



**Rezoning Application:** PD 21-0420  
**Zoning Hearing Master Date:** August 16, 2021  
**BOCC Land Use Meeting Date:** September 20, 2021 at 6:00 pm

**1.0 APPLICATION SUMMARY**

**Applicant:** 2<sup>nd</sup> Wave Development, LLC  
**FLU Category:** SMU-6  
**Service Area:** Urban  
**Site Acreage:** 21.98  
**Community Plan Area:** Brandon  
**Overlay:** None  
**Request:** Rezoning to PD



**Request Summary:**

The current zonings of the subject parcels allow residential, agricultural and non-residential uses (as detailed below). The proposed zoning is for a Planned Development (PD), which is a site controlled zoning district to allow for 472 multi-family units. Of the proposed 472 multi-family units, 120 units will be affordable housing. The project is proposed to be developed in accordance with the proposed site plan (see Section 2.4) and the proposed conditions of approval (see Section 6). This application is associated with Comprehensive Plan Amendment and Minor Modification (PRS) application.

Zoning:	Current						Proposed
	ASC-1	AS-1	RSC-6 (MH)	RSC-3	BPO -R	PD 05-1577	PD 21-0420
Uses	Agricultural and single-family	Agricultural and single-family	Single-family conventional and mobile home	Single-family	Office	Office, limited commercial and minor vehicle repair	Multi-family (472 units)
Mathematical Maximums*	19 units	2 units	4 units	1 unit	17,424 sf	13,900 sf	472 units

\*approximate maximum zoning entitlements may be reduced due to providing roads, stormwater, or other improvements at the site development state.

Development Standards:	Current						Proposed
	ASC-1	AS-1	RSC-6 (MH)	RSC-3	BPO-R	PD 05-1577	PD 21-0420
Density and Intensity	1 dwelling unit (du)/acre	1 du/acre	6 du/acre	3 du/acre	.20 Floor Area Ratio (FAR)	NA	0.21
Lot Size / Lot Width	1 acre / 150'	1 acre / 150'	7,000 square feet (sf) / 70'	14,520 sf / 75'	NA / 70'	NA	NA

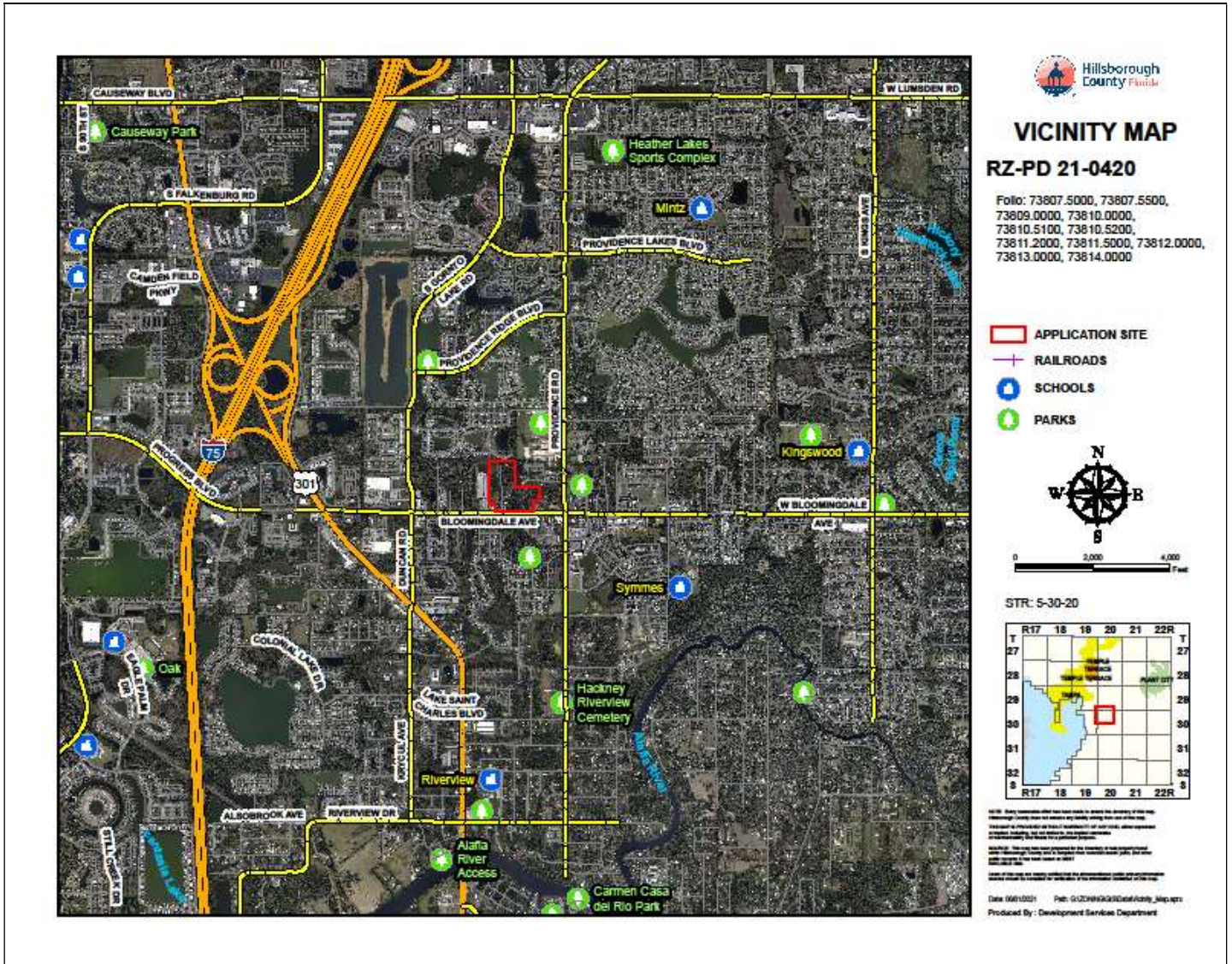
Setbacks/ Buffering and Screening	50' Front 50' Rear 15' Sides	50' Front 50' Rear 15' Sides	25' Front 25' Rear 7.5' Sides	25' Front 25' Rear 7.5' Sides	30' Front Rear and side yards dependent on adjacent use	Per site plan	Per site plan
Height	50'	50'	35'	35'	50' with 2:1 setback	35'	55' with 2:1 setback

**Additional Information:**

PD Variations	Allow a fence height increase from 6 to 8 feet along specific PD boundaries – see Figure 1 – Section 7.0 (LDC Section 6.07.00 -Fences)
Waiver(s) to the Land Development Code	None
Planning Commission Recommendation	Consistent
Development Services Department Recommendation	Approvable, subject to proposed conditions

## 2.0 LAND USE MAP SET AND SUMMARY DATA

### 2.1 Vicinity Map



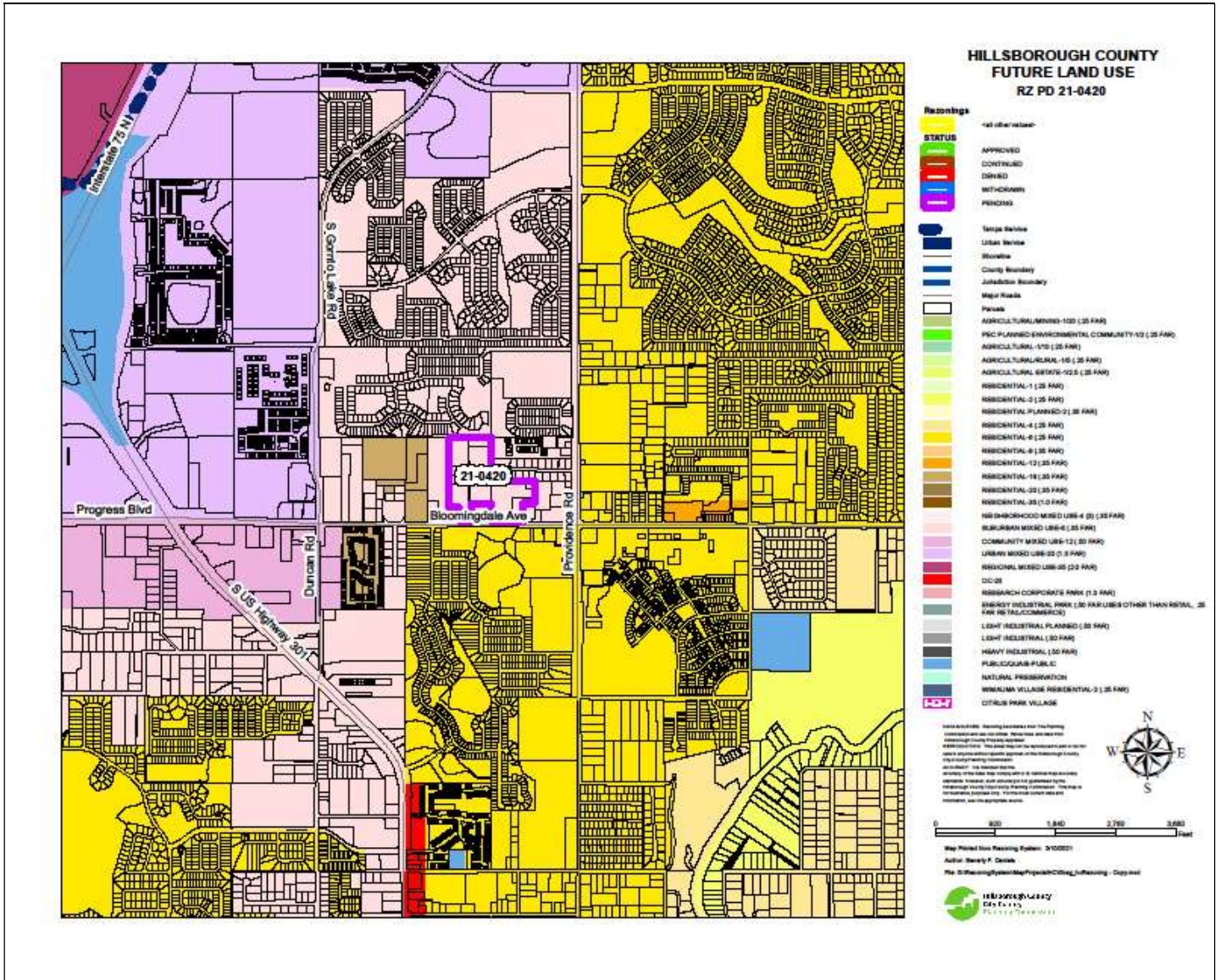
#### Context of Surrounding Area:

The area is developed in a typical suburban manner consisting of residential and non-residential uses within the Bloomingdale Avenue corridor. Non-residential uses can be found both adjacent to the project and at area intersections. Residential uses are primarily single-family with areas of townhome development.

The subject site is located on the north side of Bloomingdale Avenue, east of Gornto Lake Road and west of Providence Road. I-75 is located approximately 1 mile to the west and accessed via US Highway 301.

## 2.0 LAND USE MAP SET AND SUMMARY DATA

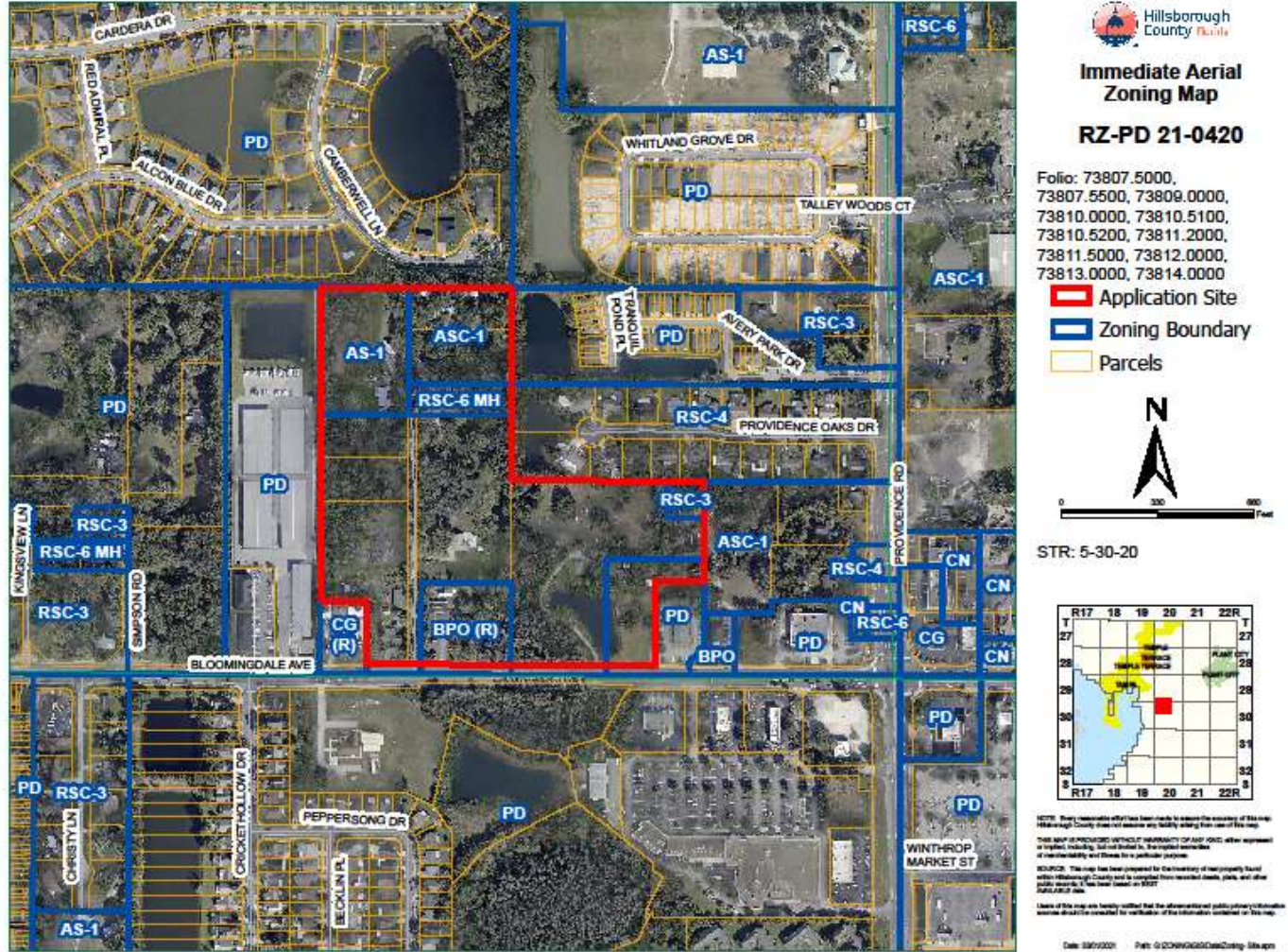
### 2.2 Future Land Use Map



Subject Site Future Land Use Category:	SMU-6 (Pending RES-16 per Comp Plan Amendment and Density Bonus Request)
Maximum Density/F.A.R.:	SMU-6: 6 units/acre RES-16 (pending): 16 units per acre Comprehensive Plan Policy 3.6.1 - Housing Element: 20 units per acre
Typical Uses:	SMU-6: residential, suburban scale neighborhood commercial, office, research corporate park, light industrial, multi-purpose and clustered residential and/or mixed use. RES-16 (pending): residential, urban scale neighborhood commercial, office, multi-purpose projects and mixed use.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Immediate Area Map

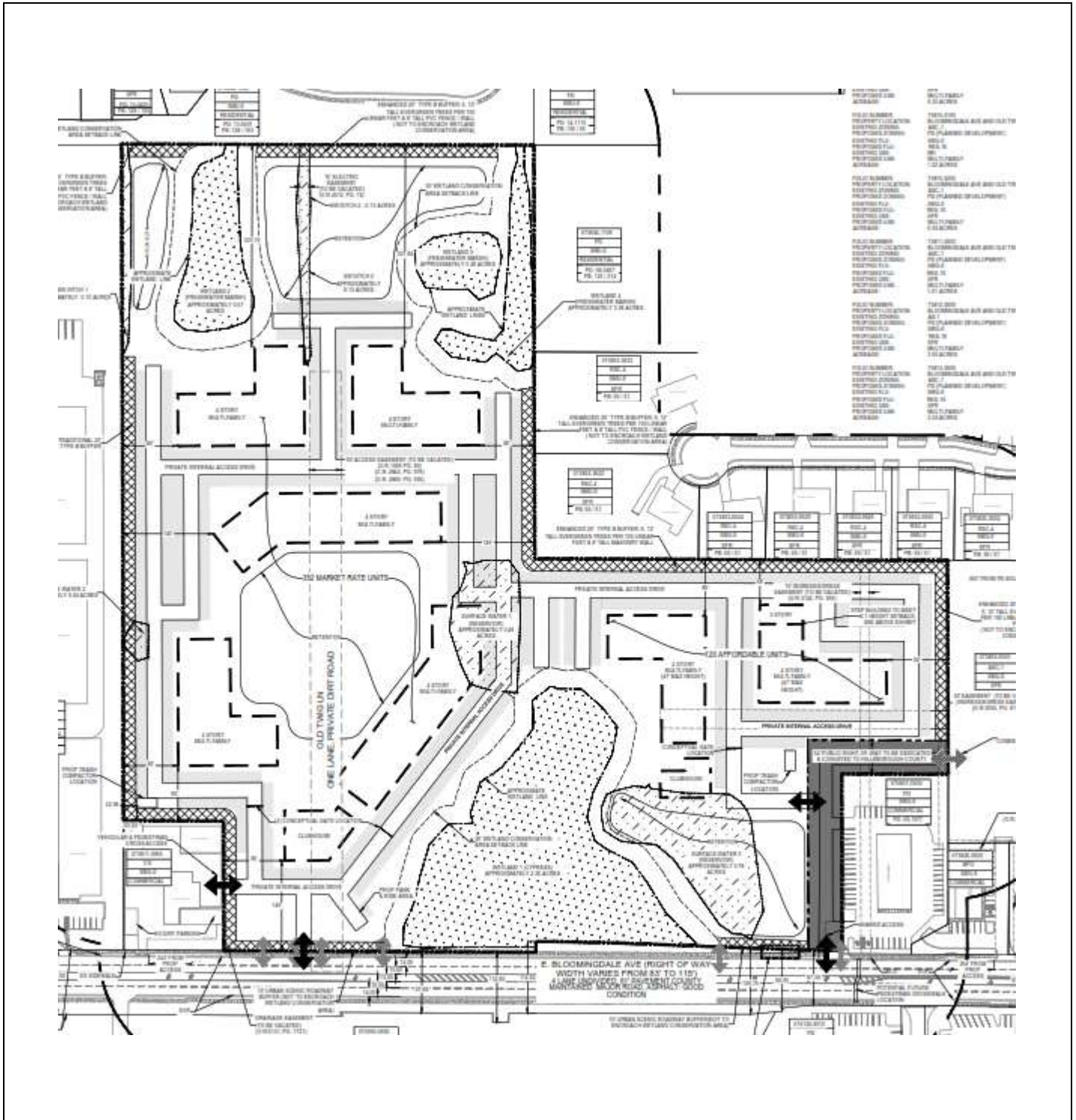


Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	PD 13-0491 (MM 19-1173)	4.23 dwelling unit (du)/acre	362 Single-family residential units	Single-family residential
South	PD 86-0103 (MM 00-0831)	0.25 FAR	Commercial, Office and Church	Church
East	PD, RSC-4, and ASC-1	1 - 4 du/acre	Limited commercial, office, minor vehicle repair, single-family residential	Single-Family and commercial
West	PD and CG-R	0.35 FAR 0.27 FAR	Mini-warehouse Contractor's office	Mini-warehouse and Contractor's office

## 2.0 LAND USE MAP SET AND SUMMARY DATA

### 2.4 Proposed Site Plan (partial provided below for size and orientation purposes. See Section 8.0 for full site plan)



**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9.0 OF STAFF REPORT)**

**Adjoining Roadways (check if applicable)**

Road Name	Classification	Current Conditions	Select Future Improvements
Bloomington Avenue	County Arterial - Urban	4 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other

**Project Trip Generation**

	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	2,071	190	188
Proposed	2,568	170	208
Difference (+/-)	(+)509	(-)20	(+)20

\*Trips reported are based on net new external trips unless otherwise noted.

**Connectivity and Cross Access**

Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	None	Vehicular & Pedestrian	Meets LDC
East		Vehicular & Pedestrian	Vehicular & Pedestrian	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC

Notes:

**Design Exception/Administrative Variance**

Road Name/Nature of Request	Type	Finding
Bloomington Avenue	Design Exception Requested	Approvable
	Choose an item.	Choose an item.

Notes:

**4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY**

<b>INFORMATION/REVIEWING AGENCY</b>				
<b>Environmental:</b>	<b>Comments Received</b>	<b>Objections</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	18% of site wetlands and surface waters
Natural Resources	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Check if Applicable: <input checked="" type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area <input type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input checked="" type="checkbox"/> Urban/Suburban/Rural Scenic Corridor <input type="checkbox"/> Adjacent to ELAPP property <input checked="" type="checkbox"/> Other <u>600' Eagle Nest Buffer (partial)</u>				
<b>Public Facilities:</b>	<b>Comments Received</b>	<b>Objections</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
<b>Transportation</b> <input checked="" type="checkbox"/> Design Exc./Adm. Variance Requested <input checked="" type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
<b>Service Area/ Water &amp; Wastewater</b> <input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	12" water main and 20" wastewater force main within Bloomingdale Ave ROW
<b>Hillsborough County School Board</b> Adequate <input checked="" type="checkbox"/> K-5 <input checked="" type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input checked="" type="checkbox"/> 9-12 <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Capacity exists for high school in adjacent concurrency service area to accommodate project
<b>Impact/Mobility Fees</b> (Fee estimate based on 1,200 sf, 2-bedroom, MF units 1-2 story) Mobility: \$4,278 (per unit)      Fire: \$249 (per unit) Parks: \$1,316 (per unit)      Multi-Family (1-2 story) per unit = \$9,734 School \$3,891 (per unit) Project may be eligible for Affordable Housing relief, or reduced Mobility rates. Not included in estimate				
<b>Comprehensive Plan:</b>	<b>Comments Received</b>	<b>Findings</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
<b>Planning Commission</b> <input type="checkbox"/> Meets Locational Criteria <input checked="" type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input checked="" type="checkbox"/> Minimum Density Met <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
<b>Other:</b>	<b>Comments Received</b>	<b>Objections</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
<b>Affordable Housing Services</b>	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	



## 5.0 IMPLEMENTATION RECOMMENDATIONS

### 5.1 Compatibility

Staff has not identified any external compatibility issues associated with this proposal. The proposed density and building heights have been mitigated for with building placement and enhanced screening along certain PD boundaries. The enhanced screening increases the fence height from 6 to 8 feet, increases the tree height from 10 to 12 feet and slightly decreases the tree spacing from 20 feet to 16.6 feet (see Figure 1, Section 7.0).

Properties to the north of the project are separated from the subject site by wetland and retention areas, resulting in multi-family buildings located over 300 feet from the common boundary line. In addition, the northern project, where adjacent to the subject site, has been developed with vegetated open space areas along their boundaries, internalizing the residential development. Enhanced screening along the northern PD boundary (exclusive of wetland areas) is proposed, which will provide tree plantings and an 8 foot high fence.

The large wetland area along the south of the site has regulated building placement along the western and eastern areas of the PD, thereby limiting the ability of the applicant to internalize the buildings. Properties to the west are developed with mini-warehouse and commercial uses. Type B screening along this boundary is proposed. Proposed building setbacks vary between 83-140 feet. Maximum building is proposed to be 55 feet. In a standard zoning district, a 2:1 additional setback would apply and would require a setback of 90 feet.

The eastern area is adjacent to existing single-family residential. Multi-family buildings will be located 63-85 feet from the common boundary line. As along other boundaries, enhanced screening, consisting of an 8 foot high fence and tree plantings, is proposed. Building heights will be 55-47 feet and limited to a maximum of 3 to 4 stories. The 2:1 setback for building heights over 20 feet (applicable in standard zoning districts) would result in a 90 foot setback. The proposed setback minimums are 90-85 feet. One building is proposed at a setback of 68 feet. For this building, the number of stories is proposed to be limited to 3 stories and to comply with the 2:1 setback.

### 5.2 Recommendation

The proposed PD Variation to increase the fence height from 6 to 8 feet is supported by staff. The applicant proposes this increase to ensure compatibility due to some multi-family buildings not meeting the 2:1 setback for the proposed maximum building height of 55 feet. The location of wetlands on the site has restricted the ability to more internally locate some of the buildings. Correspondingly, the proposed height is needed to accommodate the density needed to meet the minimum density requirements and provide affordable housing, which has been shown to be needed in the community.

Staff recommends approval, subject to conditions. The site is appropriately located for a multi-family housing project given the future land use and existing suburban development pattern. The site is also appropriately located to provide affordable housing given the criteria set forth in the the Land Development Code and Comprehensive Plan. The project design is proposing site planning techniques to ensure compatibility. Lastly, both the density bonus and use of affordable housing has been found to be supportable by both Planning Commission and Affordable Housing Services staffs.

## 6.0 PROPOSED CONDITIONS

**Approval** - Approval of the request, subject to the conditions listed below, is based on the general site plan submitted July 26, 2021.


1. The project is limited to a maximum of 472 multi-family units to consist of 120 affordable housing units and 352 market rate units.
2. Notwithstanding the development standards provided on the general site plan's project data table, buildings shall be located where generally depicted on the plan at the depicted setbacks.
3. Unless otherwise specified on the general site plan, building height shall be limited to a maximum of 55 feet / 4-stories. No additional setback of 2 feet for every 1 foot over 20 feet in height shall be provided, except where noted on the general site plan. The 3-story multi-family building located a minimum of 68 feet from the PD boundary is subject to the 2:1 additional setback. This additional setback shall be provided in addition to the minimum 68 foot setback where the structure is over 20 feet in height. Accessory garage structures, if provided, shall be limited to a maximum height of 20 feet.
4. Buffering and screening shall be provided where depicted on the general site plan. Screening shall not encroach into delineated wetlands. Screening within "enhanced" buffers shall include an 8 foot high PVC fence or wall and 6 12-foot tall (at time of planting) evergreen trees per every 100 linear feet. Screening within "traditional" buffers shall consist of Type B screening. This PD approves a PD Variation allowing an 8 foot high PVC-fence or wall as part of the Type B screening within buffers designated as "enhanced."
5. Unless otherwise stated, the project shall be developed in accordance with LDC Section 6.11.07 for the proposed 120 affordable housing units. This project will make available 120 affordable units, 25% of the total units are set aside for very low (50% MFI) and low-income households (80% MFI), with 10% of the 120 units being set aside for families emerging out of homelessness. The median family income will be adjusted for household size, consistent with the Department of Housing and Urban Development income guidelines.

This condition exceeds the 20% requirement imposed by the Comprehensive Plan. Affordability will be based on gross household income for the Tampa-St. Pete MSA. Any funding being provided by Affordable Housing Services will be encumbered on the property with a mortgage, note, and Land Use Restriction Agreement for no less than 30 years.

6. Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
7. The construction and location of any proposed wetland impacts are not approved by this correspondence, but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.
8. Prior to the issuance of any building or land alteration permits or other development, the approved wetland / other surface water (OSW) line must be incorporated into the site plan. The wetland/ OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County Land Development Code (LDC).

9. Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determinations of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.
10. If PD 21-0420 is approved, the County Engineer will approve a Design Exception (dated June 25, 2021 and revised June 29, 2021) which was found approvable by the County Engineer (on July 14, 2021). Approval of this Design Exception will allow pedestrian, lighting and safety improvements to the Bloomingdale Ave. in lieu of the standard TS-6 typical section of the Hillsborough County Transportation County Technical Manual required by Section 6.02.07. of the LDC. The specific improvements shall include:
  - a. Construction of a pedestrian crossing on Bloomingdale Avenue in the vicinity of the project including signalization to allow access to the bus stop on the south side of Bloomingdale Avenue, if warranted, after the construction of the affordable apartments and is permitted by Hillsborough County. The developer shall provide a financial guarantee for the construction for up to 3 years after the final CO for the apartments.
  - b. Widening the sidewalk on the northside of Bloomingdale Avenue from the terminus of the 6' sidewalk to be constructed by the apartments to the west to Providence Road. There may be limited deviations to the 6' sidewalk where not feasible due to right of way constraints as determined by County staff.
  - c. In coordination with TECO and Hillsborough County, the developer shall fund the construction of street lights on the north side of Bloomingdale Avenue from the terminus of the apartment property to the west to Providence Road.
  - d. In coordination with Hillsborough County School Board, the developer shall fund a crossing guard at the intersection of Bloomingdale Avenue and Providence Road for 5 years.
11. The developer shall construct and dedicate a public nonresidential local urban road consistent with the typical standard TS-3 of the Hillsborough County Transportation Technical Manual along a portion of the eastern boundary as depicted in the Planned Development Site Plan.
12. Until such time that the proposed local roadway is constructed and dedicated to the County, the developer shall continue to provide shared access between folio# 73807.5000 and the adjacent parcel, folio# 73807.0000, not included in this PD.
13. The developer shall dedicate sufficient right of way to provide the proposed transit pad and shelter.
14. The applicant shall contact FWC to ensure any suggested construction practices are adhered to during construction.
15. If the notes and/or graphics on the site plan are in conflict with specific zoning conditions and/or the LDC regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.

**Zoning Administrator Sign Off:**



J. Brian Grady  
Mon Aug 9 2021 09:25:04

**SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.**

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

### 7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS

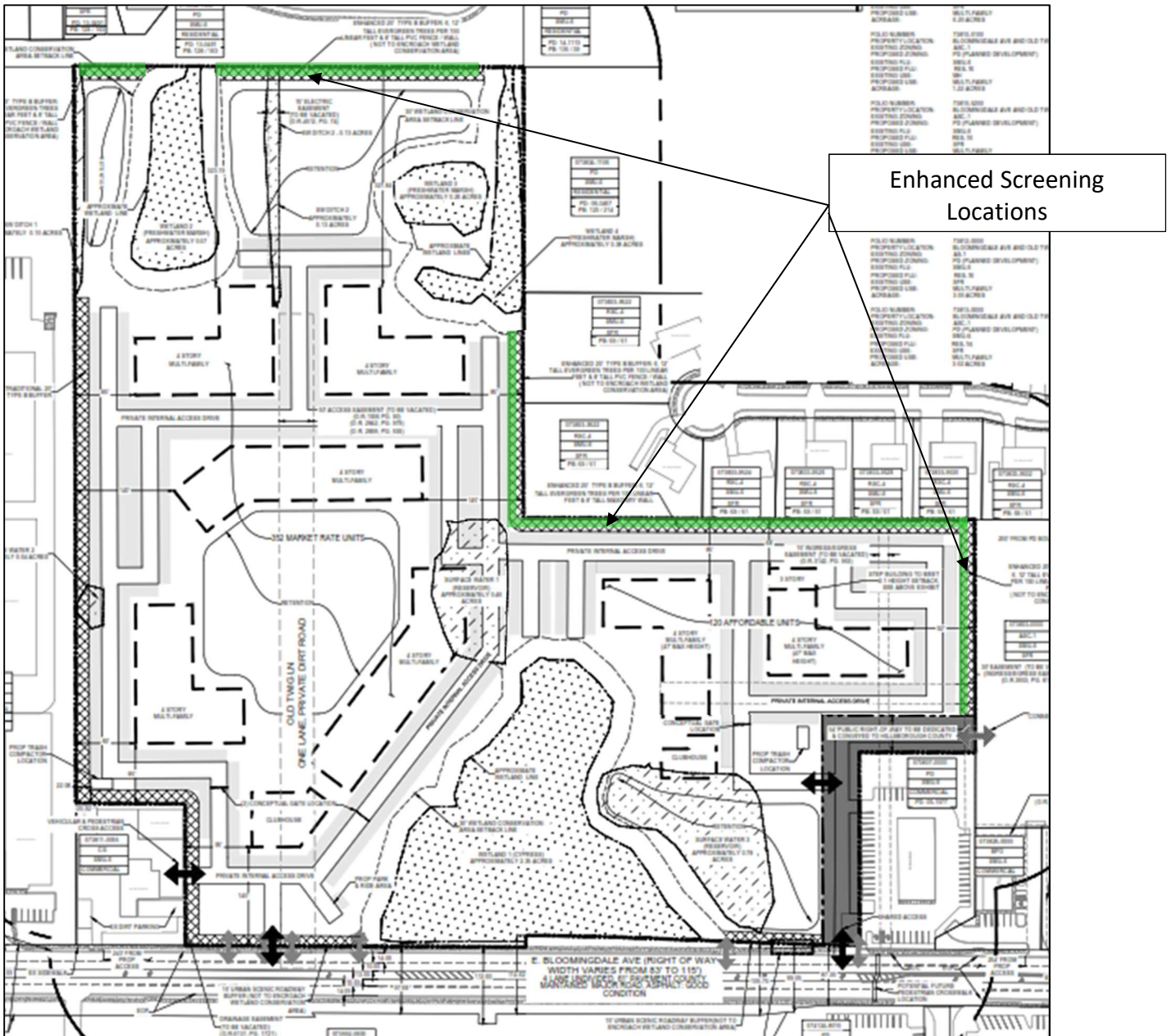


Figure 1: Enhanced Screening Locations

#### Affordable Housing Density Bonus:

The 27.05 acre site is currently within the SMU-6 FLU category, which would permit a maximum of 162 units. A comprehensive plan amendment to the RES-16 FLU category will increase the maximum to 432 units. The provision of affordable housing (in accordance with LDC regulations and Comprehensive Plan Policy 3.6.1 - Housing Element) allows a density increase to 20 units per acre, allowing a maximum of 541. The applicant proposes a maximum of 472 units, which exceeds the minimum 75% density requirement of the Comprehensive Plan.

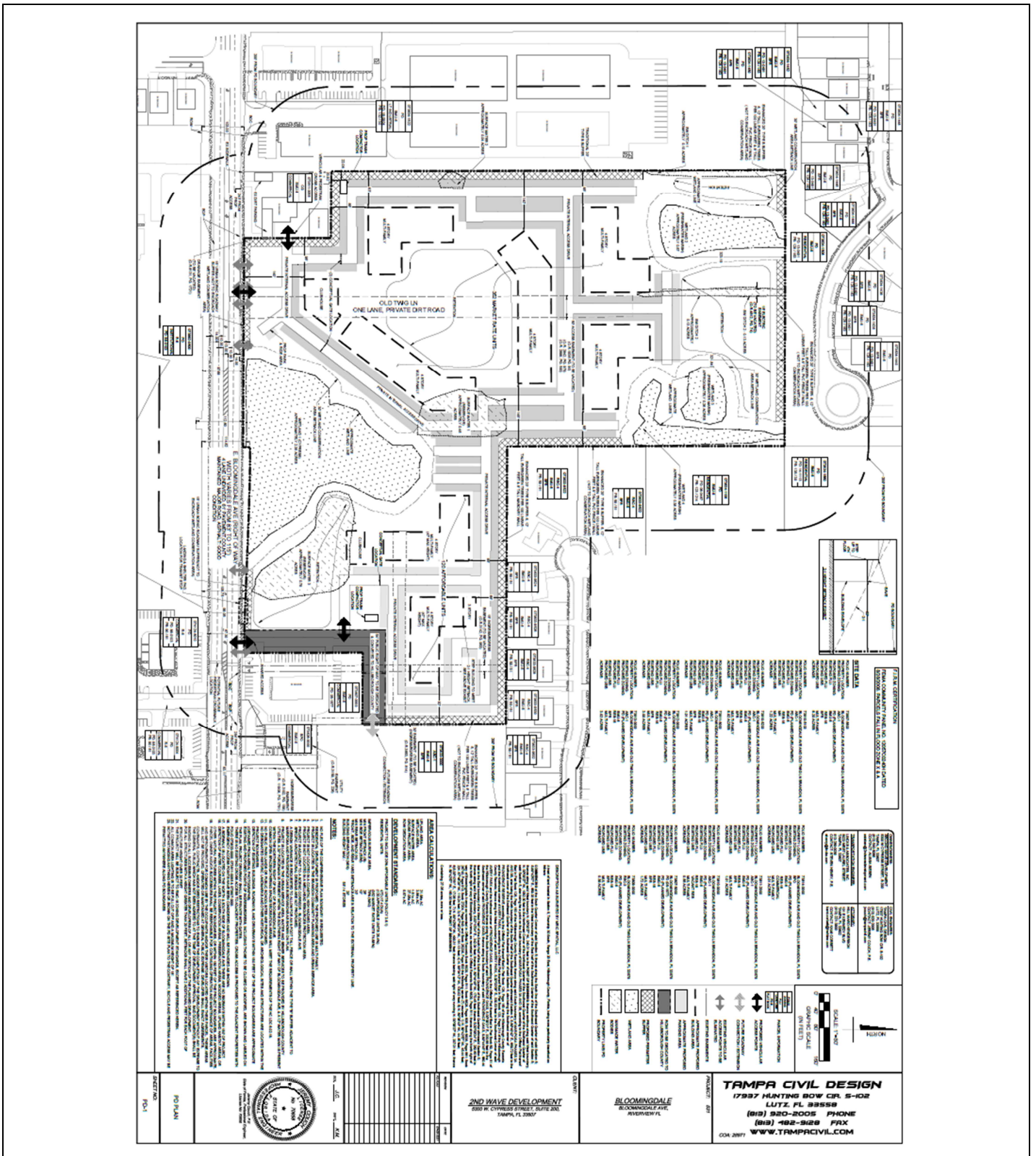
The project will provide 120 of the 472 units as affordable (25%). This exceeds the minimum 20% of the units, as required by the Land Development Code. These units will be designated for income levels specified in the Land Development Code.

Both Planning Commission and Affordable Housing Services staffs have reviewed the proposal and found that the project complies with:

- The surrounding area (3 miles around the site) is within the Urban Service Area, where infrastructure and public facilities are available.
- The project is setting aside at least 20% of the units as affordable housing units.
- Transit service is provided on Bloomindale Avenue at an LDC defined reasonable distance from the site.
- The site is located within an area consisting of 20% or more of the households within low, low and/or moderate income levels.
- Affordable housing projects are present within 1 mile of the site and there is a need for affordable housing within the area.
- The proposed location is within several employment centers, such as Progressive Insurance, Spectrum, USAA, Wal-Mart, Target and the Brandon Mall.
- Affordable Housing Services (AHS) further comments that the developer (Blue Sky) intends to submit for an award of Low-Income Housing Tax Credits and a request for funding to Hillsborough County AHS. The project will provide amenities such as clubhouses, open/green space, community gathering areas and attractive landscaping. Affordable housing units will feature full-sized refrigerators, full-sized ranges, dishwashers, walk-in closets, ceiling fans, ceramic tile floors in bathrooms and kitchens, LED lighting, keyless entry doors, marble window sills and steel entry door frames.

**8.0 PROPOSED SITE PLAN (FULL)**

Sheet 1 of 2



8.0 PROPOSED SITE PLAN (FULL)

Sheet 2 of 2





**9.0 FULL TRANSPORTATION REPORT (see following pages)**

**AGENCY REVIEW COMMENT SHEET**

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**TO:** Zoning Technician, Development Services Department

**REVIEWER:** Richard Perez, AICP

**PLANNING AREA:** BR / Central

**DATE:** 7/13/2021

*Revised: 08/3/2021*

**AGENCY/DEPT:** Transportation

**PETITION NO:** RZ- 21-0420

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- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to the listed or attached conditions.
- This agency objects for the reasons set forth below.

**REPORT SUMMARY AND CONCLUSIONS**

- The proposed rezoning would generally result in an increase of trips potentially generated by development of the subject site by 509 average daily trips and 20 trips in the p.m. peak hour; with the exception of a decrease by 20 trips in the a.m. peak hour.
- Bloomingdale Avenue is a substandard arterial roadway. The applicant requested a Design Exception from the Section 6.02.07. requirement to improve the substandard roadway to typical standards set forth in the Hillsborough County Transportation County Technical Manual, which was found approvable by the County Engineer. If the rezoning is approved, the County Engineer will approve the Design Exception, upon which the developer will make alternative improvements to Bloomingdale Ave.
- The applicant is proposing to construct and dedicate a local roadway to provided connectivity to the east of the project.
- The applicant is proposing new transit facilities to be incorporated in the project.
- Transportation Review Section staff has no objection to this request, subject to the conditions of approval provided herein.

**CONDITIONS OF ZONING APPROVAL**

1. If PD 21-0420 is approved, the County Engineer will approve a Design Exception (dated June 25, 2021 and revised June 29, 2021) which was found approvable by the County Engineer (on July 14, 2021). Approval of this Design Exception will allow pedestrian, lighting and safety improvements to the Bloomingdale Ave. in lieu of the standard TS-6 typical section of the Hillsborough County Transportation County Technical Manual required by Section 6.02.07. of the LDC. The specific improvements shall include:
  - a. Construction of a pedestrian crossing on Bloomingdale Avenue in the vicinity of the project including signalization to allow access to the bus stop on the south side of Bloomingdale Avenue, if warranted, after the construction of the affordable apartments and is permitted

by Hillsborough County. The developer shall provide a financial guarantee for the construction for up to 3 years after the final CO for the apartments.

- b. Widening the sidewalk on the northside of Bloomingdale Avenue from the terminus of the 6' sidewalk to be constructed by the apartments to the west to Providence Road. There may be limited deviations to the 6' sidewalk where not feasible due to right of way constraints as determined by County staff.
  - c. In coordination with TECO and Hillsborough County, the developer shall fund the construction of street lights on the north side of Bloomingdale Avenue from the terminus of the apartment property to the west to Providence Road.
  - d. In coordination with Hillsborough County School Board, the developer shall fund a crossing guard at the intersection of Bloomingdale Avenue and Providence Road for 5 years.
2. The developer shall construct and dedicate a public nonresidential local urban road consistent with the typical standard TS-3 of the Hillsborough County Transportation Technical Manual along a portion of the eastern boundary as depicted in the Planned Development Site Plan.
  3. Until such time that the proposed local roadway is constructed and dedicated to the County, the developer shall continue to provide shared access between folio# 73807.5000 and the adjacent parcel, folio# 73807.0000) not included in this PD.
  4. The developer shall dedicate sufficient right of way to provide the proposed transit pad and shelter.

**PROJECT SUMMARY AND ANALYSIS**

The applicant is requesting to rezone 11 parcels totaling +/- 27.08 acres from a mix of ASC-1, AS-1, BPO, RSC-3, and PD (05-1577) to a new Planned Development (PD) for multifamily housing. The site is located 600 feet west of the intersection of Providence Rd and Bloomingdale Avenue. The Future Land Use designation of the site is SMU-6.

***Trip Generation Analysis***

The applicant submitted a trip generation and site access analysis as required by the Development Review Procedures Manual (DRPM). Staff has prepared a comparison of the trips potentially generated under the existing and proposed zoning designations, utilizing a generalized worst-case scenario. Data presented below is based on the Institute of Transportation Engineer’s Trip Generation Manual, 10<sup>th</sup> Edition.

**Approved Zoning:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
ASC-1, 13 Units Single Family Detached (ITE 210)	<b>123</b>	<b>10</b>	<b>13</b>
AS-1, 2 Units Single Family Detached (ITE 210)	<b>19</b>	<b>1</b>	<b>2</b>
RSC-3/6, 41 Units Single Family Detached (ITE 210)	<b>387</b>	<b>30</b>	<b>41</b>
BPO, 21,780 SF Medical Clinic (ITE 630)	<b>831</b>	<b>80</b>	<b>71</b>
PD 05-1577, 18,621 SF Medical Clinic (ITE 630)	<b>711</b>	<b>69</b>	<b>61</b>
Total Trips	<b>2,071</b>	<b>190</b>	<b>188</b>

**Proposed Zoning:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
PD, 472 Units Multifamily – Mid-Rise (ITE 221)	<b>2,568</b>	<b>170</b>	<b>208</b>

**Trip Generation Difference:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
<b>Difference</b>	<b>(+) 509</b>	<b>(-) 20</b>	<b>(+) 20</b>

The proposed rezoning would generally result in an increase of trips potentially generated by development of the subject site by 509 average daily trips and 20 trips in the p.m. peak hour; however, the a.m. peak hour trips will decrease by 20 trips.

**TRANSPORTATION INFRASTRUCTURE SERVING THE SITE**

The site has frontage on Bloomingdale Avenue. Bloomingdale Ave. is a substandard 4-lane, undivided, County arterial roadway. As documented by the applicant the width of the existing travel lanes varies from 10 to 14 feet with 14-foot outside lanes and 10-foot inside lanes adjacent to the two-way left turn lane which is 13 feet wide. There are +/- 4-foot sidewalks on the north and south side and no bike lanes. Bloomingdale Avenue has curb and gutter on both sides. Along the project frontage, the roadway lies within a range of +/-85 to +/- 99 feet of right-of-way.

Pursuant to the Hillsborough County Transportation Technical Manual, an urban arterial roadway shall meet the typical section TS-6 standard. This includes 11-foot-wide lanes, a median separator, seven (7) foot buffered bike lanes and five (5) foot wide sidewalks within 110 feet of right of way.

The segment of Bloomingdale Avenue in front of the subject site is not included in the Hillsborough County Corridor Preservation Plan.

**SITE ACCESS**

The primary pedestrian and vehicular access will consist of two (2) access connections. One connection is planned directly on the Bloomingdale Avenue frontage. The second connection will be via a new public local road, to be constructed and dedicated to the County by the developer, to Bloomingdale Ave. Six (6) existing driveways will be removed, including the access connection to the existing private substandard road identified as Old Twig Lane.

As depicted in the PD Site Plan, the new local roadway connection, to be constructed by the developer, will provide additional connectivity from the project’s eastern access connection to Bloomingdale Ave. and to the east. The proposed new local road will stub out to allow for future local roadway connectivity to Providence Road as the adjacent property (Folio # 73833.0000) and the subsequent adjacent properties to the east of the subject property redevelop. The segment of the roadway connection will be constructed by the developer as a nonresidential local road meeting the TS-3, Local Urban Road, standard of the County Transportation Technical Manual and be dedicated to the County as a public right of way.

There is an existing shared access connection to Bloomingdale Ave. on the eastern boundary of the subject property where the future local roadway is proposed to be constructed. The shared access provide access

to folio# 73807.5000 (within this proposed PD) and folio# 73807.0000 (adjacent to this proposed PD). The shared access will remain in place until the said new local roadway connection is constructed and dedicated to the County.

A cross access facility is proposed to the adjacent parcel fronting Bloomingdale Avenue to the west consistent with LDC, Section 6.04.03. Q.

### **PROPOSED TRANSIT FACILITIES**

Additional access for Hillsborough Area Regional Transit (HART) users is proposed in the PD Site Plan. As identified in the PD Site Plan, the developer is proposing to construct a park and ride facility along the Bloomingdale Ave. frontage and a landing and shelter pad for a future transit stop. These facilities are not a requirement of the LDC, Sec. 6.03.09 and the developer has not provided documentation of HART's approval of the location of the facilities. The developer will have to coordinate with HART and the County to meet all applicable standards, including Section 9, Transit Friendly Planning and Transportation Technical Manual for Subdivisions and Site Development Projects and Americans with Disabilities Act Standards, for both facilities at the time of site/construction plan submittal. Furthermore, the developer shall dedicate sufficient right of way to accommodate the proposed landing and shelter pad, as the right of way is already constrained and does not meet minimum width for a County arterial roadway.

### **REQUESTED DESIGN EXCEPTION – BLOOMINGDALE AVE.**

As Bloomingdale Ave. is a substandard arterial roadway, the applicant's Engineer of Record (EOR) submitted a Design Exception request for Ehrlich Rd. (dated June 25, 2021 and revised June 29, 2021) to determine the specific improvements that would be required by the County Engineer. Based on factors presented in the Design Exception request, the County Engineer found the Design Exception request approvable (on July 14, 2021). To mitigate the deviations from the Hillsborough County Transportation Technical Manual (TTM) TS-6 Typical Section (for 4-Lane, divided, Urban Collector Roadways) the developer has proposed the following:

1. The developer will construct a pedestrian crossing on Bloomingdale Ave. in the vicinity of the project including signalization to allow access to the bus stop on the south side of Bloomingdale Ave., if warranted, after the construction of the affordable apartments and is permitted by Hillsborough County. The developer will provide a financial guarantee for the construction for up to 3 years after the final CO for the apartments.
2. The developer will widen the sidewalk on the northside of Bloomingdale Ave. from the terminus of the 6' sidewalk to be constructed by the apartments to the west to Providence Rd. There may be limited deviations to the 6' sidewalk where not feasible due to right of way constraints as determined by County staff.
3. The developer will work with TECO and Hillsborough County and fund the construction of street lights on the north side of Bloomingdale Ave. from the terminus of the apartment property to the west to Providence Rd.
4. The developer will work with Hillsborough County School Board and fund a crossing guard at the intersection of Bloomingdale Ave. and Providence Rd. for 5 years.

If this zoning is approved, the County Engineer will approve the Design Exception request.

**LEVEL OF SERVICE (LOS)**

Level of Service (LOS) information is reported below.

<b>FDOT Generalized Level of Service</b>				
<b>Roadway</b>	<b>From</b>	<b>To</b>	<b>LOS Standard</b>	<b>Peak Hr Directional LOS</b>
BLOOMINGDALE AVE	GORNTOLAKE RD	KINGS AVE	D	D

Source: [2020 Hillsborough County Level of Service \(LOS\) Report](#)



**LINCKS & ASSOCIATES, INC.**

July 2, 2021

Mr. Mike Williams  
Hillsborough County Government  
601 East Kennedy Blvd., 22nd Floor  
Tampa, FL 33602

Re: Bloomingdale/Providence  
PD 21-0420  
Lincks Project No. 20142

The purpose of this letter is to request a Design Exception to Section 6.02.07 of the Hillsborough County Land Development for Bloomingdale Avenue within the vicinity of the project. The developer proposes to rezone the subject property to Planned Development (PD) to allow up to 472 Multi-Family dwelling units.

The access for the project is to include two full accesses to Bloomingdale Avenue. The eastern most access is to be a shared access with the existing Hancock Tire and Auto Store.

According to the Hillsborough County Roadways Functional Classification Map, Bloomingdale Avenue is classified as an arterial roadway. The subject site is within the Hillsborough County Urban Service Area.

Table 1 provides the trip generation for the proposed development.

The following individuals met on June 11, 2021 to discuss proposed project and Design Exception for Bloomingdale Avenue.

- Mike Williams
- Sheida Tirado

The request is for a Design Exception to TS-6 of the Hillsborough County Transportation Technical Manual for Bloomingdale Avenue, which is currently a five-lane roadway. The following exceptions are requested to accommodate the proposed project.

- 1) Lane Width – TS-6 has 11' lanes. The existing roadway has 14' outside lanes and 10 feet inside lanes adjacent to the two-way left turn lane which is 13'.
- 2) Bike Lane – TS-6 has 7' buffered bike lanes. The existing road does not have bike lanes.

5023 West Laurel Street  
Tampa, FL 33607  
813 289 0039 Telephone  
8133 287 0674 Telefax  
[www.Lincks.com](http://www.Lincks.com) Website

**21-0420**

Mr. Mike Williams  
July 2, 2021  
Page 2

- 3) Sidewalk – TS-6 has 5' sidewalks on both sides of the roadway. There are currently 4' sidewalk on both sides of Bloomingdale Avenue.

To mitigate the impact of the project, the developer proposes the following:

1. The developer will construct a pedestrian crossing on Bloomingdale Avenue in the vicinity of the project including signalization to allow access to the bus stop on the south side of Bloomingdale Avenue, if warranted, after the construction of the affordable apartments and is permitted by Hillsborough County. The developer will provide a financial guarantee for the construction for up to 3 years after the final CO for the apartments.
2. The developer will widen the sidewalk on the northside of Bloomingdale Avenue from the terminus of the 6' sidewalk to be constructed by the apartments to the west to Providence Road. There may be limited deviations to the 6' sidewalk where not feasible due to right of way constraints as determined by County staff.
3. The developer will work with TECO and Hillsborough County and fund the construction of street lights on the north side of Bloomingdale Avenue from the terminus of the apartment property to the west to Providence Road.
4. The developer will work with Hillsborough County School Board and fund a crossing guard at the intersection of Bloomingdale Avenue and Providence Road for 5 years.

Based on the above, it is our opinion, the proposed improvements to Bloomingdale Avenue mitigate the impact of the project and meet the intent of the Transportation Technical Manual to the extent feasible.



Mr. Mike Williams  
July 2, 2021  
Page 3

Please do not hesitate to contact us if you have any questions or require any additional information.

Best Regards,

Steven J Henry  
President  
Lincks & Associates, Inc.  
P.E. #51555



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**Based on the information provided by the applicant, this request is:**

\_\_\_\_\_ **Disapproved**

\_\_\_\_\_ **Approved**

**If there are any further questions or you need clarification, please contact Benjamin Kniesley, P.E. at (813) 307-1758**

**Sincerely,**

**Michael J. Williams  
Hillsborough County Engineer**

Mr. Mike Williams  
July 2, 2021  
Page 4

TABLE 1  
ESTIMATED PROJECT TRIP GENERATION (1)

<u>Land Use</u>	<u>ITE</u> <u>LUC</u>	<u>Size</u>	<u>Daily</u> <u>Trip Ends</u>	<u>AM Peak Hour</u> <u>Trip Ends</u>		<u>PM Peak Hour</u> <u>Trip Ends</u>			
				<u>In</u>	<u>Out</u>	<u>Total</u>	<u>In</u>	<u>Out</u>	<u>Total</u>
Multi-Family	221	472 DU's	2,571	41	116	157	120	77	197

(1) Source: ITE Trip Generation Manual, 10th Edition, 2017.

APPENDIX



PD PLAN







HILLSBOROUGH COUNTY  
ROADWAYS FUNCTIONAL CLASSIFICATION



**HILLSBOROUGH COUNTY  
ROADWAYS  
FUNCTIONAL CLASSIFICATION**  
*Infrastructure & Development Services*



**Legend**

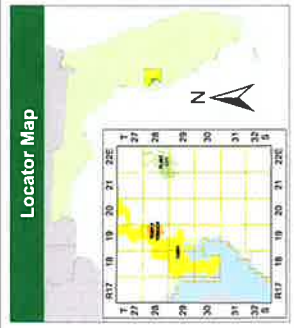
Functional Classifications  
Authority, Classification

- State, Principal Arterial
- State, Arterial
- Hillsborough, Arterial
- Hillsborough, Collector
- Urban Service Area Boundary
- City Limits

The Hillsborough County Roadway Functional Map will be used in all matters in the Hillsborough County Land Development Code (LDC) that relate to functional classification of roads. Some, but not all, examples of those matters are as follows:

PART 3.02.00 INTERSTATE-75 PLANNED DEVELOPMENT DISTRICTS  
PART 3.03.00 CITRUS PARK VILLAGE DEVELOPMENT STANDARDS  
PART 3.04.00 CITRUS PARK VILLAGE DEVELOPMENT STANDARDS  
PART 6.02.00 SUBDIVISION STANDARDS AND GUIDELINES  
PART 6.07.00 FENCES AND WALLS AND SIGNAGE STANDARDS  
PART 12.01.00 DEFINITIONS FOR SPECIAL USES  
OTHER PARTS OF THE LDC NOT LISTED ABOVE.

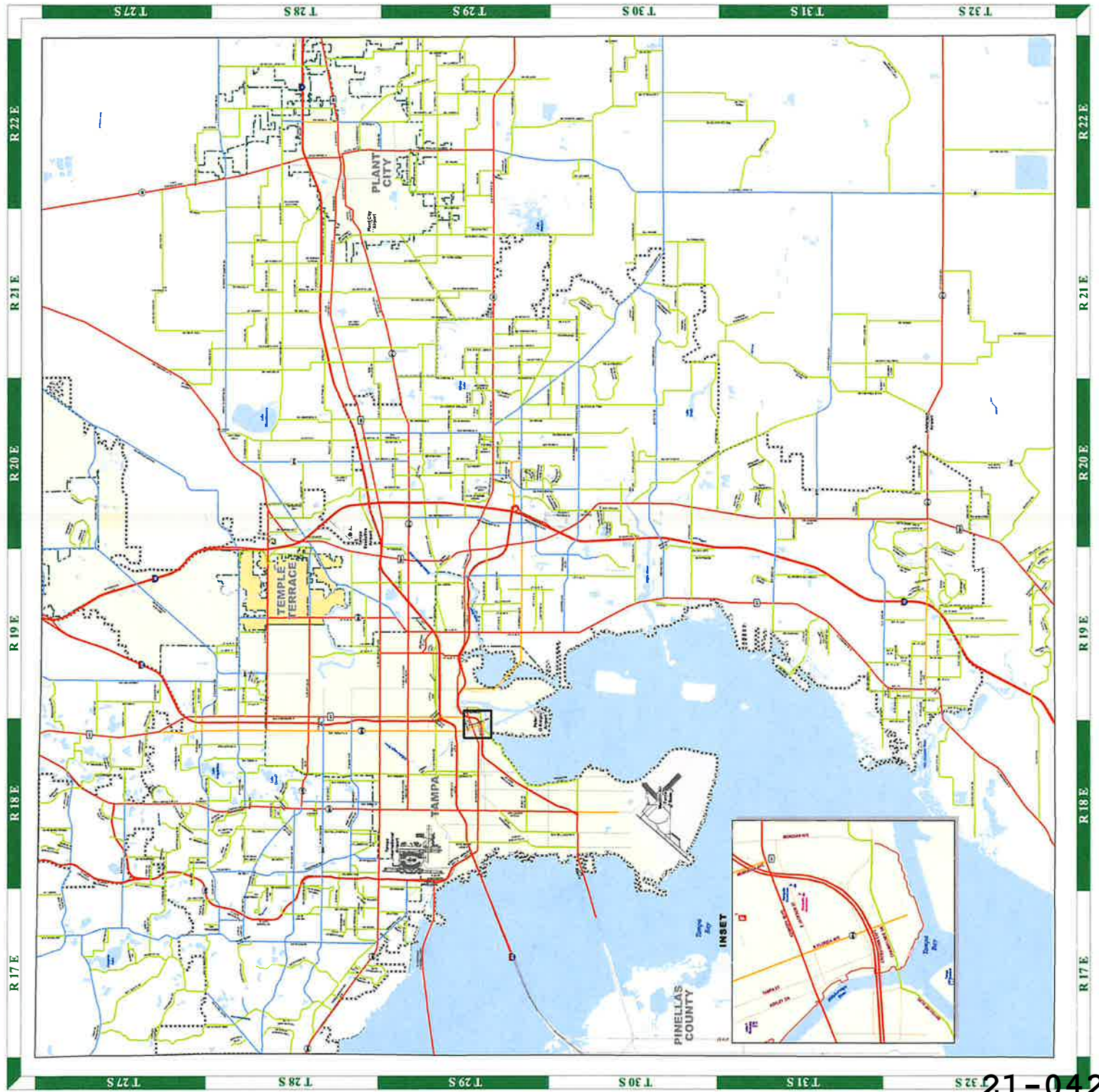
In addition to subject matters in the LDC, Functional Classification of roads plays an important role in administering the Residential Traffic Control Program and the Neighborhood Traffic Calming Program.



NOTE: Every reasonable effort has been made to ensure the accuracy of this map. Hillsborough County does not warrant the accuracy of the map. Hillsborough County does not assume any liability for errors or omissions, including but not limited to, the impact of map data, map accuracy, or any other errors or omissions. This map is provided for informational purposes only and should not be used for any other purpose.

SOURCE: This map has been prepared for the inventory of road property located within Hillsborough County and is based on the most current data available. The accuracy of the map is not guaranteed. The user should verify the information contained in this map.

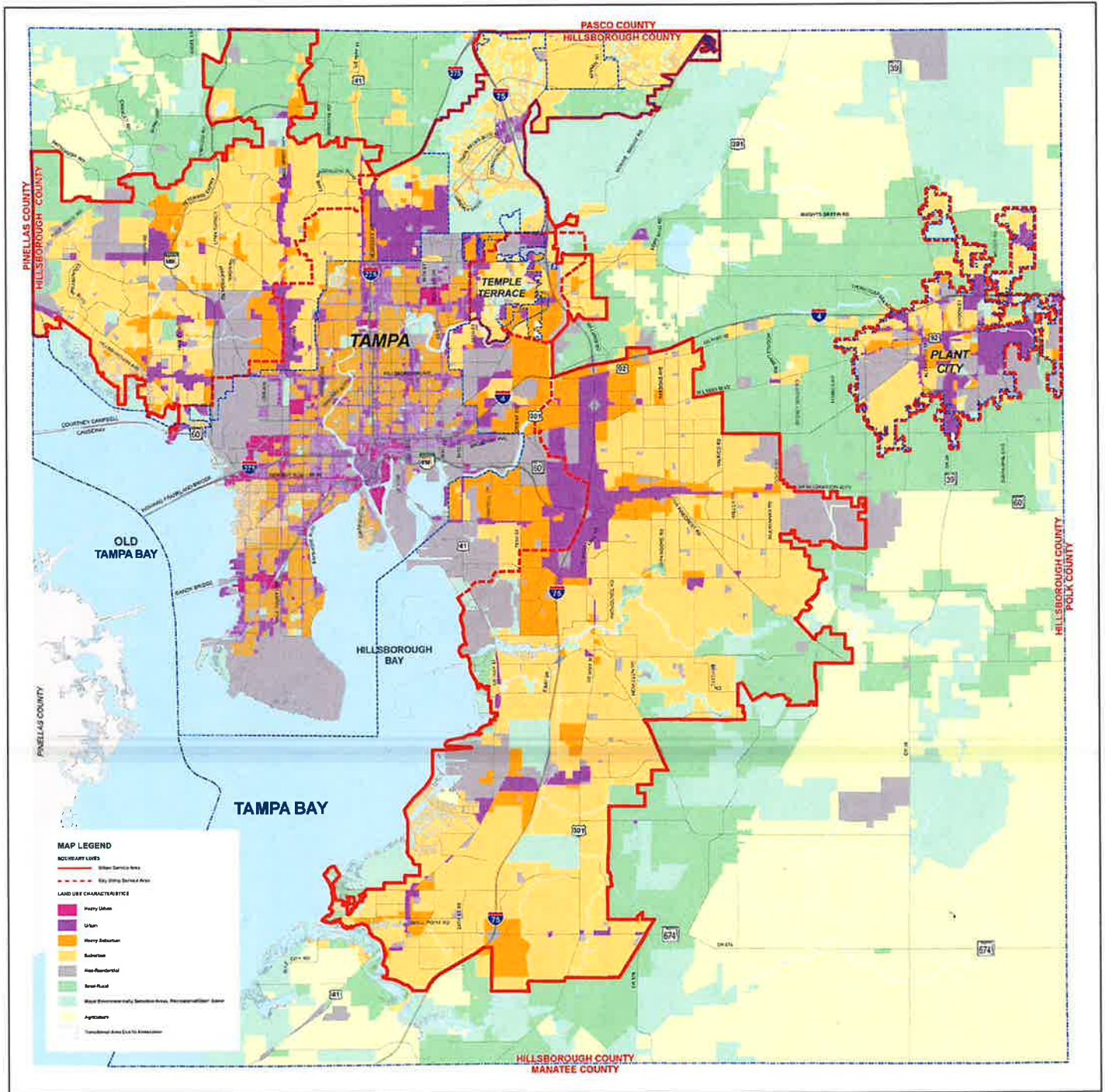
684 E. Kennedy Blvd  
Tampa, FL 33602  
(813) 272-5810  
printroom@hillsboroughcounty.org





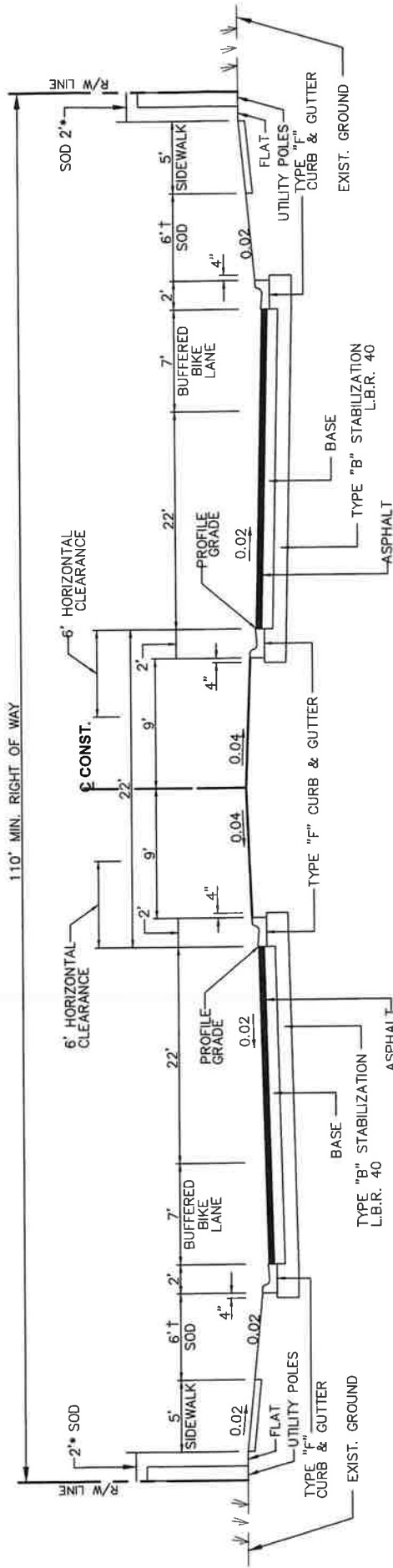
HILLSBOROUGH COUNTY  
URBAN SERVICE AREA





TS-6





**TYPICAL SECTION**  
N.T.S.

10,000 TO LESS THAN 20,000 AADT

MAX. ALLOWABLE DESIGN SPEED - 45 MPH

1. ALL DIMENSIONS SHOWN ARE MINIMUM.
2. SEE APPROPRIATE SECTIONS OF TECHNICAL MANUAL FOR DESIGN PARAMETERS.
- \* 3. PROVIDE 2' MINIMUM CLEARANCE FROM FENCES, WALLS, HEDGES, ABOVEGROUND UTILITIES OR IMPROVEMENTS, DROP OFFS, OR FROM THE TOPS OF BANKS WITH SLOPES STEEPER THAN 1 TO 4, THAT INTERFERE WITH THE SAFE, FUNCTIONAL USE OF THE SIDEWALK. INTERMITTENT ABOVEGROUND UTILITIES, OR MATURE TREES, 2' OR LESS IN DIAMETER MAY BE PLACED IN THIS 2' STRIP AS FAR FROM THE SIDEWALK AS POSSIBLE, IF NOT IN THE CLEAR ZONE.
- † 4. SEE SIDEWALK PROTECTION OPTIONS, DRAWING NO. TD-16 SHEET 7 OF 7 FOR USE WHEN TREES ARE PLANTED IN THE PARKWAY AREA (BETWEEN THE BACK OF CURB AND SIDEWALK).
5. SOD SHALL BE PLACED IN TWO ROWS STAGGERED. (BOTH TEMPORARY AND PERMANENT)

DRAWING NO. <b>TS-6</b>	<b>URBAN COLLECTORS (4 LANE DIVIDED) TYPICAL SECTION</b>	<b>Hillsborough County Florida</b>		REVISION DATE: 10/17
SHEET NO. 1 OF 1				

Transportation Comment Sheet

**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 8 OF STAFF REPORT)**

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Bloomington Avenue	County Arterial - Urban	4 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other

Project Trip Generation			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	2,071	190	188
Proposed	2,568	170	208
Difference (+/-)	(+)509	(-)20	(+)20

\*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	Vehicular & Pedestrian	Vehicular & Pedestrian	Meets LDC
East		Vehicular & Pedestrian	Vehicular & Pedestrian	Meets LDC
West		None	Vehicular & Pedestrian	Meets LDC
Notes:				

Design Exception/Administrative Variance		
Road Name/Nature of Request	Type	Finding
Bloomington Avenue	Design Exception Requested	Approvable
Notes:		

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input checked="" type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See Transportation Report

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**Hillsborough County  
City-County  
Planning Commission**

<b>Unincorporated Hillsborough County Rezoning</b>	
<b>Hearing Date:</b> August 16, 2021	<b>Petition: PD 21-0420</b>
<b>Report Prepared:</b> August 4, 2021	<i>West of Providence Road, Northside of Bloomingdale Avenue</i>
<b>Summary Data:</b>	
<b>Comprehensive Plan Finding:</b>	<b>CONSISTENT</b>
<b>Adopted Future Land Use:</b>	<b>Suburban Mixed Use-6 (6 du/ga; 0.35 FAR) * Concurrent plan amendment to Residential-16 (CPA 20-17)</b>
<b>Service Area</b>	<b>Urban</b>
<b>Community Plan:</b>	<b>Brandon</b>
<b>Requested Rezoning:</b>	Planned Development (PD) to Planned Development (PD) to allow for 427 multi-family units with an Affordable Housing density bonus
<b>Parcel Size (Approx.):</b>	27.08 +/- acres
<b>Street Functional Classification:</b>	Bloomingdale Avenue – <b>Arterial</b>
<b>Locational Criteria</b>	N/A
<b>Evacuation Zone</b>	The subject property is not located within an Evacuation Zone.



## **Context**

- The subject property is located on approximately 27.08 acres located on the north side of Bloomingdale Avenue and west of Providence Road. The site is located within the limits of the Brandon Community Plan and is in the Urban Service Area.
- The property's Future Land Use designation is Suburban Mixed Use (SMU-6), however the site is undergoing a concurrent plan amendment (CPA 20-17) to change the future land use designation to Residential-16. CPA 20-17 was approved by the Planning Commission on April 12, 2021 as well as approved to be transmitted to the state by the Board of County Commission on April 22, 2021. Residential-16 includes typical uses of residential, urban scale neighborhood commercial, office uses, multi-purpose projects and mixed- use development. Non-residential uses shall meet established locational criteria for specific land use.
- The subject property is surrounded immediately by the Suburban Mixed Use-6, to the north, east and west. Further west there is additional Residential-16. South of the site is designated as Residential-6.
- Surrounding uses include mini warehouse use to the west. Single family residential to the north and east and a church is located on the south side of Bloomingdale Avenue.
- The applicant requests a Planned Development to allow for 472 multi-family units. This application seeks to utilize affordable housing density bonus incentives, 120 of the units will be affordable housing units.

### **Compliance with Comprehensive Plan:**

The following Goals, Objectives and Policies apply to this rezoning request and are used as a basis for a consistency finding.

### ***Future Land Use Element***

#### **Urban Service Area (USA)**

***Objective 1:*** Hillsborough County shall pro-actively direct new growth into the urban service area with the goal that at least 80% of all population growth will occur within the USA during the planning horizon of this Plan. Within the Urban Service Area, Hillsborough County will not impede agriculture. Building permit activity and other similar measures will be used to evaluate this objective.

***Policy 1.4:*** Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and



architecture. Compatibility does not mean “the same as.” Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.

**Policy 1.7:** *The County will create incentives to make development within the USA desirable and cost affordable. Such incentives may include but are not limited to expedited review processes, retrofitting existing development, increased density bonuses, tax incentives, impact fee structuring and pre-zoning of vacant, underutilized lands to achieve planned densities.*

## **Neighborhood/Community Development**

**Objective 16: Neighborhood Protection** *The neighborhood is the functional unit of community development. There is a need to protect existing, neighborhoods and communities and those that will emerge in the future. To preserve, protect, and enhance neighborhoods and communities, all new development must conform to the following policies.*

**Policy 16.2:** *Gradual transitions of intensities between different land uses shall be provided for as new development is proposed and approved, through the use of professional site planning, buffering and screening techniques and control of specific land uses.*

**Policy 16.3:** *Development and redevelopment shall be integrated with the adjacent land uses through:*

- a) *the creation of like uses; or*
- b) *creation of complementary uses; or*
- c) *mitigation of adverse impacts; and*
- d) *transportation/pedestrian connections*

## **Residential-Targeted Groups & Incentives**

**Objective 20:** *The County shall encourage new development and redevelopment of residential housing for special target groups of people. The provisions specified within the Housing Element of the Comprehensive Plan shall be applied with respect to the following policies.*

**Policy 20.1:** *The provision of affordable housing shall be given high priority consideration by Hillsborough County. By 2009, development incentives shall be explored and implemented by Hillsborough County that will increase the housing opportunities for very low, low income households and workforce housing that are consistent with and further the goals, objectives and policies within the Housing Element.*

**Policy 20.2:** *Density bonuses will be utilized as an incentive to encourage the development of more affordable housing. These density bonuses are outlined in the Housing Element.*

## **Housing Element**

**Objective 3.6: Density/Intensity Bonuses for Affordable Housing**

**Policy 3.6.1:** *Density bonuses will be utilized as an incentive to encourage the development of more affordable housing these density bonuses are outlined below.*

*The maximum level of residential density and/or the maximum retail commercial Floor Area Ratio (FAR) permitted in each land use category may be increased, with project*

*specific approval of the Board Of County Commissioners and without requiring a plan amendment, when the purpose for the increase is to provide low or very low income affordable housing. Such an increase in density and/or Floor Area Ratio shall be part of an official request to rezone the subject parcel to a site plan controlled zoning district.*

### **Project Design Plan Density Bonus**

*Project Design Plan Density Bonus shall be a component of an approved, comprehensive “project design plan” which specifies the visual appearance, size, location, development costs, and sales or rental price of the residential units proposed to be built in the project. In addition, such a “project design plan” will address how the project meets the criteria listed below, provide a financial justification for the requested increase in density and/or Floor Area Ratio, and provide a listing and description of any project amenities or other planned improvements to the parcel that will enhance the project and its surrounding area.*

*In order to qualify for use of the Project Design Plan Density Bonus, the site shall adhere to the following criteria:*

- A. The area shall be in one of the residential or commercial land use categories allowing 4 or more dwelling units to the acre.*
- B. The surrounding area must be fully or partially developed and contain in-place infrastructure and public facilities which will meet the public facilities and service needs of existing and proposed residential development.*
- C. The surrounding area must contain two or more of the following conditions warranting the repair or rehabilitation of existing housing, and/or development of additional affordable housing units:*
  - 1) Evidence of existing substandard housing stock in need of rehabilitation, and/or demolition and replacement of units. Such conditions should be sufficient to represent a detriment to the maintenance and stable value of adjacent properties.*
  - 2) Evidence that existing resident households of very low, low, and/or moderate income comprise a reasonable percentage of the total existing neighborhood population, along with evidence of need, on the part of some of these households, for affordable housing assistance.*
  - 3) Association of the area with private and public initiatives to create permanent affordable housing in communities to serve the needs of the families of farm and other seasonal workers.*
  - 4) The subject area is close to a significant existing or proposed economic development project which will provide employment opportunities for proposed project residents.*
- D. Mass transit services must be available within a reasonable distance of the project site.*

**The following conditions shall apply to qualify for all of the affordable housing density and/or FAR bonuses.**

- 1. In projects using the Project Plan or Affordable Housing Infill Density Bonus, at least 20% of the total dwelling units (rounded up) must be reserved for low or very low income households. For the FAR bonus, all reserved units must be for very low income households.*

2. *The reserved units must be of compatible quality and appearance to the other units in the development. In large developments, every effort shall be made to mix the designated affordable housing units among the remaining units to avoid economic segregation.*
3. *When supplying rental housing, the applicant must, through a restrictive covenant, deed restriction, bylaws of the subdivision association, mortgage or property deed clause, or other method acceptable to the County, satisfactorily guarantee that designated affordable rental housing units, permitted under this density bonus, remain reserved for eligible households for a period of at least 12 years, or for some other time period as determined by the County.*

*All reserved dwelling units which are intended for home ownership shall be sold only to qualified "low or very low" income buyers. No further restrictions shall apply to the future sale of such homes unless required by the provisions of associated subsidized financing programs.*

*Proof of such guarantees must be submitted and/or recorded for all reserved units (both rental and home ownership) prior to the permitting of any additional units or increases in FAR in excess of that permitted by the underlying land use category.*

4. *Definitions of qualifying households (i.e. very low, low, and moderate income households) shall be determined by the U.S. Department of Housing and Urban Development and shall be adjusted annually or as appropriate.*
5. *Notwithstanding the requirements of the Affordable Housing Infill Density Bonus above, all projects utilizing the affordable housing bonus must be programmed to be served by both public water and public sewer prior to the issuance of a Certificate of Occupancy on the project.*
6. *Any density and/or FAR increase shall be compatible with existing, proposed, or planned surrounding development. Compatible densities and/or FAR intensities need not be interpreted as "comparable" or "the same as" surrounding developments, if adequate provisions for transitioning to higher densities is required and met by, but not limited to, such means as buffering, setbacks, coordinated architectural devices, and graduated height restrictions.*
7. *Nothing in this section shall be construed as guaranteeing the achievement of the density increase or FAR increase, or any portion thereof, as provided for in this section.*

## **Livable Communities Element; Brandon Community Plan**

**Goal 6:** *Re-establish Brandon's historical, hospitable, and family oriented character through thoughtful planning and forward thinking development practices by concentrating density in certain areas to preserve the semi-rural lifestyle of other areas. Attempt to buffer and transition uses in concentric circles where possible with most intense uses in an area at a node (intersection) and proceeding out from there. Create a plan for how areas could be developed and redeveloped for the future. Each of these areas would have potential for different building heights, parking configurations, fencing, buffering, landscape requirements, special use limitations, and design standards. These standards apply to new construction on infill property, redevelopment of undesirable areas and renovation of existing buildings. The primary consideration of all changes should be compatibility with existing structures to ensure neighborhood preservation.*

5. *General design characteristics for each Brandon Character District are described below. The design characteristics are descriptive as to the general nature of the vicinity and its surroundings and do not affect the Future Land Use or zoning of properties in effect at the time of adoption of the Brandon Community Plan. Any proposed changes to the zoning of property may proceed in accordance with the Land Development Code.*

**b. Urban General, including Brandon Main Street** - *Mixed use building types immediately adjacent to the Urban Center District designed to accommodate retail, offices and dwellings including row houses, town houses and multi-family housing. This district will contain a tight network of streets and blocks with wide sidewalks, consistent street tree planting and buildings 2-5 stories set close to the building setback line. Property within the Brandon Main Street (BMS) zoning districts shall be governed by the Brandon Main Street Development Regulations as set forth in the Land Development Code.*

**Staff Analysis of Goals Objectives and Policies:**

The site is located within the Suburban Mixed Use-6 Future Land Use category, however there is a concurrent plan amendment for a future land use designation change to Residential-16. The applicant is requesting an Affordable Housing Density Bonus, specifically under the Project Design Plan Density Bonus, where at least 20% of the total dwelling units are reserved for low or very low-income households.

In the RES-16 plan category, the Affordable Housing Density Bonus allows the applicant to achieve a maximum density of 20 dwelling units per acre, which is the next highest Future Land Use category. Planning Commission staff has worked with Development Services staff and Affordable Housing Services staff to determine that the proposed project meets all the criteria outlined in the Housing Element in order to receive the Affordable Housing Density Bonus. In a memo dated July 16, 2021, Cheryl Howell, the Director of Affordable Housing Services, outlined the criteria from the Land Development Code and the Comprehensive Plan that must be met in order to receive the Affordable Housing Density Bonus. In collaboration with all three agencies, it was determined that the requirements within the Land Development Code and the Comprehensive Plan to receive the Affordable Housing density bonus were met.

Policy 16.3 requires new development to be compatible with surrounding uses and promote interconnectivity via pedestrian and vehicular accesses. Cross accesses are being provided to the southeast. The proposed rezoning satisfies the intent of this policy.

The proposed rezoning furthers the goals of the Brandon Community Plan by proposing multi-family within the Urban General District as well as satisfies the intent of Objective 1, which directs 80% of the growth to occur within the Urban Service Area.

Housing Element Comprehensive Plan Policy 3.6.1 states that density bonuses will be utilized as an incentive to encourage the development of more affordable housing. The proposed project is consistent with this policy, as it meets all the requirements outlined in the Comprehensive Plan Housing Element as well as the Land Development Code requirements for the Affordable Housing Density Bonus.

Overall, Planning Commission staff finds the proposed use and density are compatible with the area while meeting the affordable housing density bonus requirements set forth

by the Housing Element. The proposed development also meets the intent of the Brandon Community Plan, specifically the site is located within the Urban General District, which supports multi-family residential uses. The proposed Planned Development would allow for a development that is consistent with the Goals, Objectives and Policies of the Future Land Use Element of the Future of Unincorporated Hillsborough County Comprehensive Plan for Unincorporated Hillsborough County.

**Recommendation**

Based upon the above considerations, Planning Commission staff finds the proposed rezoning **CONSISTENT** with the *Future of Hillsborough Comprehensive Plan for Unincorporated Hillsborough County*, subject to conditions proposed by the Development Services Department.

# HILLSBOROUGH COUNTY FUTURE LAND USE RZ PD 21-0420

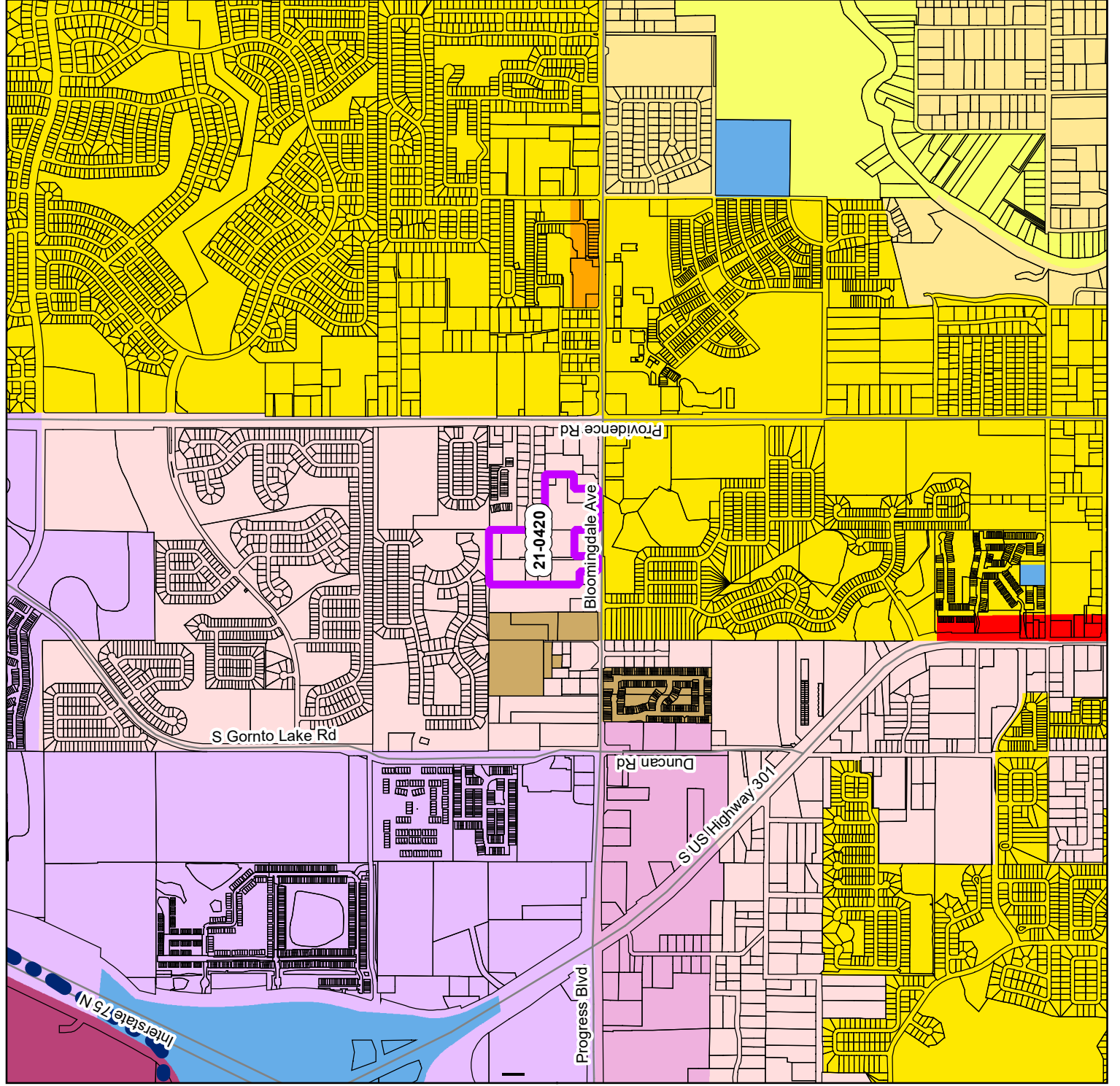
Rezonings  
STATUS

- APPROVED
- CONTINUED
- DENIED
- WITHDRAWN
- PENDING

- Tampa Service
- Urban Service
- Shoreline
- Jurisdiction Boundary
- Major Roads
- Parcels

- AGRICULTURAL/MINING-120 (.25 FAR)
- PEC PLANNED ENVIRONMENTAL COMMUNITY-1/2 (.25 FAR)
- AGRICULTURAL-1/10 (.25 FAR)
- AGRICULTURAL/RURAL-1/5 (.25 FAR)
- AGRICULTURAL/ESTATE-1/2.5 (.25 FAR)
- RESIDENTIAL-1 (.25 FAR)
- RESIDENTIAL-2 (.25 FAR)
- RESIDENTIAL PLANNED-2 (.35 FAR)
- RESIDENTIAL-4 (.25 FAR)
- RESIDENTIAL-6 (.25 FAR)
- RESIDENTIAL-9 (.35 FAR)
- RESIDENTIAL-12 (.35 FAR)
- RESIDENTIAL-16 (.35 FAR)
- RESIDENTIAL-20 (.35 FAR)
- RESIDENTIAL-35 (1.0 FAR)
- NEIGHBORHOOD MIXED USE-4 (3) (.35 FAR)
- SUBURBAN MIXED USE-6 (.35 FAR)
- COMMUNITY MIXED USE-12 (.50 FAR)
- URBAN MIXED USE-20 (1.0 FAR)
- REGIONAL MIXED USE-35 (2.0 FAR)
- OC-20
- RESEARCH CORPORATE PARK (1.0 FAR)
- ENERGY INDUSTRIAL PARK (.50 FAR USES OTHER THAN RETAIL, 25 FAR RETAIL/COMMERCE)
- LIGHT INDUSTRIAL PLANNED (.50 FAR)
- LIGHT INDUSTRIAL (.50 FAR)
- HEAVY INDUSTRIAL (.50 FAR)
- PUBLIC/QUASH-PUBLIC
- NATURAL PRESERVATION
- WIMAUMA VILLAGE RESIDENTIAL-2 (.25 FAR)
- CITRUS PARK VILLAGE

DATA SOURCES: Rezonings boundaries from The Planning Commission and are not official. Parcel lines and data from Hillsborough County Property Appraiser. The boundaries and data for this map are for informational purposes only and are not intended to be used for any legal or financial purposes. The Planning Commission and the City-County Planning Commission are not responsible for any errors or omissions in this map. The City-County Planning Commission is not responsible for any errors or omissions in this map. The City-County Planning Commission is not responsible for any errors or omissions in this map. The City-County Planning Commission is not responsible for any errors or omissions in this map.



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