffic while providing safe entry and exit from the development served. The location and design of the connection must take into account characteristics of the roadway, the site, and the *potential* users. The actual width and length of driveways shall be subject to internal and external traffic flow considerations... This will require that the entering vehicles not be confronted with maneuvering vehicles..." *[emphasis added]*

Turn Lanes and Roundabout

The applicant's traffic analysis indicates that 90% of traffic will be coming to/from the site from the east (McIntosh Rd./Interstate 4). Regarding the trips generated by this project, i.e. not including the adjacent PD traffic, of the approximately 65 peak hour trips in the highest overall peak hour (i.e. the p.m. peak hour period), 24 will be entering the site, and of those 24 trips 21 will be entering to/from the east. The project site appears to be designed such that the eastern access will serve the uses which are the subject of this PD, and the additional (westernmost) access will primarily serve the adjacent development.

Both driveways will meet Section 6.04.04.D. warrants for auxiliary (turn) lanes. The applicant is proposing to construct only the easternmost access and turn lane. Given that the westernmost access and turn lane is not needed to support project traffic, the applicant will be required to provide an access and construction easement which will allow the developer of the adjacent PD to access their property and have all rights necessary to permit construction of any driveway and sidewalk improvements within the north/south portions of Shared Access Facility, area not otherwise constructed by the developer of the project which is the subject of this PD, without further consultation from the property owner.

Previous efforts to rezone this property several years ago (which was subsequently withdrawn) resulted in the identification of a roundabout at the intersection of as a potential future improvement (by the County or others) which could would potentially increase the operational efficiency of the intersection as well as eliminate the need for turn lanes on the approaches to the intersection. The proximity of the intersection to the I-4 on ramps as well as a lack of available right-of-way along the intersection approaches would make implementation of a fully signalized conventional intersection difficult; however, the right-of-way could also be utilized for this purpose if traffic studies and designs identified that as a preferred future improvement. In order to help preserve land needed to keep the possibility of an alternative intersection design (such as the roundabout) alive, the applicant has proffered to preserve additional right-of-way within the subject PD in the vicinity of the intersection. As such, no permanent uses will be permitted within this preserved area in order to preserve the viability of its future acquisition and use.

Removal of Exit Only Driveway to McIntosh Rd.

The existing zoning approval includes an exit only driveway onto McIntosh Rd., located approximately 120 feet south of the intersection of Muck Pond Rd. and McIntosh Rd. and approximately 105 feet north of one of two existing driveways to a gas station within folio 082882.0000. That driveway does not meet the driveway spacing

standards found within today's Land Development Code, which requires a minimum connecting spacing of approximately 245 feet for a Class 5 roadway.

The transportation analysis for adjacent PD #09-0773 did not adequately analyze the worst-case trip generation potential of the subject property. The Transportation staff report at the time stated "...the applicant's site plan is noting a grouping of allowable uses which would potentially generate more traffic than that which was analyzed..." Staff was able to partially mitigate this concern since the County still had a concurrency management system at that time. Staff wrote, "If it is determined by the results of the analysis submitted by the developer, that adequate right-of-way does not exist to construct any needed improvements (i.e. turn lanes), then the developer shall re-submit an analysis showing the maximum development that could occur to still enable the needed improvements to be constructed. The development may be limited to the size as shown in the submitted analysis."

The owner of the gas station to the east of the proposed project redeveloped the subject site (via PI #4210). The new site included a significant expansion of both the convenience store and gas pumps, and also included the addition of a quick service restaurant and coffee/donut shop with drive-through. Redevelopment of that parcel involved the closure of the existing driveways on McIntosh Rd. serving that site, with a single full-access driveway connection north of the northernmost driveway (generally across from the right-in/right-out driveway serving Camping World) now being the only project access. The developer of that project was also required to construct a northbound to westbound left turn lane into their project driveway.

In previous years, area citizens have expressed specific concern regarding the proposed removal of the exit only driveway to McIntosh Rd. Given the relocation of the gas station driveway, as well as significant intensification in trip generation due to the proposed redevelopment of that property, both as described above, staff had (and continues to have) significant concerns with the proximity of the new gas station driveway to the exit only driveway.

If it were to remain, the exit only driveway would be within +/- 40 feet of the new driveway. In addition to not complying with Section 6.04.07 spacing standards, such a driveway spacing would unnecessarily create a significant number of conflict points that could otherwise be managed by directing exiting traffic on the subject property to Muck Pond Rd. (a collector roadway designated for such traffic). Staff has prepared a graphic (attached) showing some of the conflicting turning movements that would be created should the exit only driveway remain. Although staff is sensitive to the citizen concerns regarding the effects of exiting project traffic (9 trips in the a.m. peak hour and 37 trip in the p.m. peak hour) onto Muck Pond Rd., staff believes that the health, safety, and welfare of the traveling public would be better served and protected through elimination of the exit only access driveway onto McIntosh Rd.

REQUESTED DESIGN EXCEPTION – MUCK POND RD.

As Muck Pond Rd. is a substandard collector roadway, the applicant's Engineer of Record (EOR) submitted a Design Exception request for Muck Pond Rd. (dated May 31, 2022 and last revised August 8, 2022) to determine the specific improvements that would be required by the County Engineer. Based on factors presented in the Design Exception request, the County Engineer found the Design Exception request approvable with conditions (on August 9, 2022). The deviations from the Hillsborough County Transportation Technical Manual (TTM) TS-7 Typical Section (for 2-Lane, Rural Local and Collector Roadways) include:

1. The developer will be permitted to widen the pavement such that the travel lanes are 11-feet in width, rather than the 12-foot lanes required per TS-7;
2. The developer shall be permitted to construct the turn lane with 11-foot wide lanes, in lieu of the 12-foot wide lanes required per TS-7;
3. The developer will be permitted to reduce the overall right-of-way width from 96 feet required per TS-7 and utilize the existing 50-feet of right-of-way plus an additional 15-foot wide easement along the south side of Muck Pond Rd. along the project's frontage;
4. The developer will be permitted to construct a 10-foot wide multi-purpose pathway in lieu of the 5-foot wide paved shoulders serving as bicycle facilities within TS-7; and,

5. The developer shall be permitted to utilize Type-F curb along the south side of the roadway and leave the north side of the roadway in its existing condition, in lieu of the 8-foot wide stabilized shoulders, of which 5-feet are paved, required per TS-7.

The County Engineer found the request approval with two conditions. First, the Design Exception showed a 10-foot wide multi-purpose pathway being provided along the entire project frontage, where as the text described it as only being provided between the easternmost driveway and McIntosh Rd. The County Engineer’s approval requires the improvements proposed in the attached site plan be constructed (i.e. the multi-purpose path along the entire project frontage). Second, the County Engineer noted that the latest revised request was sealed but was not signed. Provided the BOCC approves the zoning modification, the County Engineer required the applicant to submit a signed letter before they will formally approve the Design Exception.

If PRS 22-0844, is approve, the County Engineer will approve the above referenced Administrative Variance, subject to the conditions described above and prosed herein the zoning conditions of approval.

ROADWAY LEVEL OF SERVICE (LOS) INFORMATION

Muck Pond Rd. is not a regulated roadway or was not included in the Hillsborough County LOS report. As such, no information for the facility can be provided. Information for the other adjacent segments is provided below.

| Roadway | From | To | LOS Standard | Peak Hour Directional LOS |
|--------------|----------|------------------|--------------|---------------------------|
| McIntosh Rd. | I-4 Ramp | Thonotosassa Rd. | C | C |

Source: Hillsborough County 2020 Level of Service Report.

Ratliff, James

From: Williams, Michael
Sent: Tuesday, August 9, 2022 12:19 PM
To: Micahel Yates (myates@palmtraffic.com); Vicki Castro
Cc: Tirado, Sheida; Ratliff, James; PW-CEIntake; Heinrich, Michelle
Subject: RE: RE PRS 22-0844
Attachments: 22-0844 DReq 08-09-22.pdf

Resent with additional cc:.

From: Williams, Michael
Sent: Tuesday, August 9, 2022 12:17 PM
To: Micahel Yates (myates@palmtraffic.com) <myates@palmtraffic.com>; Vicki Castro <vcastro@palmtraffic.com>
Subject: FW: RE PRS 22-0844

Michael/Vicki,

I have found the attached Section Design Exception (DE) for PD 22-0844 APPROVABLE with Conditions. The Condition being that the 10' multiuse path be for the limits of the frontage as reflected in the drawing which is part of this request. Also, the applicant shall provide a signed copy of the DE should the Board approve the zoning.

Please note that it is you (or your client's) responsibility to follow-up with Transportation Staff after the BOCC approves the PD zoning or PD zoning modification related to below request. This is to obtain a signed copy of the DE/AV.

If the BOCC denies the PD zoning or PD zoning modification request, staff will request that you withdraw the AV/DE. In such instance, notwithstanding the above finding of approvability, if you fail to withdraw the request, I will deny the AV/DE (since the finding was predicated on a specific development program and site configuration which was not approved).

Once I have signed the document, it is your responsibility to submit the signed AV/DE(s) together with your initial plat/site/construction plan submittal. If the project is already in preliminary review, then you must submit the signed document before the review will be allowed to progress. Staff will require resubmittal of all plat/site/construction plan submittals that do not include the appropriate signed AV/DE documentation.

Lastly, please note that it is critical to ensure you copy all related correspondence to PW-CEIntake@hillsboroughcounty.org

Mike

Michael J. Williams, P.E.
Director, Development Review
County Engineer
Development Services Department

P: (813) 307-1851
M: (813) 614-2190
E: Williamsm@HillsboroughCounty.org
W: HCFLGov.net

Hillsborough County

601 E. Kennedy Blvd., Tampa, FL 33602

[Facebook](#) | [Twitter](#) | [YouTube](#) | [LinkedIn](#) | [HCFL Stay Safe](#)

Please note: All correspondence to or from this office is subject to Florida's Public Records law.

From: Rome, Ashley <RomeA@hillsboroughcounty.org>

Sent: Tuesday, August 9, 2022 9:26 AM

To: Andrea Papandrew <papandrewa@plancom.org>; Blinck, Jim <BlinckJ@HillsboroughCounty.ORG>; Brown, Gregory <BrownGr@hillsboroughcounty.org>; Bryant, Christina <BryantC@epchc.org>; Cabrera, Richard <CabreraR@HillsboroughCounty.ORG>; Dalfino, Jarryd <DalfinoJ@hillsboroughcounty.org>; Santos, Daniel <daniel.santos@dot.state.fl.us>; David Skrelunas <David.Skrelunas@dot.state.fl.us>; DeWayne Brown <brownd2@gohart.org>; Dickerson, Ross <DickersonR@HillsboroughCounty.ORG>; Ellen Morrison <ellen.morrison@swfwmd.state.fl.us>; Franklin, Deborah <FranklinDS@hillsboroughcounty.org>; Glorimar Belangia <Glorimar.Belangia@hcps.net>; Holman, Emily - PUD <HolmanE@HillsboroughCounty.ORG>; Hummel, Christina <HummelC@hillsboroughcounty.org>; James Hamilton <jkhamilton@tecoenergy.com>; Justin Willits <WillitsJ@gohart.org>; Kaiser, Bernard <KAISERB@HillsboroughCounty.ORG>; Karla Llanos <llanosk@plancom.org>; Katz, Jonah <KatzJ@hillsboroughcounty.org>; Kyle Brown <kyle.brown@myfwc.com>; landuse-zoningreviews@tampabaywater.org; Mineer, Lindsey <Lindsey.Mineer@dot.state.fl.us>; Lindstrom, Eric <LindstromE@hillsboroughcounty.org>; Mackenzie, Jason <MackenzieJ@hillsboroughcounty.org>; Melanie Ganas <mxganas@tecoenergy.com>; Melissa Lienhard <lienhardm@plancom.org>; O'Hern Weeks, Abbie <weeksa@epchc.org>; Olivia Ryall <oryall@teamhcso.com>; Perez, Richard <PerezRL@hillsboroughcounty.org>; Petrovic, Jaksa <PetrovicJ@HillsboroughCounty.ORG>; Pezone, Kathleen <PezoneK@hillsboroughcounty.org>; Ratliff, James <RatliffJa@hillsboroughcounty.org>; Hessinger, Rebecca <HessingerR@hillsboroughcounty.org>; Renee Kamen <renee.kamen@hcps.net>; Rochelle, Randy <RochelleR@HillsboroughCounty.ORG>; Rodriguez, Dan <RodriguezD@gohart.org>; RP-Development <RP-Development@hillsboroughcounty.org>; Salisbury, Troy <SalisburyT@hillsboroughcounty.org>; Sanchez, Silvia <sanchezs@epchc.org>; Shelton, Carla <SheltonC@HillsboroughCounty.ORG>; Steady, Alex <SteadyA@hillsboroughcounty.org>; Tony Mantegna <tmantegna@tampaairport.com>; Turbiville, John (Forest) <TurbivilleJ@HillsboroughCounty.ORG>; WetlandsPermits@epchc.org; Woodard, Sterlin <Woodard@epchc.org>; Yeneka Mills <millsy@plancom.org>
Cc: Heinrich, Michelle <HeinrichM@HillsboroughCounty.ORG>; Vazquez, Bianca <VazquezB@hillsboroughcounty.org>; Tirado, Sheida <TiradoS@hillsboroughcounty.org>; Williams, Michael <WilliamsM@HillsboroughCounty.ORG>
Subject: RE PRS 22-0844

Good Day All,

Please be advised, we have received and uploaded to Optix **revised documents/plans** for the above mentioned application. Please review and comment.

For further information regarding the change/update please contact the assigned planner.

Planner assigned:

Planner: Michelle Heinrich

Contact: heinrichm@hillsboroughcounty.org

Have a good one,

Ashley Rome
Planning & Zoning Technician

Development Services Dept.

P: (813) 272-5595

E: romea@hillsboroughcounty.org

W: HCFLGov.net

Hillsborough County

601 E. Kennedy Blvd., Tampa, FL 33602

[Facebook](#) | [Twitter](#) | [YouTube](#) | [LinkedIn](#) | [HCFL Stay Safe](#)

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May 31, 2022
Revised July 20, 2022
Revised August 8, 2022

Mr. Michael Williams, P.E.
Hillsborough County
Development Services Department
Development Review Director
County Engineer
601 East Kennedy Boulevard, 20th Floor
Tampa, Florida 33602

RE: Muck Pond Road and McIntosh Road - SWC (MM 22-0844)
Folio: 82885.0050, 82881.0000, 82885.0000
Design Exception – Muck Pond Road
Palm Traffic Project No. T22047

Dear Mr. Williams:

The purpose of this letter is to provide justification for the design exception per Transportation Technical Manual (TTM) 1.7 to meet the requirements of the Hillsborough County Land Development Code (LDC) Section 6.04.03.L (existing facility) in association with the proposed development of up 55,000 square feet of RV sales and 154,000 square feet of RV storage facility located south of Muck Pond Road and west of McIntosh Road, as shown in Figure 1. This request is made based on our virtual meeting with Hillsborough County staff.

The project proposes to have one (1) full access to Muck Pond Road. Muck Pond Road is identified in the Hillsborough County Functional Classification Map as a collector roadway and was identified during our meeting as a substandard road. Muck Pond Road has a posted speed limit of 40 mph and currently has 10-foot travel lanes in approximately 50 feet of right of way.

This request is a design exception to the Hillsborough County Transportation Technical Manual for Muck Pond Road from McIntosh Road west to the property driveway. The requested exceptions to the TS-7 typical section and the justification are as follows:

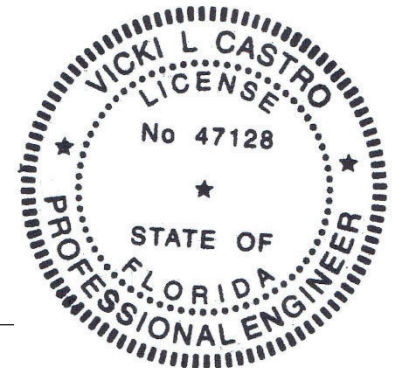
1. The existing ROW along Muck Pond Road is approximately 50 feet. The typical TS-7 section for a collector, two-lane undivided roadway requires a minimum of 96 feet of ROW with 12-foot lanes, 8-foot shoulders (5-foot paved), open ditch drainage and a 5-foot sidewalk. This section of Muck Pond Road currently has 10-foot travel lanes and no bike lanes or sidewalk.
2. The request is to provide 11-foot travel and left turn lanes instead of the 12-foot lanes. According to the Florida Green Book, on a suburban roadway where the speed limit is between 40 and 45 mph, 11-foot lanes may be used. The post speed limit on Muck Pond road is 40 mph.
3. An F type curb and 10-foot multi-use path will be provided only on the south side of Muck Pond Road from the beginning of the transition to the 3-lane section east to McIntosh Road. The multi-use path will be provided within a 15-foot public access, utility and maintenance easement. There is not enough ROW to provide a sidewalk on the north side of Muck Pond Road.
4. No modification to the northern edge of pavement, as all improvements will be made on the southside of Muck Pond Road.

The proposed typical section is shown in Figure 2.

Please do not hesitate to contact us if you have any questions or require any additional information.

Sincerely,

Vicki L Castro, P.E.
Principal



Based on the information provided by the applicant, this request is:

_____ Disapproved _____ Approved with Conditions _____ Approved

If there are any further questions or you need clarification, please contact Sheida L. Tirado, P.E.

Sincerely,

Michael J. Williams
Hillsborough County Engineer

FIGURE 1. LOCATION MAP

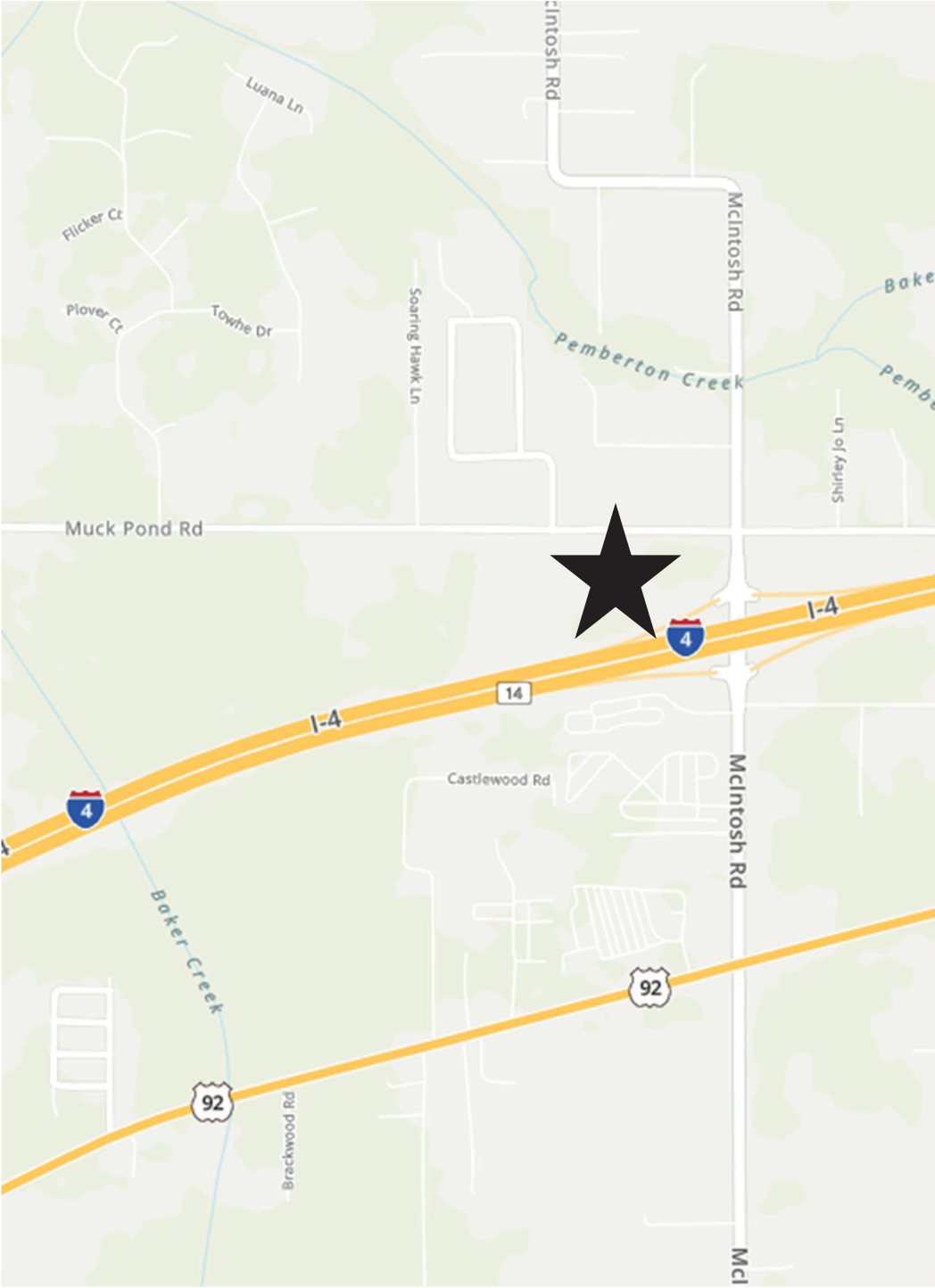


FIGURE 2. PROPOSED TYPICAL 3 LANE SECTION

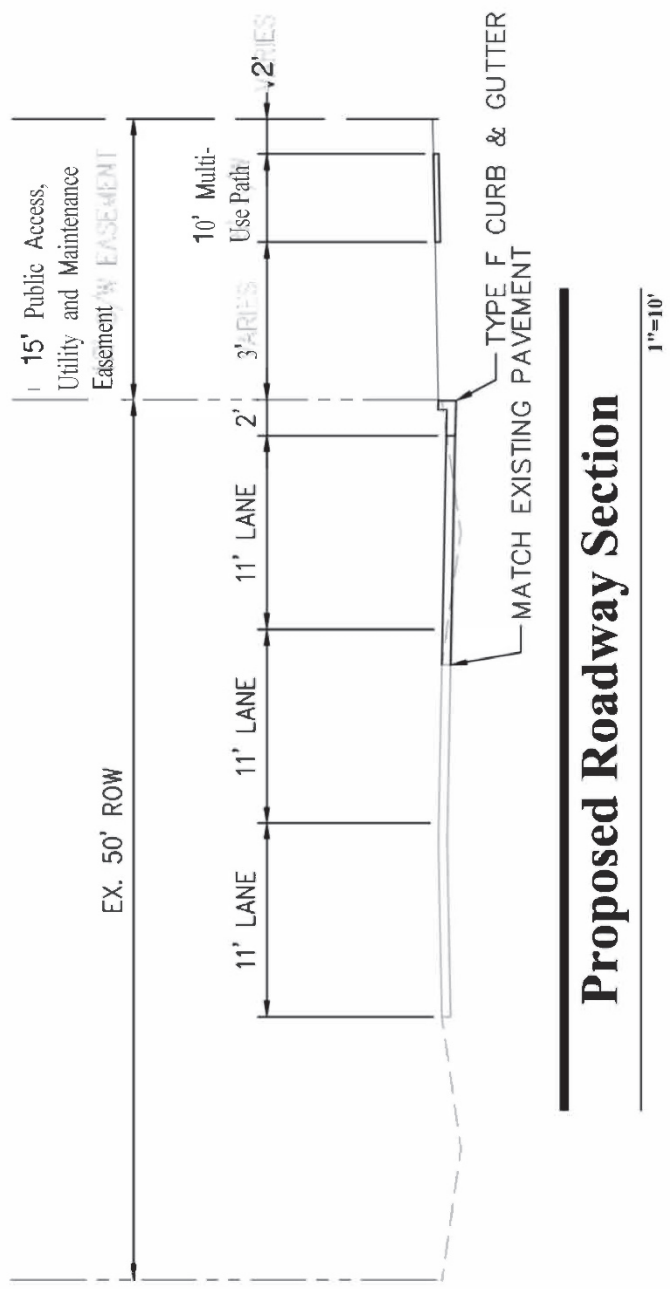


Table 210.2.1 – Minimum Travel and Auxiliary Lane Widths

| Context Classification | | Travel (feet) | | | Auxiliary (feet) | | | Two-Way Left Turn (feet) | |
|------------------------|---------------|--------------------|-------|------|--------------------|-------|------|--------------------------|----|
| | | Design Speed (mph) | | | Design Speed (mph) | | | Design Speed (mph) | |
| | | 25-35 | 40-45 | ≥ 50 | 25-35 | 40-45 | ≥ 50 | 25-35 | 40 |
| C1 | Natural | 11 | 11 | 12 | 11 | 11 | 12 | N/A | |
| C2 | Rural | 11 | 11 | 12 | 11 | 11 | 12 | | |
| C2T | Rural Town | 11 | 11 | 12 | 11 | 11 | 12 | 12 | 12 |
| C3 | Suburban | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |
| C4 | Urban General | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |
| C5 | Urban Center | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |
| C6 | Urban Core | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |

Notes:

Travel Lanes:

- (1) Minimum 11-foot travel lanes on designated freight corridors, SIS facilities, or when truck volume exceeds 10% on very low speed roadways (design speed ≤ 35 mph) (regardless of context).
- (2) Minimum 12-foot travel lanes on all undivided 2-lane, 2-way roadways (for all context classifications and design speeds). However, 11-foot lanes may be used on 2-lane, 2-way curbed roadways that have adjacent buffered bicycle lanes.
- (3) 10-foot travel lanes are typically provided on very low speed roadways (design speed ≤ 35 mph), but should consider wider lanes when transit is present or truck volume exceeds 10%.
- (4) Travel lanes should not exceed 14 feet in width.

Auxiliary Lanes:

- (1) Auxiliary lanes are typically the same width as the adjacent travel lane.
- (2) Table values for right turn lanes may be reduced by 1 foot when a bicycle keyhole is present.
- (3) Median turn lanes should not exceed 15 feet in width.
- (4) For high speed curbed roadways, 11-foot minimum lane widths are allowed for the following:
 - Dual left turn lanes
 - Single left turn lanes at directional median openings.
- (5) For RRR Projects, 9-foot right turn lanes on very low speed roadways (design speed ≤ 35 mph) are allowed.

Two-way Left Turn Lanes:

- (1) Two-way left turn lanes are typically one foot wider than the adjacent travel lanes.
- (2) For RRR Projects, the values in the table may be reduced by 1-foot.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

| Adjoining Roadways (check if applicable) | | | |
|---|-----------------------------|---|--|
| Road Name | Classification | Current Conditions | Select Future Improvements |
| Muck Pond Rd. | County Collector - Rural | 2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input checked="" type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |
| McIntosh Rd. | County Collector - Rural | 2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other – ROW Preservation |
| | Choose an item. | Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |
| | Choose an item. | Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |

| Project Trip Generation <input type="checkbox"/> Not applicable for this request | | | |
|---|-----------------------------------|-----------------------------|-----------------------------|
| | Average Annual Daily Trips | A.M. Peak Hour Trips | P.M. Peak Hour Trips |
| Existing | 498 | 39 | 65 |
| Proposed | 498 | 39 | 65 |
| Difference (+/-) | No Change | No Change | No Change |

*Trips reported are based on net new external trips unless otherwise noted.

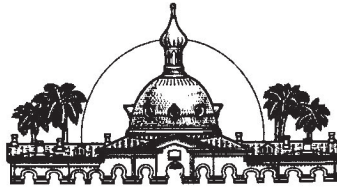
| Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request | | | | |
|---|-----------------------|---------------------------------------|------------------------|----------------|
| Project Boundary | Primary Access | Additional Connectivity/Access | Cross Access | Finding |
| North | X | Vehicular & Pedestrian | None | Meets LDC |
| South | | None | None | Meets LDC |
| East | | Pedestrian | None | Meets LDC |
| West | | Vehicular & Pedestrian | Vehicular & Pedestrian | Meets LDC |
| Notes: | | | | |

| Design Exception/Administrative Variance <input type="checkbox"/> Not applicable for this request | | |
|--|----------------------------|----------------------------|
| Road Name/Nature of Request | Type | Finding |
| Muck Pond Rd. – Substandard Road | Design Exception Requested | Approvable with Conditions |
| | Choose an item. | Choose an item. |
| Notes: | | |

Transportation Comment Sheet

| 4.0 Additional Site Information & Agency Comments Summary | | | |
|--|---|--|---------------------------------|
| Transportation | Objections | Conditions Requested | Additional Information/Comments |
| <input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input checked="" type="checkbox"/> Off-Site Improvements Provided | <input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | |

**CURRENTLY
APPROVED**



Hillsborough County
Florida

Office of the County Administrator
Patricia G. Bean

BOARD OF COUNTY COMMISSIONERS

Brian Blair
Rose V. Ferlita
Ken Hagan
Al Higginbotham
Jim Norman
Mark Sharpe
Kevin White

Deputy County Administrator
Wally Hill

Assistant County Administrators
Kenneth C. Griffin
Carl S. Harness
Manus J. O' Donnell

June 18, 2007

Reference: MM 07-0529 TH

Linda Pearson, AICP
Diaz Pearson & Associates, Inc.
Suite 204
1200 W. Platt Street
Tampa, FL 33606

Dear Applicant:

At the regularly scheduled public meeting on August 7, 2007, the Board of County Commissioners granted your request for a Major Modification to PD 98-0823, with the attached amended final conditions.

Please keep this letter for your records. If we may be of service to you in the future, feel free to contact our office at 272-5920.

Sincerely,

A handwritten signature in black ink that reads "Paula Harvey". The signature is fluid and cursive.

Paula M. Harvey, AICP, Director
Planning and Zoning Division

ps

Approval - Approval, subject to the conditions listed below, is based on site plan received July 26, 2007.

1. Parcel A shall be limited to sales and display of recreational vehicles with pre-sale maintenance and preparation services only. Said maintenance and services shall be limited to minor motor vehicle repair as defined by the Land Development Code. After-sale services, including warranty work and routine maintenance, and all general repair and maintenance services shall be prohibited unless the warranty work is for non-motorized cosmetic repair items only. Cosmetic repairs shall not include exterior body repair, grinding, sanding, sandblasting or painting. Additionally, the sale, leasing or warehousing of parts for off-site use or distribution, and the in-ground storage of fuel or sale of fuel to the public, shall be prohibited.
 - 1.1 The facility shall be permitted a maximum of 55,000 square feet of floor area. At least 30 percent of the total project area shall remain open space.
2. Parcel B shall be limited to 154,000 square feet of floor area for a condominium recreational vehicle (RV) storage facility that includes a 2,117-square-foot clubhouse. An RV wash center and septic dump station is permitted within the interior of the storage facility. Fifty percent of the project shall be open space. The recreational vehicle storage facility buildings shall have solid walls exterior to the project.
3. Maximum building height shall be 35 feet. Minimum building setbacks shall be 200 feet from the north project boundary, except in Parcel B which shall be 180 feet for the vehicle storage buildings and 70 feet for the clubhouse, 50 feet from the west PD boundary, 30 feet from the south boundary and 20 feet (10 feet for Parcel B from Parcel A) from the east boundary. Existing buildings shall be brought into conformance with these setbacks prior to any commercial use of the site. All buildings shall be architecturally finished on all sides and the use of metal exterior siding shall be prohibited. Paint shall not constitute an architectural finish. Existing buildings that do not conform with this requirement shall be finished with stucco on all exterior walls prior to any commercial use of the site.
4. All vehicle parking, display, storage and service areas shall be located a minimum of 200 feet from the north boundary of the PD site, except as referenced above. The project's internal access drive shall be located a minimum of 100 feet from the north boundary of the PD site.
5. For Parcel A, all service bay doors shall face southward and/or eastward. Existing buildings shall be brought into conformance with this requirement prior to any commercial use of the site. All service activities, excluding vehicle detailing and minor washing by hand for dust removal, shall be performed indoors only. Automated vehicle washing facilities shall include a water recycling system and runoff shall not drain into project stormwater retention ponds.
6. No outdoor loudspeakers shall be permitted in the project.

7. Overhead pole lights shall be prohibited within 200 feet of the north boundary of the project. Notwithstanding, pole lights shall be permitted along the project's internal access drive subject to the following requirements: 1) the lights shall be placed only on the north side of the drive; 2) the lights shall have a maximum height of 15 feet; and, 3) the lights shall be directed southward and utilize full cutoff fixtures to completely shield the bulbs or other sources of illumination from passersby on Muck Pond Road.
8. Project signs shall conform with Article VII of the Land Development Code, subject to the following additional restrictions:
 - 8.1. The project shall be limited to a total of two pole signs. One pole sign shall be permitted on Parcel A and one pole sign shall be permitted on Parcel B. The pole signs shall be located within 50 feet of the south boundary of the project next to Interstate-4.
 - 8.2. Wall signs shall be permitted only on the southern and eastern faces of the principal buildings.
 - 8.3. Signs at the project access points shall be monument signs with a maximum height of four feet. Illumination shall be limited to external floodlights; no internal illumination shall be permitted.
 - 8.4. No other ground signs, except traffic advisory and directional signs, shall be permitted in the project.
 - 8.5. Billboards, pennants and banners shall be prohibited.
9. If septic tanks are utilized, the drain fields for Parcel A shall be located a minimum of 200 feet from the north property boundary. The Parcel B drain field may be located as shown on the site plan. Existing septic tank locations and drainfields are permitted to remain if certified for commercial use by the Health Department.
10. All vehicle drives, parking, display, storage and service areas shall be paved.
11. Parcel A hours of operation shall be limited to the following: Monday through Saturday, 7:00 a.m. to 7:00 p.m.; and Sunday, 9:00 a.m. to 5:00 p.m. Parcel B hours of operation shall be 24 hours for the gated, owner-entry only condominium storage units; hours of operation for the clubhouse/office shall be 7:00 a.m. to 10:00 p.m. Monday through Saturdays and Sunday 9:00 a.m. to 5:00 p.m.
12. A buffer area with a minimum width of 50 feet shall be provided along the entire west boundary. The westernmost 955 feet of the northern boundary shall have a 100-foot buffer with the exception of the clubhouse, which will have a 70-foot-wide buffer. Within the buffer area, the developer shall install screening as follows:

- 12.1. Along the north boundary and extending southward along the west boundary, there shall be a landscaped berm of undulating height. The berm shall have a minimum height of four feet above grade and shall be crowned with evergreen shrubs and trees. The shrubs shall have a minimum height of two feet above berm height and minimum opacity of 75 percent at time of planting. The trees shall have a minimum height of 10 feet above berm height and minimum caliper of two inches at time of planting, and shall be placed no more than 20 feet apart on centers. The berm shall be placed along the exterior edge of the buffer area.
- 12.2. Along the remainder of the west boundary, there shall be an evergreen hedge with a minimum height of six feet and minimum opacity of 75 percent at time of planting.
- 12.3. The balance of the buffer area shall be grassed. Existing encroachments in the buffer area, such as building foundations, shall be removed prior to any commercial use of the PD site. Prior to certification, notations on the general site plan shall be revised to conform with this condition.
- 12.4. All required screening shall be installed prior to any commercial use of the PD site.
- 12.5. No fences shall be allowed within the buffer area, unless otherwise specified herein. Existing fences shall be relocated accordingly prior to any commercial use of the PD site. Fences shall conform with Section 6.07.00.C.3 of the Land Development Code,, except that for Parcel A chain link fences with a maximum height of 7 feet and topped with barbed wire shall be permitted if placed at least 100 feet from the Muck Pond Road and McIntosh Road rights-of-way.

For Parcel B, a six foot opaque decorative fence shall be permitted to be located as shown on the general site plan. The decorative fence shall be wrought iron or similar material but cannot be chain link.
13. Along the Interstate-4 frontage, the developer shall plant trees, a minimum of 10 feet in height at the time of planting, and shall be placed no more than 50 feet apart on centers.
14. The stormwater management system shall be designed and constructed in such a manner as to not adversely impact off-site surface and groundwater elevations. To the greatest extent possible, stormwater ponds shall be located in the buffer areas along the north boundaries of the project.
15. Advisory and directional signs shall be posted within the project to direct vehicles exiting the site to McIntosh Road.
16. Subject to FDOT approval, the site will be restricted to one right-out only access on McIntosh Road. The general design and location of the access point on Muck Pond Road shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (Land Development Code Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and

Growth Management Department. Final design, if approved by Hillsborough County Planning and Growth Management Department may include, but is not limited too: left turn lanes, acceleration lane(s) and deceleration lane(s). Access points may be restricted in movements.

17. Prior to Concurrency approval, the Developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic at the project driveway(s). This analysis shall include a passenger car to heavy vehicle conversion ratio to determine the increased length in turn lane being required. The turn lane(s) shall be constructed to FDOT and/or Hillsborough County standards using FDOT standard Index 301 & 526 and an asphalt overlay shall be applied over the entire portion of roadway where a left turn lane is provided. The Developer shall construct the following turn lanes at his expense:

- 17.1 Provide NB-to-WB left turn lane at the intersection of McIntosh Road @ Muck Pond Road

- 17.2 Provide a WB-to-SB left turn lane on Muck Pond Road into the site

If it is determined by the results of the analysis submitted by the developer, that adequate right-of-way does not exist to construct any needed improvements (i.e. turn lanes), then the developer shall re-submit an analysis showing the maximum development that could occur to still enable the needed improvements to be constructed. The development will be limited to the size as shown in the submitted analysis.

18. Based on the projected trip generation to the site, access onto the public road would be via "Type II" Minor Roadway Connection (50 - 1,500 trip ends per day), LDC 6.04.01.E. The Land Development Code requires that all internal access (the "throat") to the driveways must be a minimum of 50 feet from the edge of pavement of the public roadway, and shall remain free of internal connections or parking spaces that might interfere with the movement of vehicles into or out of the site. The applicant has the option of submitting an analysis showing that for his particular site, a throat of less than 50 feet is appropriate and will result in no adverse impact to the public roadway system. Because of the length of the vehicles using the site, additional throat depth may be required. If the Driveway is to be gated, then the throat depth may exceed this length to accommodate stacking vehicles.
19. Prior to Construction Plan approval, the Developer may be required to dedicate one-half of the right-of-way on Muck Pond Road to meet Hillsborough County standards. The right-of-way shall be dedicated to bring the substandard right-of-way up to Transportation Technical Manual Standards for a two lane collector roadway. Right-of-way shall be measured from the centerline of the roadway.
20. As is noted in the Land Development Code, one of the major reasons for diminished capacity of public roads is an increase in access points along roadways which increases the potential conflict points. Because of this, the applicant shall show the ability to provide cross access to adjacent parcels of like land uses. If any of the adjacent properties are developed under the

- same developer/owner, then cross access must be provided. All cross access shall be in accordance with the Hillsborough County Land Development Code Section 6.04.03 Q.
21. The applicant shall be required to pave any portion of the access drive which lies within the existing right-of-way (LDC 6.04.05).
 22. The Developer shall widen that portion of Muck Pond Road and McIntosh Road adjacent to the site to accommodate two-way RV traffic. The roadway shall be widened to Hillsborough County Transportation Technical Manual standards. The existing roadway shall be overlaid with a new layer of asphalt per Hillsborough County Standards. The Developer shall provide documentation at Preliminary Site Plan submittal regarding the existing pavement width and right-of way along Muck Pond Road and McIntosh Road. The Preliminary Site Plan shall provide notation that the roadway is to be widened to County standards. The Developer shall include the roadway improvements on the submitted Construction Plans. If the Developer can provide signed and sealed documentation that the current pavement width on Muck Pond Road and McIntosh Road meets the minimum standards for two lane collector roadways, then the Developer may not be required to widen the roadway.
 23. Where applicable and as determined by County Staff the Developer shall construct sidewalks within the right-of-way along all roadways adjacent to the property boundaries and along both sides of all internal roadways. The sidewalks shall be a minimum width of five (5) feet. Pedestrian interconnectivity shall be provided between uses and adjacent parcels. All internal sidewalks shall meet the accessibility requirements of the Americans with Disabilities Act and/or Florida Accessibility Code.
 24. The applicant shall provide internal access to any existing or future out parcels on the site (LDC 5.03.05 H).
 25. If as a result of this developments impact, a signal should be warranted at the intersection of Muck Pond Road and McIntosh Road, the developer shall install, with the concurrence of the County, a signal and appropriate interconnect adjacent signals. The development shall be responsible for the pro-rata share of their impacts. The developer shall submit the 60 % roadway construction plans for all collector or arterial roadways to Public Works (Traffic Section) for inclusion of possible signal interconnect communications conduit and pull boxes. The route, number and size of conduit will be stipulated by this review process. If signals are not immediately warranted, the cable will not be required for installation. (This is intended to prevent the need to retrofit new roadways with conduit once signals are warranted). The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department.
 26. According to FDOT, there currently is an insufficient right turn lane from McIntosh Road to westbound I-4. The design vehicle (motor homes, RVs) will require installation of an adequate turn lane to serve these large, slow moving vehicles. FDOT requires that a right turn lane be installed on McIntosh Road for entrance onto the I-4 Interstate ramp.

AMENDED
FINAL CONDITIONS
OF APPROVAL

PETITION NUMBER: MM 07-0529 TH (98-0823)
BOCC MEETING DATE: August 7, 2007
DATE TYPED: August 9, 2007

- 26.1 The size of the design vehicle should be considered regarding the width of McIntosh Rd.
- 26.2 Though the driveway onto McIntosh is proposed as a right out only, there must be a physical barrier to keep traffic from making left turns into the driveway.
- 26.3 A possible conflict exists with the auto dealership driveway on the east side of McIntosh Rd.
27. The project may be permitted a maximum of 209,000 square feet of commercial uses, subject to formal delineation of on-site conservation areas, preservation areas and water bodies and final calculation of maximum intensity (FAR) permitted by the Comprehensive Plan in accordance with the Environmentally Sensitive Lands Credits requirement of the Comprehensive Plan. If the maximum square footage for the project as permitted herein exceeds the maximum intensity (FAR) permitted by the Comprehensive Plan, per the Environmentally Sensitive Lands Credits requirement, the square footage of the project shall decrease as necessary to conform to the Comprehensive Plan.
28. The stormwater management system shall be designed and constructed in such a manner so as to not adversely impact off-site surface and groundwater elevations.
29. Approval of this application does not ensure that water will be available at the time when the applicant seeks permits to actually develop.
30. Approval of this rezoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to wetlands, and does not grant any implied or vested right to environmental approvals.
31. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
32. Development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations and ordinances of Hillsborough County.
33. Effective as of February 1, 1990, this development order/permit shall meet the concurrency requirements of Chapter 163, Part II, Florida Statutes. Approval of this development order/permit does not constitute a guarantee that there will be public facilities in place at the time of application for subsequent development orders or permits to allow issuance of such development orders or permits.



AGENCY COMMENTS

AGENCY REVIEW COMMENT SHEET

TO: Zoning Technician, Development Services Department

DATE: 07/13/2022

Revised: 8/9/2022

REVIEWER: James Ratliff, AICP, PTP, Principal Planner

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: TH/ Central

PETITION NO: PRS 22-0844

- This agency has no comment.
- This agency has no objection.
- This agency has no objection, subject to listed or attached conditions.
- This agency objects for the reasons outlined below.

NEW AND REVISED CONDITIONS OF APPROVAL

Revised Conditions

4. All vehicle parking, display, storage and service areas shall be located a minimum of 200 feet from the north boundary of the PD site, except as referenced above. ~~The project's internal access drive shall be located a minimum of 100 feet from the north boundary of the PD site.~~

[Staff is recommending modification of this condition to comport with current practice. Staff notes that minimum throat depth is governed by Sec. 6.04.03.G. and Sec. 6.04.04.A. of the LDC. The currently proposed site plan appears to include a +/- 90-foot throat depth for the easternmost access which, based upon current standards, exceeds minimum requirements. The westernmost access includes a throat depth in excess of 100 feet, which staff believes is appropriate and satisfies the minimum requirements of the LDC. Staff notes that compliance with LDC throat depth requirements and the PD site plan will be required, regardless of whether there is a specific implementing regulation to that effect.]

12. A buffer area with a minimum width of 50 feet shall be provided along the entire west boundary. The westernmost 955 feet of the northern boundary shall have a 100-foot buffer with the exception of the clubhouse, which will have a 70-foot-wide buffer. Within the buffer area, the developer shall install screening as follows:
- 12.1. Along the north boundary and extending southward along the west boundary, there shall be a landscaped berm of undulating height. The berm shall have a minimum height of four feet above grade and shall be crowned with evergreen shrubs and trees. The shrubs shall have a minimum height of two feet above berm height and minimum opacity of 75 percent at time of planting. The trees shall have a minimum height of 10 feet above berm height and minimum caliper of two inches at time of planting, and shall be placed no more than 20 feet apart on centers. The berm shall be placed along the exterior edge of the buffer area.
- 12.2. Along the remainder of the west boundary, there shall be an evergreen hedge with a minimum height of six feet and minimum opacity of 75 percent at time of planting.
- 12.3. The balance of the buffer area shall be grassed. Existing encroachments in the buffer area, such as building foundations, shall be removed prior to any commercial use of the PD site. Prior to certification, notations on the general site plan shall be revised to conform with this condition.
- 12.4. All required screening shall be installed prior to any commercial use of the PD site.
- 12.5. No fences shall be allowed within the buffer area, unless otherwise specified herein. Existing fences shall be relocated accordingly prior to any commercial use of the PD site. Fences shall conform with Section 6.07.00.C.3 of the Land Development Code., except that for Parcel A chain

link fences with a maximum height of 7 feet and topped with barbed wire shall be permitted if placed at least 100 feet from the Muck Pond Road and McIntosh Road rights-of-way. For Parcel B, a six foot opaque decorative fence shall be permitted to be located as shown on the general site plan. The decorative fence shall be wrought iron or similar material but cannot be chain link.

12.6 Notwithstanding anything herein these conditions or on the PD site plan to the contrary, buffering, screening, and fencing shall not be permitted in areas required to facilitate proposed vehicular and pedestrian access or cross access.

[Staff is recommending modification of this condition to clarify where buffering will be required. Staff notes that the developer is not constructing the westernmost access to Muck Pond Rd. with the initial increment of development, rather such connection will be constructed by the developer of the project to west. Staff notes the majority of that project's entitlements/area is required to take access through the subject PD.]

16. ~~Subject to FDOT approval, the site will be restricted to one right-out only access on McIntosh Road. The general design and location of the access point on Muck Pond Road shall be regulated by the Hillsborough County Access Management regulations as found in the Land Development Code (Land Development Code Section 6.04). The design and construction of curb cuts are subject to approval by the Hillsborough County Planning and Growth Management Department. Final design, if approved by Hillsborough County Planning and Growth Management Department may include, but is not limited too: left turn lanes, acceleration lane(s) and deceleration lane(s). Access points may be restricted in movements.~~The project shall be served by two (2) full access connections to Muck Pond Rd. Vehicular access to McIntosh Rd. shall not be permitted. Additionally:

16.1 The developer shall construct the easternmost access concurrent with the initial increment of development; and,

16.2 The developer shall have the option of constructing the westernmost access, i.e. the portion between the east/west running drive aisle (construction of which shall be the responsibility of the developer of this project) and Muck Pond Rd., together with the initial increment of development. Additionally:

16.2.1 If the developer decides not to construct such access, the developer shall record, an ingress/egress, construction and any other easements necessary to permit the owners of folio 81601.0000 (which is subject to a PD zoning approval that requires the majority of its development to take access through the subject PD) to construct and utilize the westernmost project access without further consultation of the owners of the lands within the subject PD.

16.2.2 If the developer decides to construct such access, the developer shall record an ingress and egress easement over the area, as required pursuant to Condition 18. Additionally, the developer shall be responsible for substandard road improvements on Muck Pond Rd. between the westernmost project access and the improvements described in the Design Exception, described hereinbelow.

[Staff is recommending modification of this condition to comport with the current access plan. Also, as discussed in the body of the report, the lands within the adjacent PD and subject PD were previously owned by the same owner. That owner then chose to restrict an adjacent PD such that the majority its project must take access through this PD; however, no easements were apparently necessary or formalized, due to the shared ownership at the time. Now that ownership interests have diverged, it is important that these arrangements be formalized, and site plan and zoning condition notations be brought into conformance with the current practice and wording for such facilities/concepts as contained within the current LDC.]

17. ~~Prior to Concurrency approval, the Developer shall provide a traffic analysis, signed by a Professional Engineer, showing the length of the left and right turn lanes needed to serve development traffic at the project driveway(s). This analysis shall include a passenger car to heavy vehicle conversion ratio to determine the increased length in turn lane being required. The turn lane(s) shall be constructed to FDOT and/or Hillsborough County standards using FDOT standard Index 301 & 526 and an asphalt overlay shall be applied over the entire portion of roadway where a left turn lane is provided. The Developer shall construct a westbound to southbound left turn lane on Muck Pond into the easternmost project driveway. The turn lane shall be constructed in accordance with the Design Exception described in condition 22. Construction of a westbound to southbound left turn lane into the westernmost driveway shall be the responsibility of the developer of the adjacent PD utilizing such access, if warranted pursuant to Sec. 6.04.04.D. of the LDC, the following turn lanes at his expense:~~

~~17.1 Provide NB to WB left turn lane at the intersection of McIntosh Road @ Muck Pond Road~~

~~17.2 Provide a WB to SB left turn lane on Muck Pond Road into the site~~

~~If it is determined by the results of the analysis submitted by the developer, that adequate right of way does not exist to construct any needed improvements (i.e. turn lanes), then the developer shall re-submit an analysis showing the maximum development that could occur to still enable the needed improvements to be constructed. The development will be limited to the size as shown in the submitted analysis.~~

[Staff is recommending modification of this condition, as it was based upon Hillsborough County's former system for the evaluation and mitigation of transportation impacts, i.e. transportation concurrency. Given HB 7207 (2011) and the County's adoption of a mobility fee ordinance, this condition is no longer enforceable. Staff notes future development will be required to pay mobility fees unless grandfathered in accordance with the ordinance. Staff has repurposed this condition to describe required site access improvements, as well as discuss the potential 2nd westbound left turn lane required at the westernmost access, the presence of which is contingent upon the type and amount of uses ultimately developed within the adjacent PD to the west of this site.]

18. ~~Based on the projected trip generation to the site, access onto the public road would be via "Type II" Minor Roadway Connection (50 — 1,500 trip ends per day), LDC 6.04.01.E. The Land Development Code requires that all internal access (the "throat") to the driveways must be a minimum of 50 feet from the edge of pavement of the public roadway, and shall remain free of internal connections or parking spaces that might interfere with the movement of vehicles into or out of the site. The applicant has the option of submitting an analysis showing that for his particular site, a throat of less than 50 feet is appropriate and will result in no adverse impact to the public roadway system. Because of the length of the vehicles using the site, additional throat depth may be required. If the Driveway is to be gated, then the throat depth may exceed this length to accommodate stacking vehicles. As shown on the PD site plan, certain internal driveways, drive aisles and sidewalks shall be considered Shared Access Facilities with folio 81601.0000 (and such facilities shall be treated as the sole legal means of vehicular access to certain development within the adjacent folio, as specified within the PD regulating such adjacent property)". Together with the initial increment of development within the subject PD, the property owner shall record in the Official Records of Hillsborough County a non-exclusive ingress and egress easement and any other easements necessary to permit the developer of folio 81601.0000 to take access through the areas designed as a Shared Access Facility within subject PD, without further consultation of the property owner of the subject PD. The design and locations of all connections shall be subject to the review and approval of Hillsborough County.~~

[Staff is recommending modification of this condition to repurpose it order to describe Shared Access Facility requirements. As currently approved, this condition speaks to minimum throat depth requirements, which is duplicative of the existing requirements within existing condition 4. Those existing conditions within condition 4 were themselves not needed, for the reasons explained in the staff analysis provided underneath that modified condition.]

20. ~~As is noted in the Land Development Code, one of the major reasons for diminished capacity of public roads is an increase in access points along roadways which increases the potential conflict points. Because of this, the applicant shall show the ability to provide cross access to adjacent parcels of like land uses. If any of the adjacent properties are developed under the same developer/owner, then cross access must be provided. All cross access shall be in accordance with the Hillsborough County Land Development Code Section 6.04.03~~ Prior to or concurrent with the initial increment of development, the developer shall construct an vehicular and pedestrian access/cross-access stubout to the western project boundary. This access shall consist of a minimum 24-foot-wide drive aisle between the stubout along the project's western boundary and the westernmost Muck Pond Rd. access connection. The drive aisle shall be constructed with a minimum 6-foot wide sidewalk along the north side, at the back of a raised vertical curb. If Miami curbing is utilized, then a minimum 5-foot wide sidewalk may be constructed; however, such sidewalk shall be separated from the drive aisle with a minimum 5-foot wide green strip. Nothing in this condition shall be constructed to require the developer of the subject PD to construct the north/south portion of the vehicular drive aisle within the Shared Access Facility unless the developer chooses to do so in accordance with the options specified in condition 16.2

[Staff is recommending modification of this condition to clarify the wording, conform with current practice, and also expand the condition to address certain design considerations which are needed due to the potential for a school use on the adjacent PD (which would take its sole access through the subject PD).]

21. ~~The applicant shall be required to pave any portion of the access drive which lies within the existing right of way (LDC 6.04.05).~~ Notwithstanding anything shown on the PD site plan to the contrary, the developer shall construct internal and external sidewalks in accordance with Sec. 6.03.02. of the LDC.

[Staff is recommending modification of this condition to repurpose it order include a condition which makes it clear that while some sidewalk connections are shown on the PD site plan, others may be needed into order to comply with Sec. 6.03.02. of the LDC and/or ADA and Florida Accessibility Code requirements. A detailed review of sidewalk routing and sidewalk design will not be possible until the site/construction plan review stage.]

22. If PRS 22-0844 is approved, the County Engineer will approve a Design Exception (dated May 31, 2022 and last revised August 8, 2022) which was found approvable with conditions by the County Engineer (on August 9, 2022) for the Muck Pond Rd. substandard road improvements. As Muck Pond Rd. is a substandard collector roadway, the developer will be required to make certain improvements to Muck Pond Rd. consistent with the Design Exception. Specifically, between McIntosh Rd. and the easternmost project driveway, the developer shall:

22.1 Widen the existing travel lanes such that they are 11-feet in width; and,

22.2 Install Type-F curbing along the south side of the roadway.

Additionally, along the entirety of the project's frontage, the developer shall:

22.3 Dedicate and convey to Hillsborough County, a +/- 15-foot wide easement for public access, utilities, and maintenance purposes, within which a minimum 10-foot wide multi-purpose path shall be constructed. Alternatively, at the developer's sole option, the developer may dedicate and convey the underlying fee to Hillsborough County (in lieu of an easement). All such conveyances shall be subject to the review and approval of Hillsborough County.

~~The Developer shall widen that portion of Muck Pond Road and McIntosh Road adjacent to the site to accommodate two way RV traffic. The roadway shall be widened to Hillsborough County Transportation Technical Manual standards. The existing roadway shall be overlaid with a new layer of asphalt per Hillsborough County Standards. The Developer shall provide documentation at Preliminary Site Plan~~

~~submittal regarding the existing pavement width and right of way along Muck Pond Road and McIntosh Road. The Preliminary Site Plan shall provide notation that the roadway is to be widened to County standards. The Developer shall include the roadway improvements on the submitted Construction Plans. If the Developer can provide signed and sealed documentation that the current pavement width on Muck Pond Road and McIntosh Road meets the minimum standards for two lane collector roadways, then the Developer may not be required to widen the roadway.~~

[Staff is recommending modification of this condition to comport with current practice. Given the changes to proposed access, the applicant was required to address the Muck Pond Rd. substandard road issues concurrent with this zoning modification request. This condition reflects the Design Exception requested by the applicant, and found approvable with conditions by the County Engineer.]

24. ~~The applicant shall provide internal access to any existing or future out parcels on the site (LDC 5.03.05 H).~~The developer shall construct a minimum 5-foot wide pedestrian cross access connection between the internal sidewalk network and the existing cross access stubout along the project eastern boundary (i.e. to the stubout that was constructed within and along the western boundary of folio 82882.0000). Such connection may be gated; however, if gated the access shall be available for the daily use of project employees, customers, and visitors.

[Staff is recommending modification of this condition to repurpose it for use in addressing the pedestrian cross access along the eastern project boundary. The existing condition language speaks to vehicular interconnectivity between internal uses and outparcels. This condition is not necessary. The PD site plan, together with the definition of the purpose of intent of PD districts within Sec. 5.03.04.A. and Sec. 5.03.06.C.6.a. of the LDC, make it clear that a unified PD is required with the intent of permitting creative, innovative and/or mixed-use development.]

25. ~~If as a result of this developments impact, a signal should be warranted at the intersection of Muck Pond Road and McIntosh Road, the developer shall install, with the concurrence of the County, a signal and appropriate interconnect adjacent signals. The development shall be responsible for the pro rata share of their impacts. The developer shall submit the 60 % roadway construction plans for all collector or arterial roadways to Public Works (Traffic Section) for inclusion of possible signal interconnect communications conduit and pull boxes. The route, number and size of conduit will be stipulated by this review process. If signals are not immediately warranted, the cable will not be required for installation. (This is intended to prevent the need to retrofit new roadways with conduit once signals are warranted). The placement and design of the signal shall be subject to approval by Hillsborough County Public Works Department. No vehicular or pedestrian gates shall be permitted within the portion of the site plan designated as a Shared Access Facility, nor any other area of the site plan where such gate will cause traffic to queue within the Shared Access Facility. Additionally, no parking spaces or loading zones shall be permitted within the project which are located within or back out into the Shared Access Facility.~~

[Staff is recommending repurposing of this condition for use in addressing certain internal project gates as well as drive aisle design within the Shared Access Facility. Staff is recommending removal of the existing condition, which was based upon Hillsborough County's former system for the evaluation and mitigation of transportation impacts, i.e. transportation concurrency. Given HB 7207 and the County's adoption of a mobility fee ordinance, this condition is no longer enforceable. Additionally, the applicant has proffered a condition, included herein below, which requires the applicant to preserve right-of-way to accommodate a future potential roundabout or signalization infrastructure at the intersection of McIntosh Rd. and Muck Pond Rd.]

26. As proffered by the property owner and as shown on the PD site plan, the property owner shall preserve right-of-way along the project's McIntosh Rd. frontage and at the intersection of McIntosh Rd. and Muck Pond Rd. for future potential acquisition and use by the County or others for construction of a roundabout, traffic signal, and/or other intersection improvements. Nothing within this condition shall be construed as prohibiting the developer from locating the required McIntosh Rd. sidewalk within this area and providing

~~the County with an easement for public access and maintenance purposes (or dedication of the underlying fee), if required pursuant to Sec. 6.03.02.D. of the LDC. According to FDOT, there currently is an insufficient right turn lane from McIntosh Road to westbound I 4. The design vehicle (motor homes, RVs) will require installation of an adequate turn lane to serve these large, slow moving vehicles. FDOT requires that a right turn lane be installed on McIntosh Road for entrance onto the I 4 Interstate ramp.~~

~~26.1 — The size of the design vehicle should be considered regarding the width of McIntosh Rd.~~

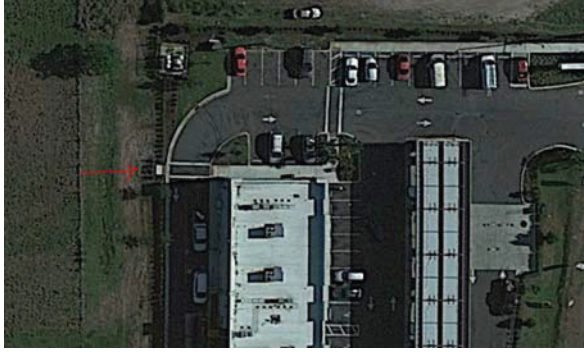
~~26.2 — Though the driveway onto McIntosh is proposed as a right out only, there must be a physical barrier to keep traffic from making left turns into the driveway.~~

~~26.3 — A possible conflict exists with the auto dealership driveway on the east side of McIntosh Rd.~~

[Staff is recommending repurposing of this condition to address the proffered right-of-way preservation at the intersection of McIntosh Rd. and Muck Pond Rd. Staff is recommending removal of the existing condition language. Portions of this condition relating to improvements at the ramp at I-4 and McIntosh Rd. were based upon Hillsborough County's former system for the evaluation and mitigation of transportation impacts, i.e. transportation concurrency. Given HB 7207 and the County's adoption of a mobility fee ordinance, this condition is no longer enforceable. Additionally, staff notes that the applicant is proposing to remove the exit only driveway onto McIntosh Rd., and that the "possible conflict" with the auto dealership driveway on the east side of McIntosh Rd. appears to have been mitigated by restriction of that access to right-in/right-out turning movements only.]

Other Conditions

- Prior to PD site plan certification, the developer shall revise the PD site plan to:
 - Revise the labels reading "12' Sidewalk Easement" to state "15' Public Access, Utility and Maintenance Easement". Staff notes such revisions are necessary for consistency with the site plan shown in the requested Design Exception.
 - Revise the labels reading "5' Sidewalk In County Dedicated Easement" to state "10' Multi-Purpose Path In County Dedicated Easement". Staff notes such revisions are necessary for consistency with the site plan shown in the requested Design Exception.
 - Revise the label reading "Voluntary ROW Preservation; See Note 27" to read, "Voluntary Right-of-way Preservation – See PD Note 27 and Zoning Condition 26".
 - Update the PD site plan for adjacent folio 82882.0000 to show the site improvements constructed within the adjacent project. Staff notes that the pedestrian infrastructure and cross access within that site are not as currently shown.
 - Revise the location of the proposed pedestrian cross access and internal pedestrian infrastructure such that the sidewalk connection occurs along the adjacent folio's western project boundary, in the location such stubout was actually constructed (see below aerial).
 - Revise Note 21 to change the words "is provided" to read "exists".
 - Revise Note 23 to add to the end of the sentence "and/or as required by the condition of approval."
 - Replace Note 28 with a note reading something similar to "Internal fencing and gating may be provided. Reference zoning conditions, including conditions 12.6 and 25 for restrictions and other pertinent information."
- Prior to PD site plan certification, if the zoning modification is approved, the Developer shall provide a signed and sealed Design Exception (the revised date of the letter shall be unchanged). Staff notes that while the first two versions of the letter were signed, the August 8, 2022 letter was sealed, but appears to have been inadvertently unsigned.



PROJECT SUMMARY AND TRIP GENERATION

The applicant is requesting a Major Modification (MM) to previously approved Planned Development (PD) #98-0823, as most recently modified via MM 07-0529. The MM consists of three parcels totaling +/- 19.07 ac., and is currently approved for 55,000 s.f. Recreational Vehicle (RV) sales/display and (pre-sale) maintenance center uses, 154,000 s.f. of RV storage facility uses, and a 2,117 s.f. clubhouse. The applicant is proposing to add an additional access driveway to Muck Pond Rd., eliminate the exit-only driveway on McIntosh Rd., and add a pedestrian cross access connection to the east. Several other changes to the site plan were made to accommodate redesign of the site based on these and/or other issues, including redesign of the stormwater ponds, and considerations for multi-purpose path placement within an easement along Muck Pond Rd.

Although the project is not modifying the project entitlements and is therefore not increasing or decreasing the maximum trip generation potential of the subject property, the project is modifying its project access (to eliminate the exit only driveway) which will impact the amount of eastbound traffic on Muck Pond Rd. As such, the developer to submit a trip generation and site access analysis consistent with the Development Review Procedures Manual (DRPM). Staff did not require the applicant to submit a trip generation and site access analysis which examined access impacts created by the adjacent PD to the west, which takes its access through the subject PD, since those impacts were previously analyzed as a part of the rezoning of that property (as further described herein below).

Staff has prepared a comparison of the potential number of peak hour trips generated under the existing and proposed zoning designations utilizing a generalized worst-case scenario. Information show below is based upon data from the 11th Edition of the Institute of Transportation Engineer’s Trip Generation Manual.

Existing Zoning:

| Land Use/Size | 24 Hour Two-Way Volume | Total Peak Hour Trips | |
|--|------------------------|-----------------------|----|
| | | AM | PM |
| PD, 55,000 s.f. RV Sales (ITE Code 842) | 275 | 25 | 42 |
| PD, 154,000 s.f. RV Storage (ITE Code 151) | 223 | 14 | 23 |
| Subtotal: | 498 | 39 | 65 |

Proposed Zoning:

| Land Use/Size | 24 Hour Two-Way Volume | Total Peak Hour Trips | |
|---|------------------------|-----------------------|----|
| | | AM | PM |
| PD, 55,000 s.f. RV Sales (ITE Code 842) | 275 | 25 | 42 |
| PD, 154,000 s.f. RV Storage | 223 | 14 | 23 |

| | | | |
|----------------|-----|----|----|
| (ITE Code 151) | | | |
| Subtotal: | 498 | 39 | 65 |

Trip Generation Difference:

| Land Use/Size | 24 Hour Two-Way Volume | Total Peak Hour Trips | |
|-------------------|------------------------|-----------------------|------------------|
| | | AM | PM |
| Difference | No Change | No Change | No Change |

TRANSPORTATION INFRASTRUCTURE SERVING THE SITE

Muck Pond Rd. a 2-lane, undivided, substandard, collector roadway characterized by +/- 19 feet of pavement in average condition. The roadway lines within a +/- 50-foot wide right-of-way along the project's frontage. There are no sidewalks or bicycle facilities along Muck Pond Rd. in the vicinity of the proposed project.

SITE ACCESS

The applicant is proposing two access driveways to Muck Pond Rd. The applicant is also adding a pedestrian connection to the convenience/gas station to the east of the proposed project, which itself constructed a pedestrian stubout to the subject property. Although the maximum trip generation potential of the proposed by itself does not warrant two access driveways per Section 6.04.03.I. of the LDC, the addition of project trips from the adjacent PD #09-0773 (as more fully discussed below), would warrant the additional access driveway. Given this, staff believes the second access driveway is necessary and appropriate.

Adjacent PD

Subsequent to approval of a modification to this PD in 2007 (via case #07-0529), the parcel to west was rezoned to PD via case #09-0773. The adjacent PD is approved for up to 206,365 s.f. of a variety of uses; however, there was a restriction placed on the project such that 7,500 s.f. of the uses had to be developed as a daycare, and that particular use was the only one permitted to take direct access to Muck Pond Rd. The remaining 198,865 s.f. of entitlements were required to access Muck Pond Rd. via a cross access connection through the PD that is the subject of this rezoning petition.

Given the above, Transportation Review Section staff estimates that up to 7,185 Average Daily Trips, 475 a.m. peak hour trips, and 710 p.m. peak hour trips could take access through the subject PD. These calculations were arrived at assuming all 198,865 s.f. were developed as Medical Office uses (ITE Code 720). It should be noted that there is the potential for significantly higher amounts of traffic to utilize the shared access driveways within the subject PD, given the list of allowable uses includes public and private K-12 schools. For example, a 1,500-student private school could generate 1,215 a.m. peak hour trips.

The adjacent PD has the potential to generate significantly more traffic that the PD which is the subject of this zoning action. Together both projects would require three (3) or more access driveways per the LDC; however, given proposed site access improvements and other considerations, staff believes the combined projects' traffic can be safely accommodated with two access driveways on Muck Pond Rd.

Additional Background on Shared Access

The Zoning Hearing Master's October 9, 2009 Recommendation for adjacent PD #09-0773, which was subsequently approved by the Hillsborough County BOCC, states the following as a Finding of Fact, "The project traffic, with the exception of the proposed day care center, will be required to circulate through the adjacent approved planned development in order to gain access to Muck Pond Road and McIntosh Road. Cross-access was approved for the adjacent PD and cross-access is required of the subject PD."

The Development Services Department's Staff Recommendation Report for adjacent PD #09-0773, dated September 28, 2009, stated, "The applicant has agreed to only the daycare use having access onto Muck Pond Road. All other uses will need to obtain access via cross access with the PD to the east, which is partially owned by the same property owner."

Shared Driveway

Given the above approvals for the adjacent PD described above, the applicant has the area to be utilized by the adjacent development to be designed as a Shared Access Facility, in accordance with current Land Development Code requirements and staff procedures.

Staff has proposed a condition requiring the Shared Access Facility be designed such that there are no parking spaces with direct access to/from the facility, nor any gates which will impede the safe and efficient functioning of this facility. This will ensure that there are no disruptions to the safe and efficient flow of traffic, both within this PD and at the project driveway(s) onto Muck Pond Rd. Section 6.04.04.G. of the LDC states, "The driveway should be constructed so that all entering and exiting movements can be accomplished with minimum disruption to traffic flow on the intersection roadway." Section 6.04.03.N. states "...all buildings and other structures, land preparation, and landscaping shall be so located and arranged as to provide safe and convenient access for emergency purposes, fire protection, serving, and off-street parking and loading..." Finally, Section 6.04.04.A. states "Driveways provide the physical transition between a site and the abutting roadway. Driveways should be located and designed to minimize impacts on traffic while providing safe entry and exit from the development served. The location and design of the connection must take into account characteristics of the roadway, the site, and the *potential* users. The actual width and length of driveways shall be subject to internal and external traffic flow considerations... This will require that the entering vehicles not be confronted with maneuvering vehicles..." *[emphasis added]*

Turn Lanes and Roundabout

The applicant's traffic analysis indicates that 90% of traffic will be coming to/from the site from the east (McIntosh Rd./Interstate 4). Regarding the trips generated by this project, i.e. not including the adjacent PD traffic, of the approximately 65 peak hour trips in the highest overall peak hour (i.e. the p.m. peak hour period), 24 will be entering the site, and of those 24 trips 21 will be entering to/from the east. The project site appears to be designed such that the eastern access will serve the uses which are the subject of this PD, and the additional (westernmost) access will primarily serve the adjacent development.

Both driveways will meet Section 6.04.04.D. warrants for auxiliary (turn) lanes. The applicant is proposing to construct only the easternmost access and turn lane. Given that the westernmost access and turn lane is not needed to support project traffic, the applicant will be required to provide an access and construction easement which will allow the developer of the adjacent PD to access their property and have all rights necessary to permit construction of any driveway and sidewalk improvements within the north/south portions of Shared Access Facility, area not otherwise constructed by the developer of the project which is the subject of this PD, without further consultation from the property owner.

Previous efforts to rezone this property several years ago (which was subsequently withdrawn) resulted in the identification of a roundabout at the intersection of as a potential future improvement (by the County or others) which could would potentially increase the operational efficiency of the intersection as well as eliminate the need for turn lanes on the approaches to the intersection. The proximity of the intersection to the I-4 on ramps as well as a lack of available right-of-way along the intersection approaches would make implementation of a fully signalized conventional intersection difficult; however, the right-of-way could also be utilized for this purpose if traffic studies and designs identified that as a preferred future improvement. In order to help preserve land needed to keep the possibility of an alternative intersection design (such as the roundabout) alive, the applicant has proffered to preserve additional right-of-way within the subject PD in the vicinity of the intersection. As such, no permanent uses will be permitted within this preserved area in order to preserve the viability of its future acquisition and use.

Removal of Exit Only Driveway to McIntosh Rd.

The existing zoning approval includes an exit only driveway onto McIntosh Rd., located approximately 120 feet south of the intersection of Muck Pond Rd. and McIntosh Rd. and approximately 105 feet north of one of two existing driveways to a gas station within folio 082882.0000. That driveway does not meet the driveway spacing

standards found within today's Land Development Code, which requires a minimum connecting spacing of approximately 245 feet for a Class 5 roadway.

The transportation analysis for adjacent PD #09-0773 did not adequately analyze the worst-case trip generation potential of the subject property. The Transportation staff report at the time stated "...the applicant's site plan is noting a grouping of allowable uses which would potentially generate more traffic than that which was analyzed..." Staff was able to partially mitigate this concern since the County still had a concurrency management system at that time. Staff wrote, "If it is determined by the results of the analysis submitted by the developer, that adequate right-of-way does not exist to construct any needed improvements (i.e. turn lanes), then the developer shall re-submit an analysis showing the maximum development that could occur to still enable the needed improvements to be constructed. The development may be limited to the size as shown in the submitted analysis."

The owner of the gas station to the east of the proposed project redeveloped the subject site (via PI #4210). The new site included a significant expansion of both the convenience store and gas pumps, and also included the addition of a quick service restaurant and coffee/donut shop with drive-through. Redevelopment of that parcel involved the closure of the existing driveways on McIntosh Rd. serving that site, with a single full-access driveway connection north of the northernmost driveway (generally across from the right-in/right-out driveway serving Camping World) now being the only project access. The developer of that project was also required to construct a northbound to westbound left turn lane into their project driveway.

In previous years, area citizens have expressed specific concern regarding the proposed removal of the exit only driveway to McIntosh Rd. Given the relocation of the gas station driveway, as well as significant intensification in trip generation due to the proposed redevelopment of that property, both as described above, staff had (and continues to have) significant concerns with the proximity of the new gas station driveway to the exit only driveway.

If it were to remain, the exit only driveway would be within +/- 40 feet of the new driveway. In addition to not complying with Section 6.04.07 spacing standards, such a driveway spacing would unnecessarily create a significant number of conflict points that could otherwise be managed by directing exiting traffic on the subject property to Muck Pond Rd. (a collector roadway designated for such traffic). Staff has prepared a graphic (attached) showing some of the conflicting turning movements that would be created should the exit only driveway remain. Although staff is sensitive to the citizen concerns regarding the effects of exiting project traffic (9 trips in the a.m. peak hour and 37 trip in the p.m. peak hour) onto Muck Pond Rd., staff believes that the health, safety, and welfare of the traveling public would be better served and protected through elimination of the exit only access driveway onto McIntosh Rd.

REQUESTED DESIGN EXCEPTION – MUCK POND RD.

As Muck Pond Rd. is a substandard collector roadway, the applicant's Engineer of Record (EOR) submitted a Design Exception request for Muck Pond Rd. (dated May 31, 2022 and last revised August 8, 2022) to determine the specific improvements that would be required by the County Engineer. Based on factors presented in the Design Exception request, the County Engineer found the Design Exception request approvable with conditions (on August 9, 2022). The deviations from the Hillsborough County Transportation Technical Manual (TTM) TS-7 Typical Section (for 2-Lane, Rural Local and Collector Roadways) include:

1. The developer will be permitted to widen the pavement such that the travel lanes are 11-feet in width, rather than the 12-foot lanes required per TS-7;
2. The developer shall be permitted to construct the turn lane with 11-foot wide lanes, in lieu of the 12-foot wide lanes required per TS-7;
3. The developer will be permitted to reduce the overall right-of-way width from 96 feet required per TS-7 and utilize the existing 50-feet of right-of-way plus an additional 15-foot wide easement along the south side of Muck Pond Rd. along the project's frontage;
4. The developer will be permitted to construct a 10-foot wide multi-purpose pathway in lieu of the 5-foot wide paved shoulders serving as bicycle facilities within TS-7; and,

5. The developer shall be permitted to utilize Type-F curb along the south side of the roadway and leave the north side of the roadway in its existing condition, in lieu of the 8-foot wide stabilized shoulders, of which 5-feet are paved, required per TS-7.

The County Engineer found the request approval with two conditions. First, the Design Exception showed a 10-foot wide multi-purpose pathway being provided along the entire project frontage, where as the text described it as only being provided between the easternmost driveway and McIntosh Rd. The County Engineer’s approval requires the improvements proposed in the attached site plan be constructed (i.e. the multi-purpose path along the entire project frontage). Second, the County Engineer noted that the latest revised request was sealed but was not signed. Provided the BOCC approves the zoning modification, the County Engineer required the applicant to submit a signed letter before they will formally approve the Design Exception.

If PRS 22-0844, is approve, the County Engineer will approve the above referenced Administrative Variance, subject to the conditions described above and prosed herein the zoning conditions of approval.

ROADWAY LEVEL OF SERVICE (LOS) INFORMATION

Muck Pond Rd. is not a regulated roadway or was not included in the Hillsborough County LOS report. As such, no information for the facility can be provided. Information for the other adjacent segments is provided below.

| Roadway | From | To | LOS Standard | Peak Hour Directional LOS |
|--------------|----------|------------------|--------------|---------------------------|
| McIntosh Rd. | I-4 Ramp | Thonotosassa Rd. | C | C |

Source: Hillsborough County 2020 Level of Service Report.

Ratliff, James

From: Williams, Michael
Sent: Tuesday, August 9, 2022 12:19 PM
To: Micahel Yates (myates@palmtraffic.com); Vicki Castro
Cc: Tirado, Sheida; Ratliff, James; PW-CEIntake; Heinrich, Michelle
Subject: RE: RE PRS 22-0844
Attachments: 22-0844 DReq 08-09-22.pdf

Resent with additional cc:.

From: Williams, Michael
Sent: Tuesday, August 9, 2022 12:17 PM
To: Micahel Yates (myates@palmtraffic.com) <myates@palmtraffic.com>; Vicki Castro <vcastro@palmtraffic.com>
Subject: FW: RE PRS 22-0844

Michael/Vicki,

I have found the attached Section Design Exception (DE) for PD 22-0844 APPROVABLE with Conditions. The Condition being that the 10' multiuse path be for the limits of the frontage as reflected in the drawing which is part of this request. Also, the applicant shall provide a signed copy of the DE should the Board approve the zoning.

Please note that it is you (or your client's) responsibility to follow-up with Transportation Staff after the BOCC approves the PD zoning or PD zoning modification related to below request. This is to obtain a signed copy of the DE/AV.

If the BOCC denies the PD zoning or PD zoning modification request, staff will request that you withdraw the AV/DE. In such instance, notwithstanding the above finding of approvability, if you fail to withdraw the request, I will deny the AV/DE (since the finding was predicated on a specific development program and site configuration which was not approved).

Once I have signed the document, it is your responsibility to submit the signed AV/DE(s) together with your initial plat/site/construction plan submittal. If the project is already in preliminary review, then you must submit the signed document before the review will be allowed to progress. Staff will require resubmittal of all plat/site/construction plan submittals that do not include the appropriate signed AV/DE documentation.

Lastly, please note that it is critical to ensure you copy all related correspondence to PW-CEIntake@hillsboroughcounty.org

Mike

Michael J. Williams, P.E.
Director, Development Review
County Engineer
Development Services Department

P: (813) 307-1851
M: (813) 614-2190
E: Williamsm@HillsboroughCounty.org
W: HCFLGov.net

Hillsborough County

601 E. Kennedy Blvd., Tampa, FL 33602

[Facebook](#) | [Twitter](#) | [YouTube](#) | [LinkedIn](#) | [HCFL Stay Safe](#)

Please note: All correspondence to or from this office is subject to Florida's Public Records law.

From: Rome, Ashley <RomeA@hillsboroughcounty.org>

Sent: Tuesday, August 9, 2022 9:26 AM

To: Andrea Papandrew <papandrewa@plancom.org>; Blinck, Jim <BlinckJ@HillsboroughCounty.ORG>; Brown, Gregory <BrownGr@hillsboroughcounty.org>; Bryant, Christina <BryantC@epchc.org>; Cabrera, Richard <CabreraR@HillsboroughCounty.ORG>; Dalfino, Jarryd <DalfinoJ@hillsboroughcounty.org>; Santos, Daniel <daniel.santos@dot.state.fl.us>; David Skrelunas <David.Skrelunas@dot.state.fl.us>; DeWayne Brown <brownd2@gohart.org>; Dickerson, Ross <DickersonR@HillsboroughCounty.ORG>; Ellen Morrison <ellen.morrison@swfwmd.state.fl.us>; Franklin, Deborah <FranklinDS@hillsboroughcounty.org>; Glorimar Belangia <Glorimar.Belangia@hcps.net>; Holman, Emily - PUD <HolmanE@HillsboroughCounty.ORG>; Hummel, Christina <HummelC@hillsboroughcounty.org>; James Hamilton <jkhamilton@tecoenergy.com>; Justin Willits <WillitsJ@gohart.org>; Kaiser, Bernard <KAISERB@HillsboroughCounty.ORG>; Karla Llanos <llanosk@plancom.org>; Katz, Jonah <KatzJ@hillsboroughcounty.org>; Kyle Brown <kyle.brown@myfwc.com>; landuse-zoningreviews@tampabaywater.org; Mineer, Lindsey <Lindsey.Mineer@dot.state.fl.us>; Lindstrom, Eric <LindstromE@hillsboroughcounty.org>; Mackenzie, Jason <MackenzieJ@hillsboroughcounty.org>; Melanie Ganas <mxganas@tecoenergy.com>; Melissa Lienhard <lienhardm@plancom.org>; O'Hern Weeks, Abbie <weeksa@epchc.org>; Olivia Ryall <oryall@teamhcso.com>; Perez, Richard <PerezRL@hillsboroughcounty.org>; Petrovic, Jaksa <PetrovicJ@HillsboroughCounty.ORG>; Pezone, Kathleen <PezoneK@hillsboroughcounty.org>; Ratliff, James <RatliffJa@hillsboroughcounty.org>; Hessinger, Rebecca <HessingerR@hillsboroughcounty.org>; Renee Kamen <renee.kamen@hcps.net>; Rochelle, Randy <RochelleR@HillsboroughCounty.ORG>; Rodriguez, Dan <RodriguezD@gohart.org>; RP-Development <RP-Development@hillsboroughcounty.org>; Salisbury, Troy <SalisburyT@hillsboroughcounty.org>; Sanchez, Silvia <sanchezs@epchc.org>; Shelton, Carla <SheltonC@HillsboroughCounty.ORG>; Steady, Alex <SteadyA@hillsboroughcounty.org>; Tony Mantegna <tmantegna@tampaairport.com>; Turbiville, John (Forest) <TurbivilleJ@HillsboroughCounty.ORG>; WetlandsPermits@epchc.org; Woodard, Sterlin <Woodard@epchc.org>; Yeneka Mills <millsy@plancom.org>
Cc: Heinrich, Michelle <HeinrichM@HillsboroughCounty.ORG>; Vazquez, Bianca <VazquezB@hillsboroughcounty.org>; Tirado, Sheida <TiradoS@hillsboroughcounty.org>; Williams, Michael <WilliamsM@HillsboroughCounty.ORG>
Subject: RE PRS 22-0844

Good Day All,

Please be advised, we have received and uploaded to Optix **revised documents/plans** for the above mentioned application. Please review and comment.

For further information regarding the change/update please contact the assigned planner.

Planner assigned:

Planner: Michelle Heinrich

Contact: heinrichm@hillsboroughcounty.org

Have a good one,

Ashley Rome
Planning & Zoning Technician

Development Services Dept.

P: (813) 272-5595

E: romea@hillsboroughcounty.org

W: HCFLGov.net

Hillsborough County

601 E. Kennedy Blvd., Tampa, FL 33602

[Facebook](#) | [Twitter](#) | [YouTube](#) | [LinkedIn](#) | [HCFL Stay Safe](#)

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May 31, 2022
Revised July 20, 2022
Revised August 8, 2022

Mr. Michael Williams, P.E.
Hillsborough County
Development Services Department
Development Review Director
County Engineer
601 East Kennedy Boulevard, 20th Floor
Tampa, Florida 33602

RE: Muck Pond Road and McIntosh Road - SWC (MM 22-0844)
Folio: 82885.0050, 82881.0000, 82885.0000
Design Exception – Muck Pond Road
Palm Traffic Project No. T22047

Dear Mr. Williams:

The purpose of this letter is to provide justification for the design exception per Transportation Technical Manual (TTM) 1.7 to meet the requirements of the Hillsborough County Land Development Code (LDC) Section 6.04.03.L (existing facility) in association with the proposed development of up 55,000 square feet of RV sales and 154,000 square feet of RV storage facility located south of Muck Pond Road and west of McIntosh Road, as shown in Figure 1. This request is made based on our virtual meeting with Hillsborough County staff.

The project proposes to have one (1) full access to Muck Pond Road. Muck Pond Road is identified in the Hillsborough County Functional Classification Map as a collector roadway and was identified during our meeting as a substandard road. Muck Pond Road has a posted speed limit of 40 mph and currently has 10-foot travel lanes in approximately 50 feet of right of way.

This request is a design exception to the Hillsborough County Transportation Technical Manual for Muck Pond Road from McIntosh Road west to the property driveway. The requested exceptions to the TS-7 typical section and the justification are as follows:

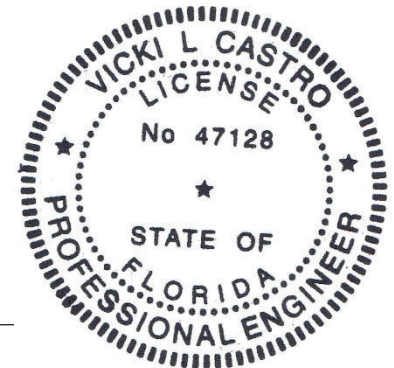
1. The existing ROW along Muck Pond Road is approximately 50 feet. The typical TS-7 section for a collector, two-lane undivided roadway requires a minimum of 96 feet of ROW with 12-foot lanes, 8-foot shoulders (5-foot paved), open ditch drainage and a 5-foot sidewalk. This section of Muck Pond Road currently has 10-foot travel lanes and no bike lanes or sidewalk.
2. The request is to provide 11-foot travel and left turn lanes instead of the 12-foot lanes. According to the Florida Green Book, on a suburban roadway where the speed limit is between 40 and 45 mph, 11-foot lanes may be used. The post speed limit on Muck Pond road is 40 mph.
3. An F type curb and 10-foot multi-use path will be provided only on the south side of Muck Pond Road from the beginning of the transition to the 3-lane section east to McIntosh Road. The multi-use path will be provided within a 15-foot public access, utility and maintenance easement. There is not enough ROW to provide a sidewalk on the north side of Muck Pond Road.
4. No modification to the northern edge of pavement, as all improvements will be made on the southside of Muck Pond Road.

The proposed typical section is shown in Figure 2.

Please do not hesitate to contact us if you have any questions or require any additional information.

Sincerely,

Vicki L Castro, P.E.
Principal



Based on the information provided by the applicant, this request is:

_____ Disapproved _____ Approved with Conditions _____ Approved

If there are any further questions or you need clarification, please contact Sheida L. Tirado, P.E.

Sincerely,

Michael J. Williams
Hillsborough County Engineer

FIGURE 1. LOCATION MAP

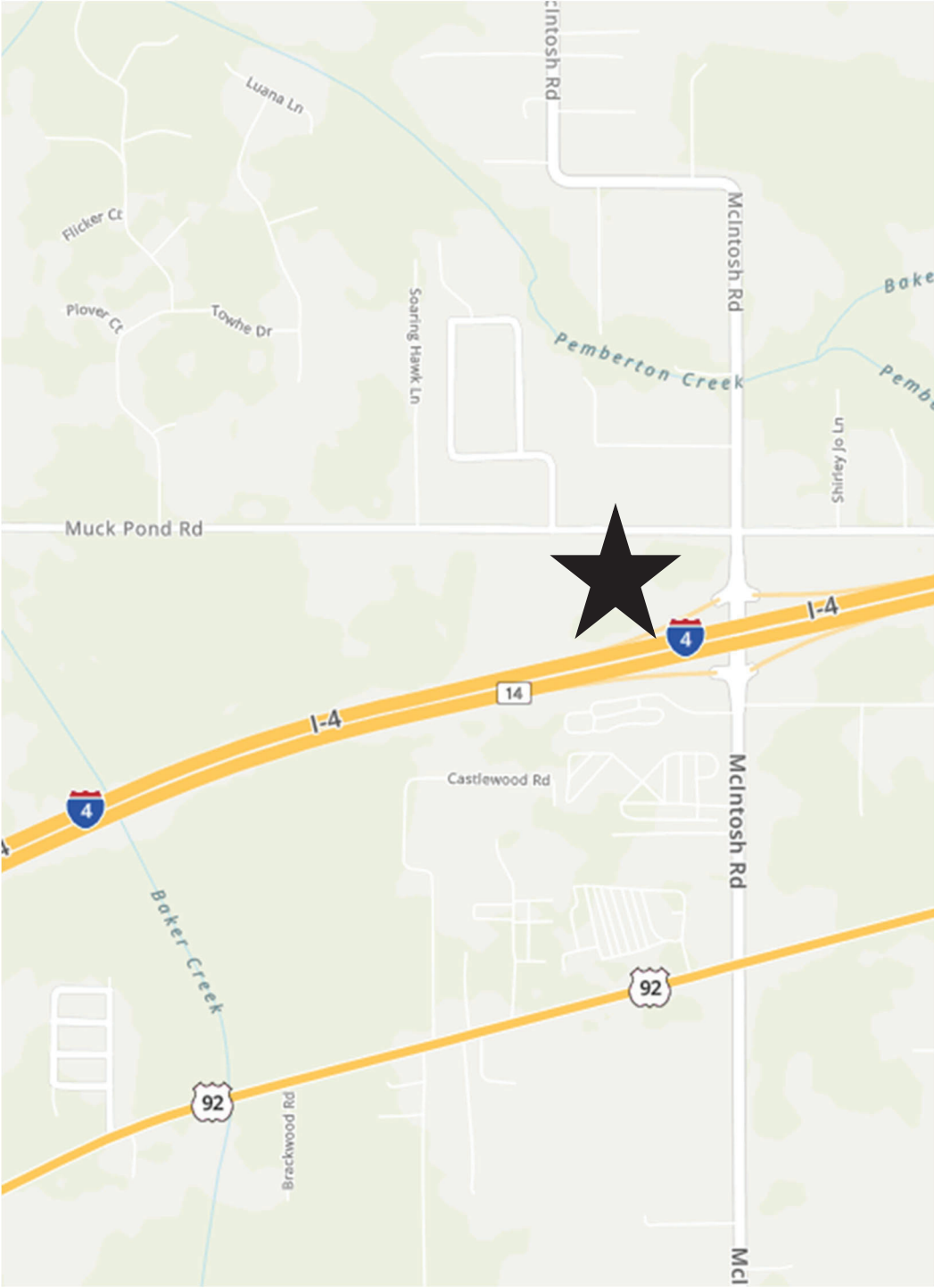
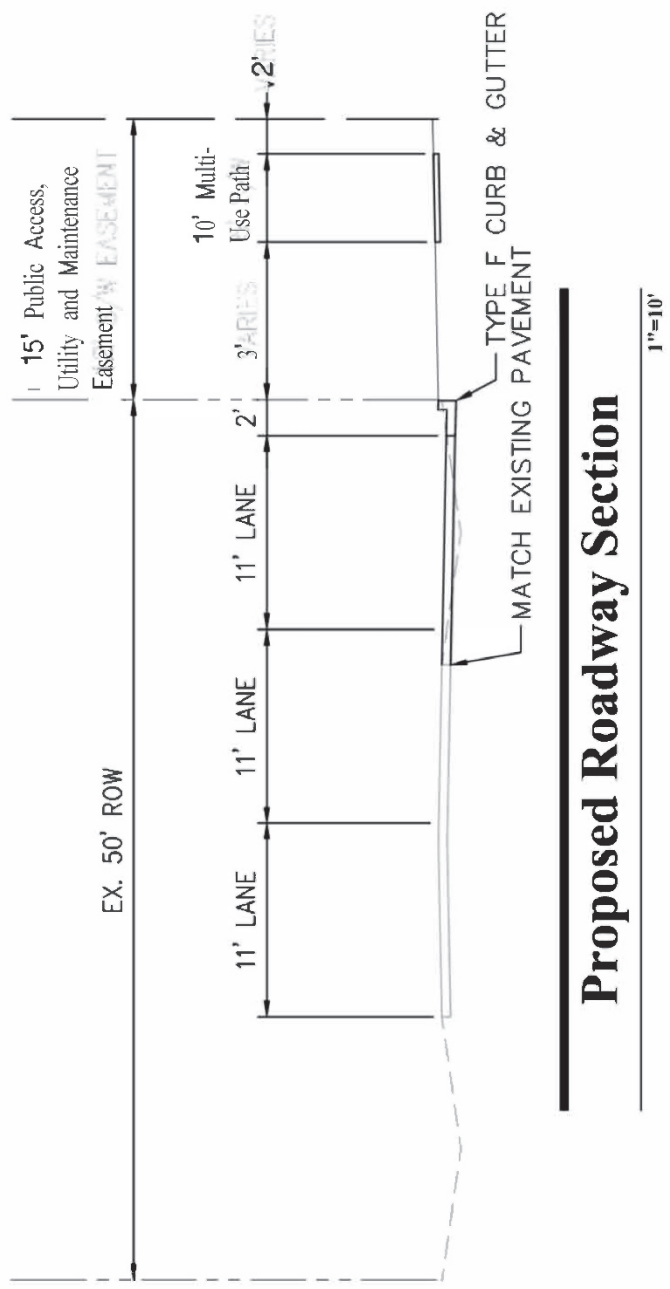
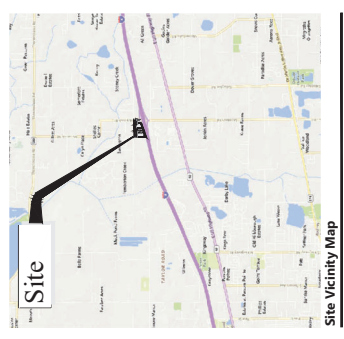


FIGURE 2. PROPOSED TYPICAL 3 LANE SECTION





501 E Kennedy Boulevard
 Suite 1010
 Tampa, FL 33602
 813.327.5450
 Certificate of Authorization
 Number FL #3932



Site
 SCALE 1" = 5,000'

Site Data Table

FOLIO NUMBERS:
 PARCEL A: 83885-0000
 PARCEL B: 82881-0000 & 8088-0000

PROPERTY SIZE:
 119.12 ACRES

EXISTING ZONING:
 PD (DT 6520)

PREVIOUS ZONINGS:
 R2 (SR 08-23) PM (SR 0134)

PROPOSED ZONING:
 PD

FUTURE LAND USE:
 RES-1

EXISTING APPROVED USES:
 SALES AND DISPLAY OF CONSTRUCTION RECREATIONAL VEHICLES WITH PRE-SALE SERVICES

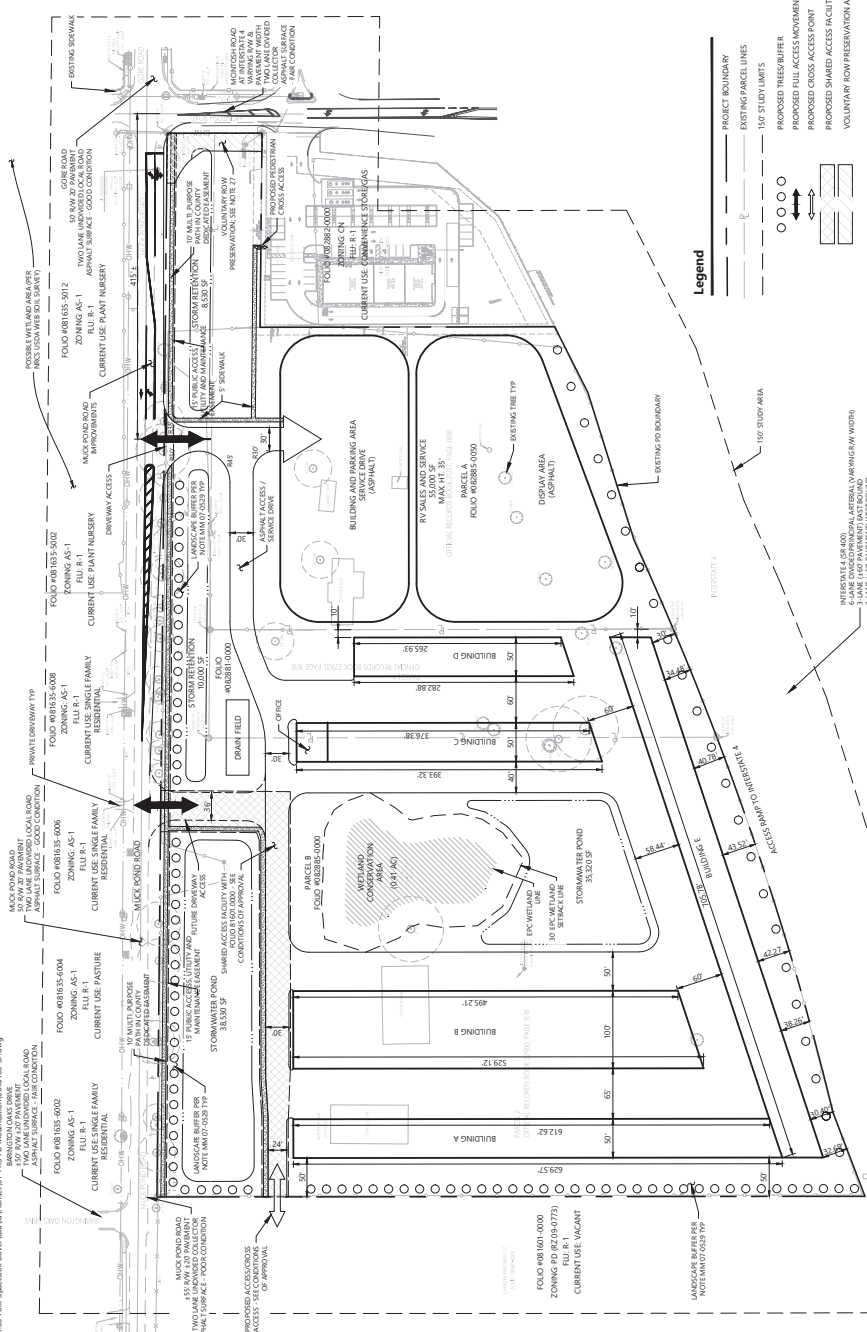
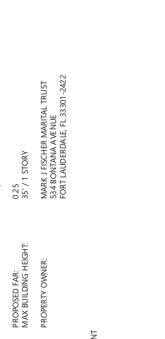
PROPOSED USES:
 PARCEL A: RV SALES AND SERVICE CLUBHOUSE OFFICE
 PARCEL B: 2,208 SF

EXISTING BUILDING AREA:
 PROPOSED BUILDING AREA:
 PARCEL A: 50,000 SF
 PARCEL B: 20,000 SF
 TOTAL: 70,000 SF

PROPOSED FAR:
 0.35

MAX BUILDING HEIGHT:
 35' / 1 STORY

PROPERTY OWNER:
 MARK J FISCHER MARITAL TRUST
 51 BENTON AVENUE
 FORT TABER, FL 3301-2622



Development Summary

Legend

- PROJECT BOUNDARY
- EXISTING PARCEL LINES
- 150' STUDY LIMITS
- PROPOSED TREE/SHRUB
- PROPOSED FULL ACCESS MOVEMENT POINT
- PROPOSED CROSS ACCESS POINT
- PROPOSED SHARED ACCESS FACILITY
- VOLUNTARY ROW PRESERVATION AREA

Legal Description

1. THE SUBJECT PROPERTY IS LOCATED WITHIN THE PARAGRAPHS OF THE HILLSBOROUGH COUNTY LAND DEVELOPMENT CODE...
 2. THE SUBJECT PROPERTY IS DEVELOPED AS A RESIDENTIAL SUBDIVISION...
 3. THE SUBJECT PROPERTY IS LOCATED WITHIN THE PARAGRAPHS OF THE HILLSBOROUGH COUNTY LAND DEVELOPMENT CODE...
 4. THE SUBJECT PROPERTY IS LOCATED WITHIN THE PARAGRAPHS OF THE HILLSBOROUGH COUNTY LAND DEVELOPMENT CODE...
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 30. THE SUBJECT PROPERTY IS LOCATED WITHIN THE PARAGRAPHS OF THE HILLSBOROUGH COUNTY LAND DEVELOPMENT CODE...

Table 210.2.1 – Minimum Travel and Auxiliary Lane Widths

| Context Classification | | Travel (feet) | | | Auxiliary (feet) | | | Two-Way Left Turn (feet) | |
|------------------------|---------------|--------------------|-------|------|--------------------|-------|------|--------------------------|----|
| | | Design Speed (mph) | | | Design Speed (mph) | | | Design Speed (mph) | |
| | | 25-35 | 40-45 | ≥ 50 | 25-35 | 40-45 | ≥ 50 | 25-35 | 40 |
| C1 | Natural | 11 | 11 | 12 | 11 | 11 | 12 | N/A | |
| C2 | Rural | 11 | 11 | 12 | 11 | 11 | 12 | | |
| C2T | Rural Town | 11 | 11 | 12 | 11 | 11 | 12 | 12 | 12 |
| C3 | Suburban | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |
| C4 | Urban General | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |
| C5 | Urban Center | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |
| C6 | Urban Core | 10 | 11 | 12 | 10 | 11 | 12 | 11 | 12 |

Notes:

Travel Lanes:

- (1) Minimum 11-foot travel lanes on designated freight corridors, SIS facilities, or when truck volume exceeds 10% on very low speed roadways (design speed ≤ 35 mph) (regardless of context).
- (2) Minimum 12-foot travel lanes on all undivided 2-lane, 2-way roadways (for all context classifications and design speeds). However, 11-foot lanes may be used on 2-lane, 2-way curbed roadways that have adjacent buffered bicycle lanes.
- (3) 10-foot travel lanes are typically provided on very low speed roadways (design speed ≤ 35 mph), but should consider wider lanes when transit is present or truck volume exceeds 10%.
- (4) Travel lanes should not exceed 14 feet in width.

Auxiliary Lanes:

- (1) Auxiliary lanes are typically the same width as the adjacent travel lane.
- (2) Table values for right turn lanes may be reduced by 1 foot when a bicycle keyhole is present.
- (3) Median turn lanes should not exceed 15 feet in width.
- (4) For high speed curbed roadways, 11-foot minimum lane widths are allowed for the following:
 - Dual left turn lanes
 - Single left turn lanes at directional median openings.
- (5) For RRR Projects, 9-foot right turn lanes on very low speed roadways (design speed ≤ 35 mph) are allowed.

Two-way Left Turn Lanes:

- (1) Two-way left turn lanes are typically one foot wider than the adjacent travel lanes.
- (2) For RRR Projects, the values in the table may be reduced by 1-foot.

Transportation Comment Sheet

3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)

| Adjoining Roadways (check if applicable) | | | |
|---|-----------------------------|---|--|
| Road Name | Classification | Current Conditions | Select Future Improvements |
| Muck Pond Rd. | County Collector - Rural | 2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input checked="" type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |
| McIntosh Rd. | County Collector - Rural | 2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other – ROW Preservation |
| | Choose an item. | Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |
| | Choose an item. | Choose an item. Lanes <input type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width | <input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other |

| Project Trip Generation <input type="checkbox"/> Not applicable for this request | | | |
|---|-----------------------------------|-----------------------------|-----------------------------|
| | Average Annual Daily Trips | A.M. Peak Hour Trips | P.M. Peak Hour Trips |
| Existing | 498 | 39 | 65 |
| Proposed | 498 | 39 | 65 |
| Difference (+/-) | No Change | No Change | No Change |

*Trips reported are based on net new external trips unless otherwise noted.

| Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request | | | | |
|---|-----------------------|---------------------------------------|------------------------|----------------|
| Project Boundary | Primary Access | Additional Connectivity/Access | Cross Access | Finding |
| North | X | Vehicular & Pedestrian | None | Meets LDC |
| South | | None | None | Meets LDC |
| East | | Pedestrian | None | Meets LDC |
| West | | Vehicular & Pedestrian | Vehicular & Pedestrian | Meets LDC |
| Notes: | | | | |

| Design Exception/Administrative Variance <input type="checkbox"/> Not applicable for this request | | |
|--|----------------------------|----------------------------|
| Road Name/Nature of Request | Type | Finding |
| Muck Pond Rd. – Substandard Road | Design Exception Requested | Approvable with Conditions |
| | Choose an item. | Choose an item. |
| Notes: | | |

Transportation Comment Sheet

| 4.0 Additional Site Information & Agency Comments Summary | | | |
|--|---|--|---------------------------------|
| Transportation | Objections | Conditions Requested | Additional Information/Comments |
| <input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input checked="" type="checkbox"/> Off-Site Improvements Provided | <input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No | <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No | |



Florida Department of Transportation

RON DESANTIS
GOVERNOR

2822 Leslie Road
Tampa, FL 33619

KEVIN J. THIBAUT, P.E.
SECRETARY

May 18, 2021

**THIS DOCUMENT IS NOT A PERMIT APPROVAL.
THIS PRE-APPLICATION FINDING MAY NOT BE USED AS A BASIS FOR PERMIT APPROVAL AFTER
11/18/2021.**

Re: PRE-APPLICATION REVIEW FOR ACCESS CONNECTION PERMIT REQUEST

| | |
|--|---------------------------------|
| Date: May 18, 2021 | State Road#: 400 |
| Time: 10:30 AM | Section ID #: 10 190 000 |
| Applicant: Dave Bartelt | Mile Post: 21.178 |
| Project: Construction Equipment Rental | Road Class: N/A |
| Location: I-4 & McIntosh SW Quadrant | MPH: N/A |
| County: Hillsborough | DW/Sig Spacing: N/A |
| Folio#: 082885-0000 & 0500, 082881-0000 | Median Spacing: N/A |

Dear Mr. Bartelt,

The Pre-application review of the subject project was conducted by your request. The purpose of the Pre-application review is to educate both the applicant and the Department of the project, the scope of work being proposed, and the requirements to obtain a permit for allowed development or modification to connections within the state Right of Way. After discussing the project and doing a thorough review of the documentation presented, the following comments are to be considered in the final design and we have determined that

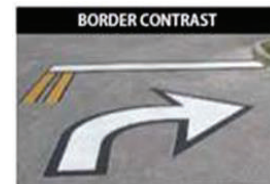
- We disapprove the concept as presented with the following considerations.
- We approve the concept as presented with the following conditions/considerations.
- We approve of the concept as submitted and we invite you to submit a permit application package to the Permit Office with engineering drawings that reflect the concept proposed in this meeting.
- We are prepared to continue the review of the concept with the District Variance Committee.
- We are prepared to continue the review of the concept as presented with the following considerations.

Conditions and Comments:

This project does not propose access to SR 400, a class 2 roadway, nor does it propose to disrupt the LA ROW. All access and construction will be in the county ROW.

FDOT Recommendations,

1. FDOT project # 447157-1 is a project on Macintosh that may involve your land. It is not due to go to design for at least a year and a half. Contact project manager, Liz Winters for more details (813) 975 – 6169/ Elizabeth.Winters@dot.state.fl.us or go to www.fdotampabay.com.
2. Manny Flores is the project manager for the I-4 project, 443319-1 if you want more details on this project. 813-975-4248.
3. All access to site will be from county roadways.
4. If the County requires sidewalk to be installed along property frontage, it may be built on private land if an easement is provided with it. Tie into sidewalk stub-out on backside of the gas station
5. With this being construction equipment on trailers, they may want to reconstruct the SW corner of McIntosh Rd @ Muck Pond with a 50'-75' radii to avoid over-tracking by trailers making the right-hand turn to head south. "Extra" pavement area Should be striped out. Additional R/W or 'corner clip' may be needed to accommodate this. See example of Gibsonton Drive & Kenlake/Fernhill below. Check with Hillsborough County on these requirements as well.
6. Additional R/W may be needed along McIntosh and Muck Pond Road; no additional R/W is anticipated to be needed for I-4.
7. A drainage exception will be required, it appears that this project does not drain towards the FDOT R/W.
8. Maintain 20' pedestrian sight triangles and draw the triangles on the plans to show there are no obstructions taller than 24" within the triangles. (See example below)
9. All typical driveway details to be placed properly:
 - a. 6' wide, high emphasis, ladder style crosswalk straddling the detectable warning mats.
 - b. 24" thermoplastic white stop bar equal to the lane width placed 4' behind crosswalk.
 - c. 36" stop sign mounted on a 3" round post, aligned with the stop bar.
 - d. If applicable, a "right turn only" sign mounted below the stop sign (FTP-55R-06 or FTP-52-06).
 - e. Double yellow 6" lane separation lines.
 - f. Directional arrow(s) 25' behind the stop bar.
 - g. Warning mats to be red in color unless specified otherwise.
 - h. All markings on concrete are to be high contrast (white with black border).
 - i. All striping within and approaching FDOT R/W shall be thermoplastic.
10. **Make a note in the plans** stating that vegetation control, maintenance and restoration is the responsibility of the UAO or Contractor for the duration of the work being done in the ROW for six months after after work is complete or until restored vegetation is well established per UAM sections 3.17 and 13.18



11. **Include a copy of this letter in the application submittal.**
12. **Plans shall be per the current Standard Plans and FDM.**
13. **Lighting of sidewalks and/or shared paths must be to current standards** (FDM section 231). Newly implemented FDOT Context classifications updated the required sidewalk widths (FDM section 222.2.1.1). Where sidewalk is being added and/or widened, the lighting will be analyzed to ensure sidewalks are properly lighted per FDOT FDM standards. Reference the following link for details: https://fdotwww.blob.core.windows.net/sitefinity/docs/default-source/roadway/fdm/2020/2020fdm231lighting.pdf?sfvrsn=2ad35fbf_2
14. Any relocation of utilities, utility poles, signs, or other agency owned objects must be coordinated with the Department and the **existing and proposed location** must be clearly labeled on the plans. Contact the Permits Department for more details and contact information.
15. Any project that falls within the limit of a FDOT project must contact project manager, provide a work schedule, and coordinate construction activities prior to permit approval. Contact the Permits Department for more details and contact information.
16. **All the following project identification information must be on the Cover Sheet of the plans:**
 - a. **All Associated FDOT Permit #'s)**
 - b. **SR # (& Local Road Name) and Road Section ID #**
 - c. **Mile Post # and Lt or Rt Roadway**
 - d. **Roadway Classification # and Speed Limit (MPH)**
17. All Plans and Documents submitted in OSP need to be **signed and sealed**.
18. The following FDOT Permits may additionally be required:
 - a. Access Permit / Construction Agreement
 - b. Drainage Permit or Exemption
 - c. Utility Permit – for any utility connections within the FDOT R/W (Except those that are exempt from permitting by the 2017 FDOT Utility Accommodation Manual).
 - d. Temporary Driveway Permit

If you do not have access to a computer, and are unable to apply through our One Stop Permitting website, you must submit your application to,

**Florida Department of Transportation
2822 Leslie Rd.
Tampa, Fl. 33619
Attn: Mecale' Roth**

Favorable review of the proposed generally means that you may develop plans that comply with the review comments and submit them with a permit application, within six months, to the Department for permit processing and further review. The Pre-application is for the applicant to discuss, with Department staff, the proposed site design for compliance and constructability in relation to the Standard Plans, and look at options, potential obstacles, or unforeseen issues. The review findings are not binding and are subject to change. The applicant's Engineer of Record is responsible for the technical accuracy of the plans. In keeping with the intent of the Rule, the Department will attempt to abide with the review comments to

the extent that necessary judgment is available to the Permits Engineer. Unfavorable review generally means that a permit application, based on the design proposal, would likely be denied.

If you do not agree with Pre-Application meeting results and would like to schedule an AMRC meeting, contact Traffic Ops, David Ayala at 813-975-6717.

For any other questions or assistance, please feel free to contact me.

Sincerely,

Mecale' Roth

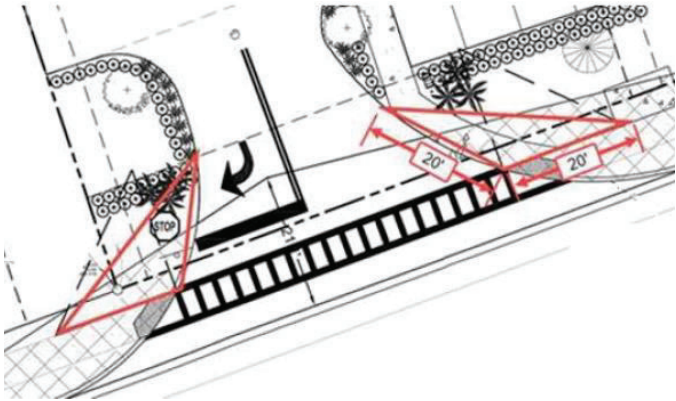
Permit Coordinator II
Tampa Operations
Office - 813-612-3237
Cell- 813-460-1121

Meeting Attendees:

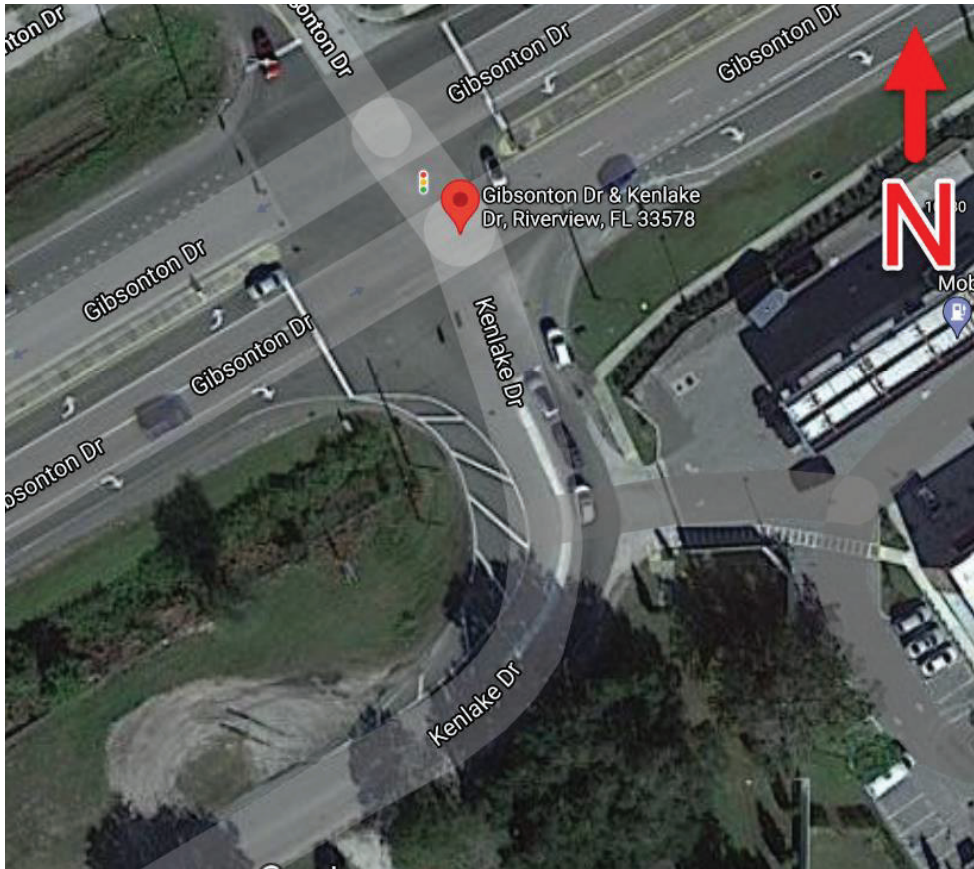
| | | |
|----------------|----------------|--|
| Guests- | Dave Bartelt | dbartelt@flaengineering.com |
| FDOT- | Matt Campbell | matthew.campbell@dot.state.fl.us |
| | Mecale' Roth | mecale.roth@dot.state.fl.us |
| | Holly Champion | holly.champion@dot.state.fl.us |
| | Ryan Bogan | ryan.bogan@dot.state.fl.us |
| | Lindsey Mineer | lindsey.miner@dot.state.fl.us |
| | Amanda Serra | amanda.serra@dot.state.fl.us |
| | Ahmad Chehab | ahmad.chehab@dot.state.fl.us |
| | Liz Winters | elizabeth.winters@dot.state.fl.us |

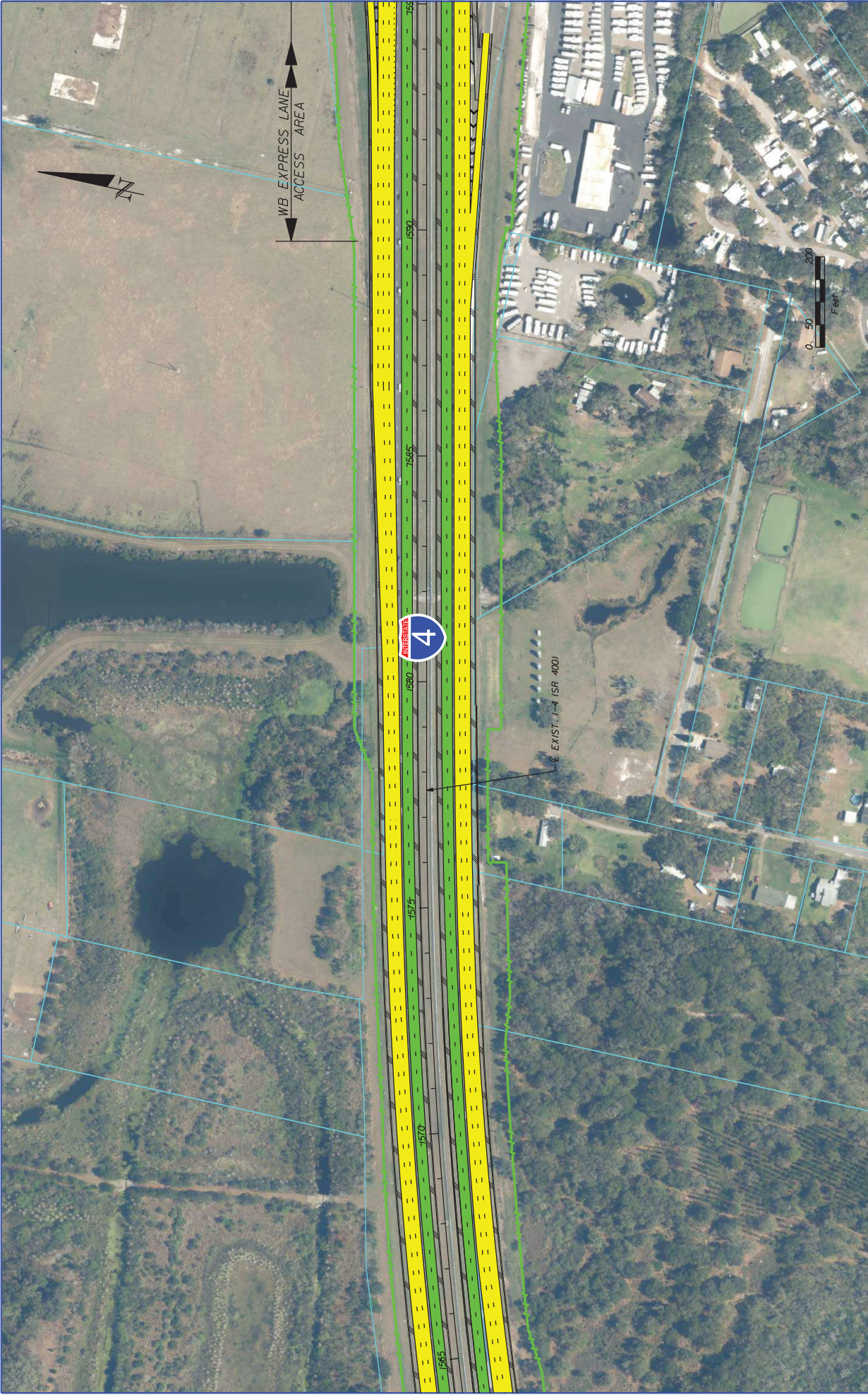
Pedestrian Sight Triangle Example:

Driveways leading onto state roads need to have min. 20' x 20' clear 'pedestrian sight triangles' on each side of the driveway, at the edge of the sidewalk. It should be measured as 20' up the sidewalk and 20' up the driveway from the point at which the sidewalk meets the driveway. Nothing above 2' in height from the pavement elevation (except for the stop sign post) should be placed in these triangles. Also, no parking spaces should be in these triangles, not even partially. Please draw in and label these 'pedestrian sight triangles' on the plans. Here is an example of what these triangles look like and how they are positioned (see red triangles in the attached example)

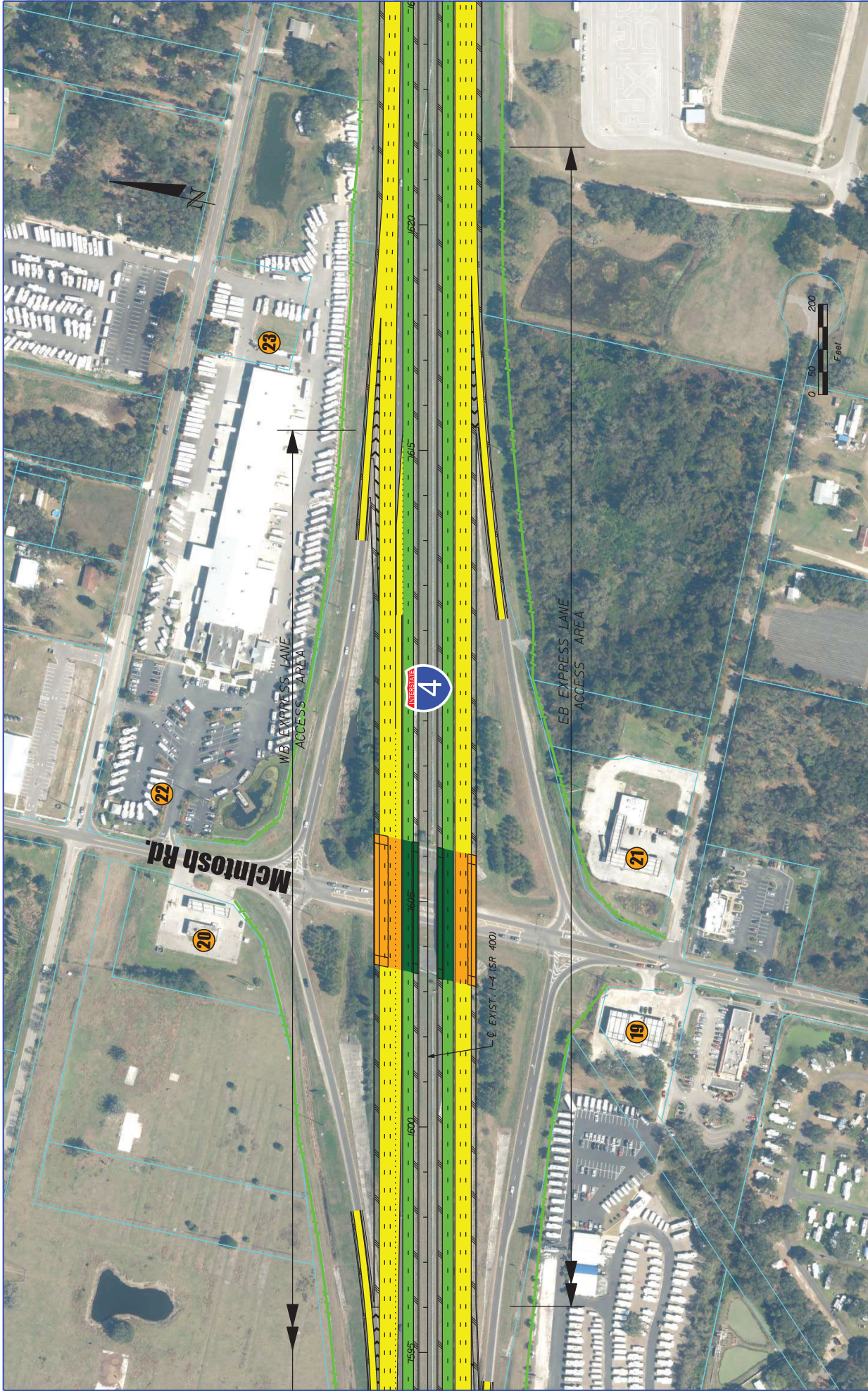


Striping Example @ Gibsonton & Kenlake:





| | | | |
|--|--|---|-----------------------|
| LEGEND: EXPRESS LANES - BRIDGE EXPRESS LANES - BRIDGE GENERAL USE LANES - BRIDGE NON-INTERSTATE ROWY. & BRIDGE EXIST. R/W EXIST. L/A R/W PROP. L/A R/W PROP. R/W POTENTIAL NOISE BARRIER PARCEL LINES POTENTIAL RESIDENTIAL RELOCATION POTENTIAL CONTAMINATION SITE | STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION | SR 400 (I-4) CONCEPT PLANS (2040) PREFERRED BUILD ALTERNATIVE | SHEET NO. 19 |
| | ROAD NO. 400 COUNTY HILLSBOROUGH | FINANCIAL PROJECT ID 431746-1-22-01 | SOURCES STINES SFILES |
| | AERIAL FLOWM DATE: 2014 |  | |
| | | | |



LEGEND:

- EXPRESS LANES - BRIDGE
- EXPRESS LANES - BRIDGE
- GENERAL USE LANES - BRIDGE
- NON-INTERSTATE ROW, & BRIDGE
- EXIST. R/W
- EXIST. L/A R/W
- PROP. L/A R/W
- PROP. R/W
- POTENTIAL NOISE BARRIER
- PARCEL LINES
- POTENTIAL RESIDENTIAL RELOCATION
- POTENTIAL CONTAMINATION
- SITE

STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION

ROAD NO. 400 **COUNTY HILLSBOROUGH** **FINANCIAL PROJECT ID 431746-1-22-01**

SR 400 (I-4)
CONCEPT PLANS (2040)
PREFERRED BUILD ALTERNATIVE

SHEET NO. 20

DATE: 2014

AERIAL FLOWN

SCALE: 0 50 200 Feet

STATE FILES





COMMISSION

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 Pat Kemp VICE-CHAIR
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 Steffanie L. Wickham WASTE DIVISION
 Sterlin Woodard, P.E. WETLANDS DIVISION

AGENCY COMMENT SHEET

| REZONING | |
|--|---|
| <p>HEARING DATE: June 7, 2022</p> <p>PETITION NO.: 22-0844</p> <p>EPC REVIEWER: Sarah Hartshorn</p> <p>CONTACT INFORMATION: (813) 627-2600 X 1237</p> <p>EMAIL: hartshorns@epchc.org</p> | <p>COMMENT DATE: May 5, 2022</p> <p>PROPERTY ADDRESS: 7351, 7345 Muck Pond Rd and 4820 McIntosh Rd, Dover, FL 33527</p> <p>FOLIO #: 0828810000, 0828850000, 0828850050</p> <p>STR: 30-28S-21E</p> |
| <p>REQUESTED ZONING: Minor Mod to PD</p> | |
| FINDINGS | |
| WETLANDS PRESENT | YES |
| SITE INSPECTION DATE | November 12, 2021 |
| WETLAND LINE VALIDITY | Expired |
| WETLANDS VERIFICATION (AERIAL PHOTO, SOILS SURVEY, EPC FILES) | Pond excavated out of hydric soils in the central portion of the property and ditches throughout |
| <p>The EPC Wetlands Division has reviewed the proposed rezoning. In the site plan's current configuration, a resubmittal is not necessary. If the zoning proposal changes and/or the site plans are altered, EPC staff will need to review the zoning again. This project as submitted is conceptually justified to move forward through the zoning review process as long as the following conditions are included:</p> <ul style="list-style-type: none"> Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals. The construction and location of any proposed wetland impacts are not approved by this correspondence, but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property. Prior to the issuance of any building or land alteration permits or other development, the approved wetland / other surface water (OSW) line must be incorporated into the site plan. The wetland / OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be | |

Environmental Excellence in a Changing World

Environmental Protection Commission - Roger P. Stewart Center
 3629 Queen Palm Drive, Tampa, FL 33619 - (813) 627-2600 - www.epchc.org

labeled as "Wetland Conservation Area" pursuant to the Hillsborough County Land Development Code (LDC).

- Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.

INFORMATIONAL COMMENTS:

The following specific comments are made for informational purposes only and to provide guidance as to the EPC review process. However, future EPC staff review is not limited to the following, regardless of the obviousness of the concern as raised by the general site plan and EPC staff may identify other legitimate concerns at any time prior to final project approval.

- The subject property contains wetland/OSW areas, which have not been delineated. Knowledge of the actual extent of the wetland and OSW are necessary in order to verify the avoidance of wetland impacts pursuant to Chapter 1-11. Prior to the issuance of any building or land alteration permits or other development, the wetlands/OSWs must be field delineated in their entirety by EPC staff or Southwest Florida Water Management District staff (SWFWMD) and the wetland line surveyed. Once delineated, surveys must be submitted for review and formal approval by EPC staff.
- Chapter 1-11, prohibits wetland impacts unless they are necessary for reasonable use of the property. Staff of the EPC recommends that this requirement be taken into account during the earliest stages of site design so that wetland impacts are avoided or minimized to the greatest extent possible. The size, location, and configuration of the wetlands may result in requirements to reduce or reconfigure the improvements depicted on the plan.
- The Hillsborough County Land Development Code (LDC) defines wetlands and other surface waters as Environmentally Sensitive Areas. Pursuant to the LDC, wetlands and other surface waters are further defined as Conservation Areas or Preservation Areas and these areas must be designated as such on all development plans and plats. A minimum setback must be maintained around the Conservation/Preservation Area and the setback line must also be shown on all future plan submittals.
- Any activity interfering with the integrity of wetland(s) or other surface water(s), such as clearing, excavating, draining or filling, without written authorization from the Executive Director of the EPC or authorized agent, pursuant to Section 1-11.07, would be a violation of Section 17 of the Environmental Protection Act of Hillsborough County, Chapter 84-446, and of Chapter 1-11.

Sjh/mst

**WATER RESOURCE SERVICES
REZONING REVIEW COMMENT SHEET: WATER & WASTEWATER**

PETITION NO.: PRS22-0844 **REVIEWED BY:** Randy Rochelle **DATE:** 6/29/2022

FOLIO NO.: 82881.0000, 82885.0000 & 82885.0050

WATER

- The property lies within the _____ Water Service Area. The applicant should contact the provider to determine the availability of water service.

- A ___ inch water main exists (adjacent to the site), (approximately ___ feet from the site) _____. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.

- Water distribution system improvements will need to be completed prior to connection to the County's water system. The improvements include _____ and will need to be completed by the _____ prior to issuance of any building permits that will create additional demand on the system.

WASTEWATER

- The property lies within the _____ Wastewater Service Area. The applicant should contact the provider to determine the availability of wastewater service.

- A ___ inch wastewater gravity main exists (adjacent to the site), (approximately ___ feet from the site) _____. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.

- Wastewater collection system improvements will need to be completed prior to connection to the County's wastewater system. The improvements include _____ and will need to be completed by the _____ prior to issuance of any building permits that will create additional demand on the system.

COMMENTS: The subject site is located outside of the Hillsborough County Urban Service Area, therefore water and/or wastewater service is not generally allowed. if the site is required or otherwise allowed to connect to the potable water and/or wastewater systems, there will be offsite improvements required that extend beyond a connection to the closest location with existing infrastructure. These points-of-connection will have to be determined at time of application of service as additional analysis will be required to make the final determination .