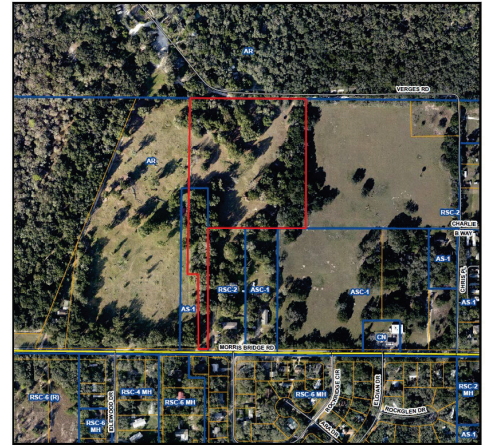


**Rezoning Application:** PD 23-0042  
**Zoning Hearing Master Date:** April 17, 2023  
**BOCC Land Use Meeting Date:** June 13, 2023

**1.0 APPLICATION SUMMARY**

**Applicant:** First Tampa Development Corporation  
**FLU Category:** Residential - 6  
**Service Area:** Rural  
**Site Acreage:** Approximately 10.67 acres  
**Community Plan Area:** None  
**Overlay:** None



**Introduction Summary:**

The applicant seeks to develop an approximately 10.67-acre unified development consisting of one folio. The request is for a rezoning from Agricultural Single-family-1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) to allow for a 10-dwelling-unit, single-family development.

Zoning:	Existing		Proposed
District(s)	AR (Agricultural Rural)	AS-1 (Agricultural, Single-family)	PD
Typical General Use(s)	Single-Family Residential/Agricultural	Single-Family Residential/Agricultural	Single-family
Acreage	Approximately 9.27 acres	Approximately 1.4 acres	10.67 ac.
Density/Intensity	1 dwelling per 5 acres	1 dwelling per acre	0.94 dwelling per acre

Development Standards:	Existing		Proposed
District(s)	AR	AS-1	PD Dimensional Standards
Setbacks/Buffering and Screening	Front: 50 ft. Side: 25 ft. Rear: 50 ft. Lot Width: 150 ft.	Front: 50 ft. Side: 15 ft. Rear: 50 ft. Lot Width: 150 ft.	Front: 25 feet Side: 7.5 ft. / 12.5 ft. corner lot 2 <sup>nd</sup> side Rear: 25 ft. Lot Width: 80 ft.
Height	50 ft. Max. Ht.	50 ft. Max. Ht.	50 ft. Max. Ht.

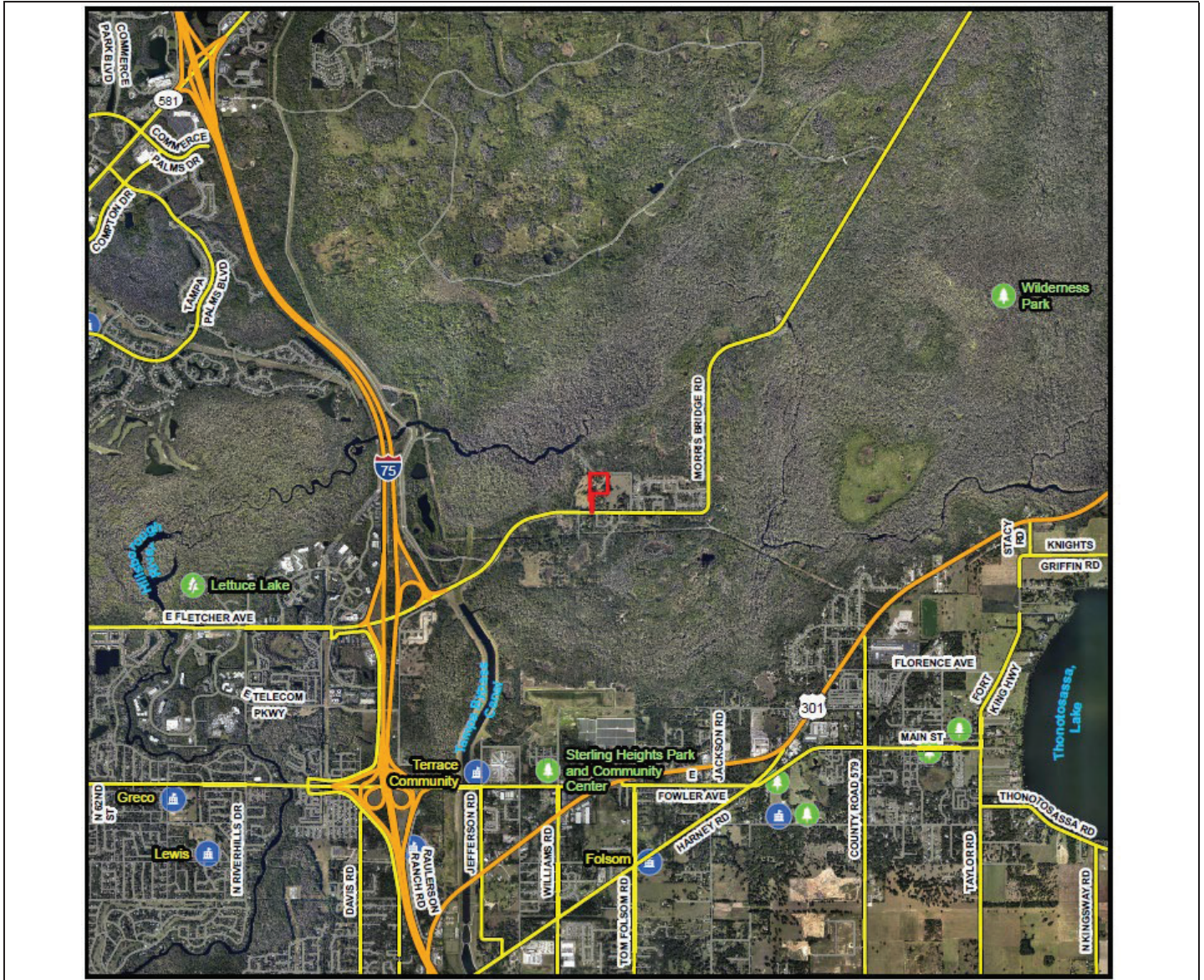
**Additional Information:**

PD Variation(s)	None requested as part of this application
Waiver(s) to the Land Development Code	None requested as part of this application.

<b>Planning Commission Recommendation:</b> CONSISTENT	<b>Development Services Recommendation:</b> APPROVABLE, Subject to Conditions.
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## 2.0 LAND USE MAP SET AND SUMMARY DATA

### 2.1 Vicinity Map



#### Context of Surrounding Area:

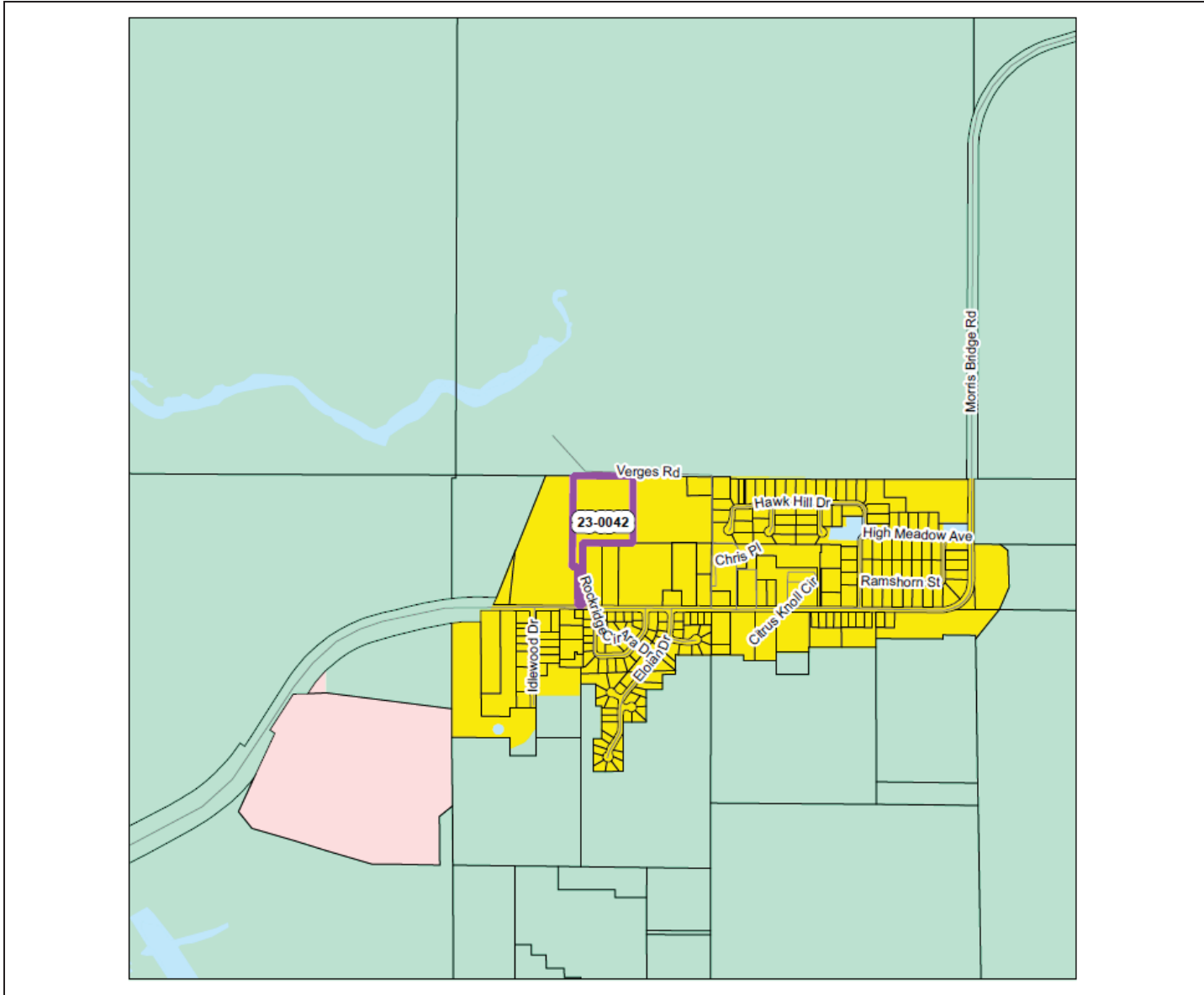
The subject property is located on the north side of Morris Bridge Road, approximately 6,280 feet east of the Interstate-75 interchange. The subject site is located in the Rural Service Area. The property is not located within a community planning area.

The immediate area surrounding the property is predominantly zoned for agricultural. In addition, there are single-family homes, mobile homes, and single-family development. South across Morris Bridge Road is a single-family development zoned RSC-6. Further west are more agricultural lands; further east are pockets of neighborhood commercial areas off of Morris Bridge Road.



**2.0 LAND USE MAP SET AND SUMMARY DATA**

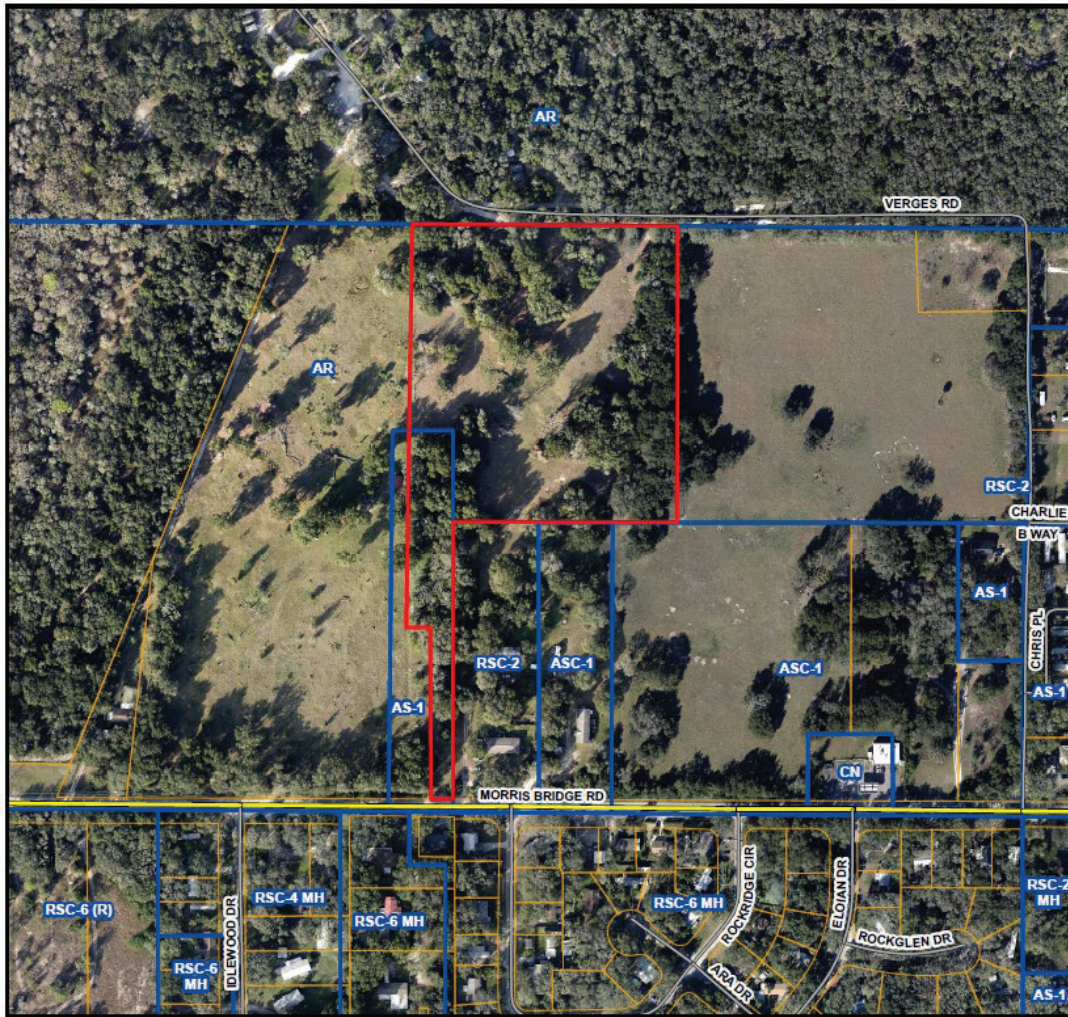
**2.2 Future Land Use Map**



Subject Site Future Land Use Category:	Residential -6
Maximum Density/F.A.R.:	6 dwelling per acre / 0.25 Maximum FAR
Typical Uses:	Typical uses in the Res-6 future land use category include residential, suburban commercial, offices, multi-purpose, mixed-use development.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Immediate Area Map

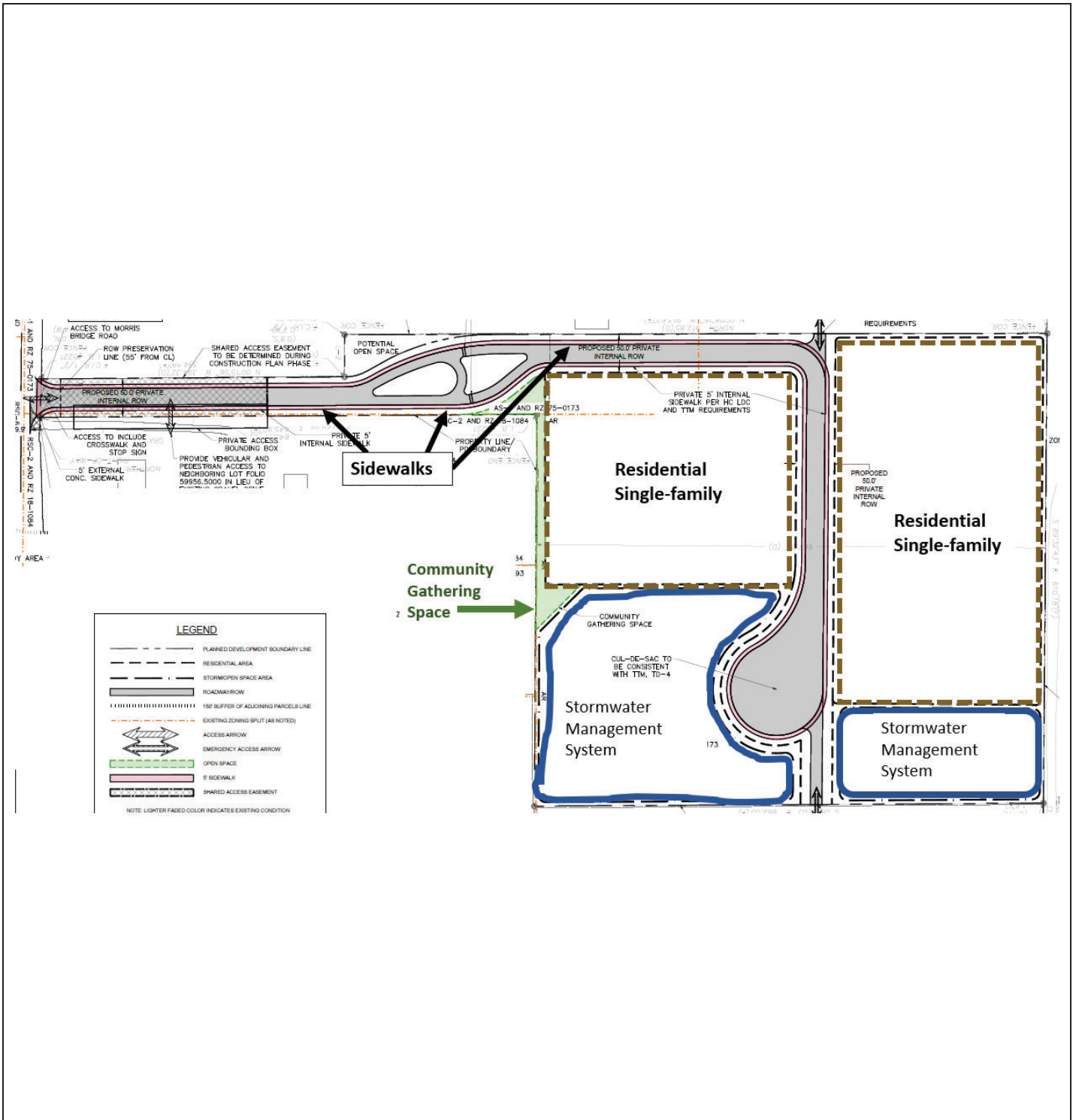


Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	AR	AR: 1 du/5 acres	Located in Hillsborough River Corridor Policy Overlay Area	Verges Road ROW & Southwest Florida Water Management District Public Facility (632 acres)
South	RSC-6	Min. 7,000 sf lot	Single-family residential	Single-family residential
East	AS-1, AR, and RSC-2	AS-1: 1 du/acre AR: 1 du/5 acres RSC-2: 2 du/acre	Single-family (SF) and Agricultural Uses	SF and Agriculture
West	AS-1, AR	AS-1: 1 du/acre AR: 1 du/5 acres	Single-family (SF) and Agricultural Uses	Agricultural and Vacant



2.4 Proposed Site Plan (partial provided below for size and orientation purposes. See Section 8.0 for full site plan)



**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)**

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Morris Bridge Rd.	County Arterial - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	18	2	2
Proposed	94	7	9
Difference (+/-)	+76	+5	+7

\*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	None	None	Meets LDC
East		Emergency Access	None	Meets LDC
West		Emergency Access	None	Meets LDC
Notes:				

Design Exception/Administrative Variance <input type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
Morris Bridge Rd./Access Spacing	Administrative Variance Requested	Approvable
Notes: Shared access proposed to eliminate adjacent driveway conflict.		

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See report.



**4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY**

INFORMATION/REVIEWING AGENCY				
Environmental:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Natural Resources	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Check if Applicable: <input type="checkbox"/> Wetlands/Other Surface Waters <input type="checkbox"/> Use of Environmentally Sensitive Land Credit <input type="checkbox"/> Wellhead Protection Area <input type="checkbox"/> Surface Water Resource Protection Area <input checked="" type="checkbox"/> Potable Water Wellfield Protection Area <input type="checkbox"/> Significant Wildlife Habitat <input type="checkbox"/> Coastal High Hazard Area <input checked="" type="checkbox"/> Urban/Suburban/Rural Scenic Corridor (Morris Bridge Road) <input type="checkbox"/> Adjacent to ELAPP property <input type="checkbox"/> Other _____				
Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
<b>Transportation</b> <input checked="" type="checkbox"/> Design Exc./Adm. Variance Requested <input type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See Transportation Report.
<b>Service Area/ Water &amp; Wastewater</b> <input type="checkbox"/> Urban <input checked="" type="checkbox"/> City of Tampa <input checked="" type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	See Water Resource Services Comment Sheet Water & Wastewater
<b>Hillsborough County School Board</b> Adequate <input checked="" type="checkbox"/> K-5 <input checked="" type="checkbox"/> 6-8 <input checked="" type="checkbox"/> 9-12 <input type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input type="checkbox"/> N/A	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<b>See School Board Report located in backup materials.</b> Although Armwood High School is projected to be at capacity given existing approved development and the proposed amendment, state law requires the school district to consider whether capacity exists in adjacent concurrency service areas (i.e., school attendance boundaries). At this time, additional capacity exists in adjacent concurrency service areas at the high school level.
<b>Impact/Mobility Fees</b> Single Family Detached (Fee estimate is based on a 2,000 s.f.) Mobility: \$13,038 * 10 = \$130,380 Parks: \$2,145 * 10 = \$21,450 School: \$8,227 * 10 = \$82,270				

Fire:  $\$335 * 10 = \$3,350$   
 Total per House:  $\$23,745 * 10 = \$237,450$

Rural Mobility, Northeast Parks/Fire - 10 single family homes

Comprehensive Plan:	Comments Received	Findings	Conditions Requested	Additional Information/Comments
<b>Planning Commission</b> <input type="checkbox"/> Meets Locational Criteria <input checked="" type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input checked="" type="checkbox"/> Minimum Density Met <input type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	See Planning Commission Report



## 5.0 IMPLEMENTATION RECOMMENDATIONS

### 5.1 Compatibility

The applicant seeks to develop an approximately 10.67-acre unified development consisting of one folio. The request is for a rezoning from Agricultural Single-family-1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) to allow for a 10-dwelling-unit, single-family development with a minimum lot size of 0.5 acre per lot. The approximately 10.67-acre subject property is located north of Morris Bridge Road and southwest of Verges Road within the Rural Service Area and not located within the limits of a community plan.

The immediate area surrounding the property is predominantly zoned for agricultural. In addition, there are single-family homes, mobile homes, and single-family development. South across Morris Bridge Road is a single-family development zoned RSC-6. Further west are more agricultural lands; further east are pockets of neighborhood commercial areas off of Morris Bridge Road. On the north side, across Verges Road is a 632-acre Southwest Florida Water Management District Public Facility property with a governmental structure.

The site plan illustrates the areas proposed for the residential development located to the north and west of the overall subject site. Within the eastern portion of the subject site is 2.7-acre stormwater area. Also shown is a gathering space and internal sidewalks promoting connectivity, health, and safety within the proposed development. The applicant's narrative states in part, *"design of the Project protects and preserves the area's natural and physical assets. It is responsive to the existing environmental site conditions while also providing neighborhood infrastructure and street furnishings. Consistent with the Comprehensive Plan and LDC, the Project incorporates site design techniques including buffering, setbacks, open space, height, circulation and access that are compatible with the community and minimize any potential impacts."*

The subject site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land Development Code. The proposed Planned Development would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated Hillsborough County Comprehensive Plan.

### Transportation Administrative Variance

1. Morris Bridge Access Spacing: The applicant's Engineer of Record (EOR) submitted a Section 6.04.02.B. Administrative Variance Request (dated March 30, 2023) from the Section 6.04.07. LDC requirement, governing spacing for the proposed Morris Bridge Rd. access. Per the LDC, Morris Bridge Rd. is a Class 6 roadway, which requires a minimum connection spacing of 245 feet. The applicant is proposing the driveway in a location which is within 245 feet of 3 connections located to the west and 3 connections located to the east shown in full in the Transportation Report. Based on factors presented in the Administrative Variance Request, the County Engineer found the request approvable.

### 5.2 Recommendation

Based on the above considerations, staff finds the request **APPROVABLE**.

## 6.0 PROPOSED CONDITIONS

Approval of the request, subject to the conditions listed below, is based on the general site plan submitted March 28, 2023.


1. The project shall be limited to up to 10 single-family homes. Interim agricultural uses shall be permitted.
2. The development shall comply with the following development standards.
  - a. Minimum Front Yard Setback: 25 feet
  - b. Minimum Side Yard Setback: 7.5 feet
  - c. Minimum Side Yard Setback
    - i. Functioning as a front lot (Corner Lot): 12.5 feet
  - d. Maximum Height: 50 feet
3. Development standards for the single family lots shall be as shown on the site plan under Typical Lot Detail.
4. Lots shall have a minimum of one-half acre of upland acreage in order to use septic.
5. The subject property shall be subject to buffering and screening requirements of Section 6.06.06 of the Hillsborough County Land Development Code.
6. The project is adjacent to residential property and agricultural and pastureland property. Given the existing uses and the rural/suburban nature of the area, natural landscaping shall be provided as part of the common space and stormwater retention, and per Section 6.06.06, specific buffering and fencing shall not be required. However, at the Developer's choosing, the Project may provide fencing around the Project boundary. The exact location and type of such fencing, if proposed, shall be determined at time of construction plan approval.
7. Location of retention ponds and internal roads and driveways shall generally conform with the General Site Plan.
8. Primary vehicular and pedestrian access to the project shall be provided from Morris Bridge Rd., as shown on the PD Site Plan.
9. If PD 23-0042 is approved, the County Engineer will approve an Administrative Variance (dated March 30, 2023) which was found approvable by the County Engineer (on April 3, 2023) for minimum access connection spacing. This administrative variance will allow for the location of the project's access connection on Morris Bridge Rd., as shown on the PD site plan.
10. Gated emergency access shall be provided to folio#59954.0050 and folio#59957.0000.
11. Prior to or concurrent with the initial increment of development, shared access shall be provided to folio#59956.5000 and the driveway serving folio# 59956.5000 shall be removed and restored to County typical section with the County right of way, as shown on the PD site plan.
12. As Morris Bridge Rd. is identified on the Hillsborough County Corridor Preservation Plan as a future 4-lane arterial roadway improvement, the developer shall designate +/-25 of right of way preservation along the



project frontage on Morris Bridge Rd. as shown on the PD site plan. Building setbacks shall be calculated from the future right-of-way line.

- 13. Notwithstanding anything shown in the PD site plan or in the PD conditions to the contrary, pedestrian access may be permitted anywhere along the project boundary.
- 14. Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
- 15. If the notes and/or graphic on the site plan are in conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval, unless otherwise stated herein.
- 16. The development of the project shall proceed in strict accordance with the terms and conditions contained in the Development Order, the General Site Plan, the land use conditions contained herein, and all applicable rules, regulations, and ordinances of Hillsborough County.
- 17. In accordance with LDC Section 5.03.07.C, the certified PD general site plan shall expire for the internal transportation network and external access points, as well as for any conditions related to the internal transportation network and external access points, if site construction plans, or equivalent thereof, have not been approved for all or part of the subject Planned Development within 5 years of the effective date of the PD unless an extension is granted as provided in the LDC. Upon expiration, re-certification of the PD General Site Plan shall be required in accordance with provisions set forth in LDC Section 5.03.07.C

**Zoning Administrator Sign Off:**



J. Brian Grady  
Mon Apr 10 2023 07:39:36

*IntegrISign Desktop*

**APPLICATION NUMBER: PD 23-0042**

ZHM HEARING DATE: April 17, 2023

BOCC LUM MEETING DATE: June 13, 2023

Case Reviewer: Tim Lampkin, AICP

**SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.**

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

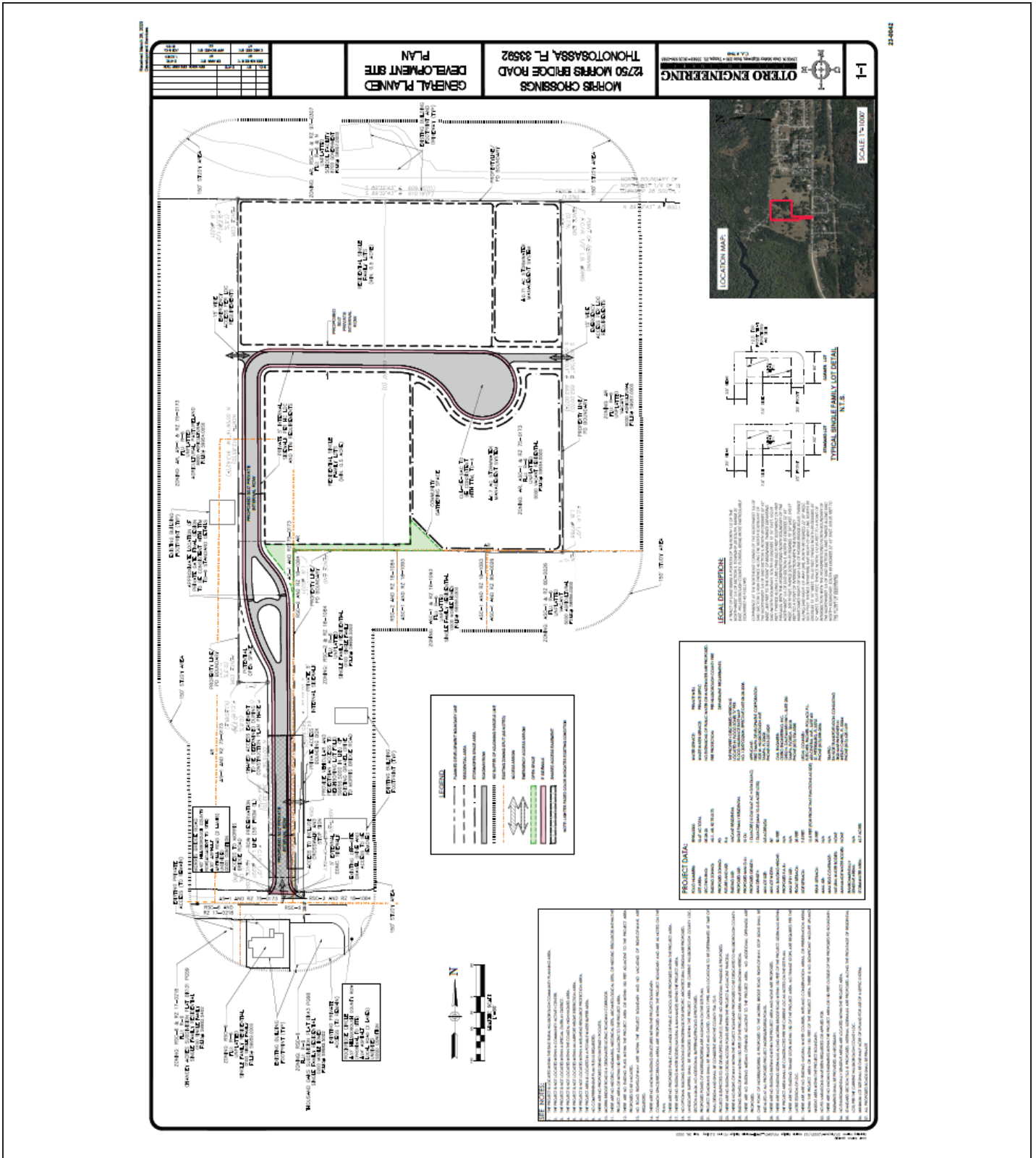


**SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDNACE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.**

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

## 7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS

8.0 PROPOSED SITE PLAN (FULL)





**9.0 FULL TRANSPORTATION REPORT (see following pages)**

## AGENCY REVIEW COMMENT SHEET

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**TO:** Zoning Technician, Development Services Department  
**REVIEWER:** Richard Perez, AICP  
**PLANNING AREA:** East Rural/Northeast

**DATE:** 4/06/2023  
**AGENCY/DEPT:** Transportation  
**PETITION NO:** PD 23-0042

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- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to the listed or attached conditions.
- This agency objects for the reasons set forth below.

### CONDITIONS OF ZONING APPROVAL

- Primary vehicular and pedestrian access to the project shall be provided from Morris Bridge Rd., as shown on the PD Site Plan.
- If PD 23-0042 is approved, the County Engineer will approve an Administrative Variance (dated March 30, 2023) which was found approvable by the County Engineer (on April 3, 2023) for minimum access connection spacing. This administrative variance will allow for the location of the project's access connection on Morris Bridge Rd., as shown on the PD site plan.
- Gated emergency access shall be provided to folio#59954.0050 and folio#59957.0000.
- Prior to or concurrent with the initial increment of development, shared access shall be provided to folio#59956.5000 and the driveway serving folio# 59956.5000 shall be removed and restored to County typical section with the County right of way, as shown on the PD site plan.
- As Morris Bridge Rd. is identified on the Hillsborough County Corridor Preservation Plan as a future 4-lane arterial roadway improvement, the developer shall designate +/-25 of right of way preservation along the project frontage on Morris Bridge Rd. as shown on the PD site plan. Building setbacks shall be calculated from the future right-of-way line.
- Notwithstanding anything shown in the PD site plan or in the PD conditions to the contrary, pedestrian access may be permitted anywhere along the project boundary.

### PROJECT SUMMARY AND ANALYSIS

The applicant is requesting to rezone the subject property from Agricultural Single Family 1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) to construct 10 single family residential lots on +/- 10.67 acres. The site is located on the north side of Morris Bridge Rd., approximately 405 feet east of Idlewood Dr. The Future Land Use designation of the site is Residential 6 (R-6).

***Trip Generation Analysis***

The applicant submitted a trip generation analysis as required by the Development Review Procedures Manual (DRPM). Staff has prepared a comparison of the trips potentially generated under the existing and proposed zoning designations, utilizing a generalized worst-case scenario. Data presented below is based on the Institute of Transportation Engineer’s Trip Generation Manual, 11<sup>th</sup> Edition.

**Approved Zoning:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
AS-1: 1 Units, Single Family Detached (ITE 210)	9	1	1
AR: 1 Units, Single Family Detached (ITE 210)	9	1	1
<b>TOTAL TRIPS</b>	<b>18</b>	<b>2</b>	<b>2</b>

**Proposed Zoning:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
PD: 10 Units, Single Family Detached (ITE 210)	94	7	9

**Trip Generation Difference:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
<b>Difference (+/-)</b>	<b>+76</b>	<b>+5</b>	<b>+7</b>

The proposed rezoning will result in an increase in potential trip generation by +76 daily trips, +5 AM peak hour trips, +7 PM peak hour trips.

**TRANSPORTATION INFRASTRUCTURE SERVING THE SITE**

The subject property fronts the northside of Morris Bridge Rd.

Morris Bridge Rd. is a substandard, rural, 2-lane arterial roadway, maintained by the County. The roadway predominately consists of +/-10-foot wide travel lanes with 5-foot paved shoulders/bicycle lanes, no sidewalks and no curb and gutter. The roadway lies within +/- 40 feet of right-of-way.

By policy of the County Engineer, projects that generate less than 11 peak hour trips are considered de minimis provided the roadways meet minimum life safety standards (i.e. 15 feet of pavement in a 20-foot clear area).

Morris Bridge Rd. is identified in the Hillsborough County Corridor Preservation Plan as a future 4-lane arterial roadway. A future 4-lane roadways requires a total of 110 feet of right of way pursuant to County TTM, TS-6, 4-lane divided typical section. The applicant has submitted a PD site plan showing the right of way preservation required being measured 55 feet from the roadway centerline. As such +/-25 feet of corridor preservation will be required to be designated along the project frontage.



## **SITE ACCESS & CONNECTIVITY**

The proposed PD site plan shows primary vehicular and pedestrian access to Morris Bridge Rd via private local roadway. The project frontage is limited to 50 feet and, therefore, does not have any other location to construct the project access. This proposed access connection does not meet the minimum spacing criteria of 245 feet pursuant to LDC, Sec. 6.04.07. The applicant has submitted a Sec. 6.04.02. B. administrative variance request to allow the proposed vehicular access connection location as discussed in greater detail herein under the section titled Request Administrative Variance.

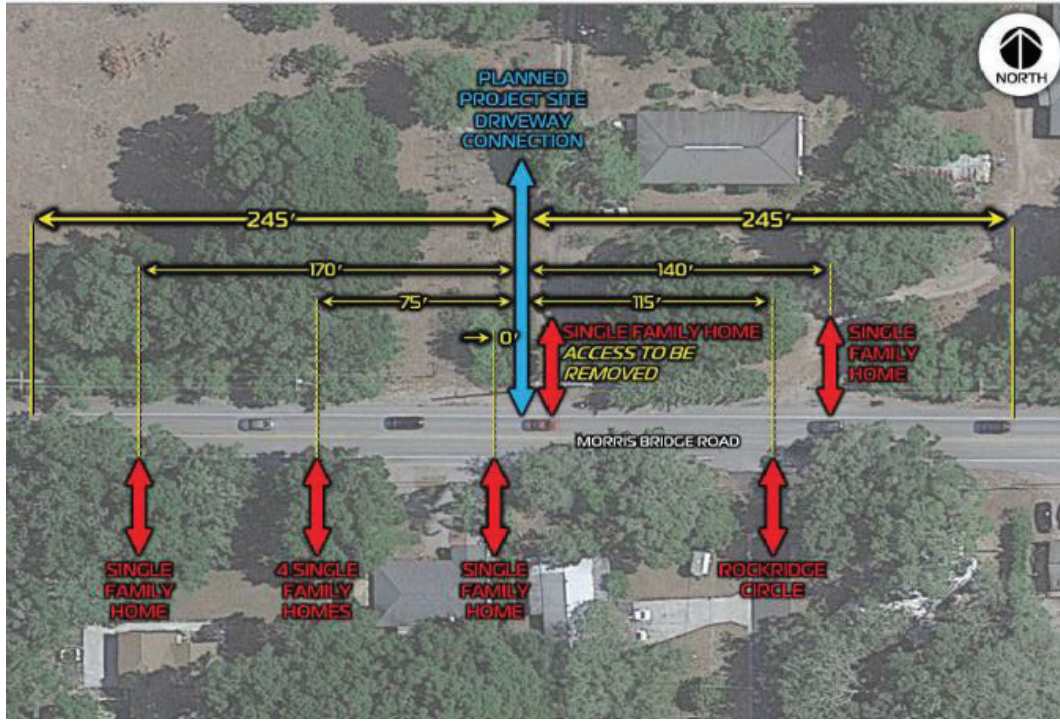
The project proposes to designate the project access as a shared access facility with the adjacent residential property to the east (folio#59956.5000), which also fronts on Morris Bridge Rd. Folio##59956.5000 is currently served by a driveway located approximately +/-8 feet from the project's proposed access connection. At the time of construction, the developer will be required to construct a driveway access to serve Folio##59956.5000 and remove the existing driveway on Morris Bridge Rd. and restore it to Hillsborough County typical standard.

The proposed PD site plan provides emergency access connections to the adjacent residentially designated properties to the east (folio#59957.0000) and west (folio#59954.0050) consistent with LDC, Sec. 6.02.01. H. The emergency access shall be gated and accessible via a Knox Box Rapid Entry System by County Emergency Services.

The PD site plan proposes the internal subdivision road to be private and gated. Said roadway will be constructed consistent with the County Transportation Technical Manual TS-3 local roadway typical section.

## **REQUESTED ADMINISTRATIVE VARIANCE – MORRIS BRIDGE RD. ACCESS SPACING:**

The applicant's Engineer of Record (EOR) submitted a Section 6.04.02.B. Administrative Variance Request (dated March 30, 2023) from the Section 6.04.07. LDC requirement, governing spacing for the proposed Morris Bridge Rd. access. Per the LDC, Morris Bridge Rd. is a Class 6 roadway, which requires minimum connection spacing of 245 feet. The applicant is proposing the driveway in a location which is with 245 feet of 3 connections located to the west and 3 connections located to the east shown in the graphic below. Based on factors presented in the Administrative Variance Request, the County Engineer found the request approvable on April 3, 2023. If this rezoning is approved, the County Engineer will approve the above referenced Administrative Variance Request.



**LEVEL OF SERVICE (LOS) INFORMATION**

FDOT Generalized Level of Service				
Roadway	From	To	LOS Standard	Peak Hr Directional LOS
Morris Bridge Rd.	I-75	Cross Creek Blvd.	D	C

Source: 2022 Hillsborough County Multimodal Level of Service (LOS) Report.

Transportation Comment Sheet

**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)**

Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Morris Bridge Rd.	County Arterial - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	18	2	2
Proposed	94	7	9
Difference (+/-)	+76	+5	+7

\*Trips reported are based on net new external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	None	None	Meets LDC
East		Emergency Access	None	Meets LDC
West		Emergency Access	None	Meets LDC
Notes:				

Design Exception/Administrative Variance <input type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
Morris Bridge Rd./Access Spacing	Administrative Variance Requested	Approvable
Notes: Shared access proposed to eliminate adjacent driveway conflict.		

4.0 Additional Site Information & Agency Comments Summary

Transportation	Objections	Conditions Requested	Additional Information/Comments
<input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See report.

**COUNTY OF HILLSBOROUGH**

**RECOMMENDATION OF THE  
LAND USE HEARING OFFICER**

**APPLICATION NUMBER:** RZ PD 23-0042

**DATE OF HEARING:** April 17, 2023

**APPLICANT:** First Tampa Development Corporation

**PETITION REQUEST:** A request to rezone property from AS-1 and AR to PD to permit 10 single-family homes

**LOCATION:** 12750 Morris Bridge Road

**SIZE OF PROPERTY:** 10.67 acres, m.o.l.

**EXISTING ZONING DISTRICT:** AS-1 and AR

**FUTURE LAND USE CATEGORY:** RES-6

**SERVICE AREA:** Rural

**COMMUNITY PLAN:** N/A



## DEVELOPMENT REVIEW STAFF REPORT

**\*Note:** Formatting issues prevented the entire Development Services Department staff report from being copied into the Hearing Master's Recommendation. Therefore, please refer to the Development Services Department web site for the complete staff report.

### 1.0 APPLICATION SUMMARY



Applicant: First Tampa Development Corporation

FLU Category: Residential - 6

Service Area: Rural

Site Acreage: Approximately 10.67 acres

Community Plan Area: None

Overlay: None

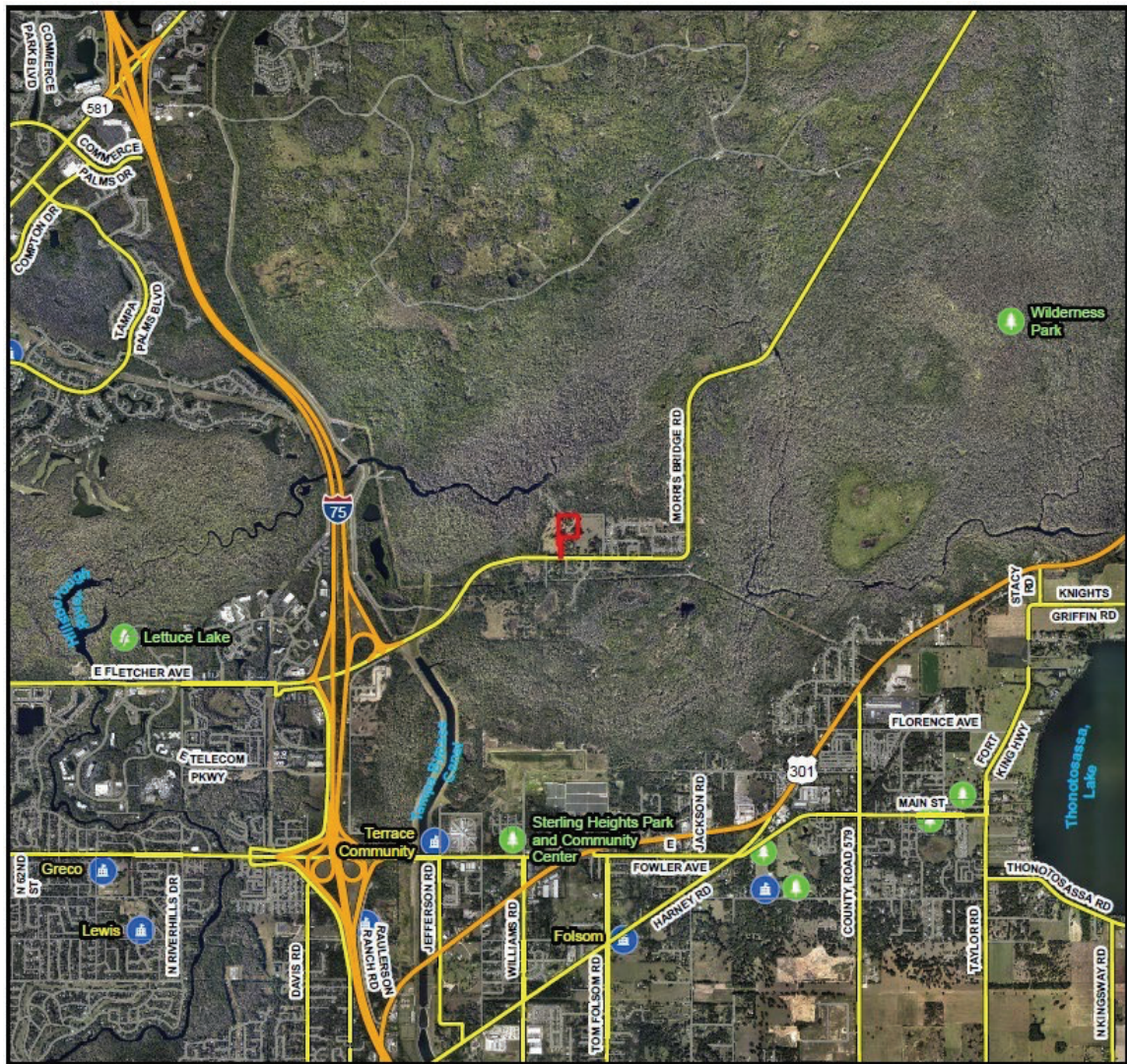
#### Introduction Summary:

The applicant seeks to develop an approximately 10.67-acre unified development consisting of one folio. The request is for a rezoning from Agricultural Single-family-1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) to allow for a 10-dwelling-unit, single-family development.

Setbacks/Buffering and Screening	Front: 50 ft. Side: 25 ft. Rear: 50 ft. Lot Width: 150 ft.	Front: 50 ft. Side: 15 ft. Rear: 50 ft. Lot Width: 150 ft.	Front: 25 feet Side: 7.5 ft. / 12.5 ft. corner lot 2 <sup>nd</sup> side Rear: 25 ft. Lot Width: 80 ft.
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<b>Planning Commission Recommendation:</b>  CONSISTENT	<b>Development Services Recommendation:</b>  APPROVABLE, Subject to Conditions.
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## 2.0 LAND USE MAP SET AND SUMMARY DATA 2.1 Vicinity Map

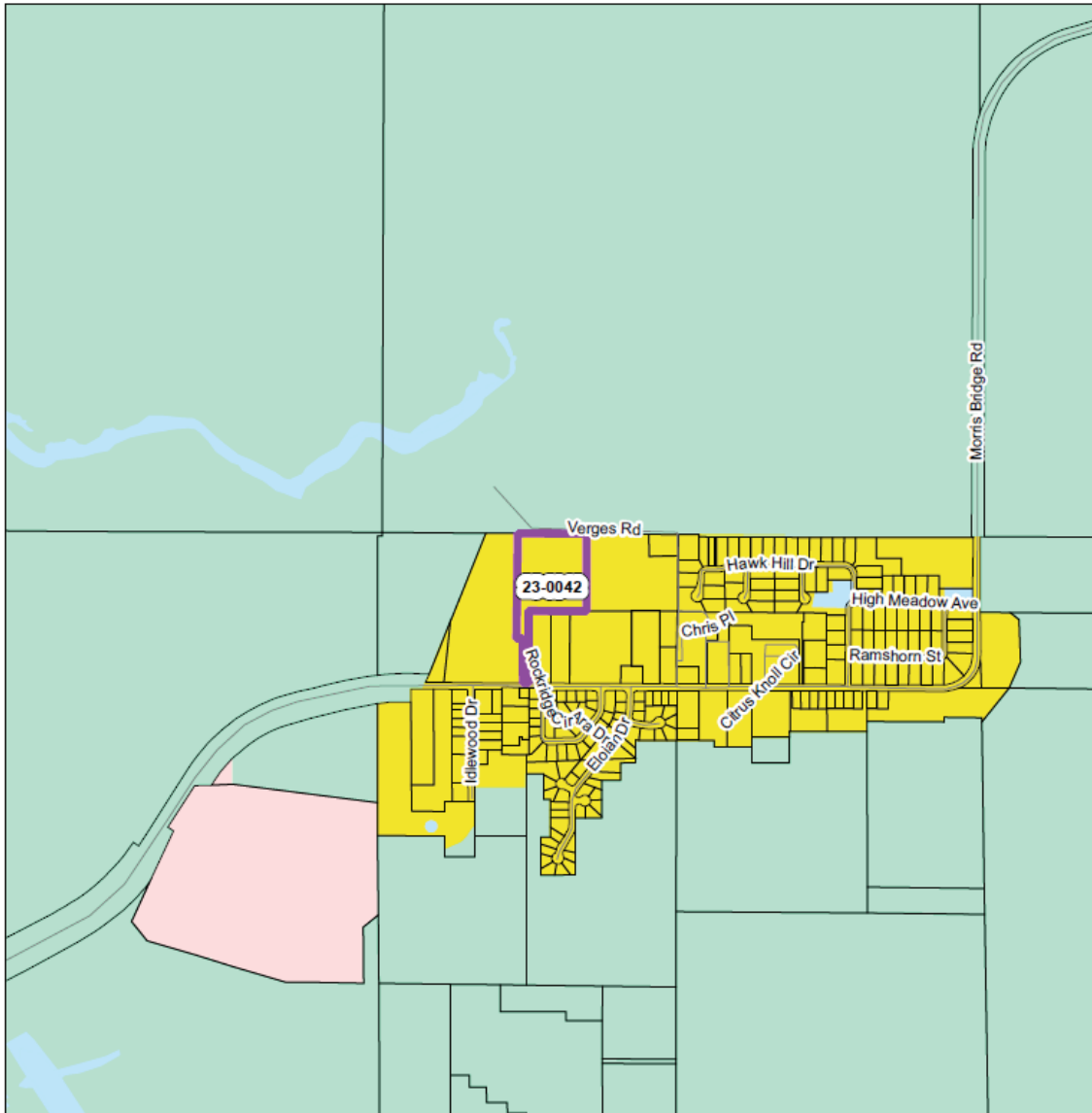


**Context of Surrounding Area:**

The subject property is located on the north side of Morris Bridge Road, approximately 6,280 feet east of the Interstate- 75 interchange. The subject site is located in the Rural Service Area. The property is not located within a community planning area.

The immediate area surrounding the property is predominantly zoned for agricultural. In addition, there are single- family homes, mobile homes, and single-family development. South across Morris Bridge Road is a single-family development zoned RSC-6. Further west are more agricultural lands; further east are pockets of neighborhood commercial areas off of Morris Bridge Road.

## 2.0 LAND USE MAP SET AND SUMMARY DATA 2.2 Future Land Use Map



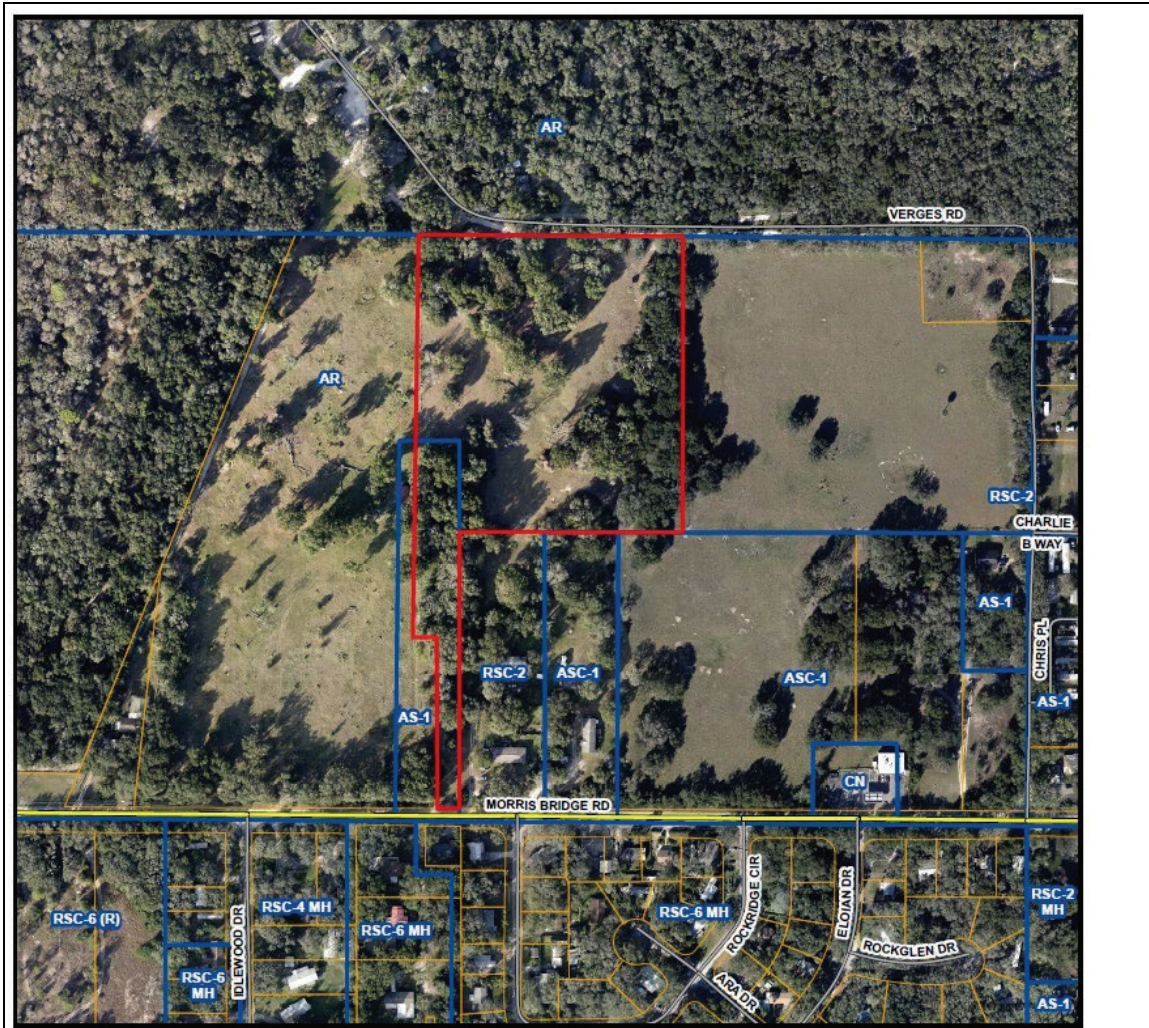
Subject Site Future Land Use Category: Residential -6

Maximum Density/F.A.R.: 6 dwelling per acre / 0.25 Maximum FAR

Typical Uses:	Typical uses in the Res-6 future land use category include residential, suburban commercial, offices, multi-purpose, mixed-use development.
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**2.0 LAND USE MAP SET AND SUMMARY DATA 2.3 Immediate Area Map**

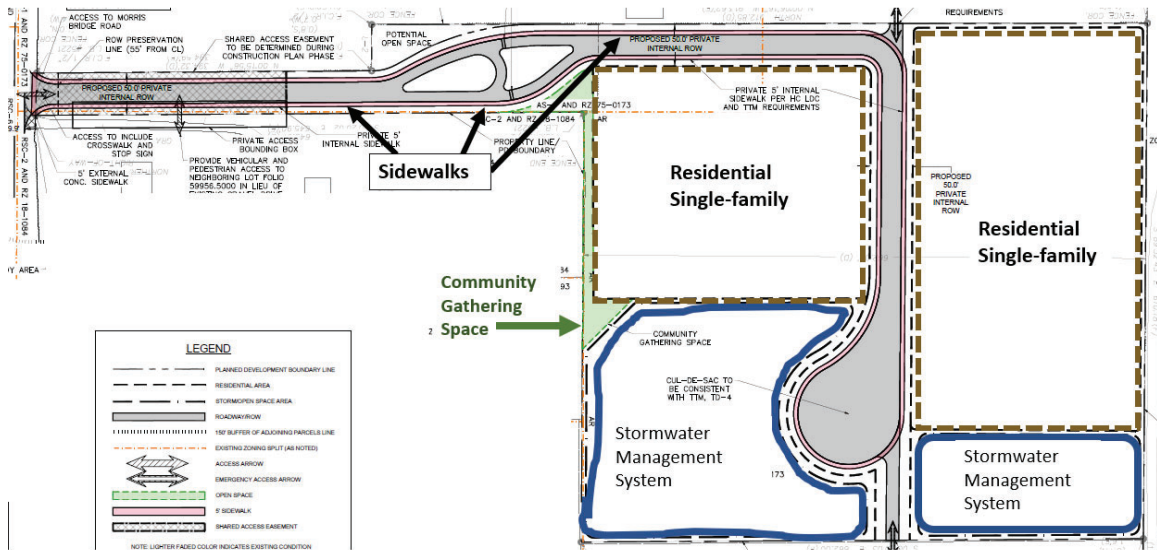


**Adjacent Zonings and Uses**

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	AR	AR: 1 du/5 acres	Located in Hillsborough River Corridor Policy Overlay Area	Verges Road ROW & Southwest Florida Water Management District Public Facility (632 acres)

East	AS-1, AR, and RSC-2	AS-1: 1 du/acre AR: 1 du/5 acres RSC-2: 2 du/acre	Single-family (SF) and Agricultural Uses	SF and Agriculture
West	AS-1, AR	AS-1: 1 du/acre AR: 1 du/5 acres	Single-family (SF) and Agricultural Uses	Agricultural and Vacant

**2.4 Proposed Site Plan** (partial provided below for size and orientation purposes. See Section 8.0 for full site plan)



**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)**

<b>Adjoining Roadways (check if applicable)</b>			
Road Name	Classification	Current Conditions	Select Future Improvements
Morris Bridge Rd.	County Arterial - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

<b>Project Trip Generation</b> <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	18	2	2
Proposed	94	7	9
Difference (+/-)	+76	+5	+7

\*Trips reported are based on net new external trips unless otherwise noted.

<b>Connectivity and Cross Access</b> <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	None	None	Meets LDC
East		Emergency Access	None	Meets LDC
West		Emergency Access	None	Meets LDC
Notes:				

<b>Design Exception/Administrative Variance</b> <input type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
Morris Bridge Rd./Access Spacing	Administrative Variance Requested	Approvable
Notes: Shared access proposed to eliminate adjacent driveway conflict.		

<b>4.0 Additional Site Information &amp; Agency Comments Summary</b>			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See report.

**4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY**

<p>Check if Applicable:</p> <p><input type="checkbox"/> Wetlands/Other Surface Waters</p> <p><input type="checkbox"/> Use of Environmentally Sensitive Land Credit</p> <p><input type="checkbox"/> Wellhead Protection Area</p> <p><input type="checkbox"/> Surface Water Resource Protection Area</p>
--

- Potable Water Wellfield Protection Area
- Significant Wildlife Habitat
- Coastal High Hazard Area
- Urban/Suburban/Rural Scenic Corridor (Morris Bridge Road)  Adjacent to ELAPP property
- Other \_\_\_\_\_

Public Facilities:	Comments Received	Objections	Conditions Requested	Additional Information/Comments
<b>Transportation</b>  <input checked="" type="checkbox"/> Design Exc./Adm. Variance Requested <input type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See Transportation Report.
<b>Service Area/ Water &amp; Wastewater</b>  <input type="checkbox"/> Urban <input checked="" type="checkbox"/> City of Tampa <input checked="" type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	See Water Resource Services Comment Sheet Water & Wastewater
<b>Hillsborough County School Board</b>  Adequate <input checked="" type="checkbox"/> K-5 <input checked="" type="checkbox"/> 6-8 <input checked="" type="checkbox"/> 9-12 <input type="checkbox"/> N/A Inadequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input type="checkbox"/> N/A	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<b>See School Board Report located in backup materials.</b>  Although Armwood High School is projected to be at capacity given existing approved development and the proposed amendment, state law requires the school district to consider whether capacity exists in adjacent concurrency service areas (i.e., school attendance boundaries). At this time, additional capacity exists in adjacent



				concurrency service areas at the high school level.
<b>Impact/Mobility Fees</b>				
Single Family Detached (Fee estimate is based on a 2,000 s.f.) Mobility: \$13,038 * 10 = \$130,380				
Parks: \$2,145 * 10 = \$21,450				
School: \$8,227 * 10 = \$82,270				

## 5.0 IMPLEMENTATION RECOMMENDATIONS

### 5.1 Compatibility

The applicant seeks to develop an approximately 10.67-acre unified development consisting of one folio. The request is for a rezoning from Agricultural Single-family-1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) to allow for a 10-dwelling-unit, single-family development with a minimum lot size of 0.5 acre per lot. The approximately 10.67- acre subject property is located north of Morris Bridge Road and southwest of Verges Road within the Rural Service Area and not located within the limits of a community plan.

The immediate area surrounding the property is predominantly zoned for agricultural. In addition, there are single-family homes, mobile homes, and single-family development. South across Morris Bridge Road is a single-family development zoned RSC-6. Further west are more agricultural lands; further east are pockets of neighborhood commercial areas off of Morris Bridge Road. On the north side, across Verges Road is a 632-acre Southwest Florida Water Management District Public Facility property with a governmental structure.

The site plan illustrates the areas proposed for the residential development located to the north and west of the overall subject site. Within the eastern portion of the subject site is 2.7-acre stormwater area. Also shown is a gathering space and internal sidewalks promoting connectivity, health, and safety within the proposed development. The applicant’s narrative states in part, *“design of the Project protects and preserves the area’s natural and physical assets. It is responsive to the existing environmental site conditions while also providing neighborhood infrastructure and street furnishings. Consistent with the Comprehensive Plan and LDC, the Project incorporates site design techniques including buffering, setbacks, open space, height, circulation and access that are compatible with the community and minimize any potential impacts.*

The subject site will comply with and conform to all other applicable policies and regulations, including but not limited to, the Hillsborough County Land



Development Code. The proposed Planned Development would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated Hillsborough County Comprehensive Plan.

### **Transportation Administrative Variance**

1. Morris Bridge Access Spacing: The applicant's Engineer of Record (EOR) submitted a Section 6.04.02.B. Administrative Variance Request (dated March 30, 2023) from the Section 6.04.07. LDC requirement, governing spacing for the proposed Morris Bridge Rd. access. Per the LDC, Morris Bridge Rd. is a Class 6 roadway, which requires a minimum connection spacing of 245 feet. The applicant is proposing the driveway in a location which is within 245 feet of 3 connections located to the west and 3 connections located to the east shown in full in the Transportation Report. Based on factors presented in the Administrative Variance Request, the County Engineer found the request approvable.

### **5.2 Recommendation**

Based on the above considerations, staff finds the request **APPROVABLE**.

Zoning conditions, which were presented Zoning Hearing Master hearing, were reviewed and are incorporated by reference as a part of the Zoning Hearing Master recommendation.

## **SUMMARY OF HEARING**

THIS CAUSE came on for hearing before the Hillsborough County Land Use Hearing Officer on April 17, 2023. Ms. Michelle Heinrich of the Hillsborough County Development Services Department introduced the petition.

Ms. Anne Pollack 433 Central Avenue St. Petersburg testified on behalf of the applicant First Tampa Development Corporation. Ms. Pollack introduced members of the development team and stated that the request is to rezone from AS-1 and AR to PD to permit a 10-unit single-family subdivision located at 17250 Morris Bridge Road. She added that the property is just over 10 acres. Because the site is designated RES-6, it has the potential for 64 units but the applicant is requesting 10 which equates to just over one unit per acre. The developer will provide the necessary utilities and will not request to extend public water and sewer. Ms. Pollack showed graphics to discuss the flag shape of the property and access points. The roadway will be improved with sidewalks on both sides which will permit pedestrian connectivity in the area. A gate will be install just north of the access point for the neighboring property owner. Two emergency stub outs will be provided per the County's request. Ms. Pollack added that she was aware there was opposition to the stub outs but stated that it was a County requirement. She testified that it was her understanding that the stub outs were for emergency services like fire trucks but would not be used unless the

neighboring properties were developed. Ms. Pollack detailed the transportation and environmental conditions and stated that a wildlife survey was conducted and determined that there are gopher tortoises on-site which will be addressed in full compliance with all regulations. No bald eagles were found on-site. Ms. Pollack discussed the compatibility of the proposed 10-units with the surrounding area and stated that the applicant reached out to the several adjacent neighbors before filing the application, talked to them and then did not hear much back.

Mr. Tim Lampkin, Development Services Department testified regarding the County's staff report. Mr. Lampkin described the location of the property and surrounding area and stated that the request is to rezone from AS-1 and AR to PD to develop 10 single-family homes. No variations or waivers are requested. An administrative variance was requested regarding the Morris Bridge access spacing which the County Engineer has found approvable. Mr. Lampkin concluded his comments by stating that staff finds the request approvable.

Hearing Master Finch asked Mr. Perez of the County's Transportation section about the gated emergency access to confirm that it is a request from the County per the Technical Manual. Mr. Perez replied that was correct and cited the Code Section as the development triggers the need for emergency access. He added that at this time, the developer would provide a stub out to either side of the project which would be in place for the future if the adjacent parcels were to develop residential in which case they would connect.

Hearing Master Finch asked Mr. Perez that because the properties to the east and west are not currently developed, then the subject 10-lot subdivision would install the gate and Knox Box. Mr. Perez replied that the first project in installs the gate and Knox Box with the emergency access to the property line. It is then in place if any potential development occurs on the adjacent properties. Hearing Master Finch asked Mr. Perez to confirm that even though the subject property owner is required to install the gate, it would not be used because the adjacent properties are not developed. Mr. Perez replied that was correct.

Ms. Melissa Lienhard of the Planning Commission staff stated that the property is designated Residential-6 Future Land Use category and located in the Rural Service Area. She discussed the surrounding area and stated that the request is consistent with the area as the lots will be a minimum of one-half acre in size. Ms. Lienhard testified that the Planning Commission staff finds the rezoning is consistent with the Comprehensive Plan.

Hearing Master Finch asked audience members if there were any proponents of the application. None replied.

Hearing Master Finch asked audience members if there were any opponents of the application.

Ms. Amira Zeinelabdin 12728 Morris Bridge Road testified in opposition and

stated that her family owns the property next door which is 12744 Morris Bridge Road. Ms. Zeinelabdin stated that she opposes the rezoning and hopes to preserve the sanctity of the agricultural and wildlife aspects of the area. She added that the proposed number of homes does not fit the culture of the single-family homes in the area. She testified that there is wildlife in the area and Nature's Classroom which is a residential environmental education program is in the area. Wild deer and bald eagles nest just west of the property in a tower as well as to the east on a power line. There are grey and red foxes as well as gopher turtles. Ms. Zeinelabdin stated that the project will increase traffic and that the applicant's traffic report falsely claimed that there were no fatalities in the course of five years. She stated that in November of 2021 there was a vehicle fatality that occurred in front of her house. She cited an article from the Tampa Bay News that stated that since 2019 there were a total of five fatal accidents as well as 150 crashes on Morris Bridge Road. She testified that her family are deer farmers and the influx of traffic will endanger her deer. Ms. Zeinelabdin concluded her comments by stating that the applicant's statement that neighbors do not object is false and that they did not permit access for the proposed access points.

Ms. Cheryl Chewning 12858 Morris Bridge Road testified in opposition. She stated that her family originally owned the subject property. Her family sold it to people that wanted rural agricultural. A church then bought half of the site. The church then sold it to a trucking company and now it is proposed for development. Ms. Chewning referred to a road analysis that was done when her family sold the property in 2006 and cited the number of cars and level of service. She stated that 16 years later, the road is a rural highway and very dangerous. She spoke with the Sheriff's Office to see what could be done to slow the traffic down and was told there was nothing that could be done because it's a rural highway. The speed limit was lowered from 45 to 40 MPH and it often takes 30 minutes to leave her driveway. Regarding the proposed emergency access, Ms. Chewning stated that she and the property to the west have no intention of developing. She expressed concerns regarding the potential for sink holes and stated that putting septic tanks near the Hillsborough River is an issue. She wrapped up her comments by stating that she is concerned about the impacts to the animals in the area.

Ms. Sharon Bass 9835 North 56<sup>th</sup> Street testified and stated that she is an attorney representing Ms. Chewning and her Mother June McClarnon. She stated that her concern relates to Comprehensive Plan Policy 1.1 regarding property owners having a vested right in their property as they own it. The properties are being used by current land owners that have been there a significant amount of time. She added that there are concerns about the impact of the Planned Development with ten homes with septic tanks and the possible impact to the water on the adjacent parcels. Ms. Bass stated that there are concerns regarding light pollution and described compatibility issues with the adjacent properties.

Development Services Department staff did not have additional comments.

Ms. Pollack testified during the rebuttal period. She asked for her transportation consultant to address the citizens' concerns.

Mr. Michael Raysor 19046 Bruce B. Downs Blvd. testified as a Registered Professional Engineer with 26 years of experience in the field of traffic engineering. Mr. Raysor stated that while he did not have access to the 2006 report referred to by the citizen, the staff report stated that Morris Bridge Road has a Level of Service C compared to a D standard. He added that the operational capacity in the 2006 report may have been incorrectly evaluated as an arterial road rather than a highway segment. He added that as concurrency has been eliminated and replaced with mobility fees it would not have been material. Mr. Raysor testified that the crash data was obtained from the County's crash data base and discussions with the County Engineer, Mr. Williams, have not brought up any safety concerns on Morris Bridge Road. He testified that the permitting process would include a review of site visibility and other factors and the developer would not be permitted to construct a driveway that is not safe. Mr. Raysor concluded his rebuttal comments by stating that the subject property is located on a segment of Morris Bridge Road that is close to the Interstate and the conversations with the County Engineer have not included mention of any atypical safety characteristics.

Ms. Pollack continued her rebuttal testimony by stating that she would be submitting a geo tech email and report that was done to show their findings and that additional geo technical testing would be done if it was determined that there was a sink hole or other type of issue on-site. She stated that the owners are not interested in building a development that would be unsafe for either their home owners or cause a problem on neighboring sites. Regarding animals, Ms. Pollack testified that there are animals in the area and will be required to submit a study within 90 days of submitting construction plans. She added that the development would recognize the agricultural uses nearby.

Mr. Dimitri Artzibushev with First Tampa Development 1525 West Hillsborough stated that is his intent to develop with a nice project the best he can. He added that trees will be preserved and that studies will be done to check for sink holes and other issues.

Ms. Pollack concluded her rebuttal testimony by stating that there have been several meetings with SWFWMD regarding stormwater and how to adjust the ponds. The development will have the least impact possible on the surrounding properties.

The hearing was then concluded.

## **EVIDENCE SUBMITTED**

Ms. Chewing submitted a study regarding transportation and written comments from a neighbor in opposition into the record.

## **PREFACE**

All matters that precede the Summary of Hearing section of this Decision are hereby incorporated into and shall constitute a part of the ensuing Findings of Fact and Conclusions of Law.

## **FINDINGS OF FACT**

1. The subject site is 10.67 acres in size and is zoned Agricultural Single-Family -1 (AS-1) and Agricultural Rural (AR) and designated Residential-6 (RES-6) by the Comprehensive Plan. The property is located in the Rural Service Area.
2. The rezoning to Planned Development (PD) is requested to permit 10 single-family homes.
3. No Planned Development Variations or waivers are requested.
4. The Planning Commission staff testified that the request is consistent with the area as the lots will be a minimum of one-half acre in size. Staff testified that the rezoning is consistent with the Comprehensive Plan.
5. The surrounding parcels are zoned AR, AS-, RSC-2 and RSC-6 and developed with single-family homes and agricultural land uses.
6. Testimony in opposition was provided at the Zoning Hearing Master hearing and submitted into the record. The property owners to the east and west expressed concerns regarding the County required gated emergency access stub out points to the east and west of the project. Other comments pertained to safety concerns existing on Morris Bridge Road and the impact of the additional project traffic. Environmental and wildlife impacts from the proposed 10 homes were also included in the opposition's testimony.

County transportation staff testified that the County's Technical Manual required the emergency access points. Staff also stated that the subject project would be required to provide the access stub out, gate and Knox Box but that the access would not be used until the adjacent parcels are developed with residential land uses.

The applicant's representative testified that all applicable regulations would be followed regarding environmental and wildlife standards.



The applicant's Professional Transportation Engineer testified that Morris Bridge Road has a Level of Service C compared to a D standard. Additionally, he testified that the site access will comply with all requirements regarding site visibility and safety.

7. The proposed zoning conditions require a minimum lot size of one-half acre of upland for the use of a septic tank.

The applicant's representative testified that the developer will not extend water and sewer lines to the subject property.

8. The rezoning to Planned Development for 10 single-family homes is compatible with the surrounding development pattern and meets the intent of the Land Development Code and Comprehensive Plan.

### **FINDINGS OF COMPLIANCE/NON-COMPLIANCE WITH THE HILLSBOROUGH COUNTY COMPREHENSIVE PLAN**

The rezoning request is in compliance with and does further the intent of the Goals, Objectives and the Policies of the Future of Hillsborough Comprehensive Plan.

### **CONCLUSIONS OF LAW**

Based on the Findings of Fact cited above, there is substantial competent evidence to demonstrate that the requested Planned Development rezoning is in conformance with the applicable requirements of the Land Development Code and with applicable zoning and established principles of zoning law.

### **SUMMARY**

The request is to rezone 10.67 acres from Agricultural Single-Family-1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) is to develop a maximum of 10 single-family homes.

The Planning Commission staff testified that the request is consistent with the area as the lots will be a minimum of one-half acre in size. Staff testified that the rezoning is consistent with the Comprehensive Plan.

Testimony in opposition was provided at the Zoning Hearing Master hearing. The property owners to the east and west expressed concerns regarding the County required gated emergency access stub out points to the east and west of the project. Other comments pertained to safety concerns existing on Morris Bridge Road and the impact of the additional project traffic. Environmental and wildlife

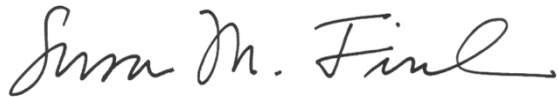
impacts from the proposed 10 homes were also included in the opposition's testimony.

County transportation staff testified that the County's Technical Manual required the emergency access points. Staff also stated that the subject project would be required to provide the access stub out, gate and Knox Box but that the access would not be used until the adjacent parcels are developed with residential land uses. The applicant's representative testified that all applicable regulations would be followed regarding environmental and wildlife standards. The applicant's Professional Transportation Engineer testified that Morris Bridge Road has a Level of Service C compared to a D standard. Additionally, he testified that the site access will comply with all requirements regarding site visibility and safety.

The rezoning to Planned Development for 10 single-family homes is compatible with the surrounding development pattern and meets the intent of the Land Development Code and Comprehensive Plan.

### **RECOMMENDATION**

Based on the foregoing, this recommendation is for **APPROVAL** of the Planned Development rezoning request as indicated by the Findings of Fact and Conclusions of Law stated above subject to the proposed zoning conditions prepared by the Development Services Department.

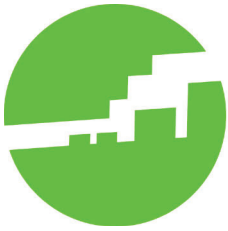


May 8, 2023

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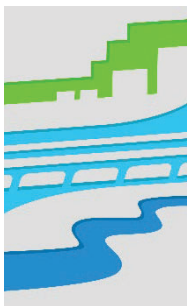
**Susan M. Finch, AICP**  
**Land Use Hearing Officer**

**Date**



**Hillsborough County  
City-County  
Planning Commission**

<b>Unincorporated Hillsborough County Rezoning</b>	
<b>Hearing Date:</b> April 17, 2023  <b>Report Prepared:</b> April 5, 2023	<b>Petition: PD 23-0042</b>  <b>12750 Morris Bridge Road</b>  <i>North of Morris Bridge Road and southwest of Verges Road</i>
<b>Summary Data:</b>	
<b>Comprehensive Plan Finding</b>	<b>CONSISTENT</b>
<b>Adopted Future Land Use</b>	<b>Residential-6 (6du/ga; 0.25 FAR)</b>
<b>Service Area</b>	<b>Rural Area</b>
<b>Community Plan</b>	<b>N/A</b>
<b>Request</b>	Rezoning from Agricultural Rural (AR) and Agricultural, Single-Family (AS-1) to a Planned Development (PD) to allow for a 10-unit single family residential development
<b>Parcel Size</b>	10.67 ± acres
<b>Street Functional Classification</b>	Morris Bridge Road - <b>County Arterial</b> Verges Road - <b>Local Road</b>
<b>Locational Criteria</b>	N/A
<b>Evacuation Zone</b>	D



## **Context**

- The approximately 10.67-acre subject property is located north of Morris Bridge Road and southwest of Verges Road.
- The site is located within the Rural Area and is not located within the limits of any Community Plan.
- The subject property is located within the Residential-6 (RES-6) Future Land Use category, which can be considered for a maximum density of 6 dwelling units per gross acre and a maximum intensity of 0.25 FAR. The RES-6 Future Land Use category is designated to areas that are suitable for low density residential development. Typical uses include residential, suburban scale neighborhood commercial, office uses, multi-purpose projects, and mixed-use development.
- The subject site abuts Morris Bridge Road to the south. RES-6 surrounds the site to the east, south, and west. To the north, northwest, and northeast, as well as further south from the site is the Natural Preservation (N) Future Land Use category. Further southwest, there is Suburban Mixed Use-6 (SMU-6).
- The subject property is currently vacant. The area abutting the site to the east and west is agricultural. To the north there are public/quasi-public institutional uses owned by the Southwest Florida Water Management District. To the south it is mostly developed with single-family residential homes, vacant, public/quasi-public/institutional, and agricultural uses. There are a variety of existing land uses further east with mobile home parks, light commercial, homeowner associations, vacant, public communications/utilities, and single-family residential uses.
- The site is currently zoned as Agricultural Single-Family (AS-1) and Agricultural Rural (AR). Directly south from the property is Residential Single-Family-2 (RSC-2), Agricultural Single-Family Conventional-1 (ASC-1), and AS-1. Land to the north, west, and east is zoned as AR. Further south, there are Residential Single-Family Conventional-6 (RSC-6), Residential Single-Family Conventional-4 (RSC-4), and AS-1 uses. Further east from the site are ASC-1, RSC-2, AR, AS-1, as well as Commercial Neighborhood (CN), RSC-6 and Planned Development (PD) uses.
- The applicant is requesting a rezoning from Agricultural Rural (AR) and Agricultural Single-Family (AS-1) to a Planned Development (PD) to allow for a 10-unit single family residential development.
- The property is adjacent to Significant Wildlife Habitat to the north.

## **Compliance with Comprehensive Plan:**

The following Goals, Objectives and Policies apply to this rezoning request and are used as a basis for a consistency finding.

## **FUTURE LAND USE ELEMENT**

### **URBAN SERVICE AREA**

**Policy 1.4:** *Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and architecture. Compatibility does not mean “the same as.” Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.*

### **RURAL AREA**

**Objective 4:** *The Rural Area will provide areas for long term, agricultural uses and large lot, low density rural residential uses which can exist without the threat of urban or suburban encroachment, with the goal that no more than 20% of all population growth within the County will occur in the Rural Area.*

**Policy 4.1:** *Rural Area Densities Within rural areas, densities shown on the Future Land Use Map will be no higher than 1 du/5 ga unless located within an area identified with a higher density land use category on the Future Land Use Map as a suburban enclave, planned village, a Planned Development pursuant to the PEC ½ category, or rural community which will carry higher densities.*

### **Land Use Categories**

**Objective 8:** *The Future Land Use Map will include Land Use Categories which outline the maximum level of intensity or density and range of permitted land uses allowed and planned for an area. A table of the land use categories and description of each category can be found in Appendix A.*

**Policy 8.1:** *The character of each land use category is defined by building type, residential density, functional use, and the physical composition of the land. The integration of these factors sets the general atmosphere and character of each land use category. Each category has a range of potentially permissible uses which are not exhaustive, but are intended to be illustrative of the character of uses permitted within the land use designation. Not all of those potential uses are routinely acceptable anywhere within that land use category.*

### **Relationship To Land Development Regulations**

**Objective 9:** *All existing and future land development regulations shall be made consistent with the Comprehensive Plan, and all development approvals shall be consistent with those development regulations as per the timeframe provided for within Chapter 163, Florida Statutes. Whenever feasible and consistent with Comprehensive Plan policies, land development regulations shall be designed to provide flexible, alternative solutions to problems.*

**Policy 9.1:** *Each land use plan category shall have a set of zoning districts that may be permitted within that land use plan category, and development shall not be approved for zoning that is inconsistent with the plan.*



**Policy 9.2:** *Developments must meet or exceed the requirements of all land development regulations as established and adopted by Hillsborough County, the state of Florida and the federal government unless such requirements have been previously waived by those governmental bodies.*

### **Neighborhood/Community Development**

**Objective 16: Neighborhood Protection** *The neighborhood is a functional unit of community development. There is a need to protect existing neighborhoods and communities and those that will emerge in the future. To preserve, protect and enhance neighborhoods and communities, all new development must conform to the following policies.*

**Policy 16.1:** *Established and planned neighborhoods and communities shall be protected by restricting incompatible land uses through mechanisms such as: locational criteria for the placement of non-residential uses as identified in this Plan, limiting commercial development in residential land use categories to neighborhood scale; requiring buffer areas and screening devices between unlike land uses.*

**Policy 16.2:** *Gradual transitions of intensities between different land uses shall be provided for as new development is proposed and approved, through the use of professional site planning, buffering and screening techniques and control of specific land uses.*

**Policy 16.3:** *Development and redevelopment shall be integrated with the adjacent land uses through:*

- a) the creation of like uses; or*
- b) creation of complementary uses; or*
- c) mitigation of adverse impacts; and*
- d) transportation/pedestrian connections*

**Policy 16.7:** *Residential neighborhoods shall be designed to include an efficient system of internal circulation and street stub-outs to connect adjacent neighborhoods together.*

**Policy 16.8:** *The overall density and lot sizes of new residential projects shall reflect the character of the surrounding area, recognizing the choice of lifestyles described in this Plan.*

**Policy 16.9:** *All land use categories allowing residential development may permit clustering of residences within the gross residential density limit for the land use category.*

**Policy 16.10:** *Any density increase shall be compatible with existing, proposed or planned surrounding development. Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and architecture. Compatibility does not mean “the same as”. Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.*

**Community Design Component**

**4.0 COMMUNITY LEVEL DESIGN**  
**4.1 Rural Residential Character**

*Goal 7: Preserve existing rural uses as viable residential alternatives to urban and suburban areas.*

**5.0 NEIGHBORHOOD LEVEL DESIGN**  
**5.1 COMPATIBILITY**

*Goal 12: Design neighborhoods which are related to the predominant character of the surroundings.*

*Objective 12-1: New developments should recognize the existing community and be designed in a way that is compatible (as defined in FLUE policy 1.4) with the established character of the surrounding neighborhood.*

**5.3 RURAL**

*Goal 14: Provide standards within the land development code for development in the rural areas, which allow for developments of a specifically rural character.*

**ONE WATER SECTION**

*Objective 4.3: Limit public potable water and wastewater lines from being extended into the Rural Area, except under specified conditions.*

**Staff Analysis of Goals, Objectives and Policies:**

The 10.67 ± acre subject property is located north of Morris Bridge Road and southwest of Verges Road. The site is in the Rural Area and is not located within the limits of a Community Plan. The subject site’s Future Land Use classification on the Future Land Use Map (FLUM) is Residential-6 (RES-6). The applicant is requesting to rezone the subject property from Agricultural Single-Family (AS-1) and Agricultural Rural (AR) to Planned Development (PD) allowing for a 10-unit single-family residential development on 0.5 acre lots. Per the conditions of approval, the applicant is also requesting pastureland to be allowed in the interim.

Policy 1.4 of the Future Land Use Element (FLUE) promotes compatibility and defines it not solely as “the same as” but similar to development proposals that are consistent with maintaining the existing character of the area. The FLUE permits for new development within the Rural Area that is similar in character to the existing community.

Objective 4 of the Future Land Use Element (FLUE) of the Comprehensive Plan notes that 20% of the growth in the region will occur within the Rural Area. The proposed site is within the RES-6 Future Land Use category; therefore, it is permitted to develop up to a maximum density of 64 units. According to the site-plan and narrative submitted by the applicant dated April 4, 2023, the project proposes to construct only 10 single-family lots at a minimum of a half-acre each. FLUE Policy 4.1 characterizes the Rural Area as low-density, large lot residential. The proposed development is similar in nature to the existing single-

family residential land uses, especially to the south of the site. Thus, the proposed rezoning from AS-1 and AR to PD is consistent with the surrounding area.

FLUE Objective 9 and Policy 9.2 require that all developments be consistent with the Plan and meet all Land Development Regulations in Hillsborough County. The applicant has submitted Section 6.04.02.B Administrative Variance from Section 6.04.7 Land Development Code requirement. In addition, at the time of filing this report, Transportation comments were not yet available in Optix and thus were not taken into consideration for analysis of this request.

The proposed rezoning, including the interim pastureland use, meets the intent of the Neighborhood Protection Policies of FLUE Objective 16 and its accompanying Policies 16.1, 16.2, 16.3, 16.8, 16.9 and 16.10. The development pattern of the surrounding area shows several other single-family and other residential uses along Morris Bridge Road, Verges Road, and the surrounding area. The areas to the south of the site and further east are all existing residential uses and are all zoned for Residential Single-Family Conventional-2, 4, and 6 (RSC-2, RSC-4, and RSC-6). The applicant is proposing cross access points for future connectivity to the east and west. The applicant is also proposing a minimum of 0.5 acre lots which are compatible with surrounding lot sizes, ranging from over 2 acres to under 0.20 acres. A PD rezoning would reflect a development pattern that is aligned with the existing development pattern and consistent with the policy direction of the surrounding area. The proposed development of single family residential would provide a transition in intensity between the urban development further west, residential to the south, and the public/quasi-public/institutional uses to the north and east of the area.

The applicant meets the intent of Policy 16.7. The applicant is proposing an access point to Morris Bridge Road and may be gated north of the access driveway. The applicant proposes emergency access only to the adjacent parcels to the east (Folio 59957.0000) and west (Folio 59954.0050). Vehicular and pedestrian access will be provided to the adjacent folio 59956.5000 located southeast of the site. Internal and external sidewalks on Morris Bridge Road will also be provided.

The Community Design Component (CDC) in the Future Land Use Element provides guidance on residential developments. Goal 7 encourages the preservation of existing rural uses as viable residential alternatives to urban and suburban areas. Goal 12 and Objective 12-1 seek to facilitate patterns of development that are both compatible and related to the predominate character of their surroundings. Goal 14 provides a standard for the rural area which promotes development that is aligned with the rural character of the area. The applicant is proposing natural landscaping with the option for a future fence around the project boundary. Thus, the proposed planned development would be consistent with this policy direction, as it seeks to promote low-density residential options within the Rural Area.

The site is within the Tampa Bay Water Mitigation Area. The applicant has stated the proposed development will comply with the State Uniform Mitigation Assessment Method. The proposed will not extend public potable water and wastewater lines and meets the intent of One Water Objective 4.3. In addition, lots will have a minimum of one-half acre of upland acreage to use septic.

Overall, the proposed rezoning would allow for development that is consistent with the Goals, Objectives, and Policies of the Unincorporated *Hillsborough County Comprehensive Plan* and is compatible with the existing and planned development pattern found in the surrounding area.

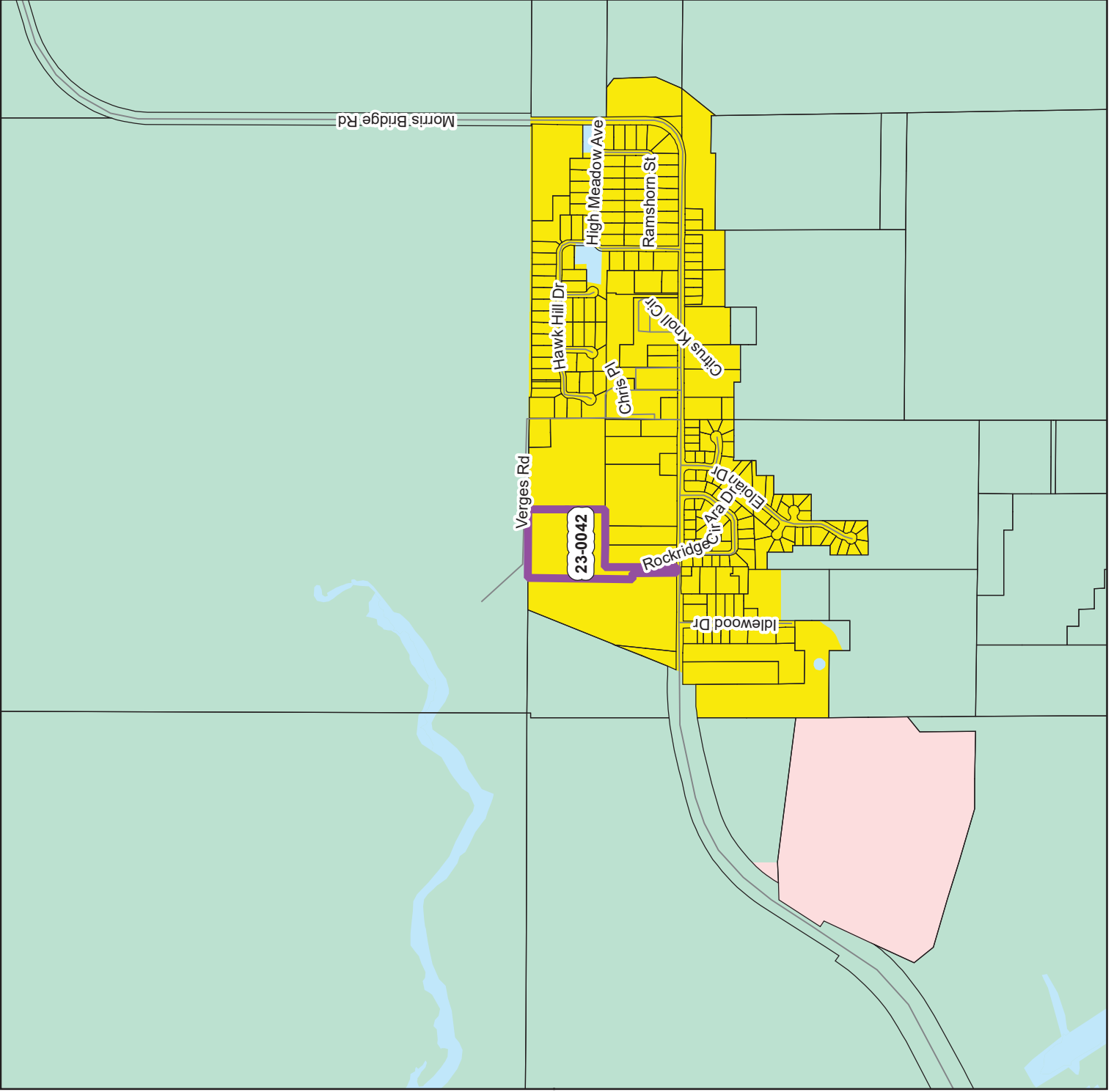
**Recommendation**

Based upon the above considerations, the Planning Commission staff finds the proposed Planned Development **CONSISTENT** with the *Unincorporated Hillsborough County Comprehensive Plan*, subject to the conditions proposed by the Department of Development Services.

# HILLSBOROUGH COUNTY FUTURE LAND USE RZ PD 23-0042

<all other values>

- Rezoning Status Legend:**
- APPROVED (Yellow)
  - CONTINUED (Green)
  - DENIED (Red)
  - WITHDRAWN (Purple)
  - PENDING (Blue)
- Other Features Legend:**
- Tampa Service (Dark Blue)
  - Urban Service (Medium Blue)
  - Shoreline (Light Blue)
  - County Boundary (Dashed Blue)
  - Jurisdiction Boundary (Dotted Blue)
  - Roads (Grey)
  - Parcels (Thin Black)
  - WATER NATURAL LULC, Wet Poly (Light Green)
  - AGRICULTURAL/MINING-1/20 (.25 FAR) (Light Green)
  - PEC PLANNED ENVIRONMENTAL COMMUNITY-1/2 (.25 FAR) (Light Green)
  - AGRICULTURAL-RURAL-1/10 (.25 FAR) (Light Green)
  - AGRICULTURAL ESTATE-1/2.5 (.25 FAR) (Light Green)
  - RESIDENTIAL-1 (.25 FAR) (Light Green)
  - RESIDENTIAL-2 (.25 FAR) (Light Green)
  - RESIDENTIAL-4 (.25 FAR) (Light Green)
  - RESIDENTIAL-6 (.25 FAR) (Light Green)
  - RESIDENTIAL-9 (.35 FAR) (Light Green)
  - RESIDENTIAL-12 (.35 FAR) (Light Green)
  - RESIDENTIAL-16 (.35 FAR) (Light Green)
  - RESIDENTIAL-20 (.35 FAR) (Light Green)
  - RESIDENTIAL-35 (1.0 FAR) (Light Green)
  - NEIGHBORHOOD MIXED USE-4 (3) (.35 FAR) (Light Green)
  - SUBURBAN MIXED USE-6 (.35 FAR) (Light Green)
  - COMMUNITY MIXED USE-12 (.50 FAR) (Light Green)
  - URBAN MIXED USE-20 (1.0 FAR) (Light Green)
  - REGIONAL MIXED USE-35 (2.0 FAR) (Light Green)
  - OC-20 (Light Green)
  - RESEARCH CORPORATE PARK (1.0 FAR) (Light Green)
  - ENERGY INDUSTRIAL PARK (.50 FAR USES OTHER THAN RETAIL, .25 FAR RETAIL/COMMERCE) (Light Green)
  - LIGHT INDUSTRIAL PLANNED (.50 FAR) (Light Green)
  - LIGHT INDUSTRIAL (.50 FAR) (Light Green)
  - HEAVY INDUSTRIAL (.50 FAR) (Light Green)
  - PUBLIC/QUASIPUBLIC (Light Green)
  - NATURAL PRESERVATION (Light Green)
  - WIMAUMA VILLAGE RESIDENTIAL-2 (.25 FAR) (Light Green)
  - CITRUS PARK VILLAGE (Light Green)



**Map Information:**

- Map Printed from Rezoning System: 1/18/2023
- Author: Beverly F. Daniels
- File: G:\Rezoning\System\MapProjects\H\Greg\_H\Rezoning - Copy.mxd

**DISCLAIMER:** This map is for illustrative purposes only. It is not intended to be used as a legal document. The map is based on the most current data and information available at the time of printing. The Planning Commission and the Board of County Commissioners are not responsible for any errors or omissions on this map. All rezoning requests must be approved by the Hillsborough County Planning Commission.







**GENERAL  
SITE PLAN  
FOR  
CERTIFICATION**



**DEVELOPMENT SERVICES**

PO Box 1110, Tampa, FL 33601-1110  
(813) 272-5600

**HILLSBOROUGH COUNTY  
DEVELOPMENT SERVICES DEPARTMENT**

**GENERAL SITE PLAN REVIEW/CERTIFICATION**

**BOARD OF COUNTY  
COMMISSIONERS**

Harry Cohen  
Ken Hagan  
Pat Kemp

Gwendolyn "Gwen" Myers  
Michael Owen  
Donna Cameron Cepeda  
Joshua Wostal

**COUNTY**

**ADMINISTRATOR** Bonnie  
M. Wise **COUNTY**

**ATTORNEY** Christine M.  
Beck **INTERNAL AUDITOR**  
Peggy Caskey

**DEPUTY COUNTY ADMINISTRATOR**

Gregory S. Horwedel

Project Name: Morris Crossings

Zoning File: RZ-PD (23-0042) Modification: None

Atlas Page: None Submitted: 05/11/23

To Planner for Review: 05/11/23 Date Due: ASAP

Contact Person: Anne Pollack, Fletcher Fischer Pollack PL Phone: 813-898-2836/apollack@ffplegal.com

Right-Of-Way or Land Required for Dedication: Yes  No

The Development Services Department HAS NO OBJECTION to this General Site Plan.

The Development Services Department RECOMMENDS DISAPPROVAL of this General Site Plan for the following reasons:

Reviewed by: Tim Lampkin Date: 5-11-23

Date Agent/Owner notified of Disapproval: \_\_\_\_\_





# **AGENCY COMMENTS**

## AGENCY REVIEW COMMENT SHEET

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**TO:** Zoning Technician, Development Services Department  
**REVIEWER:** Richard Perez, AICP  
**PLANNING AREA:** East Rural/Northeast

**DATE:** 4/06/2023  
**AGENCY/DEPT:** Transportation  
**PETITION NO:** PD 23-0042

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- This agency has no comments.
- This agency has no objection.
- This agency has no objection, subject to the listed or attached conditions.
- This agency objects for the reasons set forth below.

### CONDITIONS OF ZONING APPROVAL

- Primary vehicular and pedestrian access to the project shall be provided from Morris Bridge Rd., as shown on the PD Site Plan.
- If PD 23-0042 is approved, the County Engineer will approve an Administrative Variance (dated March 30, 2023) which was found approvable by the County Engineer (on April 3, 2023) for minimum access connection spacing. This administrative variance will allow for the location of the project's access connection on Morris Bridge Rd., as shown on the PD site plan.
- Gated emergency access shall be provided to folio#59954.0050 and folio#59957.0000.
- Prior to or concurrent with the initial increment of development, shared access shall be provided to folio#59956.5000 and the driveway serving folio# 59956.5000 shall be removed and restored to County typical section with the County right of way, as shown on the PD site plan.
- As Morris Bridge Rd. is identified on the Hillsborough County Corridor Preservation Plan as a future 4-lane arterial roadway improvement, the developer shall designate +/-25 of right of way preservation along the project frontage on Morris Bridge Rd. as shown on the PD site plan. Building setbacks shall be calculated from the future right-of-way line.
- Notwithstanding anything shown in the PD site plan or in the PD conditions to the contrary, pedestrian access may be permitted anywhere along the project boundary.

### PROJECT SUMMARY AND ANALYSIS

The applicant is requesting to rezone the subject property from Agricultural Single Family 1 (AS-1) and Agricultural Rural (AR) to Planned Development (PD) to construct 10 single family residential lots on +/- 10.67 acres. The site is located on the north side of Morris Bridge Rd., approximately 405 feet east of Idlewood Dr. The Future Land Use designation of the site is Residential 6 (R-6).



***Trip Generation Analysis***

The applicant submitted a trip generation analysis as required by the Development Review Procedures Manual (DRPM). Staff has prepared a comparison of the trips potentially generated under the existing and proposed zoning designations, utilizing a generalized worst-case scenario. Data presented below is based on the Institute of Transportation Engineer’s Trip Generation Manual, 11<sup>th</sup> Edition.

**Approved Zoning:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
AS-1: 1 Units, Single Family Detached (ITE 210)	9	1	1
AR: 1 Units, Single Family Detached (ITE 210)	9	1	1
<b>TOTAL TRIPS</b>	<b>18</b>	<b>2</b>	<b>2</b>

**Proposed Zoning:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
PD: 10 Units, Single Family Detached (ITE 210)	94	7	9

**Trip Generation Difference:**

Zoning, Lane Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
<b>Difference (+/-)</b>	<b>+76</b>	<b>+5</b>	<b>+7</b>

The proposed rezoning will result in an increase in potential trip generation by +76 daily trips, +5 AM peak hour trips, +7 PM peak hour trips.

**TRANSPORTATION INFRASTRUCTURE SERVING THE SITE**

The subject property fronts the northside of Morris Bridge Rd.

Morris Bridge Rd. is a substandard, rural, 2-lane arterial roadway, maintained by the County. The roadway predominately consists of +/-10-foot wide travel lanes with 5-foot paved shoulders/bicycle lanes, no sidewalks and no curb and gutter. The roadway lies within +/- 40 feet of right-of-way.

By policy of the County Engineer, projects that generate less than 11 peak hour trips are considered de minimis provided the roadways meet minimum life safety standards (i.e. 15 feet of pavement in a 20-foot clear area).

Morris Bridge Rd. is identified in the Hillsborough County Corridor Preservation Plan as a future 4-lane arterial roadway. A future 4-lane roadways requires a total of 110 feet of right of way pursuant to County TTM, TS-6, 4-lane divided typical section. The applicant has submitted a PD site plan showing the right of way preservation required being measured 55 feet from the roadway centerline. As such +/-25 feet of corridor preservation will be required to be designated along the project frontage.

## **SITE ACCESS & CONNECTIVITY**

The proposed PD site plan shows primary vehicular and pedestrian access to Morris Bridge Rd via private local roadway. The project frontage is limited to 50 feet and, therefore, does not have any other location to construct the project access. This proposed access connection does not meet the minimum spacing criteria of 245 feet pursuant to LDC, Sec. 6.04.07. The applicant has submitted a Sec. 6.04.02. B. administrative variance request to allow the proposed vehicular access connection location as discussed in greater detail herein under the section titled Request Administrative Variance.

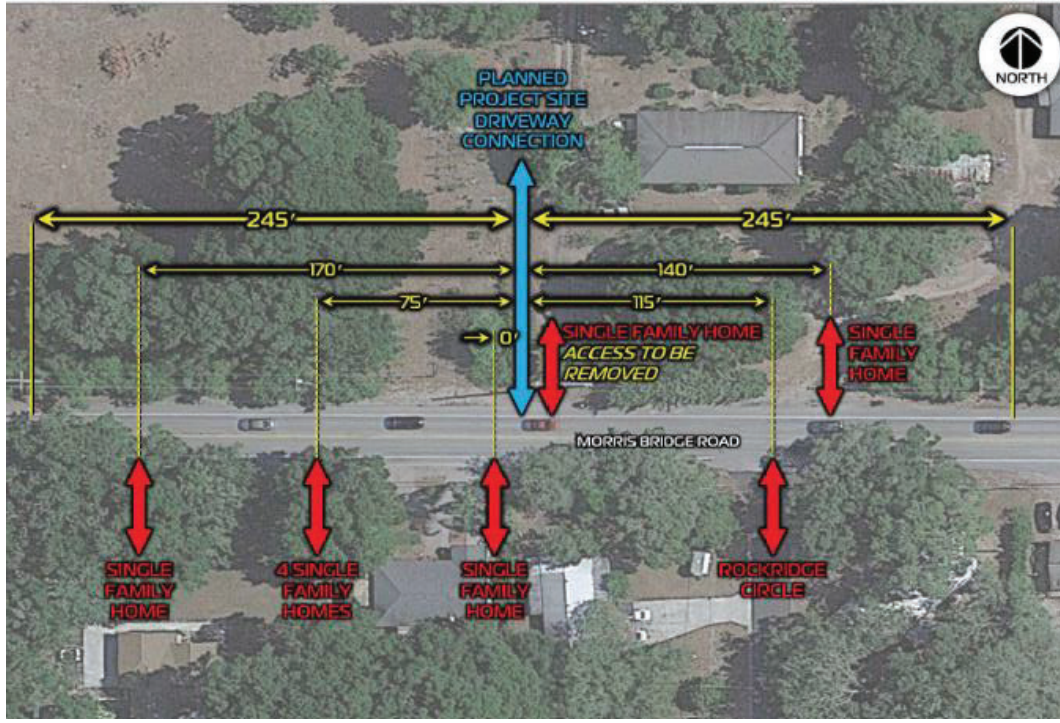
The project proposes to designate the project access as a shared access facility with the adjacent residential property to the east (folio#59956.5000), which also fronts on Morris Bridge Rd. Folio##59956.5000 is currently served by a driveway located approximately +/-8 feet from the project's proposed access connection. At the time of construction, the developer will be required to construct a driveway access to serve Folio##59956.5000 and remove the existing driveway on Morris Bridge Rd. and restore it to Hillsborough County typical standard.

The proposed PD site plan provides emergency access connections to the adjacent residentially designated properties to the east (folio#59957.0000) and west (folio#59954.0050) consistent with LDC, Sec. 6.02.01. H. The emergency access shall be gated and accessible via a Knox Box Rapid Entry System by County Emergency Services.

The PD site plan proposes the internal subdivision road to be private and gated. Said roadway will be constructed consistent with the County Transportation Technical Manual TS-3 local roadway typical section.

## **REQUESTED ADMINISTRATIVE VARIANCE – MORRIS BRIDGE RD. ACCESS SPACING:**

The applicant's Engineer of Record (EOR) submitted a Section 6.04.02.B. Administrative Variance Request (dated March 30, 2023) from the Section 6.04.07. LDC requirement, governing spacing for the proposed Morris Bridge Rd. access. Per the LDC, Morris Bridge Rd. is a Class 6 roadway, which requires minimum connection spacing of 245 feet. The applicant is proposing the driveway in a location which is with 245 feet of 3 connections located to the west and 3 connections located to the east shown in the graphic below. Based on factors presented in the Administrative Variance Request, the County Engineer found the request approvable on April 3, 2023. If this rezoning is approved, the County Engineer will approve the above referenced Administrative Variance Request.



**LEVEL OF SERVICE (LOS) INFORMATION**

FDOT Generalized Level of Service				
Roadway	From	To	LOS Standard	Peak Hr Directional LOS
Morris Bridge Rd.	I-75	Cross Creek Blvd.	D	C

Source: 2022 Hillsborough County Multimodal Level of Service (LOS) Report.

**From:** Williams, Michael [WilliamsM@HillsboroughCounty.ORG]  
**Sent:** Monday, April 3, 2023 7:07 PM  
**To:** Michael D. Raysor (mdr@raysor-transportation.com) [mdr@raysor-transportation.com]  
**CC:** Tirado, Sheida [TiradoS@hillsboroughcounty.org]; Perez, Richard [PerezRL@hillsboroughcounty.org]; Lampkin, Timothy [LampkinT@hillsboroughcounty.org]; De Leon, Eleonor [DeLeonE@hillsboroughcounty.org]; PW-CEIntake [PW-CEIntake@hillsboroughcounty.org]; Anne Pollack [apollack@ffplegal.com]  
**Subject:** FW: RZ PD 23-0042 Administrative Variance  
**Attachments:** 23-0042 AVAddIn 03-31-23.pdf

**Importance:** High

Mike,

I have found the attached Section 6.04.02.B. Administrative Variance (AV) for PD 23-0042 APPROVABLE.

Please note that it is you (or your client's) responsibility to follow-up with my administrative assistant, Eleonor De Leon ([DeLeonE@hillsboroughcounty.org](mailto:DeLeonE@hillsboroughcounty.org) or 813-307-1707) after the BOCC approves the PD zoning or PD zoning modification related to below request. This is to obtain a signed copy of the DE/AV.

If the BOCC denies the PD zoning or PD zoning modification request, staff will request that you withdraw the AV/DE. In such instance, notwithstanding the above finding of approvability, if you fail to withdraw the request, I will deny the AV/DE (since the finding was predicated on a specific development program and site configuration which was not approved).

Once I have signed the document, it is your responsibility to submit the signed AV/DE(s) together with your initial plat/site/construction plan submittal. If the project is already in preliminary review, then you must submit the signed document before the review will be allowed to progress. Staff will require resubmittal of all plat/site/construction plan submittals that do not include the appropriate signed AV/DE documentation.

Lastly, please note that it is critical to ensure you copy all related correspondence to [PW-CEIntake@hillsboroughcounty.org](mailto:PW-CEIntake@hillsboroughcounty.org)

Mike

**Michael J. Williams, P.E.**  
**Director, Development Review**  
**County Engineer**  
Development Services Department

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P: (813) 307-1851  
M: (813) 614-2190  
E: [WilliamsM@HillsboroughCounty.org](mailto:WilliamsM@HillsboroughCounty.org)  
W: HCFLGov.net

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**Hillsborough County**

601 E. Kennedy Blvd., Tampa, FL 33602

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**From:** Tirado, Sheida <TiradoS@hillsboroughcounty.org>  
**Sent:** Friday, March 31, 2023 9:49 AM  
**To:** Williams, Michael <WilliamsM@HillsboroughCounty.ORG>  
**Cc:** Perez, Richard <PerezRL@hillsboroughcounty.org>  
**Subject:** RZ PD 23-0042 Administrative Variance  
**Importance:** High

Hello Mike,

The attached Administrative Variance is approvable to me. Please include the following people in your response email:

[mdr@raysor-transportation.com](mailto:mdr@raysor-transportation.com)  
[apollack@ffplegal.com](mailto:apollack@ffplegal.com)  
[lampkint@hillsboroughcounty.org](mailto:lampkint@hillsboroughcounty.org)  
[perezrl@hillsboroughcounty.org](mailto:perezrl@hillsboroughcounty.org)

Best Regards,

**Sheida L. Tirado, PE** *(she/her/hers)*  
**Transportation Review Manager**  
Development Services Department

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P: (813) 276-8364  
E: [tirados@HCFLGov.net](mailto:tirados@HCFLGov.net)  
W: [HCFLGov.net](http://HCFLGov.net)

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**Hillsborough County**

601 E. Kennedy Blvd., Tampa, FL 33602

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**Hillsborough  
County Florida**  
Development Services

# Supplemental Information for Transportation Related Administrative Reviews

**Instructions:**

- This form must be provided separately for each request submitted (including different requests of the same type).
- This form must accompany all requests for applications types shown below. Staff will not log in or assign cases that are not accompanied by this form, or where the form is partially incomplete.
- A response is required in every field. Blank fields or non-responsive answers will result in your application being returned.
- All responses must be typed.
- Please contact Ingrid Padron at [padroni@hcpaf.fl.gov](mailto:padroni@hcpaf.fl.gov) or via telephone at (813) 307-1709 if you have questions about how to complete this form.

<b>Request Type</b> (check one)	<input type="checkbox"/> Section 6.04.02.B. Administrative Variance <input type="checkbox"/> Technical Manual Design Exception Request <input type="checkbox"/> Alternative Parking Plan Request (Reference LDC Sec. 6.05.02.G3.) <input type="checkbox"/> Request for Determination of Required Parking for Unlisted Uses (Reference LDC Sec. 6.05.02.G.1. and G.2.)		
<b>Submittal Type</b> (check one)	<input type="checkbox"/> New Request	<input type="checkbox"/> Revised Request	<input type="checkbox"/> Additional Information
<b>Submittal Number and Description/Running History</b> (check one and complete text box using instructions provided below)	<input type="checkbox"/> 1.	<input type="checkbox"/> 2.	<input type="checkbox"/> 3.
		<input type="checkbox"/> 4.	<input type="checkbox"/> 5.
		<input type="checkbox"/> 6.	
<p><b>Important:</b> To help staff differentiate multiple requests (whether of the same or different type), please use the above fields to assign a unique submittal number/name to each separate request. Previous submittals relating to the same project/phase shall be listed using the name and number previously identified. It is critical that the applicant reference this unique name in the request letter and subsequent filings/correspondence. If the applicant is revising or submitting additional information related to a previously submitted request, then the applicant would check the number of the previous submittal.</p>			
<b>Project Name/ Phase</b>			
<p><b>Important:</b> The name selected must be used on all future communications and submittals of additional/revised information relating to this variance. If request is specific to a discrete phase, please also list that phase.</p>			
<b>Folio Number(s)</b>			
<input type="checkbox"/> Check This Box If There Are More Than Five Folio Numbers			
<p><b>Important:</b> List all folios related to the project, up to a maximum of five. If there are additional folios, check the box to indicate such. Folio numbers must be provided in the format provided by the Hillsborough County Property Appraiser's website (i.e. 6 numbers, followed by a hyphen, followed by 4 additional numbers, e.g. "012345-6789"). Multiple records should be separated by a semicolon and a space e.g. "012345-6789; 054321-9876").</p>			
<b>Name of Person Submitting Request</b>			
<p><b>Important:</b> For Design Exception (DE) Requests, the person submitting must be a Professional Engineer (PE) licensed within the state of Florida. The DE request letter must be signed and sealed.</p>			
<b>Current Property Zoning Designation</b>			
<p><b>Important:</b> For Example, type "Residential Multi-Family Conventional – 9" or "RMC-9". This is not the same as the property's Future Land Use (FLU) Designation. Typing "N/A" or "Unknown" will result in your application being returned. This information may be obtained via the Official Hillsborough County Zoning Atlas, which is available at <a href="https://maps.hillsboroughcounty.org/maphillsborough/maphillsborough.html">https://maps.hillsboroughcounty.org/maphillsborough/maphillsborough.html</a>. For additional assistance, please contact the <a href="#">Zoning Counselors</a> at the Center for Development Services at (813) 272-5600 Option 3.</p>			
<b>Pending Zoning Application Number</b>			
<p><b>Important:</b> If a rezoning application is pending, enter the application number preceded by the case type prefix, otherwise type "N/A" or "Not Applicable". Use PD for PD rezoning applications, MM for major modifications, PRS for minor modifications/personal appearances.</p>			
<b>Related Project Identification Number (Site/Subdivision Application Number)</b>			
<p><b>Important:</b> This 4-digit code is assigned by the Center for Development Services Intake Team for all Certified Parcel, Site Construction, Subdivision Construction, and Preliminary/Final Plat applications. If no project number exists, please type "N/A" or "Not Applicable".</p>			



March 30, 2023 (Revision No. 2)

Michael J. Williams, P.E.  
County Engineer/Director, Development Review Division  
Hillsborough County Development Services  
601 East Kennedy Boulevard  
Tampa, Florida 33602

**SUBJECT: ACCESS SPACING AV (v1)  
MORRIS CROSSINGS (PD 23-0042)  
ADMINISTRATIVE VARIANCE FOR ACCESS SPACING  
FOLIO No. 059954-0000**

Dear Mr. Williams,

This letter documents a request for a Section 6.04.02.B. **ADMINISTRATIVE VARIANCE** to Hillsborough County Land Development Code (LDC) §6.04.07. (Minimum Spacing) in association with PD 23-0042 for the **MORRIS CROSSINGS** project.

The subject project site is located on the north side of Morris Bridge Road, approximately 1.2 miles east of Interstate 75, in Hillsborough County, Florida; as shown in **ATTACHMENT A**. The project site is currently vacant and is proposed for development consisting of 10 single family residential units, with access to the site planned to be provided via one full access driveway connection to Morris Bridge Road; as shown in **ATTACHMENT B**. It is noted that upon development of the project site, the planned site access driveway connection will replace an existing driveway connection to Morris Bridge Drive for Hillsborough County folio no. 059956-5000; where access to the referenced folio will be restored via new access using the planned site access driveway connection.

The purpose of this variance is to ensure that the development is provided with reasonable access.

The applicable connection spacing criteria for the referenced segment of Morris Bridge Road is identified as 245 feet pursuant to §6.04.07. (Minimum Spacing), for an Access Class 6 facility with a posted speed limit of less than or equal to 45 mph (the posted speed limit is 40 mph).

Within 245 feet from the location of the planned site access driveway connection there are multiple existing driveway connections; with 3 connections located to the west and 3 connections located to the east, as summarized below and shown in **ATTACHMENT C**.

- WEST-1:** SOUTH SIDE OF MORRIS BRIDGE ROAD AT ± 0' FROM PLANNED CONNECTION (WESTERLY OFFSET) [SERVES 1 SINGLE FAMILY HOME]
- WEST-2:** SOUTH SIDE OF MORRIS BRIDGE ROAD AT ± 75' FROM PLANNED CONNECTION [SERVES 4 SINGLE FAMILY HOMES]
- WEST-3:** SOUTH SIDE OF MORRIS BRIDGE ROAD AT ± 170' FROM PLANNED CONNECTION [SERVES 1 SINGLE FAMILY HOME]
- EAST-1\*:** NORTH SIDE OF MORRIS BRIDGE ROAD AT ± 15' FROM PLANNED CONNECTION [SERVES 1 SINGLE FAMILY HOME]
- EAST-2:** SOUTH SIDE OF MORRIS BRIDGE ROAD AT ± 115' FROM PLANNED CONNECTION [ROCKRIDGE CIRCLE]
- EAST-3:** NORTH SIDE OF MORRIS BRIDGE ROAD AT ± 140' FROM PLANNED CONNECTION [SERVES 1 SINGLE FAMILY HOME]

**\*TO BE REMOVED AND REPLACED WITH PLANNED SITE ACCESS DRIVEWAY CONNECTION/CROSS-ACCESS**

**RAYSOR Transportation Consulting**

MICHAEL J. WILLIAMS, P.E.  
ACCESS SPACING AV (v1)  
MORRIS CROSSINGS (PD 23-0042)  
ADMINISTRATIVE VARIANCE FOR ACCESS SPACING  
MARCH 30, 2023 (REVISION NO. 2)  
PAGE 2 OF 3

The peak hour trip generation for the project site was estimated using trip characteristic data pursuant to the Institute of Transportation Engineers (ITE) *Trip Generation Manual* (11<sup>th</sup> edition), as documented in **ATTACHMENT D**; which identified 122 daily trips with 9 trips during the AM peak hour and 11 trips during the PM peak hour.

A crash data evaluation has been prepared, as documented in **ATTACHMENT E**, which indicates 8 crashes within the prior five years on Morris Bridge Road within 500 feet in each direction from the planned site access driveway connection. Five of these crashes occurred at the intersection of Rockridge Circle, with the other 3 crashes occurring at different & distinct locations within the 1,000 foot evaluation area. Of these crashes, 4 were rear-end crashes, 3 were angle crashes, and 1 involved hitting a fixed object.

None of these crashes occurred at the existing connections within 245 feet of the planned site access driveway connection, or were influenced by those existing connections; except for Rockridge Circle, where Rockridge Circle provides access to 26 residential lots, plus connectivity to Ara Drive/Eloian Drive/Rockglen Drive which collectively provide access to an additional 45 residential lots.

This request has been prepared in accordance with LDC §6.04.02.B., to address the following: (a) there is an unreasonable burden on the applicant, (b) the exception would not be detrimental to the public health, safety, and welfare, and (c) without the exception, reasonable access cannot be provided; as discussed herein.

**THERE IS AN UNREASONABLE BURDEN ON THE APPLICANT** as the subject site access driveway connection to Morris Bridge Road is planned for construction in the only location possible, noting that the project site frontage along Morris Bridge Road consists of only 50 feet (i.e., flag lot configuration); where meeting the 245 foot connection spacing standard is not feasible. ***Therefore, approval of this ADMINISTRATIVE VARIANCE is necessary such that an unreasonable burden is not unduly imparted upon the applicant.***

**THE ADMINISTRATIVE VARIANCE WOULD NOT BE DETRIMENTAL TO THE PUBLIC HEALTH, SAFETY, AND WELFARE** in consideration that (A) the trip generation to be served by the subject site access driveway connection is very low at only 11 trips during the worst-case peak hour (7 trips entering & 4 trips exiting), (B) the subject site access driveway connection is replacing an existing driveway connection, and (C) the existing “low volume” site access driveway connections to Morris Bridge Road within 500 feet of the planned site access driveway connection were found to not exhibit safety deficiencies and did not have crash patterns that would indicate a potential for future safety concerns associated with the proposed driveway connection. ***Therefore, approval of this ADMINISTRATIVE VARIANCE would not adversely affect public health, safety, or welfare.***

**WITHOUT THE ADMINISTRATIVE VARIANCE, REASONABLE ACCESS CANNOT BE PROVIDED** as the subject site access driveway connection is planned for construction in the only location possible due to the referenced flag lot configuration. ***Therefore, approval of this ADMINISTRATIVE VARIANCE is necessary to provide reasonable access to the project.***



**RAYSOR Transportation Consulting**

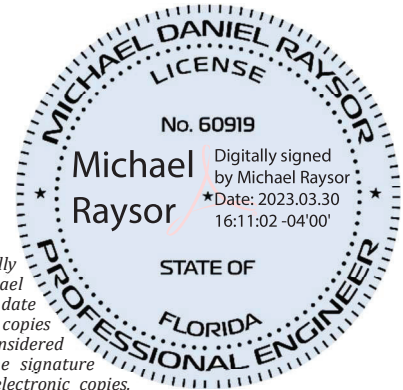
MICHAEL J. WILLIAMS, P.E.  
ACCESS SPACING AV (v1)  
MORRIS CROSSINGS (PD 23-0042)  
ADMINISTRATIVE VARIANCE FOR ACCESS SPACING  
MARCH 30, 2023 (REVISION NO. 2)  
PAGE 3 OF 3

The foregoing documents a request for an **ADMINISTRATIVE VARIANCE** to Hillsborough County Land Development Code (LDC) §6.04.07. (Minimum Spacing) in association with PD 23-0042 for the **MORRIS CROSSINGS** project, to allow for a site access driveway connection at a spacing less than the applicable minimum LDC requirement, and is recommended for approval by the County Engineer.

Sincerely,

RAYSOR Transportation Consulting, LLC

Michael D. Raysor, P.E.  
President



*This item has been digitally signed and sealed by Michael Daniel Raysor P.E., on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.*

**BASED ON THE INFORMATION PROVIDED BY THE APPLICANT, THIS REQUEST IS HEREBY**

**APPROVED**.....

**APPROVED WITH CONDITIONS**.....

**DENIED**.....

---

MICHAEL J. WILLIAMS, P.E., COUNTY ENGINEER date  
HILLSBOROUGH COUNTY DEVELOPMENT REVIEW DIVISION

**MORRIS CROSSINGS**  
**ADMINISTRATIVE VARIANCE**  
*ACCESS SPACING*

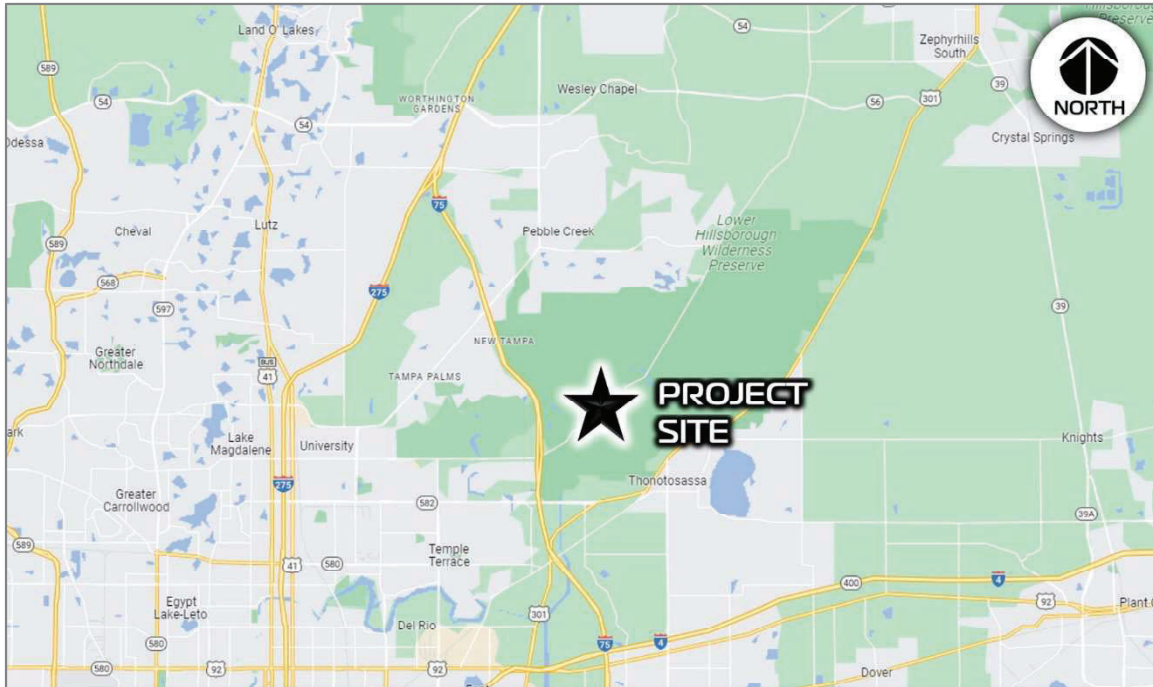
**ATTACHMENT A**  
**PROJECT SITE LOCATION MAP**







**MORRIS CROSSINGS**  
*Project Site Location Map*



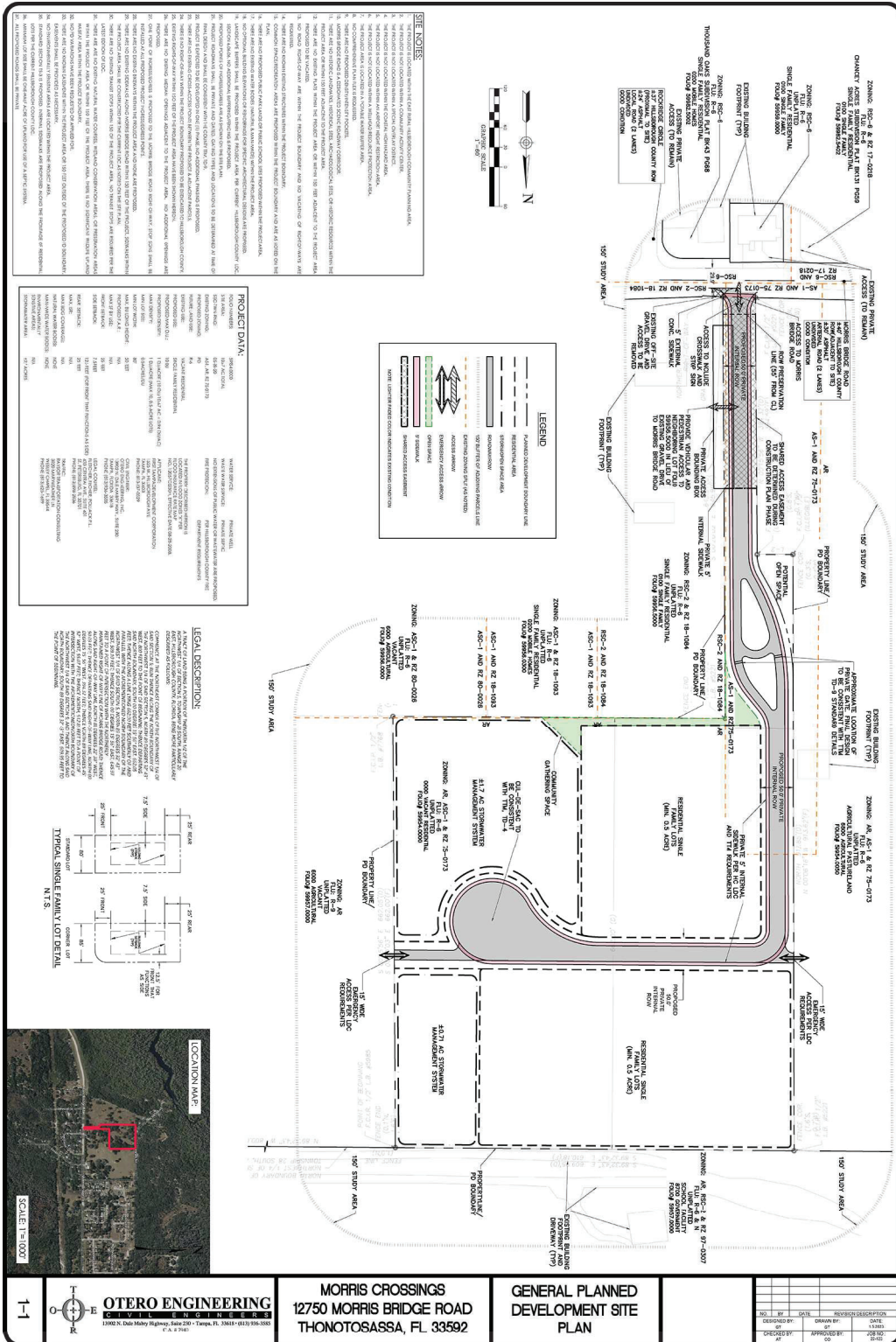
**MORRIS CROSSINGS**  
**ADMINISTRATIVE VARIANCE**  
*ACCESS SPACING*

**ATTACHMENT B**

**PROJECT SITE GENERAL  
DEVELOPMENT PLAN**







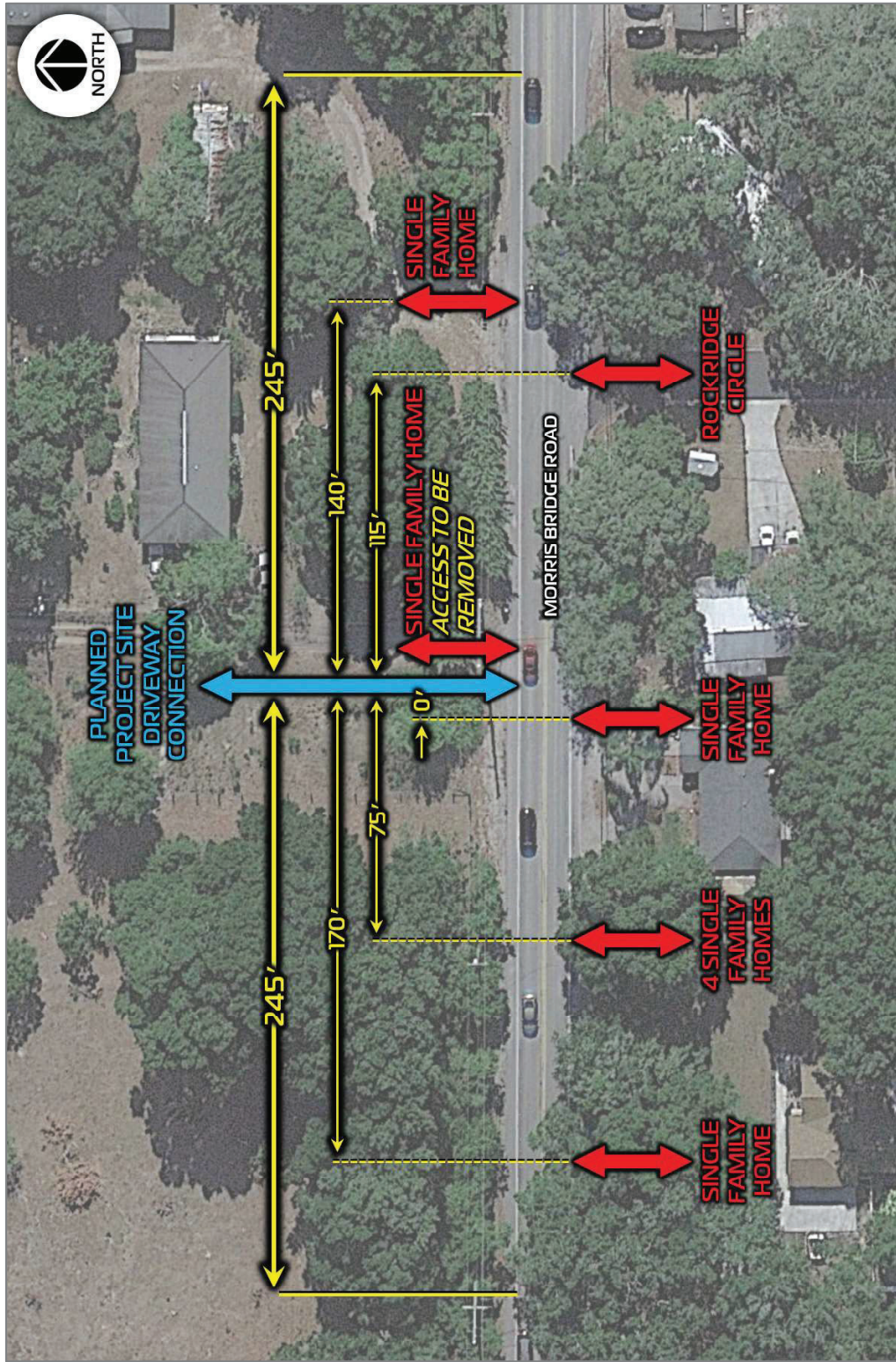
**MORRIS CROSSINGS**  
**ADMINISTRATIVE VARIANCE**  
*ACCESS SPACING*

**ATTACHMENT C**

**CONNECTION  
SPACING DIAGRAM**









**MORRIS CROSSINGS**  
**ADMINISTRATIVE VARIANCE**  
*ACCESS SPACING*

**ATTACHMENT D**

**TRIP GENERATION ESTIMATE**





**MORRIS CROSSINGS**  
Trip Generation Estimate

ITE LUC	Land Use Description	Size	Weekday		AM Peak Hour				PM Peak Hour			
			Formula	Trips	Formula	Trips	Enter	Exit	Formula	Trips	Enter	Exit
210	Single Family Residential	10 units	$\ln(T)=0.92^* \ln(X)+2.68$	122	$\ln(T)=0.91^* \ln(X)+0.12$	9	2	7	$\ln(T)=0.94^* \ln(X)+0.27$	11	7	4

**MORRIS CROSSINGS**  
**ADMINISTRATIVE VARIANCE**  
*ACCESS SPACING*

**ATTACHMENT E**  
**CRASH REPORT**



CDMS - Crash Data Management System

---

5 Year Crash Report

**Report Memo:**

Morris Bridge Road within 500' of Site Driveway



**Selections used to generate this report:**

Date Range: 1/1/2018 - 12/31/2022  
Saved Area 1: Extent(-82.33326133506264,28.08006428107594,-82.32999440448279,28.080338793340612)

5 Year Crash Report

CDMS - Crash Data Management System

Intersection Summary		Injury Severity			Ped/Bike		Crash Type				Strategic Highway Safety Plan															
		Total Crashes	Total Fatalities	Total Serious Injuries	Fatal Crashes	Incap	Non Incap	Possible Injury	Ped	Bike	Angle	Left Turn	Right Turn	Head On	Comm. Veh	Work Zone	No Restraint	Speed Agr. Driving	Lane Depart	At Int.	Distract Driving	Teen Driver 15-19	Aging Driver 65+	Impaired	Motor Cycle	
CR 579 @ ROCKRIDGE CIR	5	0	0	0	0	0	0	1	0	0	2	0	0	0	0	0	0	3	1	2	3	2	1	1	1	0
CR 579 @ ROCKRIDGE CIR	1	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	1	1	0	1	0	0	0	0	0	

\* Total Injuries = Total Incapacitating and Total Non-Incapacitating Injuries. Possible Injuries are not included in total.

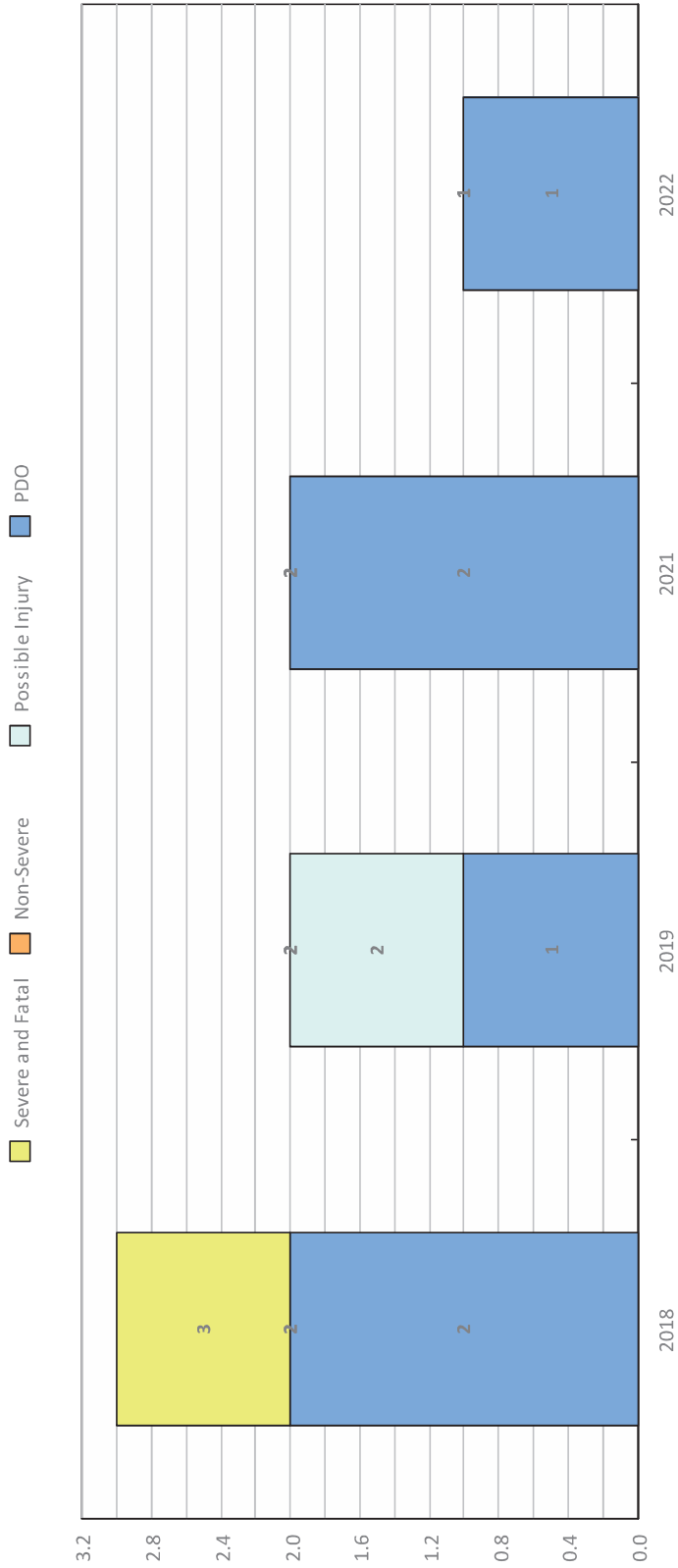
\* Ped and Bike totals are for all crashes involving a Pedestrian and/or Bicycle



5 Year Crash Report

Crashes by Year

CDMS - Crash Data Management System



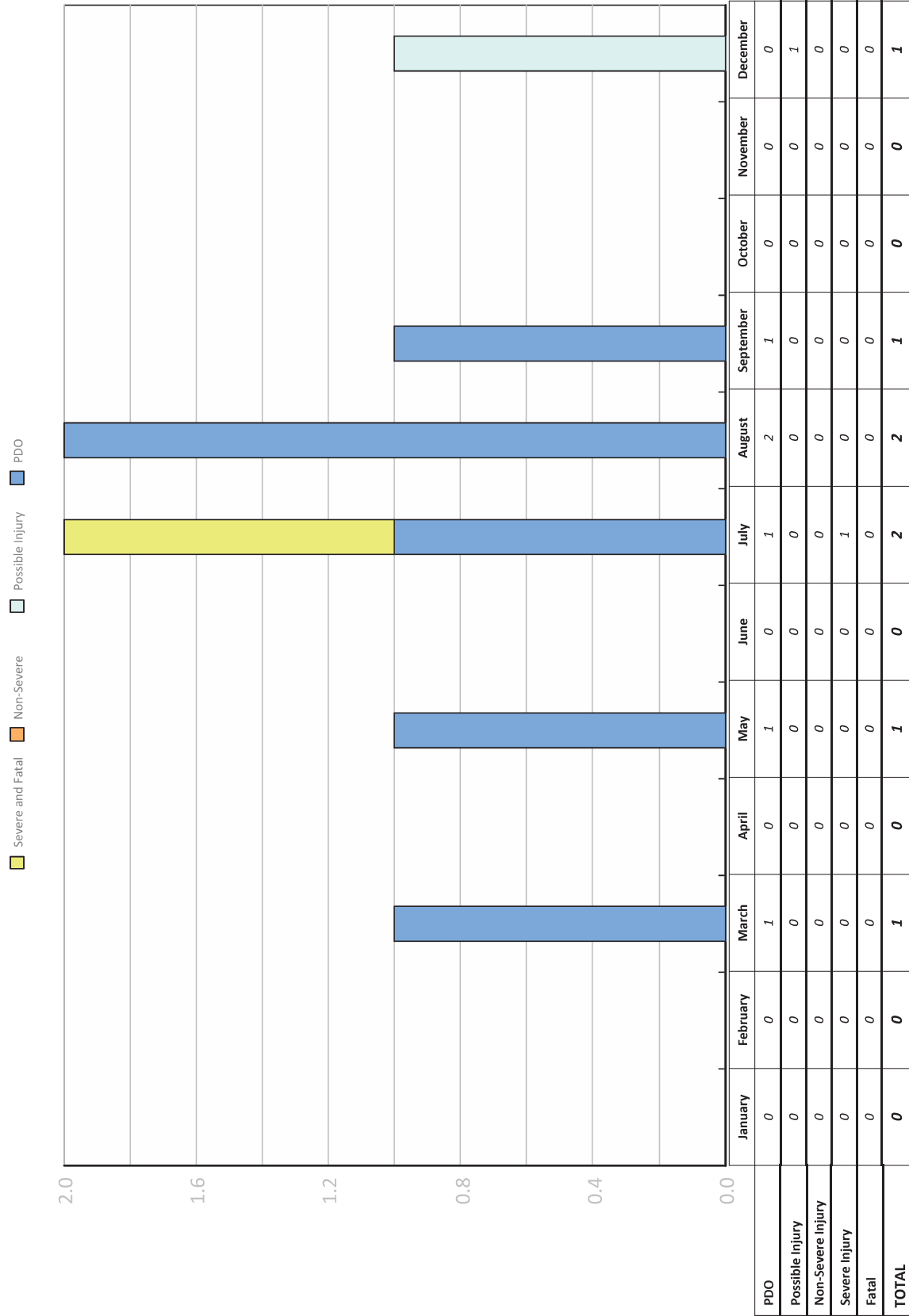
	2018	2019	2021	2022	TOTAL
PDO	2	1	2	1	6
Possible Injury	0	1	0	0	1
Non-Severe Injury	0	0	0	0	0
Severe Injury	1	0	0	0	1
Fatal	0	0	0	0	0
<b>TOTAL</b>	<b>3</b>	<b>2</b>	<b>2</b>	<b>1</b>	<b>8</b>

\* PDO = Property Damage Only

5 Year Crash Report

Crashes by Month

CDMS - Crash Data Management System



\* PDO = Property Damage Only

5 Year Crash Report

CDMS - Crash Data Management System

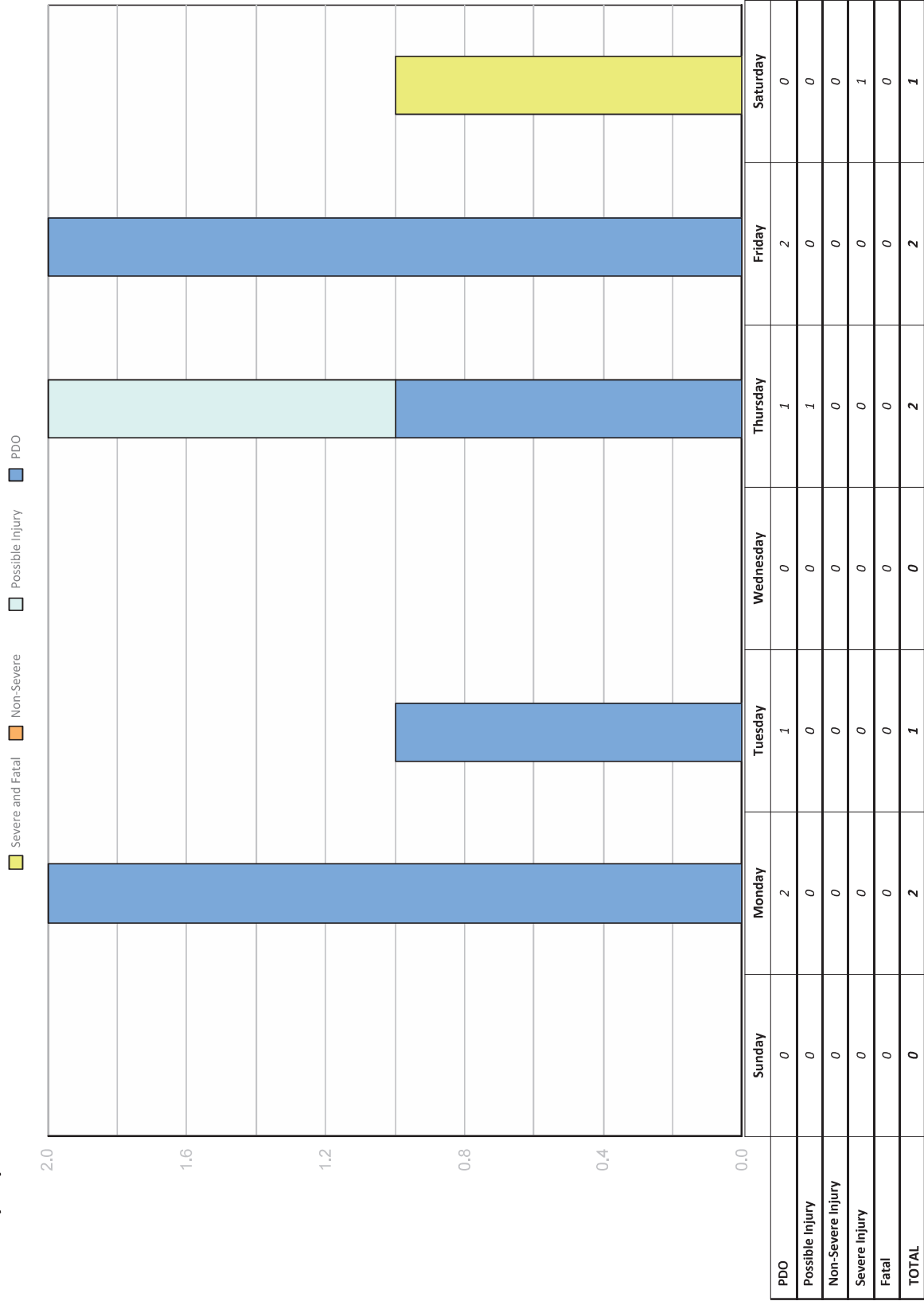
Crashes by Month/Year

	January	February	March	April	May	June	July	August	September	October	November	December
<b>2018</b>												
PDO	0	0	0	0	0	0	0	1	1	0	0	0
Possible Injury	0	0	0	0	0	0	0	0	0	0	0	0
Non-Severe	0	0	0	0	0	0	0	0	0	0	0	0
Severe Injury	0	0	0	0	0	0	1	0	0	0	0	0
Fatal	0	0	0	0	0	0	0	0	0	0	0	0
<b>2019</b>												
PDO	0	0	0	0	0	0	1	0	0	0	0	0
Possible Injury	0	0	0	0	0	0	0	0	0	0	0	1
Non-Severe	0	0	0	0	0	0	0	0	0	0	0	0
Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0
Fatal	0	0	0	0	0	0	0	0	0	0	0	0
<b>2021</b>												
PDO	0	0	0	0	1	0	0	1	0	0	0	0
Possible Injury	0	0	0	0	0	0	0	0	0	0	0	0
Non-Severe	0	0	0	0	0	0	0	0	0	0	0	0
Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0
Fatal	0	0	0	0	0	0	0	0	0	0	0	0
<b>2022</b>												
PDO	0	0	1	0	0	0	0	0	0	0	0	0
Possible Injury	0	0	0	0	0	0	0	0	0	0	0	0
Non-Severe	0	0	0	0	0	0	0	0	0	0	0	0
Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0
Fatal	0	0	0	0	0	0	0	0	0	0	0	0

5 Year Crash Report

CDMS - Crash Data Management System

Crashes by Day of Week



\* PDO = Property Damage Only

5 Year Crash Report

CDMS - Crash Data Management System

Crashes by Month / Day of Week

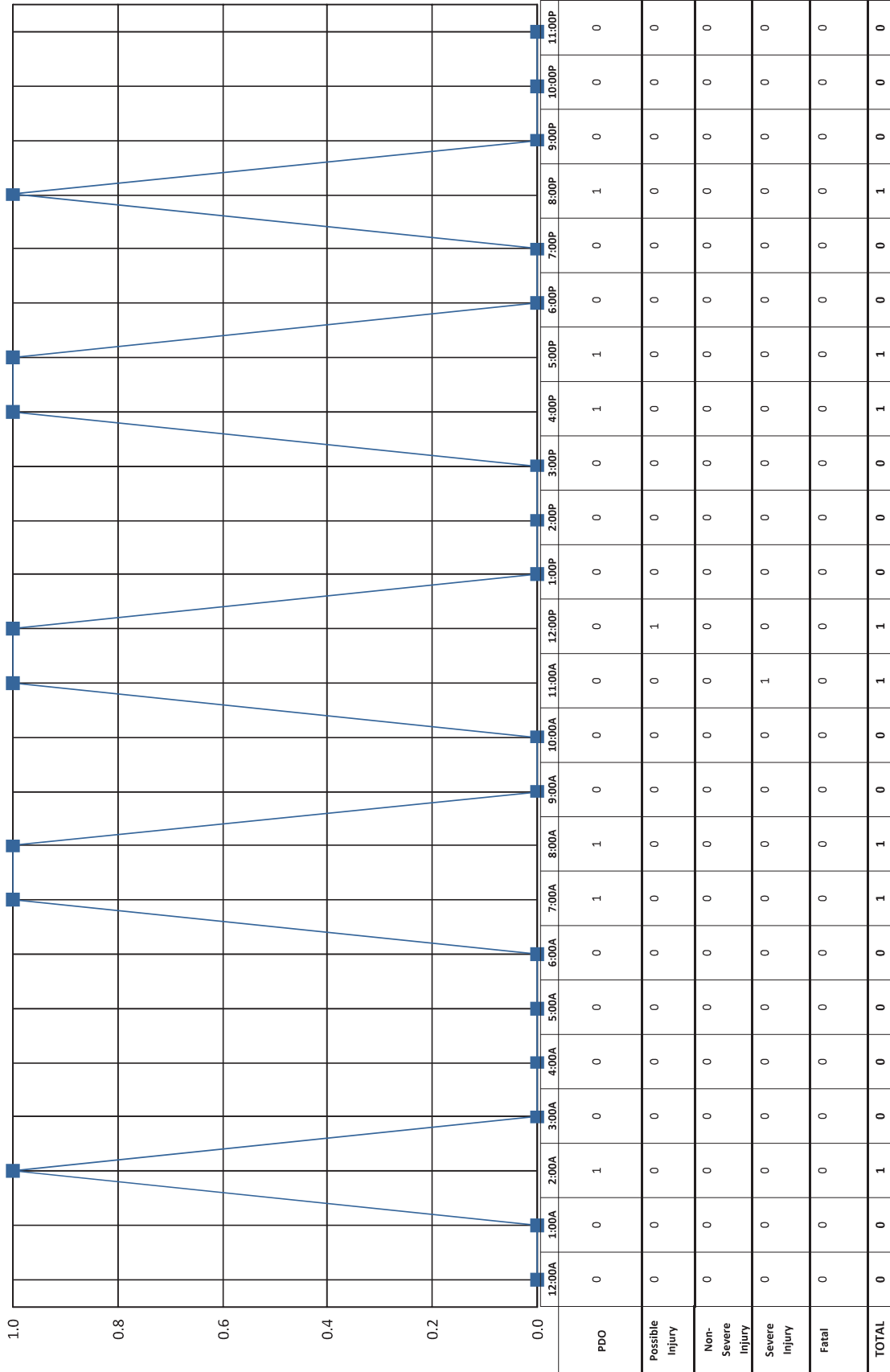
		Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday		Sunday	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday
January	PDO	0	0	0	0	0	0	0	July	0	1	0	0	0	0	0
	Possible Injury	0	0	0	0	0	0	0		Possible Injury	0	0	0	0	0	0
	Non-Severe Injury	0	0	0	0	0	0	0		Non-Severe Injury	0	0	0	0	0	0
	Severe Injury	0	0	0	0	0	0	0		Severe Injury	0	0	0	0	0	0
	Fatal	0	0	0	0	0	0	0		Fatal	0	0	0	0	0	1
	TOTAL	0	0	0	0	0	0	0		TOTAL	0	1	0	0	0	1
February	PDO	0	0	0	0	0	0	0	August	0	0	0	0	0	2	0
	Possible Injury	0	0	0	0	0	0	0		Possible Injury	0	0	0	0	0	0
	Non-Severe Injury	0	0	0	0	0	0	0		Non-Severe Injury	0	0	0	0	0	0
	Severe Injury	0	0	0	0	0	0	0		Severe Injury	0	0	0	0	0	0
	Fatal	0	0	0	0	0	0	0		Fatal	0	0	0	0	0	0
	TOTAL	0	0	0	0	0	0	0		TOTAL	0	0	0	0	2	0
March	PDO	0	0	1	0	0	0	0	September	0	1	0	0	0	0	0
	Possible Injury	0	0	0	0	0	0	0		Possible Injury	0	0	0	0	0	0
	Non-Severe Injury	0	0	0	0	0	0	0		Non-Severe Injury	0	0	0	0	0	0
	Severe Injury	0	0	0	0	0	0	0		Severe Injury	0	0	0	0	0	0
	Fatal	0	0	0	0	0	0	0		Fatal	0	0	0	0	0	0
	TOTAL	0	0	1	0	0	0	0		TOTAL	0	1	0	0	0	0
April	PDO	0	0	0	0	0	0	0	October	0	0	0	0	0	0	0
	Possible Injury	0	0	0	0	0	0	0		Possible Injury	0	0	0	0	0	0
	Non-Severe Injury	0	0	0	0	0	0	0		Non-Severe Injury	0	0	0	0	0	0
	Severe Injury	0	0	0	0	0	0	0		Severe Injury	0	0	0	0	0	0
	Fatal	0	0	0	0	0	0	0		Fatal	0	0	0	0	0	0
	TOTAL	0	0	1	0	0	0	0		TOTAL	0	0	0	0	0	0
May	PDO	0	0	0	0	1	0	0	November	0	0	0	0	0	0	0
	Possible Injury	0	0	0	0	0	0	0		Possible Injury	0	0	0	0	0	0
	Non-Severe Injury	0	0	0	0	0	0	0		Non-Severe Injury	0	0	0	0	0	0
	Severe Injury	0	0	0	0	0	0	0		Severe Injury	0	0	0	0	0	0
	Fatal	0	0	0	0	0	0	0		Fatal	0	0	0	0	0	0
	TOTAL	0	0	0	0	1	0	0		TOTAL	0	0	0	0	0	0
June	PDO	0	0	0	0	1	0	0	December	0	0	0	0	0	0	0
	Possible Injury	0	0	0	0	0	0	0		Possible Injury	0	0	0	0	1	0
	Non-Severe Injury	0	0	0	0	0	0	0		Non-Severe Injury	0	0	0	0	0	0
	Severe Injury	0	0	0	0	0	0	0		Severe Injury	0	0	0	0	0	0
	Fatal	0	0	0	0	0	0	0		Fatal	0	0	0	0	0	0
	TOTAL	0	0	0	0	1	0	0		TOTAL	0	0	0	0	1	0

\* PDO = Property Damage Only

5 Year Crash Report

CDMS - Crash Data Management System

Crashes by Time of Day



\* PDO = Property Damage Only



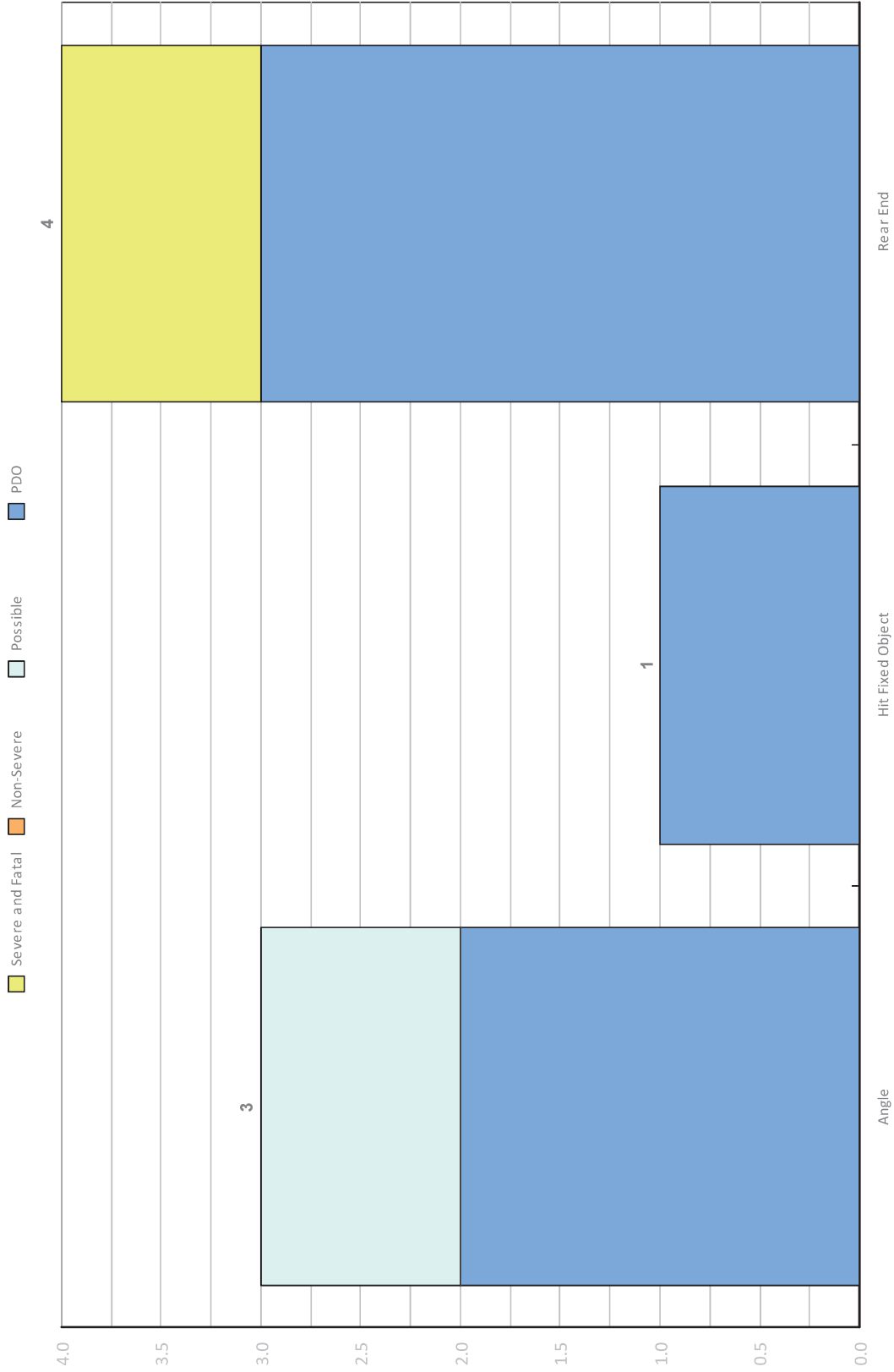
Crashes by Crash Type

	2018	2019	2021	2022	Total
<b>Angle</b>					
PDO	1	0	1	0	2
Possible Inj	0	1	0	0	1
Non Severe	0	0	0	0	0
Severe	0	0	0	0	0
Fatal	0	0	0	0	0
<b>Total</b>	<b>1</b>	<b>1</b>	<b>1</b>	<b>0</b>	<b>3</b>
<b>Rear End</b>					
PDO	1	0	1	1	3
Possible Inj	0	0	0	0	0
Non Severe	0	0	0	0	0
Severe	1	0	0	0	1
Fatal	0	0	0	0	0
<b>Total</b>	<b>2</b>	<b>0</b>	<b>1</b>	<b>1</b>	<b>4</b>

Crashes by Crash Type

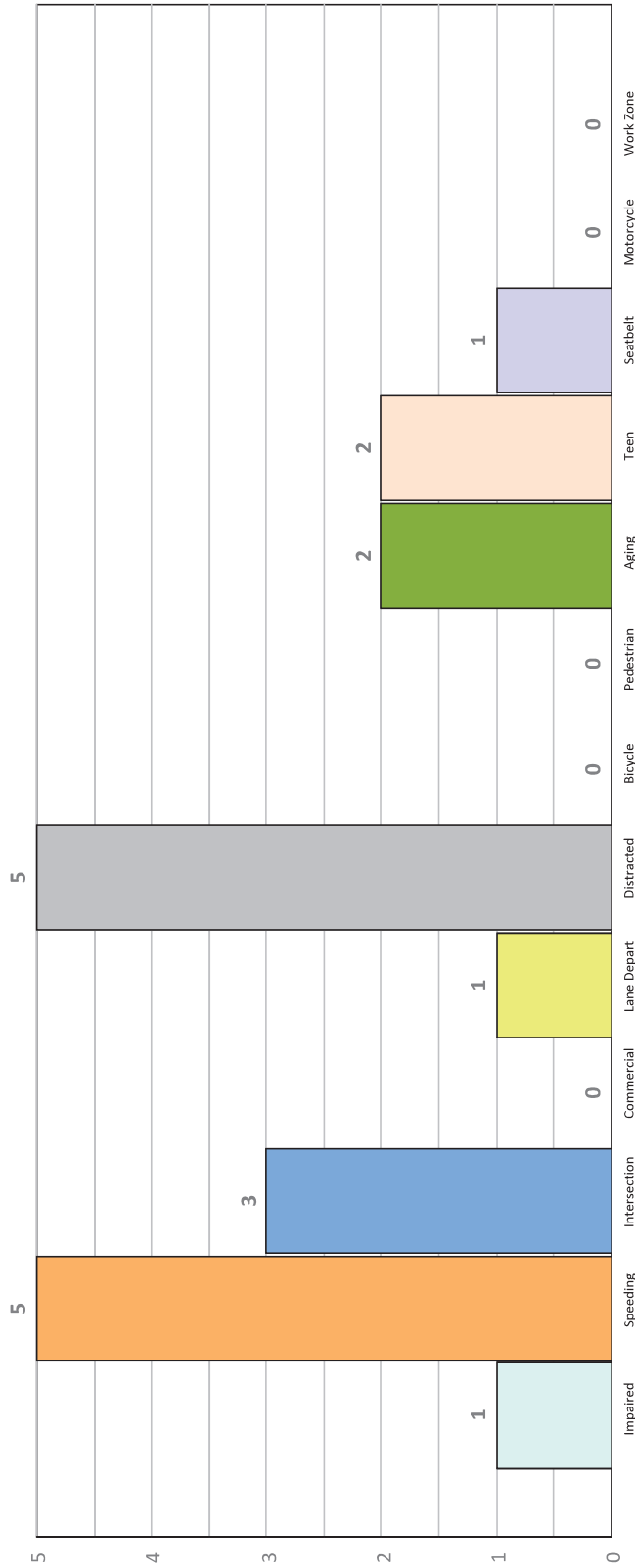
	2019	Total
Hit Fixed Object	1	1
PDO	0	0
Possible Injury	0	0
Non-Severe	0	0
Severe	0	0
Fatal	0	0
<b>Total</b>	<b>1</b>	<b>1</b>

### Crashes by Crash Type



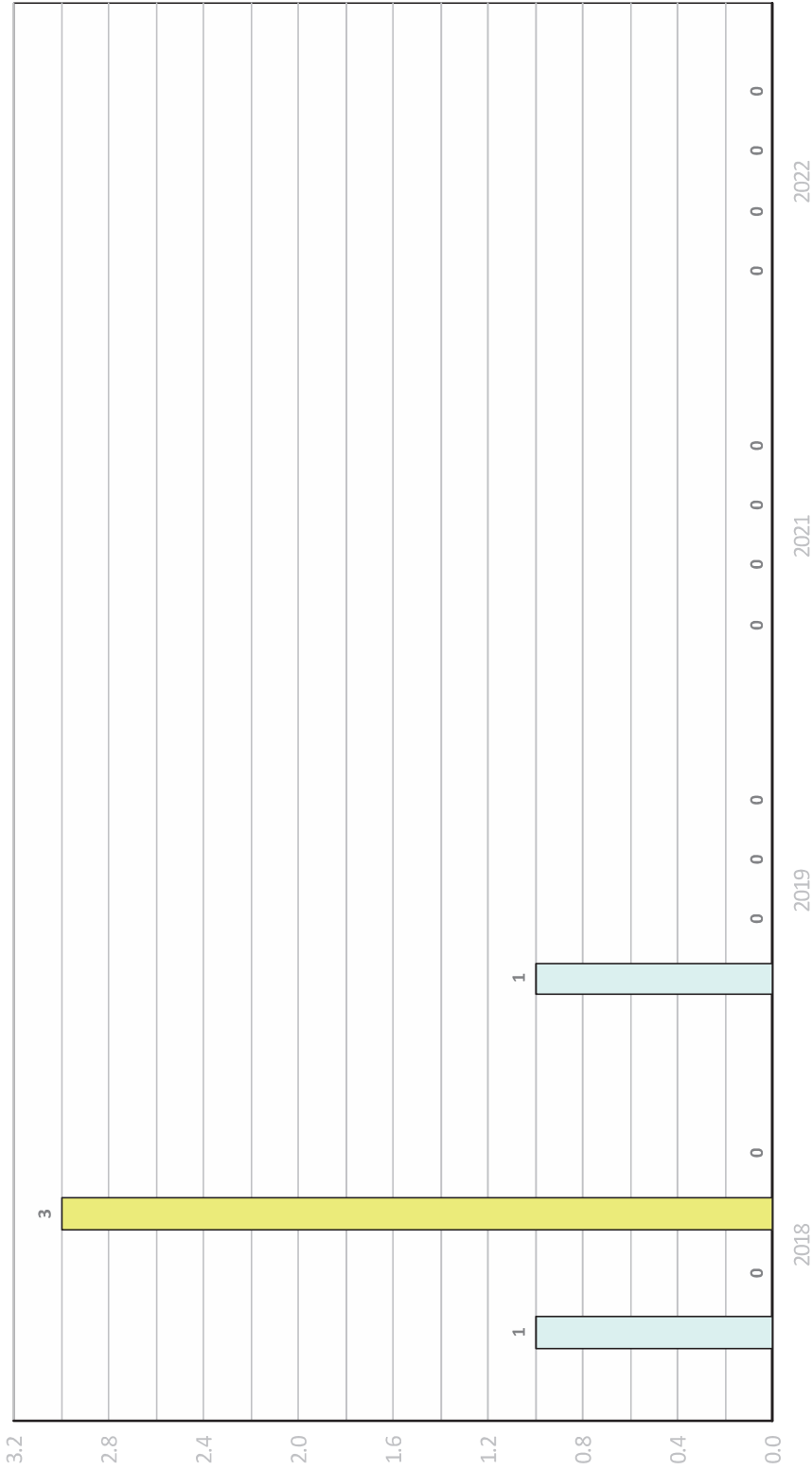
Crashes by Strategic Highway Safety Plan Category

[More Information](#)



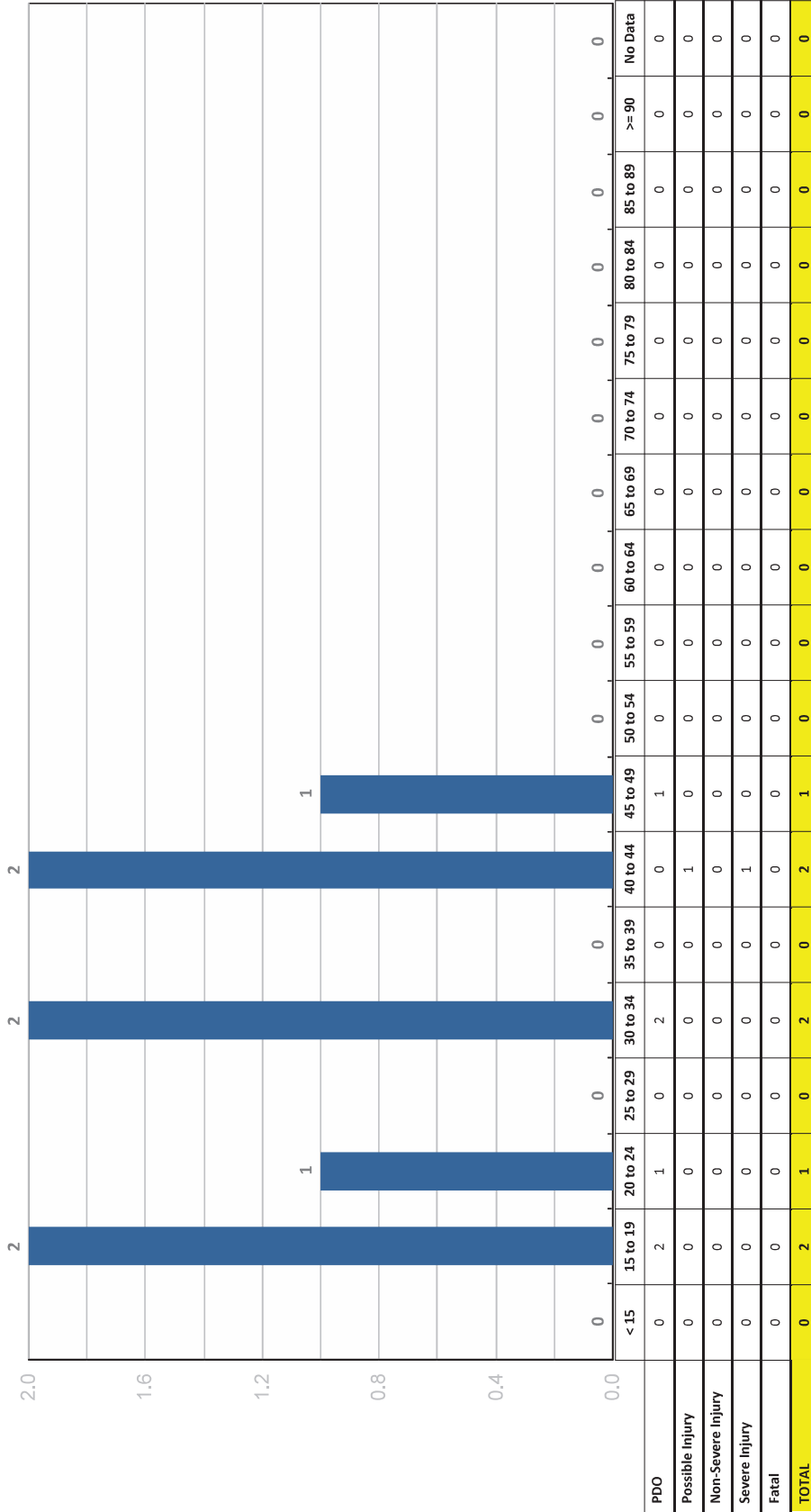
	2018			2019			2021			2022			Total		
	Crashes	Severe	Fatal	Crashes	Severe	Fatal	Crashes	Severe	Fatal	Crashes	Severe	Fatal	Crashes	Severe	Fatal
Impaired	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0
Speeding and Aggressive	2	1	0	2	0	0	1	0	0	0	0	0	5	1	0
Intersection	2	1	0	0	0	0	1	0	0	0	0	0	3	1	0
Commercial	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lane Departure	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0
Distracted	2	0	0	0	0	0	2	0	0	1	0	0	5	0	0
Bicycle Involved	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Pedestrian Involved	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Aging Road User	0	0	0	0	0	0	2	0	0	0	0	0	2	0	0
Teen Driver	1	0	0	1	0	0	0	0	0	0	0	0	2	0	0
Seatbelt	1	1	0	0	0	0	0	0	0	0	0	0	1	1	0
Motorcycle	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Work Zone	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Injuries per Year



	2018	2019	2021	2022	Total
Possible Injuries	1	1	0	0	2
Non-Severe Injuries	0	0	0	0	0
Severe Injuries	3	0	0	0	3
Fatalities	0	0	0	0	0

Crashes by Driver 1 Age



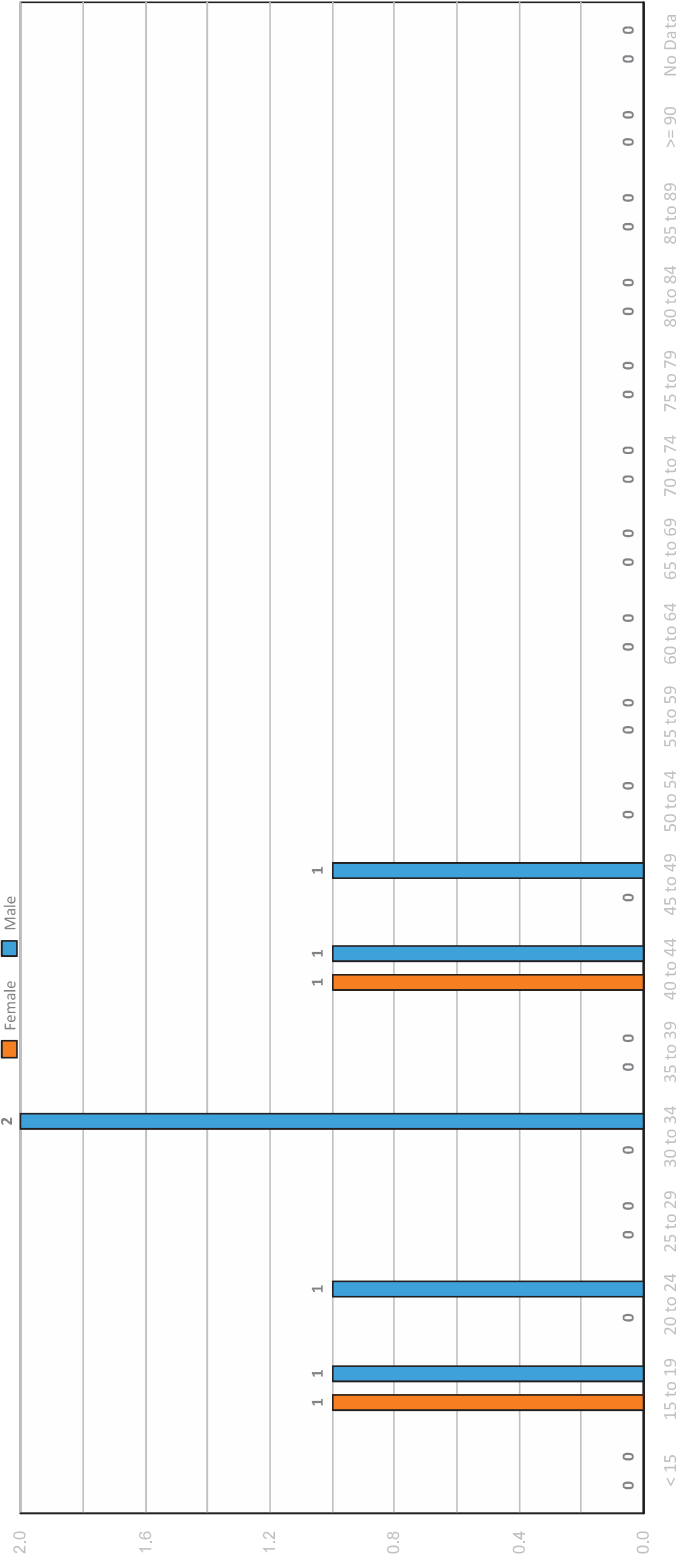
\* PDO = Property Damage Only



5 Year Crash Report

CDMS - Crash Data Management System

Crashes by Driver 1 Age / Gender



	< 15		15 to 19		20 to 24		25 to 29		30 to 34		35 to 39		40 to 44		45 to 49		50 to 54		
	Female	Male	Female	Male	Female	Male	Female	Male	Female	Male	Female	Male	Female	Male	Female	Male	Female	Male	
PDO	0	0	1	1	0	1	0	0	0	2	0	0	0	0	0	0	1	0	0
Possible Injury	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0
Non-Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0
Fatal	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>TOTAL</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>1</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>
	55 to 59		60 to 64		65 to 69		70 to 74		75 to 79		80 to 84		85 to 89		>= 90		No Data		
PDO	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Possible Injury	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Non-Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Severe Injury	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Fatal	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>TOTAL</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

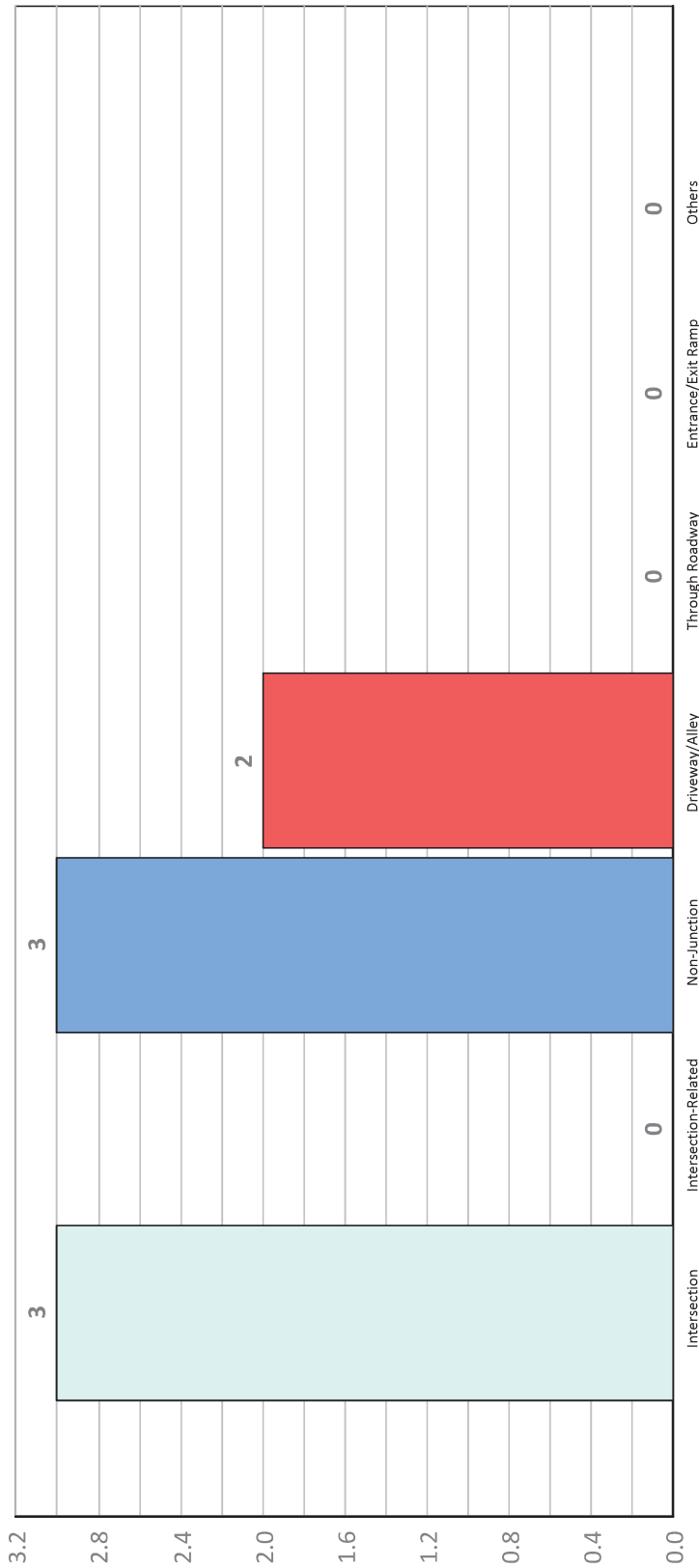
\* PDO = Property Damage Only

5 Year Crash Report

CDMS - Crash Data Management System

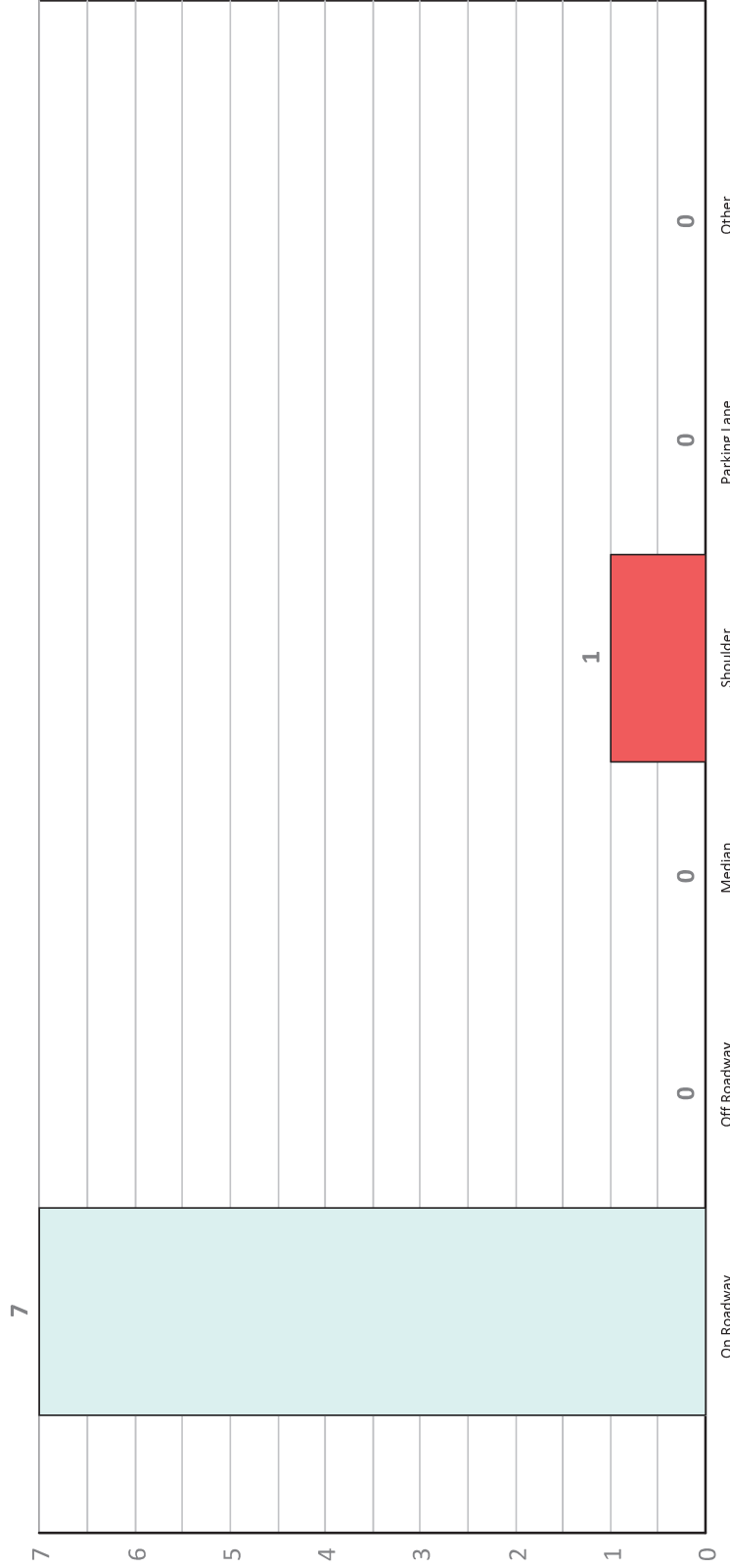
Driver Contributing Cause (Driver 1)		2018	2019	2021	2022	Total
Failed to Keep in Proper Lane	Crashes	0	1	0	0	1
	Severe	0	0	0	0	0
	Fatal	0	0	0	0	0
Failed to Yield Right-of-Way	Crashes	0	1	1	0	2
	Severe	0	0	0	0	0
	Fatal	0	0	0	0	0
Improper Passing	Crashes	1	0	0	0	1
	Severe	1	0	0	0	1
	Fatal	0	0	0	0	0
Operated MV in Careless or Negligent Manner	Crashes	1	0	1	1	3
	Severe	0	0	0	0	0
	Fatal	0	0	0	0	0
Ran Stop Sign	Crashes	1	0	0	0	1
	Severe	0	0	0	0	0
	Fatal	0	0	0	0	0

Relation to Intersection



	2018		2019		2021		2022		Total	
	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal
Intersection	2	1	0	0	1	0	0	0	3	1
Intersection-Related	0	0	0	0	0	0	0	0	0	0
Non-Junction	1	0	1	0	0	0	1	0	3	0
Driveway/Alley	0	0	1	0	1	0	0	0	2	0
Through Roadway	0	0	0	0	0	0	0	0	0	0
Entrance/Exit Ramp	0	0	0	0	0	0	0	0	0	0
Others	0	0	0	0	0	0	0	0	0	0

Location on Roadway

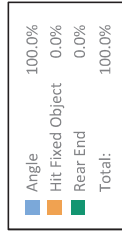
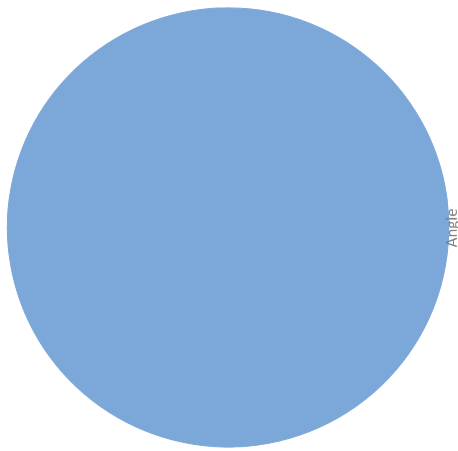


	2018		2019		2021		2022		Total	
	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal
On Roadway	3	1	0	0	2	0	1	0	7	1
Off Roadway	0	0	0	0	0	0	0	0	0	0
Median	0	0	0	0	0	0	0	0	0	0
Shoulder	0	0	1	0	0	0	0	0	1	0
Parking Lane	0	0	0	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	0	0	0	0

5 Year Crash Report

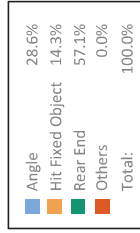
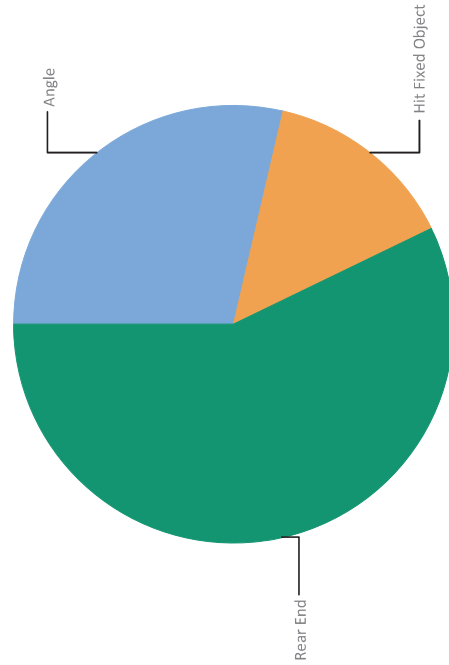
CDMS - Crash Data Management System

Wet Crashes by Crash Type



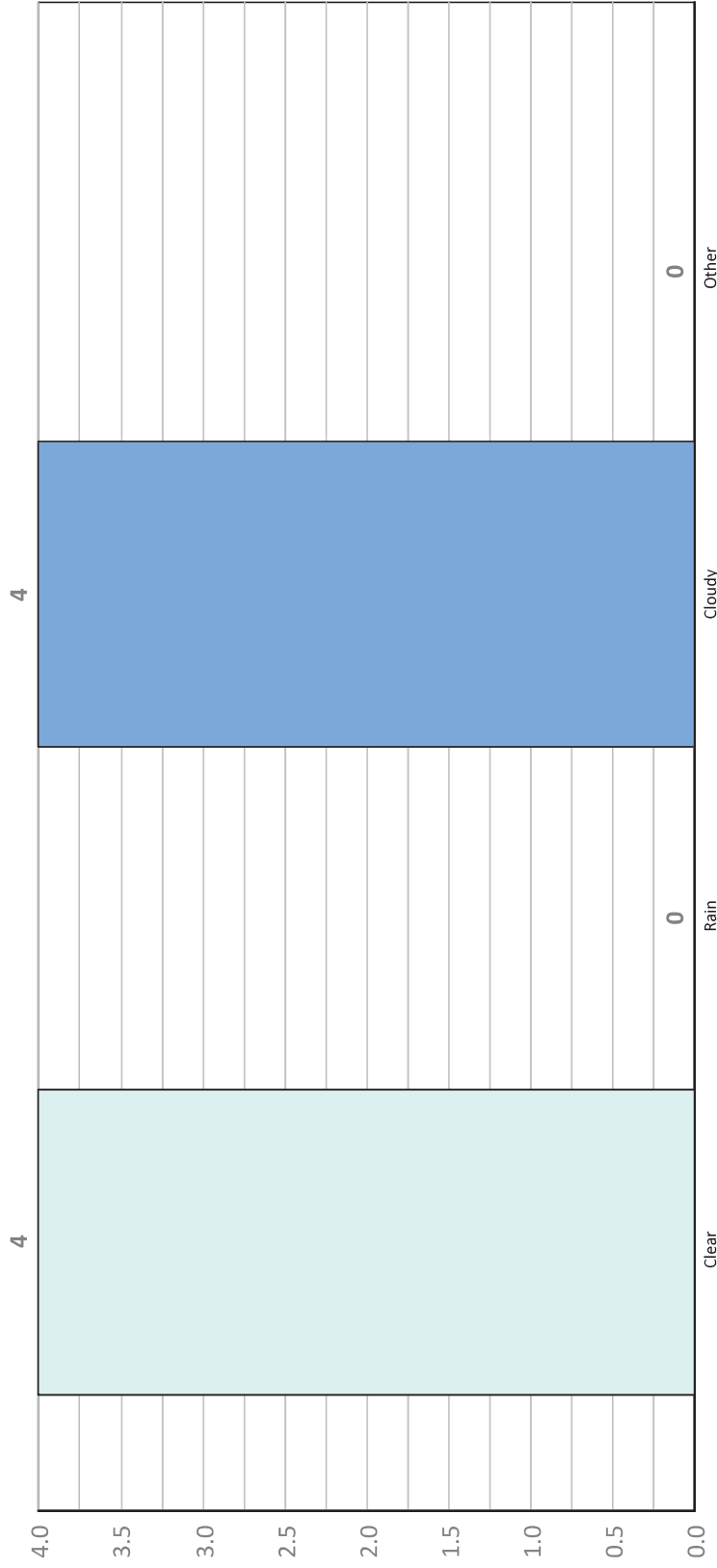
	Wet Crashes		Severe	Fatal
Angle	1	0	0	0
Hit Fixed Object	0	0	0	0
Rear End	0	0	0	0
<b>Total</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>

Dry Crashes by Crash Type



	Dry Crashes		Severe	Fatal
Angle	2	0	0	0
Hit Fixed Object	1	0	0	0
Rear End	4	1	1	0
<b>Total</b>	<b>7</b>	<b>1</b>	<b>1</b>	<b>0</b>

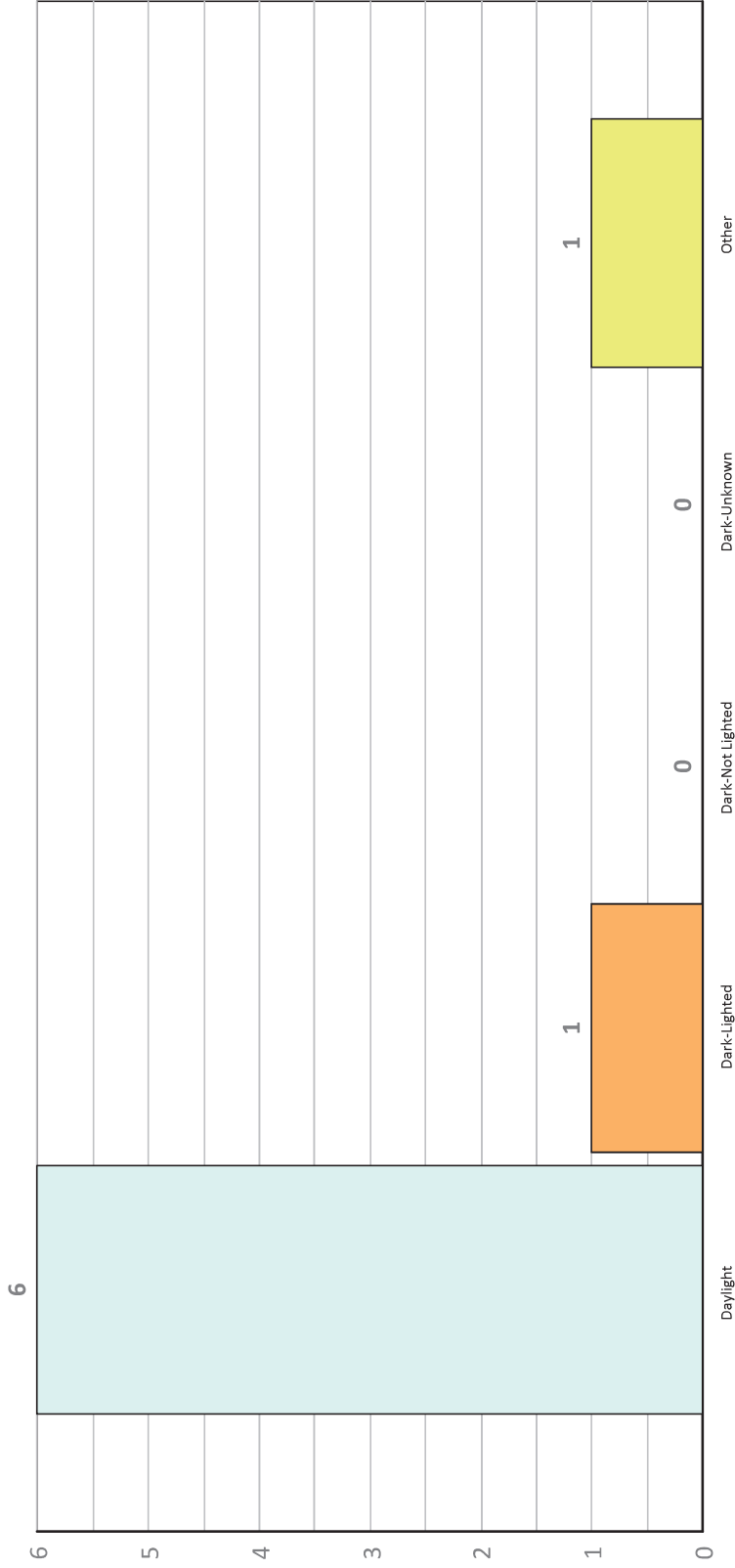
Weather Condition



	2018		2019		2020		2021		2022		Total	
	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal	Crashes	Fatal
Clear	1	0	1	0	1	0	1	0	1	0	4	0
Rain	0	0	0	0	0	0	0	0	0	0	0	0
Cloudy	2	1	1	0	1	0	0	0	0	0	4	1
Other	0	0	0	0	0	0	0	0	0	0	0	0



Lighting Condition



	2018			2019			2021			2022			Total		
	Crashes	Severe	Fatal	Crashes	Severe	Fatal	Crashes	Severe	Fatal	Crashes	Severe	Fatal	Crashes	Severe	Fatal
Daylight	3	1	0	1	0	0	1	0	0	1	0	0	6	1	0
Dark-Lighted	0	0	0	1	0	0	0	0	0	0	0	0	1	0	0
Dark-Not Lighted	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dark-Unknown	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Other	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0

5 Year Crash Report

CDMS - Crash Data Management System

Located Crashes

Area	Crashes	Fatalities	Severe Injuries
E LAKE ORIENT PARK	1	0	0
NEW TAMPA	1	0	3
UNINCORPORATED	2	0	0
UNINCORPORATED H.C.	2	0	0
<b>Totals:</b>	<b>6</b>	<b>0</b>	<b>3</b>

Private Property, Parking Lot, and Unlocated Crashes

Area	Crashes	Fatalities	Severe Injuries
UNINCORPORATED H.C.	2	0	0
<b>Totals:</b>	<b>2</b>	<b>0</b>	<b>0</b>

Transportation Comment Sheet

**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9 OF STAFF REPORT)**

<b>Adjoining Roadways (check if applicable)</b>			
Road Name	Classification	Current Conditions	Select Future Improvements
Morris Bridge Rd.	County Arterial - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input type="checkbox"/> Sufficient ROW Width	<input checked="" type="checkbox"/> Corridor Preservation Plan <input type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

<b>Project Trip Generation</b> <input type="checkbox"/> Not applicable for this request			
	Average Annual Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	18	2	2
Proposed	94	7	9
Difference (+/-)	+76	+5	+7

\*Trips reported are based on net new external trips unless otherwise noted.

<b>Connectivity and Cross Access</b> <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North		None	None	Meets LDC
South	X	None	None	Meets LDC
East		Emergency Access	None	Meets LDC
West		Emergency Access	None	Meets LDC
Notes:				

<b>Design Exception/Administrative Variance</b> <input type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
Morris Bridge Rd./Access Spacing	Administrative Variance Requested	Approvable
Notes: Shared access proposed to eliminate adjacent driveway conflict.		

4.0 Additional Site Information & Agency Comments Summary			
Transportation	Objections	Conditions Requested	Additional Information/Comments
<input checked="" type="checkbox"/> Design Exception/Adm. Variance Requested <input type="checkbox"/> Off-Site Improvements Provided	<input type="checkbox"/> Yes <input type="checkbox"/> N/A <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See report.

**COMMISSION**

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**AGENCY COMMENT SHEET**

REZONING	
<b>HEARING DATE:</b> 6/13/2023  <b>PETITION NO.:</b> 23-0042  <b>EPC REVIEWER:</b> Melissa Yañez  <b>CONTACT INFORMATION:</b> (813) 627-2600 X 1360  <b>EMAIL:</b> <a href="mailto:yanezm@epchc.org">yanezm@epchc.org</a>	<b>COMMENT DATE:</b> 2/2/2023  <b>PROPERTY ADDRESS:</b> 12750 Morris Bridge Rd, Thonotosassa, FL 33592  <b>FOLIO #:</b> 0599540000  <b>STR:</b> 05-28S-20E
<b>REQUESTED ZONING:</b> AR and ASC-1 to PD	
FINDINGS	
<b>WETLANDS PRESENT</b>	NO
<b>SITE INSPECTION DATE</b>	01/25/2023
<b>WETLAND LINE VALIDITY</b>	NA
<b>WETLANDS VERIFICATION (AERIAL PHOTO, SOILS SURVEY, EPC FILES)</b>	No wet per site visit
<b>INFORMATIONAL COMMENTS:</b>  Wetlands Division staff of the Environmental Protection Commission of Hillsborough County (EPC) inspected the above referenced site in order to determine the extent of any wetlands and other surface waters pursuant to Chapter 1-11, Rules of the EPC. This determination was performed using the methodology described within Chapter 62-340, Florida Administrative Code, and adopted into Chapter 1-11. The site inspection revealed that no wetlands or other surface waters exist within the above referenced parcel.  Please be advised this wetland determination is informal and non-binding. A formal wetland delineation may be applied for by submitting a "WDR30 - Delineation Request Application". Once approved, the formal wetland delineation would be binding for five years.	

my / cb

*Environmental Excellence in a Changing World*

**Environmental Protection Commission - Roger P. Stewart Center**  
 3629 Queen Palm Drive, Tampa, FL 33619 - (813) 627-2600 - [www.epchc.org](http://www.epchc.org)



**Adequate Facilities Analysis: Planned Development**

**Date:** 4/3/23 **Acreage:** 10.67 (+/- acres)  
**Jurisdiction:** Hillsborough County **Proposed Zoning:** Planned Development  
**Case Number:** PD 23-0042 **Future Land Use:** RES-6  
**HCPS #:** RZ-512 **Maximum Residential Units:** 10  
**Address:** 17250 Morris Bridge Road **Residential Type:** Single-Family Detached  
**Parcel Folio Number(s):** 059954.000

School Data	Thonotosassa Elementary	Jennings Middle	Armwood High
<b>FISH Capacity</b> Total school capacity as reported to the Florida Inventory of School Houses (FISH)	551	1203	2465
<b>2022-23 Enrollment</b> K-12 enrollment on 2022-23 40 <sup>th</sup> day of school. This count is used to evaluate school concurrency per Interlocal Agreements with area jurisdictions	373	799	2401
<b>Current Utilization</b> Percentage of school capacity utilized based on 40 <sup>th</sup> day enrollment and FISH capacity	68%	66%	97%
<b>Concurrency Reservations</b> Existing concurrency reservations due to previously approved development. Source: CSA Tracking Sheet as of 4/3/23	71	126	64
<b>Students Generated</b> Estimated number of new students expected in development based on adopted generation rates. Source: Duncan Associates, School Impact Fee Study for Hillsborough County, Florida, Dec. 2019	2	1	1
<b>Proposed Utilization</b> School capacity utilization based on 40 <sup>th</sup> day enrollment, existing concurrency reservations, and estimated student generation for application	81%	77%	100%

**Notes:** At this time, adequate capacity exists at Thonotosassa Elementary and Jennings Middle schools for the proposed rezoning. Although Armwood High School is projected to be at capacity given existing approved development and the proposed amendment, state law requires the school district to consider whether capacity exists in adjacent concurrency service areas (i.e., school attendance boundaries). At this time, additional capacity exists in adjacent concurrency service areas at the high school level.

**This is an analysis for adequate facilities only and is NOT a determination of school concurrency. A school concurrency review will be issued PRIOR TO preliminary plat or site plan approval.**

*Andrea A. Stingone*

Andrea A. Stingone, M.Ed.  
 Department Manager, Planning & Siting  
 Growth Management Department  
 Hillsborough County Public Schools  
 E: [andrea.stingone@hcps.net](mailto:andrea.stingone@hcps.net)  
 P: 813.272.4429 C: 813.345.6684



**NOTE:** THIS IS ONLY FOR ESTIMATE PURPOSES, BASED ON THE FEES AT THE TIME THE REVIEW WAS MADE. ACTUAL FEES WILL BE ASSESSED BASED ON PERMIT APPLICATIONS RECEIVED AND BASED ON THE FEE SCHEDULE AT THE TIME OF BUILDING PERMIT APPLICATION.

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**TO:** Zoning Review, Development Services

**DATE:** 04/05/2023

**REVIEWER:** Ron Barnes, Impact & Mobility Fee Coordinator

**APPLICANT:** First Tampa Development Corporation

**PETITION NO:** 23-0042

**LOCATION:** 12750 Morris Bridge Rd

**FOLIO NO:** 59954.0000

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**Estimated Fees:**

Single Family Detached (Fee estimate is based on a 2,000 s.f.)

Mobility: \$13,038 \* 10 = \$130,380

Parks: \$2,145 \* 10 = \$21,450

School: \$8,227 \* 10 = \$82,270

Fire: \$335 \* 10 = \$3,350

Total per House: \$23,745 \* 10 = \$237,450

**Project Summary/Description:**

Rural Mobility, Northeast Parks/Fire - 10 single family homes

**WATER RESOURCE SERVICES  
REZONING REVIEW COMMENT SHEET: WATER & WASTEWATER**

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PETITION NO.: PD23-0042      REVIEWED BY: Randy Rochelle      DATE: 2/6/2023

FOLIO NO.: 59954.0000

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**WATER**

- The property lies within the City of Tampa Water Service Area. The applicant should contact the provider to determine the availability of water service.
  
- A \_\_\_ inch water main exists  (adjacent to the site),  (approximately \_\_\_ feet from the site) \_\_\_\_\_. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.
  
- Water distribution system improvements will need to be completed prior to connection to the County's water system. The improvements include \_\_\_\_\_ and will need to be completed by the \_\_\_\_\_ prior to issuance of any building permits that will create additional demand on the system.

**WASTEWATER**

- The property lies within the City of Tampa Wastewater Service Area. The applicant should contact the provider to determine the availability of wastewater service.
  
- A \_\_\_ inch wastewater force main exists  (adjacent to the site),  (approximately \_\_\_ feet from the site) \_\_\_\_\_. This will be the likely point-of-connection, however there could be additional and/or different points-of-connection determined at the time of the application for service. This is not a reservation of capacity.
  
- Wastewater collection system improvements will need to be completed prior to connection to the County's wastewater system. The improvements include \_\_\_\_\_ and will need to be completed by the \_\_\_\_\_ prior to issuance of any building permits that will create additional demand on the system.

COMMENTS: \_\_\_\_\_.



**AGENCY REVIEW COMMENT SHEET**

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**TO: ZONING TECHNICIAN, Planning Growth Management**

**DATE: 30 January 2023**

**REVIEWER: Bernard W. Kaiser, Conservation and Environmental Lands Management**

**APPLICANT: Anne Pollack**

**PETITION NO: RZ-PD 23-0042**

**LOCATION: Not listed**

**FOLIO NO: 59954.0000**

**SEC: \_\_\_\_\_ TWN: \_\_\_\_\_ RNG: \_\_\_\_\_**

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- This agency has no comments.
  
- This agency has no objection.
  
- This agency has no objection, subject to listed or attached conditions.
  
- This agency objects, based on the listed or attached conditions.

COMMENTS: \_\_\_\_\_.



# **VERBATIM TRANSCRIPT**

HILLSBOROUGH COUNTY, FLORIDA  
BOARD OF COUNTY COMMISSIONERS

-----X  
  )  
IN RE:  )  
  )  
ZONE HEARING MASTER                          )  
HEARINGS                                      )  
  )  
-----X

ZONING HEARING MASTER HEARING  
TRANSCRIPT OF TESTIMONY AND PROCEEDINGS

BEFORE:  Susan Finch  
  Land Use Hearing Master

DATE:  Monday, April 17, 2023

TIME:  Commencing at 6:00 p.m.  
  Concluding at 9:43 p.m.

Reported via Cisco Webex Videoconference by:  
  Diane DeMarsh, CER No. 1654

1 MS. HEINRICH: The last case is Item D.20, PD  
2 Application 23-0042. The applicant is requesting to rezone  
3 property currently zoned AS-1 and AR to PD. Tim Lampkin with  
4 Development Services will provide staff comments after the  
5 applicant's presentation.

6 HEARING MASTER: Good evening.

7 MS. POLLACK: Hi. Good evening. My name is an  
8 Anne Pollack, 433 Central Avenue, Saint Pete, Florida,  
9 representing the applicant, First Tampa Development Corporation.  
10 Tonight with me, Dimitri Artzibusheu with the developer. Online  
11 we have another representative, Mike Cassins (phonetically). We  
12 have Michael Raysor, who is with Raysor Transportation  
13 Consulting and Amber Tomas with Otero. And there may be more  
14 from our team online.

15 We are requesting a rezoning from AS-1 and AR to PD to  
16 allow a ten-unit single-family subdivision at 17250 Morris  
17 Bridge Road. The property is just over ten acres. It is in the  
18 rural service area, but it is in a suburban enclave with a RES-6  
19 Comp Plan designation. The confine would allow 64 units, but we  
20 are only requesting ten, which is just above one unit per acre.

21 The developer will provide the necessary utilities, as  
22 required in the rural service area and will not request an  
23 extension of public water or sewer. And the lots would be a  
24 minimum size to meet the requirements for sewer -- for septic in  
25 the rural service area. The property is flag shaped, as you've

1 seen. I -- I thought that was a good -- a good map of the area.  
2 It will have the single main access onto Morris Bridge Road,  
3 which is -- it's a 50-foot wide access point right there. To  
4 limit the number of access points onto Morris Bridge, the  
5 project site driveway connection there will replace the existing  
6 driveway for the property that is directly south of that flag  
7 and east of the flag poll, so-to-speak. It's folio 59956.55.  
8 So it will move their direct access and will then access our  
9 driveway and then exit the area onto Morris Bridge at our access  
10 point.

11           Typically, with a ten-unit subdivision in the rural  
12 service area, we can design the project to the standards of a  
13 low volume road. However, because we are granting that access  
14 to that other property, it's considered an 11th lot, so we no  
15 longer meet those standards. So we are providing the roadway  
16 standards of the higher level TS-3, local urban road. And to --  
17 to do that -- as part of that, we are providing sidewalks on  
18 both sides of the street, not only does that fill that  
19 requirement, but it also allows for significant pedestrian  
20 connectivity in this rural suburban development that we're  
21 creating here.

22           We're also proposing a gate. The gate will be just  
23 north of that access point for the neighboring property owner  
24 and will be built to code. We have been asked by the County to  
25 provide two emergency stub outs. One, to the property on the

1 east and one to the property to west. We -- I noticed in the  
2 record there's some opposition to that. And you know, we don't  
3 really have a, you know, any fight about this, but it appears to  
4 be, you know, it's a county requirement. From my understanding,  
5 and this could be confirmed by the County, but it's only for  
6 emergency services like fire trucks and -- and so and it  
7 wouldn't actually be used unless those neighboring properties  
8 were developed. So my understanding is those fire trucks will  
9 not be driving across their field or anything like that. So I  
10 don't think they should worry if it ends up being up to them if  
11 they develop and they want to -- or required to connect to our  
12 emergency set up.

13           So generally, we are compatible with the present  
14 configuration of Morris Bridge Road. We have requested an  
15 administrative variance because of driveway spacing issues along  
16 Morris Bridge, even though we are moving at one driver, there  
17 are still others that are closer than the minimum required that  
18 we are supposed to have. But the county engineer has deemed it  
19 approvable for this.

20           The traffic analysis submitted shows nine p.m. peak  
21 hours, which is pretty insignificant compared to the de minimus  
22 setting of 50, which you typically go into these developments  
23 with. And it's -- because it's at that low nine peak, even with  
24 at 11th lot coming onto our thing, it's still at nine p.m. peak  
25 hours. The -- the determination was done. No further analysis

1 or improvements would be required on Morris Bridge Road.

2           So from environmental perspective, as you can see it's  
3 surrounded by Hillsborough County park to the north and parks  
4 and areas and there's some the west side directly to the west of  
5 the neighboring property owner. We're almost completely  
6 surrounded by it. The vicinity map, the County I think really  
7 shows clearly this little area that we're talking about here  
8 amidst this natural area of -- of this North Hillsborough County  
9 area.

10           So then really even though we are in this location,  
11 there are no environmental -- environmental extensive concerns,  
12 features, overlays on the property. There are no wetlands.

13           Another comment in the opposition stuff that we saw  
14 online was, somebody mentioned that they saw a bald eagle fly --  
15 fly by. We did a wildlife study and we determined that there  
16 are in deed in fact gopher tortoises on the property, which is  
17 kind of expected in this type of area in Florida. And the  
18 developer will, of course, deal with that as appropriate and in  
19 full compliance with all regulations as they develop. But no  
20 bald eagles were found on the property. In fact, they found the  
21 only record of a bald eagle nest is from 2013 and it's more than  
22 a mile away. And that is determined to be farther than the  
23 appropriate -- farther than the distance that would really be  
24 impactful by development of this sort, which is only 700 feet.  
25 So I can submit that wildlife study into the record if -- if



1 that would be of assistance.

2           Attractive stormwater retention facilities throughout  
3 the property that will meet with the requirements and help  
4 create a situation where almost 30% of the property is in the  
5 open space. Another -- and I'm -- I'm just taking these points  
6 that were made by the commenters online because, you know, I  
7 want to make sure that you understand that we have looked at  
8 them. One comment was to suggest that there are sink holes at  
9 the property. We don't feel this is relevant to the zoning  
10 decision that you're going to be making, as the applicant would  
11 need to make sure that it meets all regulatory requirements if  
12 something should show up. But certainly, as part of the  
13 development process for doing this, they've gone out and they  
14 hired a geo tech, they've hired the wildlife and the -- all the  
15 environmental studies that might be necessary. And nothing was  
16 determined to be an issue on the site. The -- the geo tech  
17 report found that it had suitable subgrade conditions for the  
18 planned low rise residential construction and that with the  
19 proper construction procedures, there would be no issue.  
20 Certainly, if we went out there and it was discovered that there  
21 were sink holes or things that looked like sink holes because  
22 there are a lot of other possibilities for what those could be.  
23 We also don't know where they are. They weren't identified a  
24 location. But animal activity could -- could even be from bulls  
25 rolling around or something. So we just don't know. But we

1 would certainly look into that as part of the development  
2 process.

3           We did submit a detailed consistency analysis in our  
4 submittal. And Planning Commission Staff have submitted a  
5 report finding the project consistent with the comprehensive  
6 plan. The property is surrounded by RES-6 land use, which as  
7 for this low density residential development, as well as some  
8 suburban scale neighborhood commercial, which we're of course  
9 not proposing, but it shows to you the intensity that is  
10 considered appropriate for this area.

11           And again, we're only providing one unit per acre as  
12 opposed to six, which would be otherwise permitted. It is  
13 compatible with the established character of the surrounding  
14 neighborhood. It brings in much needed housing, but does so  
15 that we feel the specs of the rural character of the area,  
16 surrounding a natural and environment. It incorporates large  
17 lots throughout the development and provides sidewalks to allow  
18 good pedestrian connectivity.

19           There are no buffering requirements for this because  
20 it's adjacent to residential and agriculture uses. However,  
21 we're going to provide an actual landscaping along the project  
22 access road, around the project boundaries and use that -- the  
23 open space that is coming from the design of where the  
24 stormwater ponds are to assist in also providing that transition  
25 and that buffering.

1           We also feel that it is truly compatible with the  
2 surrounding area. Compatibility is not same as, it's similar to  
3 its existing and in harmony with. And really, although this  
4 area is outside the urban service area, it is developed to a  
5 more suburban development pattern. The Section 1.3 of the  
6 Future Land Use Element defines suburban as being in transition  
7 between urban and rural and can be a little bit of either,  
8 depending on the situation. Typically, it's two to nine units  
9 per acre. And in this case, we could theoretically go to six  
10 units per acre and we're only asking for one unit per acre.

11           Among other things, suburban is characterized by being  
12 less compact and intense than urban development, having tracks of  
13 undeveloped land interest verse with more developed lots,  
14 generally larger than 5,000 square feet, which we definitely  
15 have here. Most of the nearby zonings allow for density equal  
16 to or greater than what we're proposing, AS-1, RSC-6, 4, 2. And  
17 we are really relatively close to commercial development, social  
18 services, other major roads. We're only about two miles from  
19 I-75 to the west and Temple Terrace over there.

20           And then we are, of course, bound by county parkland  
21 and the preserve. To the west is a vacant pasture land and to  
22 the east is vacant pasture land right now. But immediately  
23 adjacent to that eastern property is a 50-lot subdivision called  
24 Pine Ridge Estates with half-acre lots generally right there.  
25 To the south are agricultural and residentially zoned lots,

1 larger ones that have been split, even though they weren't  
2 platted. But also a 88 sub -- of lot subdivision of quarter  
3 acre lots called Thousand Oaks (phonetically). This is all  
4 within this general area, this enclave here on Morris Bridge  
5 Road.

6           And then I think best of all is that idea that the --  
7 this little enclave here is surrounded by parkland almost  
8 completely. And so, you know, it's not going to get any bigger.  
9 It's not an issue of sprawl. It's an issue keeping this  
10 suburban rural type development here in an appropriate location  
11 and bound by the parkland.

12           We feel that the -- the project is compatible with the  
13 character of the surrounding area, the residential and storm  
14 water has designed to be compatible with the adjoining land  
15 uses. The roadways are designed to ensure adequate movement of  
16 vehicles and certainly, while it's relatively close to the  
17 nearby activity center, I-75, it does provide that transition.  
18 It helps to provide that transition between that more commercial  
19 area and the rural and parkland area to the north and west.  
20 We -- the applicant has reached out to several adjacent  
21 neighbors had a filing application. For the most part, they  
22 didn't really get much response. They -- they talked with a few  
23 of them and didn't really hear much fact. I haven't received  
24 anything certainly since sending the mailing out. So that's why  
25 we wanted to address those particular concerns that we saw

1 online. And I don't know if they're here tonight, but we're  
2 happy to discuss any other concerns that they might have or any  
3 questions that you might have.

4 But with that, I'm done. Thank you.

5 HEARING MASTER: Thank you. I appreciate it. Don't  
6 forget to sign in. I don't have any further questions. Thank  
7 you. Development Services.

8 MR. LAMPKIN: Good evening. Tim Lampkin.  
9 Hillsborough County Development Services for 23-0042.

10 This property has a Future Land Use designation of  
11 Residential-6. The subject property is located on the north  
12 side of Morris Bridge Road. It's approximately 6,300 feet east  
13 of Interstate 75 interchange. And such property is located in  
14 the rural service area. The property is not located within a  
15 community planning area.

16 The median area surrounding the property is  
17 predominantly zoned for agricultural. In addition, there are  
18 single-family homes, mobile homes and single-family development.  
19 South across Morris Bridge Road is a single-family development  
20 zoned RSC-6. Further west are more agricultural lands. Further  
21 east are pockets of neighborhood and commercial areas along  
22 Morris Bridge Road.

23 The applicant seeks to develop the approximately ten  
24 and a half acre unit by development consisting of one folio.  
25 The request is for rezoning from agricultural single-family one

1 and agricultural rural to plan development to allow for 12 --  
2 I'm sorry, a ten developing units -- single-family development.

3           The applicant requests no variations or waivers. The  
4 Planning Commission found the application to be consistent with  
5 Unincorporated Hillsborough County Development Services. There  
6 was -- there is one transportation administrative variance or  
7 the Morris Bridge access spacing. Pursuant to 6.04.07 and the  
8 county engineer found that request to be approvable. That  
9 concludes my presentation.

10           HEARING MASTER: Okay. Thank you so much. I  
11 appreciate it. Before we move to the Planning Commission, if I  
12 could ask Mr. Perez from the transportation -- County's  
13 Transportation Review Section is still online?

14           MR. PEREZ: Good evening.

15           HEARING MASTER: Good evening. Thank you. Regarding  
16 the gated emergency access, I saw in your comments and the  
17 applicant, Ms. Pollack's testimony that, you know, there are  
18 concerns from the neighbors. I read the letter myself that is  
19 in Optix. It's a requirement of the County Technical Manual, is  
20 that correct?

21           MR. PEREZ: That is correct. Per Section 602.01.H of  
22 the Land Development Code. The development does trigger the  
23 need for an emergency access. So at this point in time, they  
24 would stub out an emergency access to either side as they're  
25 showing on their site plan. And that would be in place for when

1 at some future point, the adjacent properties which also have a  
2 similar future land use for residential, could potentially come  
3 in for approval for a subdivision. And at that point, they  
4 would have to reciprocate and meet the same emergency access  
5 requirement in which they would connect.

6 But until such time, the -- the emergency access stub  
7 outs would be there for -- for that future scenario.

8 HEARING MASTER: So in -- this is just a fine point,  
9 but I just want to clarify because it -- it will become an  
10 issue, I think.

11 So in that those properties to the east and the west  
12 adjacent where these gated emergency access points are located,  
13 are not developed currently. Then this ten lot subdivision  
14 comes in, are they required to put up the gate and the Knox Box  
15 system at this point, even though the adjacent parcels are not  
16 developed?

17 MR. PEREZ: My understanding is that as development  
18 occurs, they have to put the infrastructure in so that it's  
19 there for the future. It's the first one in when you're putting  
20 the gate in so they would have a Knox Box system that they'd  
21 have to construct at the time that they build the subdivision,  
22 stub out the emergency access road to the property line. And  
23 then that gate would be -- would be constructed in place and  
24 acted for that -- that future, any potential future development  
25 that would occur on the adjacent properties that would have to



1 also connect.

2 HEARING MASTER: But to close the issue. So even  
3 though perhaps this property owner would be required to do that,  
4 it wouldn't be used because the adjacent properties are not  
5 developed yet. Only at that time would it be used,; is that  
6 correct?

7 MR. PEREZ: That -- that's correct.

8 HEARING MASTER: All right. Thank you so much. I  
9 just wanted to clarify that for the record.

10 All right. We'll go to the Planning Commission.

11 MS. LIENHARD: Thank you. The subject property is  
12 located in the Residential-6 Future Land Use Category. It is in  
13 the rural area. And it is not located within the limits of a  
14 community plan.

15 The subject site abuts Morris Bridge Road to the south  
16 and the Residential-6 Future Land Use Category surrounds the  
17 site to the east, south and west. To the north and further  
18 south is natural preservation. And then further southwest  
19 there's a suburban mixed use Future Land Use Category  
20 designation.

21 Future Land Use Element Objective 4 of the  
22 Comprehensive Plan notes that 20% of the growth from the region  
23 will incur within an rural area. The request includes ten  
24 single-family lots at a minimum of one-half acre each. Future  
25 Land Use Element Policy 4.1 characterizes the rural area as

1 low-density large lot residential. The proposed development is  
2 similar in nature to the existing single-family residential land  
3 uses, especially to the south of the site.

4 Therefore, the proposed rezoning is consistent with  
5 the surrounding area. The area the south of the site and  
6 further east are all existing residential uses and are all zoned  
7 residential single-family conventional 2, 4 or 6. The  
8 application -- or the applicant is proposing cross access points  
9 for future activity to the east and west.

10 The applicant is also proposing a minimum of half acre  
11 lot -- lots which are compatible with the surrounding lot sizes  
12 that range from over two acres to approximately 8,000 square  
13 feet in size. A PD rezoning would reflect a development pattern  
14 that is aligned with the existing development pattern and  
15 consistent policy direction.

16 Based upon those considerations, Planning Commission  
17 Staff finds the proposed land development consistent with the  
18 Unincorporated Hillsborough County Comprehensive Plan, subject  
19 to the conditions proposed by Development services. Thank you.

20 HEARING MASTER: Thank you. I appreciate it. Is  
21 there anyone that would like to speak in support? Anyone in  
22 favor of this application, either in the room or online? I see  
23 no one.

24 Anyone to testify in opposition? All right. So we  
25 have two people in room. Go ahead and come forward. Is there

1 anyone online that would like to speak in opposition?

2 MS. ZEINELABDIN: Yes, I would like to.

3 HEARING MASTER: Okay. So we have three people, so 15  
4 minutes. That gives you five minutes a piece. Joe, if you  
5 could start the timer. Did you have any preference to who goes  
6 first?

7 UNIDENTIFIED SPEAKER: You can let them go first.

8 HEARING MASTER: All right. Ma'am you -- if you'd  
9 like to go online, go first. You have a maximum of five  
10 minutes. If you could give us your name and address to start.

11 MS. ZEINELABDIN: So I go first.

12 HEARING MASTER: Yes. Go ahead.

13 MS. ZEINELABDIN: Okay. Okay. So my name is  
14 Amira Zeinelabdin. And the property that we -- me and my family  
15 own is 12728 Morris Bridge Road, Thonotosassa, Florida 33592.  
16 We also own the property right next door, which is -- and the  
17 address is 12744 Morris Bridge, Thonotosassa, Florida, Morris  
18 Bridge Road. And I speak on behalf of my family when I say that  
19 we oppose the plan and we hope to preserve the sanctity of  
20 agricultural and wildlife aspects of this area.

21 The proposed number of houses outlined in their report  
22 does not hit the culture of the single-family wildlife and  
23 farmland neighborhood. Contrary to the project narrative that  
24 claims that there are no significant wildlife up -- upland  
25 habitat area or environmental sensitive areas within the project

1 boundary. The land is surrounded by nature's classroom, which  
2 is a residential environmental education program that preserves  
3 habitats, as well as Southwest Florida, Water Management  
4 District or a wetland. Development of these housing,  
5 subdivisions will disturb the habitat of all wild animals that  
6 are conserved by all neighborhoods. There are wild deer that  
7 brows in the neighboring properties. And the woman who spoke  
8 earlier said that there were no recent bald eagle sightings and  
9 that's completely false. There's bald eagles that nest in the  
10 west of that -- of that property in a tower, as well -- sorry,  
11 yeah to the west in a tower, as well as to the east on the power  
12 line.

13           There are also gray foxes and red foxes and also fox  
14 heads that travel through this area. The gopher turtles also  
15 burrow in that land and they haven't been -- it hasn't been  
16 shown -- it hasn't been recorded in that project summary. And  
17 they even mark the holes with pink flags in that property. Not  
18 only will this development disrupt wildlife, but it will also  
19 generate an increase in traffic causing congestion and an upward  
20 of traffic accidents and fatalities. The crash report that they  
21 provided falsely claimed that there were no fatalities during  
22 the course of five years. In November of 2021, there was a  
23 vehicle fatality that occurred directly in front of my  
24 residence. The man lost control of his vehicle and crashed into  
25 the woods inevitably causing his death.

1           Moreover, according to an article published by Ten  
2 Tampa Bay News, over the past -- over the past five years since  
3 2019, there were a total of five fatal accidents, as well as 150  
4 traffic crashes that occurred on Morris Bridge Road in  
5 Thonotassassa. There's also an interview by Denny Alvarez, who  
6 was the Hillsborough County Sheriff's Office Chief Communication  
7 Officer at the time. And it was also confirmation that traffic  
8 enforcement is experiencing difficulty, maintain safe travel in  
9 this area.

10           Also my family, we -- we are deer farmers. An influx  
11 of traffic from this project will distress and endanger my  
12 captive wild deer. They're known to be easily spooked and are  
13 highly sensitive to stressful situations. They're -- there's  
14 signage of high risk of severely injuring and killing themselves.  
15 Therefore, frequent accidents resulting from this development  
16 will undoubtedly trigger them to harm themselves.  
17 This is a liability that can be -- that can and should be  
18 avoided.

19           Also, the project narrative documents stated that the  
20 applicant has reached out to several adjacent neighbors ahead of  
21 the filing application, but we've -- and have not received any  
22 objection, but this is also false. None of us agreed to this  
23 plan. There was also statement that there were -- there --  
24 there's also false that -- in their statement in the plan that  
25 they -- there are no negotiate -- negotiations for future

1 acquisitions of our property, nor did we allow access for the  
2 the access points on their plan.

3 HEARING MASTER: All right. Thank you for your  
4 comments. I really appreciate it. All right. We'll go to  
5 opposition that's in the room. Good evening. If you could  
6 please just give us your name and address to start?

7 MS. CHEWNING: Hello. My name is Cheryl Chewning and  
8 I live at 12858 Morris Bridge Road. And I'm speaking in  
9 opposition to this subdivision. Just to kind of give you a  
10 little bit of an idea. My family was the original owners of  
11 this property. It has been in our family for 80 years. And we  
12 had to sell the property, there's 26 acres and we had to sell  
13 the property in 2006 due to a death in the family. And so, when  
14 we sold it, we had an analysis done, our attorneys had an  
15 analysis done for agriculture and the road and everything.

16 So when we sold it, the people we sold it to, we  
17 specifically wanted rural agriculture. And a Church bought half  
18 of the property because it was going to remain an agriculture  
19 and then you heard from Amira, her family bought the other half.  
20 And they are deer farmers and have animals.

21 So moving forward, the church sold to this trucking  
22 company because the church was going to build a church, but they  
23 couldn't because they didn't have the money for it. So they  
24 sold it to the trucking county. So now we're talking about a  
25 development. But initially when the property was sold, it was

1 all understood and a resolving agreement that would remain  
2 agriculture.

3           So when the property sold, we had to have an analysis  
4 done of -- of the property and the attorneys hired was  
5 Sheers and Associates to do an analogy of the -- the property  
6 and -- and everything that is around it. And I have a copy of  
7 this to give to you. They did a road analysis, now keep in  
8 mind, this was back in 2006, and what they said on here was that  
9 the Morris Bridge Road from I-75 across creek has an AADT of  
10 13,100 cars with a right-of-way level of service of E. The  
11 current Land Development Code requires a level of service to be  
12 between A and D. Therefore, according to the Hillsborough  
13 County Traffic Services Department, any development of the  
14 subject property that would involve sub dividing the property  
15 would also require some mitigation of the roadways in order to  
16 gain approvals. Mitigation could include, but may not be  
17 limited to installation of turn lanes, in terms of the concurrency,  
18 the level of service, a number of average daily trips, 13,100,  
19 both causes, the roadway to be at maximum capacity and  
20 mitigation would be necessary in order to meet concurrency  
21 should the property be suddenly divided and developed as such.  
22 And that was 2006.

23           So here we are 16 years later and I am here to tell  
24 you that that is a rural highway. And the traffic on there, the  
25 crashes, the fatalities that were mentioned previously, is



1 very -- it's a very dangerous road. And I -- Ashley spoke with  
2 the Hillsborough County Sheriff's Department to see what they  
3 could do to slow traffic down on that road. And they told me  
4 there's nothing they can do because it's a rural highway. And  
5 he said it's a rural highway through your neighborhood. And so  
6 they said the only thing they could do -- and it's rural highway  
7 because when you get off from I-75, there's nothing to slow  
8 anybody down until they get to Cross Creek. Okay, which is very  
9 far away. This Pine Ridge Estates that she was talking about is  
10 not right where this is. It's down the road on a bad curve and  
11 there's been many accidents down here and -- and there's been a  
12 couple of fatalities on that curve.

13           So with that being said, what the Sheriff said the  
14 only thing they can do is drop the speed limit down there. So  
15 they dropped the speed limit from 45 to 40. And they put up  
16 those you know, big signs that when you're driving and it  
17 telling you, hey, you're going this fast, you know, you need to  
18 slow down. They're doing that. But they said there's  
19 absolutely nothing they can do. And I'm here to tell you that  
20 it has grown out past that area and the cars are unbelievable.  
21 I mean, you have to sit in your driveway sometimes for 30  
22 minutes to even get out on the road.

23           And where this issue went into this subdivision is, if  
24 you're coming from the east side, you come up on a hill. And  
25 the entryway is right there on the top of the hill. And people

1 pass all the time on that double yellow line. My family has  
2 almost been hit. You know, the neighbors, if you talked to the  
3 neighbors out there, because we have all been talking about  
4 this. You know, it's -- it's terrible.

5 So there is a lot of concern about bringing that out  
6 there, you know, this subdivision with nothing with the roads.  
7 So that -- and I have a copy of that for you as well. So that's  
8 one of the -- the issues, you know, that we're concerned about.

9 The other thing is, in the narrative that the  
10 developer submitted, he's talking about the -- and you -- and  
11 you brought it up, emergency access, it's on either side. My  
12 family's property is on the east side and we have said  
13 repeatedly, we are not selling, we are not developing. Amira's  
14 family that just spoke, they're on the west side and they're  
15 like, we're not selling, we're not developing. But with that  
16 being said, and here, you -- that the projections there could be  
17 a subdivision there, what does that mean to us, you know?

18 So that being said, there's some concern because  
19 that's not accurate. We're not selling, we're in no  
20 negotiations and we're -- it's just -- we're not doing it.

21 The other thing is, is I have a small cattle business  
22 and my cattle -- my property backs right up to where this is  
23 going. There are multiple areas in that pasture land where it's  
24 sinking. We actually had a sink hole, 20-foot sink hole open up  
25 and it swallowed one of my cows and broke her neck. And I had

1 called the State and the State came out there to -- to look at  
2 that sink hole because I was concerned because it was 20-foot  
3 deep and I was concerned about would this dead cow in the sink  
4 hole affect the water. And they came out and they said Aquifer  
5 was like 40 or well, I think like 60 feet down. It was okay.  
6 However, there is -- that area is prone to the indentations.  
7 And they said, because you're close to the river, there's a lot  
8 going on underneath the -- that property there. And that sink  
9 hole was -- is probably maybe 100 feet from where they want to  
10 put their -- their -- where the retention pumps are going to be.  
11 So that -- you know, with the water issue, it's an issue. Plus,  
12 putting the septic right there on the Hillsborough River where  
13 you know, you're going to be -- that goes to Tampa, as well.  
14 So --

15 HEARING MASTER: If I could ask you to wrap it up.  
16 We're well past the --

17 MS. CHEWNING: Okay.

18 HEARING MASTER: -- five minutes.

19 MS. CHERNING: So with the animals, the animals are  
20 there. It depends on what time of the day you're there that you  
21 can see them. Okay. But they are there, the ones that she  
22 mentioned, they're there. And we're concerned about them as  
23 well, the safety of swiftmud has animals and Nature's Classroom  
24 has animals there. And we're concerned about all of that safety  
25 out there. Thank you very much.

1 HEARING MASTER: Thank you. And you're welcome to  
2 submit anything you would like into the record with Joe and  
3 sign-in as well.

4 Yes, ma'am. Good evening.

5 MS. BASS: Good evening, Madam Hearing Officer. My  
6 name is Sharon Bass. I'm an attorney here in Hillsborough  
7 County and I represent Ms. Chewning and her mother,  
8 June McClarnon (phonetically), who live at 12858 Morris Bridge  
9 Road. My address is 9835 North 56th Street, Temple Terrace,  
10 Florida 33617. I'll keep my comments very brief.

11 The concern that I have relates directly to the  
12 Hillsborough County Comprehensive Plan, Article 11, Goal 1,  
13 Object -- Policy 1.1. The concern is that homeowners in  
14 Hillsborough County do have a best of right their property as  
15 they commit. And the impacts from development in these areas  
16 certainly has impacts on their property rights as well. And so  
17 I'm here this evening, Madam Hearing Officer, just to remind  
18 both your department and Hillsborough County Commission that  
19 this property is being used currently by land owners who have  
20 been there for a significant amount of time. The concerns about  
21 the impacts that this type of planned development's going to  
22 have on their particular business, which happens to sit right in  
23 the middle of two established agricultural uses. Because of the  
24 ten plots being on septic, we have no idea what's going to  
25 happen with the water rights with regard the land owners to the

1 east or to the west. There's concerns about light pollution and  
2 other issues will come with having a plan development in that  
3 area. And further, that entire north side of the road there is  
4 agricultural. There's a few single-family homes, but there's  
5 nothing else there. All of the RES-6 is on the south side of  
6 the road. Simply put, this area, while it is proposed to be a  
7 RES-6 area, it doesn't have to be. Is that the right use for  
8 this particular area given the homeowners who are currently in  
9 place and the use of their property?

10 And so we're simply just asking for consideration from  
11 you of what our Hillsborough County Comprehensive goals and  
12 objectives are. These are the existing land owners. Thank you.

13 HEARING MASTER: Thank you so much. If you could  
14 please sign-in. All right. I'm seeing no other opposition  
15 testimony. We'll close that portion of the hearing. We'll go  
16 to Development Services. Ms. Heinrich, do you have anything  
17 else?

18 MS. HEINRICH: Nothing further.

19 HEARING MASTER: All right. Then we'll go back to  
20 Ms. Pollack, you have five minutes for rebuttal.

21 MS. POLLACK: Thank you. I'd like to first direct to  
22 Michael Raysor, who's our transportation consultant who can  
23 speak to someone the concerns. He's online.

24 HEARING MASTER: Okay. Good evening.

25 MR. RAYSOR: Good evening. My name is Michael Raysor.

1 I'm the president of Raysor Transportation Consulting. I'm a  
2 registered professional engineer. 26 years of experience with a  
3 specialization in traffic engineering. My address is 19046  
4 Bruce B. Downs Boulevard, Suite 308, Tampa 33647. I have not  
5 been sworn.

6 HEARING MASTER: All right. You can and anybody else  
7 in the room, I think we're past this point, but do you swear  
8 that the testimony you're about to provide is the truth, the  
9 whole truth and nothing but the truth?

10 MR. RAYSOR: I do.

11 HEARING MASTER: Thank you. Go ahead.

12 MR. RAYSOR: So I'd like to talk about the -- some of  
13 the traffic items that have come up in the testimony. I'd like  
14 to start with the Morris Bridge 2006 evaluation that was  
15 referenced. The -- the traffic volumes on Morris Bridge Road  
16 are approximately 13,000. However, I don't have access to the  
17 2006 analysis, but in the staff report reviewed by County Staff,  
18 the level of service identified for this segment of Morris  
19 Bridge Road was C compared to a D standard. The -- the  
20 operational capacity may have been incorrectly evaluated back in  
21 2006 as a -- on an arterial road rather than a highway segment,  
22 which is as we heard. So again, level service is -- is that.  
23 And that is in the Staff Report.

24 There's also been a change in the regulation since  
25 2006, where they county (indiscernible) program, so regardless

1 of there being a deficiency on the road, you're now mobility  
2 (indiscernible). So that wouldn't have been material  
3 regardless, but in fact, level service C compared to a D  
4 standard.

5           The crash data that was referenced previously was from  
6 the County Sources, the County's traffic crash database. So if  
7 there was something missing from that database, that would be  
8 a -- a county issue. We have spoken with the county engineer on  
9 multiple occasions. I've been involved in two specific meetings  
10 with William -- Williams and there's been no discussion  
11 regarding concerns about any A-typical crash safety for this  
12 segment of Morris Bridge Road.

13           There was also a reference to a curve perhaps  
14 providing adverse conditions for the location of the driveway  
15 connection. I can assure you that going through the permitting,  
16 we're not quite there yet, but it goes through the permitting  
17 process, sight, visibility, environmental and vertical will be  
18 looked at in detail. And we certainly couldn't build a driveway  
19 connection that is not safe.

20           So I'd like to conclude back to the -- back to traffic  
21 safety. And I apologize for jumping around, my notes were --  
22 were plentiful. Morris Bridge Road is in Hillsborough County  
23 from an interstate segment of Pasco County. It's about ten  
24 miles. We're in the first approximate mile of -- of that  
25 distance. There's no doubt that in the easterly and northerly



1 portions of that, which -- which consists of eight and a half  
2 miles or so on that road, it is a fairly straight shot, you  
3 know, two lanes and -- and nature. So speeds are probably  
4 excesses in that particular area due to the -- the nature of  
5 that road and this configuration and alignment and the lack of  
6 development, along that -- along that road. And so where you  
7 have excessive speeds, excessive crashes can follow. Again, we  
8 are on a segment that's relatively close to the interstate  
9 and -- and speaking with the county engineer on multiple  
10 occasions, there have been no discussion regarding any of the  
11 A-typical safety characteristics.

12 MS. POLLACK: Thank you very much.

13 MR. RAYSOR: I'm here if you have any questions.  
14 Thank you.

15 I just want to make sure I have enough time just to  
16 address a few other things.

17 HEARING MASTER: Ms. Pollack, I'll give you a little  
18 extra --

19 MS. POLLACK: Okay.

20 HEARING MASTER: -- time as the opposition testimony  
21 went a little long too.

22 MS. POLLACK: I appreciate that. I'm going to submit  
23 into the record just a -- an email and a -- a brief summary of  
24 the geo tech report that we had done, so they could see their  
25 findings and that additional geo technical testing would occur

1 if they were to determine that there were -- was a sink hole  
2 issue or some other type of issue out on the property.

3           Again, the owners are not interested in building a  
4 development that would be unsafe either for their own homeowners  
5 or cause a problem on the -- the neighboring properties and  
6 really wouldn't be allowed to anyway due to regulations.

7           With regard to the animals, there's no doubt there's  
8 animals in this area. We're right near this major parkland  
9 area. We don't expect there to not be animals, but the -- the  
10 owners are required to test and determine study to see if there  
11 are those animals, I believe, within 90 days of submitting for  
12 permits for construction plan. And so at that point, they would  
13 go out even though our report from 2022 says no bald eagle nests  
14 were identified near the property, closer than even a mile away.  
15 You know, that would be something if they saw bald eagles nests  
16 close, gopher tortoises on the property. Those would have to be  
17 you know, the property would have to be -- the development would  
18 have to look like their location or they would have to be  
19 relocated as necessary. But they would have to do that to meet  
20 the needs.

21           And, you know, they -- they expect to -- to develop in  
22 a way that reflects that we are in this area of -- of sort of a  
23 rural, suburban development and recognizing that there is  
24 agricultural nearby. That would be part of their development  
25 plan. And if I can just have the owners just give a one-minute

1 just to --

2 HEARING MASTER: Good evening.

3 MR. ARTZIBUSHEU: Good evening. Dimitri Artzibusheu,  
4 First Tampa Development, 1525 West Hillsborough, Tampa, Florida.  
5 Our intent is to develop with a nice development as best as we  
6 can. We're going to leave as many trees as we can on the  
7 outside wherever we can. We'll save what we can. As you know,  
8 it's hard to do that always, but we will do what we can to try  
9 to keep as many trees up as possible. So the squirrels and --  
10 and all those will be fine out there.

11 Yes, we do our own studies and we've got to check for  
12 the geo tech issues and problem with the sink hole issues and  
13 (indiscernible). A lot of studies to be sure there's no issues  
14 (indiscernible)

15 HEARING MASTER: Thank you. I you appreciate it.

16 MS. POLLACK: Just one more comment with regard to the  
17 water table and water flow. We've had to have several meetings  
18 with Swiftmud already about how we're adjusting stormwater and  
19 how we're adjusting our ponds. And based on the topography and  
20 the requirements that they already told us and we're not even to  
21 that next step. That is something that is definitely being  
22 taken into consideration as this development goes forward so  
23 that we have the least impact on surrounding property, whatever  
24 happens with water. Thank you very much.

25 HEARING MASTER: Thank you. I appreciate that. Then

1 with that, we'll close Rezoning 23-0042. And adjourn the  
2 hearing. Thank you all for your time and testimony.

3 (Off the record at 9:43 p.m.)  
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**EXHIBITS SUBMITTED  
DURING THE ZHM HEARING**

DATE/TIME: 4-17-23

HEARING MASTER: Susan Finch

PLEASE **PRINT CLEARLY**, THIS INFORMATION WILL BE USED FOR MAILING

<p>APPLICATION # RZ 23-0082</p>	<p>PLEASE PRINT NAME <u>Jay A MUFFIX</u> MAILING ADDRESS <u>102 5TH AVE SE</u> CITY <u>LUTZ</u> STATE <u>FL</u> ZIP <u>33549</u> PHONE <sup>813</sup> <u>949-2224</u></p>
<p>APPLICATION # RZ 23-0203</p>	<p>PLEASE PRINT NAME <u>SUSAN SWIFT</u> MAILING ADDRESS <u>607 S. Alexander St.</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP _____ PHONE <u>703946</u> <u>6792</u></p>
<p>APPLICATION # RZ 23-0082</p>	<p>PLEASE PRINT NAME <u>Todd Pressman</u> MAILING ADDRESS <u>200 2nd Ave. S #451</u> CITY <u>St. Pete</u> STATE <u>FL</u> ZIP <u>33709</u> PHONE <sup>727</sup> <u>804</u> <u>1760</u></p>
<p>APPLICATION # RZ 23-0115</p>	<p>PLEASE PRINT NAME <u>J. Michael Shea, Esq</u> MAILING ADDRESS <u>40301 Bayshore Blvd</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33611</u> PHONE <sup>813</sup> <u>310-8057</u></p>
<p>APPLICATION # RZ 23-0115</p>	<p>PLEASE PRINT NAME <u>Dilip Agarwal</u> MAILING ADDRESS <u>301 W. PL A-77 ST</u> CITY <u>TAMPA</u> STATE <u>FL</u> ZIP <u>33606</u> PHONE <sup>813</sup> <u>412</u> <u>5719</u></p>
<p>APPLICATION # RZ 22-1431</p>	<p>PLEASE PRINT NAME <u>Todd Pressman</u> MAILING ADDRESS <u>200 2nd Ave. S. #451</u> CITY <u>St. Pete</u> STATE <u>FL</u> ZIP <u>33709</u> PHONE <sup>727</sup> <u>804</u> <u>1760</u></p>



DATE/TIME: 4-17-23HEARING MASTER: Susan FinchPLEASE **PRINT CLEARLY**, THIS INFORMATION WILL BE USED FOR MAILING

APPLICATION # RZ 22-1431	PLEASE PRINT NAME <u>Ariel Quintela (Virtual)</u> MAILING ADDRESS <u>9511 Aqua Lane</u> CITY <u>Odessa</u> STATE <u>FL</u> ZIP <u>33556</u> PHONE <u>813-263-5727</u>
APPLICATION # RZ 22-1431	PLEASE PRINT NAME <u>Clara Lawhead, President KCA</u> MAILING ADDRESS <u>7340 Colley Rd.</u> CITY <u>Odessa</u> STATE <u>FL</u> ZIP <u>33556</u> PHONE <u>813-376-0474</u>
APPLICATION # RZ - 1431	PLEASE PRINT NAME <u>Elizabeth White</u> MAILING ADDRESS <u>17905 Burrell Rd</u> CITY <u>Odessa</u> STATE <u>FL</u> ZIP <u>33556</u> PHONE <u>813/404-3125</u>
APPLICATION # <u>22-1431</u>	PLEASE PRINT NAME <u>Melissa Nurdbeck</u> MAILING ADDRESS <u>1008 Hammock Woods Dr</u> CITY <u>Odessa</u> STATE <u>FL</u> ZIP <u>33556</u> PHONE <u>813-505-9315</u>
APPLICATION # RZ 22-1431	PLEASE PRINT NAME <u>Joe Dunan</u> MAILING ADDRESS <u>11311 Hutchins AVE</u> CITY <u>ODessa</u> STATE <u>FL</u> ZIP <u>33556</u> PHONE <u>813-877-0026</u>
APPLICATION # RZ 22-1431	PLEASE PRINT NAME <u>Ward Netscher</u> MAILING ADDRESS <u>18421 Gunn Highway</u> CITY <u>Odessa</u> STATE <u>FL</u> ZIP <u>33556</u> PHONE <u>813-9202442</u>



DATE/TIME: 4-17-23HEARING MASTER: Susan FinchPLEASE **PRINT CLEARLY**, THIS INFORMATION WILL BE USED FOR MAILING

APPLICATION # RZ 23-0081	PLEASE PRINT NAME <u>Alexa Vargas Abarca</u> MAILING ADDRESS <u>115 Railroad St Wimauma FL</u> CITY <u>Wimauma</u> STATE <u>FL</u> ZIP <u>33598</u> PHONE <u>727-589-0225</u>
APPLICATION # RZ 23-0081	PLEASE PRINT NAME <u>Joel Avila Perez</u> MAILING ADDRESS <u>115 Railroad St Wimauma FL</u> CITY <u>Wimauma</u> STATE <u>FL</u> ZIP <u>33598</u> PHONE <u>813-416-8800</u>
APPLICATION # RZ 23-0100	PLEASE PRINT NAME <u>David Wright (virtual)</u> MAILING ADDRESS <u>P.O. Box 273417</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33688</u> PHONE <u>813-230-7473</u>
APPLICATION # RZ 23-0149	PLEASE PRINT NAME <u>TOM AMADEN</u> MAILING ADDRESS <u>8515 Palm Rural</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33619</u> PHONE <u>813-217-8411</u>
APPLICATION # RZ 23-0149	PLEASE PRINT NAME <u>DAVID WEBB</u> MAILING ADDRESS <u>3903 CRESTWOOD DR</u> CITY <u>VALRICO</u> STATE <u>FL</u> ZIP <u>33596</u> PHONE <u>813-748-0810</u>
APPLICATION # MM 22-1116	PLEASE PRINT NAME <u>David Wright (virtual)</u> MAILING ADDRESS <u>P.O. Box 273417</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33688</u> PHONE <u>813-230-7473</u>

DATE/TIME: 4-17-23HEARING MASTER: Susan FinchPLEASE **PRINT CLEARLY**, THIS INFORMATION WILL BE USED FOR MAILING

APPLICATION # MM 22-1116	PLEASE PRINT NAME <u>Chris Pello</u> MAILING ADDRESS <u>10477 Gardens Dr</u> CITY <u>Riverview</u> STATE <u>FL</u> ZIP <u>33518</u> PHONE <u>813 404 1598</u>
APPLICATION # MM 22-1116	PLEASE PRINT NAME <u>Mrs. J. Ammersey</u> MAILING ADDRESS <u>908 Greenwell Dr</u> CITY <u>Brandon</u> STATE <u>FL</u> ZIP <u>33511</u> PHONE <u>813-71-132</u>
APPLICATION # MM 22-1236	PLEASE PRINT NAME <u>CANTON BRICKLEY</u> MAILING ADDRESS <u>101 E. KENNEDY BLVD, SUITE 2700</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>(813) 506-5078</u>
APPLICATION # MM 22-1392	PLEASE PRINT NAME <u>J Dallas Evans</u> MAILING ADDRESS <u>3810 Northdale Blvd</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33604</u> PHONE <u>813-949-7449</u>
APPLICATION # RZ 22-1401	PLEASE PRINT NAME <u>Ava Russo (virtual)</u> MAILING ADDRESS <u>4809 Ehrlich Road # 202</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33624</u> PHONE <u>609-513-7501</u>
APPLICATION # RZ 22-1401	PLEASE PRINT NAME <u>Luis Pagan Marchand (virtual)</u> MAILING ADDRESS <u>2808 Hideaway Lane</u> CITY <u>Valrico</u> STATE <u>FL</u> ZIP <u>33596</u> PHONE <u>813-447-6090</u>



DATE/TIME: 4-17-23HEARING MASTER: Susan FinchPLEASE **PRINT CLEARLY**, THIS INFORMATION WILL BE USED FOR MAILING

APPLICATION # MM 22-1501	PLEASE PRINT NAME <u>Brice Pinson</u> MAILING ADDRESS <u>1000 N Ashley Dr. Ste 900</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813-425-6200</u>
APPLICATION # MM 22-1501	PLEASE PRINT NAME <u>Jeff Anderson</u> MAILING ADDRESS <u>3811 S Frongate Rd</u> CITY <u>Plant City</u> STATE <u>FL</u> ZIP <u>33547</u> PHONE <u>813-259-0609</u>
APPLICATION # RZ 22-1702	PLEASE PRINT NAME <u>Ely Payne</u> MAILING ADDRESS <u>2054 W Central Ave</u> CITY <u>St Pete</u> STATE <u>FL</u> ZIP <u>33712</u> PHONE <u>813-679-9908</u>
APPLICATION # RZ 22-1702	PLEASE PRINT NAME <u>Steph Sprints Lindup</u> MAILING ADDRESS <u>505 E Jackson</u> CITY <u>Tamp</u> STATE <u>FL</u> ZIP <u>33602</u> PHONE <u>813-375-0616</u>
APPLICATION # RZ 22-1702	PLEASE PRINT NAME <u>STEVEN HENRY</u> MAILING ADDRESS <u>5023 W. LAMAR ST</u> CITY <u>TPA</u> STATE <u>FL</u> ZIP <u>33607</u> PHONE <u>813-289-0039</u>
APPLICATION # RZ 22-0042	PLEASE PRINT NAME <u>Anne Dollack</u> MAILING ADDRESS <u>433 Central Ave #402</u> CITY <u>St Pete</u> STATE <u>FL</u> ZIP <u>33701</u> PHONE <u>813-898-2836</u>

DATE/TIME: 4-17-23HEARING MASTER: Susan FinchPLEASE **PRINT CLEARLY**, THIS INFORMATION WILL BE USED FOR MAILING

APPLICATION # RZ 23-0042	PLEASE PRINT NAME <u>Amira Zeinelabdin (Virtual)</u> MAILING ADDRESS <u>12728 Morris Bridge Road</u> CITY <u>Thonotosassa</u> STATE <u>FL</u> ZIP <u>33592</u> PHONE <u>813-765-2123</u>
APPLICATION # RZ 23-0042	PLEASE PRINT NAME <u>Sheryl Chewing</u> MAILING ADDRESS <u>12858 Morris Bridge Road</u> CITY <u>Thonotosassa</u> STATE <u>FL</u> ZIP <u>33592</u> PHONE <u>813-928-1490</u>
APPLICATION # RZ 23-0042	PLEASE PRINT NAME <u>Sharon Alves Bass</u> MAILING ADDRESS <u>9385 96th St Ste 311</u> CITY <u>Temple Terrace</u> STATE <u>FL</u> ZIP <u>33617</u> PHONE <u>813 988-4040</u>
APPLICATION # RZ 23-0042	PLEASE PRINT NAME <u>Michael Raysor (Virtual)</u> MAILING ADDRESS <u>19046 Bruce B. Downs Boulevard #308</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33647</u> PHONE <u>813-625-1699</u>
APPLICATION # RZ 23-0042	PLEASE PRINT NAME <u>DIMITRI ARTZIBUSHEV</u> MAILING ADDRESS <u>1525 W. Hillsborough Ave</u> CITY <u>Tampa</u> STATE <u>FL</u> ZIP <u>33613</u> PHONE <u>813-239-1103</u>
APPLICATION #	PLEASE PRINT NAME _____ MAILING ADDRESS _____ CITY _____ STATE _____ ZIP _____ PHONE _____

HEARING TYPE: **ZHM**, PHM, VRH, LUHO

DATE: December 12, 2022

HEARING MASTER: Pamela Jo Hatley

PAGE:    OF 1

<b>APPLICATION #</b>	<b>SUBMITTED BY</b>	<b>EXHIBITS SUBMITTED</b>	<b>HRG. MASTER YES OR NO</b>
RZ 22-1431	Todd Pressman	1. Application Videos	No
RZ 22-1431	Todd Pressman	2. Applicant presentation packet	No
RZ 22-1401	Brian Grady	1. Revised staff report	Yes (Copy)
MM 22-1236	Brian Grady	1. Revised staff report	Yes (Copy)
RZ 22-1702	Stephen Sposato	1. Application presentation packet	No
RZ 22-0042	Sheryl Chewning	1. Applicant presentation packet	No

APRIL 17, 2023 - ZONING HEARING MASTER

The Land Use Hearing Officer (LUHO), Hillsborough County, Florida, met in Regular Meeting, scheduled for Monday, April 17, 2023, at 6:00 p.m., in the Boardroom, Frederick B. Karl County Center, Tampa, Florida, and held virtually.

▶ Susan Finch, ZHM, called the meeting to order at 6:00 p.m. and led in the pledge of allegiance to the flag.

A. WITHDRAWALS AND CONTINUANCES

▶ Michelle Heinrich, Development Services, reviewed changes/withdrawals/continuances.

▶ Susan Finch, ZHM, overview of ZHM process.

▶ Senior Assistant County Attorney Mary Dorman, overview of oral argument/ZHM process.

▶ Susan Finch, ZHM, oath.

B. REMANDS

C. REZONING STANDARD (RZ-STD):

C.1. RZ 22-1431

▶ Michelle Heinrich, DS, called RZ 22-1431.

▶ Testimony provided.

▶ Susan Finch, ZHM, continued RZ 22-1431.

C.2. RZ 22-1681

▶ Michelle Heinrich, DS, called RZ 22-1681.

▶ Testimony provided.

▶ Susan Finch, ZHM, continued RZ 22-1681.

C.3. RZ 23-0081

▶ Michelle Heinrich, DS, called RZ 23-0081.

▶ Testimony provided.

▶ Susan Finch, ZHM, closed RZ 23-0081.

C.4. RZ 23-0082

- ▶ Michelle Heinrich, DS, called RZ 23-0082.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, continued RZ 23-0082.

C.5. RZ 23-0100

- ▶ Michelle Heinrich, DS, called RZ 23-0100.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed RZ 23-0100.

C.6. RZ 23-0115

- ▶ Michelle Heinrich, DS, called RZ 23-0115.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, continued RZ 23-0115.

C.7. RZ 23-0149

- ▶ Michelle Heinrich, DS, called RZ 23-0149.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed RZ 23-0149.

C.8. RZ 23-0203

- ▶ Michelle Heinrich, DS, called RZ 23-0203.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, continued RZ 23-0203.

D. REZONING-PLANNED DEVELOPMENT (RZ-PD) & MAJOR MODIFICATION (MM) :

D.1. MM 22-1116

- ▶ Michelle Heinrich, DS, called MM 22-1116.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed MM 22-1116.



D.3. MM 22-0042

- ▶ Michelle Heinrich, DS, called MM 22-1236.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed MM 22-1236.

D.4. MM 22-1392

- ▶ Michelle Heinrich, DS, called MM 22-1392.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed MM 22-1392.

D.5. RZ 22-1401

- ▶ Michelle Heinrich, DS, called RZ 22-1401.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed RZ 22-1401.

D.6. MM 22-1501

- ▶ Michelle Heinrich, DS, called MM 22-1501.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed MM 22-1501.

D.8. RZ 22-1702

- ▶ Michelle Heinrich, DS, called RZ 22-1702.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, continued RZ 22-1702.

D.10. RZ 23-0042

- ▶ Michelle Heinrich, DS, called RZ 23-0042.
- ▶ Testimony provided.
- ▶ Susan Finch, ZHM, closed RZ 23-0042.

MONDAY, APRIL 17, 2023

ADJOURNMENT

▶ Susan Finch, ZHM, adjourned the meeting at 9:43 p.m.

Application No. 23-0042  
Name: Sheryl Chenang  
Entered at Public Hearing: 9-17-23  
Exhibit # 1 Date: ZHM

**Summary Appraisal Report**  
Current Valuation On:  
September 29, 2006

Thonotosassa Vacant Land  
12750 Morris Bridge Rd  
Thonotosassa, FL 33592

Land.4145 - Chancey/Griffis -  
12750 Morris Bridge Road

Prepared For:

The Client & Intended User(s):

Mr. Mark Wall  
Hill, Ward & Henderson, P.A.  
Bank of America Plaza 101 E Kennedy Blvd,  
Suite 3700  
Tampa, FL 33602

Report Date:

October 2, 2006

Prepared By:



610 South Albany Avenue  
Tampa, FL 33606  
813.254.2885  
813.254.0193

*Page 17 - roadways  
if - future land use  
Page 42 - remain rural in nature  
page 45 - rezone 6 months to 2 years  
page 52 - final value - 21,200 / acre = \$ 830,000*

**PROPERTY DESCRIPTION**

**B.1 Site Description**

<p><b>TOPOGRAPHY AND DRAINAGE</b></p>	<p>The site consists of undulating terrain and at or near road grade. Drainage appears adequate and typical for the area.</p>	
<p><b>SOILS</b></p>	<p>A soil analysis for the site has not been provided for the preparation of this appraisal. In the absence of a soil report, it is a specific assumption that the sites have adequate soils to support the highest and best use.</p>	
<p><b>ENVIRONMENTAL &amp; ENGINEERING CONDITIONS</b></p>	<p>Unless otherwise stated in this report, we have no knowledge of any hidden or unapparent conditions of the subject site, (including wetlands or unstable soil), or adverse environmental conditions (including the presence of hazardous wastes, toxic substances, etc.) that would make the subject site more or less valuable. We have assumed that there are no such conditions and make no guarantees or warranties, express or implied, regarding the condition of the property. We will not be responsible for any such conditions that do exist or for any engineering or testing that might be required to discover whether such conditions exist. Because we are not experts in the field of site engineering or environmental hazards, this report must not be considered as an engineering or environmental assessment of the property. Unless otherwise stated in the report we have assumed the subject site is 100% usable.</p>	
<p><b>EASEMENTS COVENANTS, CONDITIONS AND RESTRICTIONS</b></p>	<p>There are no other known covenants, conditions and restrictions impacting the site which are considered to affect the marketability or highest and best use, other than zoning restrictions. Therefore, we have valued the subject as free of easements.</p>	
<p><b>CONCURRENCY AND IMPACT FEES</b></p>	<p>Impact Fee Estimate</p>	<p>\$N/A</p>
	<p>Level of Service (LOS)</p>	<p>E</p>
<p>Virtually all real estate markets in the state of Florida, must now contend with strict growth control legislation. The 1985 Florida Growth Management Act limits new development to the capacity of existing or planned and funded infrastructure. The Florida Growth Management Act has at its heart the concurrency provision. The Concurrency Requirement has the principle purpose of assuring that adequate infrastructure including transportation, sewer and water, schools and fire protection is in place prior or concurrent with new developments. Generally, concurrency is defined as follows adequate infrastructure is in place or specifically planned at the time a development permit is issued or when impact of the development necessitates mitigation. Insufficient infrastructure capacity can restrict new development even if a proposed use meets all other zoning and land use codes. Many communities have begun charging impact fees on new development to pay for infrastructure required under the concurrency requirement.</p> <p>Roadway level of service (LOS) is a qualitative assessment of the road user's perception of the quality of flow. Much like a student's report card, the letters "A" through "F" represents LOS, with "A" generally representing the most favorable driving conditions and "F" representing the least favorable. The LOS reflects the quality of flow as measured by a scale of driver satisfaction. The definitions and measures of LOS reflect a national consensus of driver quality of flow. The indicated level of Service designates the lowest quality operating conditions for the 100<sup>th</sup> highest volume hour in predominant traffic flow direction from the present through a 20-year planning horizon. The 100<sup>th</sup> highest hour approximates the typical peak hour during the peak season.</p>		
<p><b>NOTES</b></p>	<p>Morris Bridge Road from Interstate 75 to Cross Creek Boulevard has an AADT of 13,100 cars with a roadway level of service of "E." The current land development code requires the level of service to be between A and D. Therefore, according to the Hillsborough County Traffic Service Department, any development of the subject property that would involve subdividing the property would also require some mitigation of the roadways in order to gain approvals. Mitigation could include, but may not be limited to, installation of turn lanes. In terms of concurrency, the level of service and number of average daily trips (13,100) both cause the roadway to be at maximum capacity, and mitigation would be necessary in order to meet concurrency should the property be subdivided and developed as such.</p>	

*Roadway at maximum Capacity*

My family and I wish to preserve the sanctity of agricultural and wildlife aspects of this area. The proposed number of houses outlined in the report does not fit the culture of this single-family, wildlife, and farm-life neighborhood. The development of these housing subdivisions will disturb the habitat of all wild animals that are conserved by all neighbors, specifically Nature's Classroom and South-West Florida Water Management District (SWIFTMUD). There are deer that browse in the neighboring properties, as well as bald eagles that hunt and fly through. Protected animals including red fox, gray fox, and bobcat also travel through this area.

Gopher tortoise burrows are also well-established in all properties, including the property where this project will ensue. However, there was no record of them in the project summary report despite their holes being marked with pink flags.

Not only will this development disrupt wildlife, but it will also generate an increase in traffic, causing congestion and an upward trend of traffic accidents and fatalities. The crash report provided falsely claimed that there were no fatalities over the course of 5 years. In November of 2021, there was a vehicle fatality that occurred directly in front of my residence. The man lost control of his vehicle and crashed into the woods, inevitably resulting in his death. Moreover, according to the article published by 10 Tampa Bay News, over the past five years since 2019, there were a total of "five fatal accidents and 158 traffic crashes have occurred on Morris Bridge Road in Thonotosassa." Their interview with Danny Alvarez, the Hillsborough County Sheriff's Office Chief Communications Officer, is confirmation that traffic enforcement is experiencing difficulty in maintaining safe travel in this area. There was also a fatality from traffic accidents reported in March of 2022.

Furthermore, as a deer farmer, the influx of traffic from this project will distress and endanger my captive, wildlife deer. Deer are known to be easily spooked animals and are highly sensitive to stressful situations. Their shy nature puts them at a higher risk of severely injuring and killing themselves. Frequent accidents resulting from this development will undoubtedly trigger them to harm themselves. This is a liability that can and should be avoided.



**PARTY OF  
RECORD**

## Rome, Ashley

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**From:** Hearings  
**Sent:** Tuesday, April 4, 2023 7:40 AM  
**To:** Rome, Ashley  
**Subject:** FW: Documents for opposition on application RZ-PD 23-0042  
**Attachments:** Documents for land hearing 4-3-23.docx

---

**From:** Sheryl Chewning <schewning42@gmail.com>  
**Sent:** Monday, April 3, 2023 9:32 PM  
**To:** Hearings <Hearings@HillsboroughCounty.ORG>  
**Subject:** Documents for opposition on application RZ-PD 23-0042

**External email:** Use caution when clicking on links, opening attachments or replying to this email.

Please kindly accept these documents in opposition for the rezoning of the property on Morris Bridge Road for application RZ-PD 23-0042



# Attachment A

RZ – PD 23-0042



RZ-PD 23 -0042

# Attachment B

RZ – PD 23-0042





**RZ-PD 23-0042**

# Attachment C

RZ – PD 23-0042





**RZ-PD 23-0042**

# Attachment D

RZ-PD 23-0042





**RZ-PD 23-0042**

# Attachment E

RZ-PD 12-0042





**RZ-PD 23-0042**

# Attachment F

RZ-PD 23-0042





**RZ-PD 23-0042**

# Attachment G

RZ-PD 23-0042





**RZ-PD 23-0042**



# Attachment H

## RZ-PD 23-0042

Drawings show emergency access via property that is **not for sale**. There is no emergency access there as indicated on the drawing.



## Rome, Ashley

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**From:** Hearings  
**Sent:** Friday, April 14, 2023 7:56 AM  
**To:** Rome, Ashley  
**Cc:** Timoteo, Rosalina  
**Subject:** FW: Second set of photos for RZ-PD 23-0042  
**Attachments:** Road pictures for land development oppositiion.docx

---

**From:** Sheryl Chewning <schewning42@gmail.com>  
**Sent:** Thursday, April 13, 2023 4:52 PM  
**To:** Hearings <Hearings@HillsboroughCounty.ORG>  
**Subject:** Second set of photos for RZ-PD 23-0042

**External email:** Use caution when clicking on links, opening attachments or replying to this email.

This is the second set of pictures for the opposition of the rezoning of the property located on Morris Bridge Road RZ-PD 23-0042 for a hearing scheduled on 4/17/23.

Kind Regards,  
Sheryl Chewning



# Attachment A

RZ – PD 23-0042



# Attachment B

## RZ – PD 23-0042



Curve before  
subdivision entry



# Attachment C

## RZ – PD 23-0042

Sign knocked down from  
sliding car by Nature's  
Classroom Entry which is two





# Attachment D



RZ – PD 23-0042