

**Rezoning Application: PD 26-0349**
**Zoning Hearing Master Date: March 23, 2026**
**BOCC Land Use Meeting Date: May 12, 2026**
**1.0 APPLICATION SUMMARY**

**Applicant:** Hill Ward Henderson, P.A.  
**FLU Category:** RES-4  
**Service Area:** Urban  
**Site Acreage:** ~648.8 acres  
**Community Plan Area:** SouthShore Areawide Systems and Wimauma Village  
**Overlay:** None


**Introduction Summary:**

This is a request to rezone a site from a Planned Development (PD) to a new Planned Development in order to add approximately 13.1 acres into the project. The request also seeks to make some amendments to conditions implemented by the Transportation Department. The number of approved residential units will remain unchanged.

Zoning:	Existing		Proposed
District(s)	AR	PD 25-0371 (PRS 25-1373)	PD 26-0349
Typical General Use(s)	Single-Family Residential/Agricultural	Single-Family Detached & Attached (Townhomes) / K-5 Public School	Single-Family Detached & Attached (Townhomes) / K-5 Public School
Acreage	~13.1 acres	~635.7 acres	~648.8 acres
Density/Intensity	1 DU/ 5 AC	3.6 DU/AC with School or 3.9 DU/AC	3.5 DU/AC with School or 3.8 DU/AC
Mathematical Maximum*	2 dwelling units	2,300 dwelling units with School or 2,492 dwelling units	2,595 units per FLU

\*number represents a pre-development approximation

Development Standards:	Existing			Proposed	
District(s)	AR	PD 25-0371		PD 26-0349	
Lot Size / Lot Width	5 acres / 150'	SF Detached	Townhomes	SF Detached	Townhomes
		4,400 sq. ft. / 40'	1,200 sq. ft. / 15'	4,400 sq. ft. / 40'	1,200 sq. ft. / 15'
Setbacks/Buffering and Screening	Front: 50' Side: 25' Rear: 50'	Front: 20' (Garage 25') Side: 5' (Corner 10'/20') Rear: 15'	Front: 20' Side: 5' (Corner 15') Rear: 10'	Front: 20' (Garage 25') Side: 5' (Corner 10'/20') Rear: 15'	Front: 20' Side: 5' (Corner 15') Rear: 10'
Height	50'	35'	35'	35'	35'

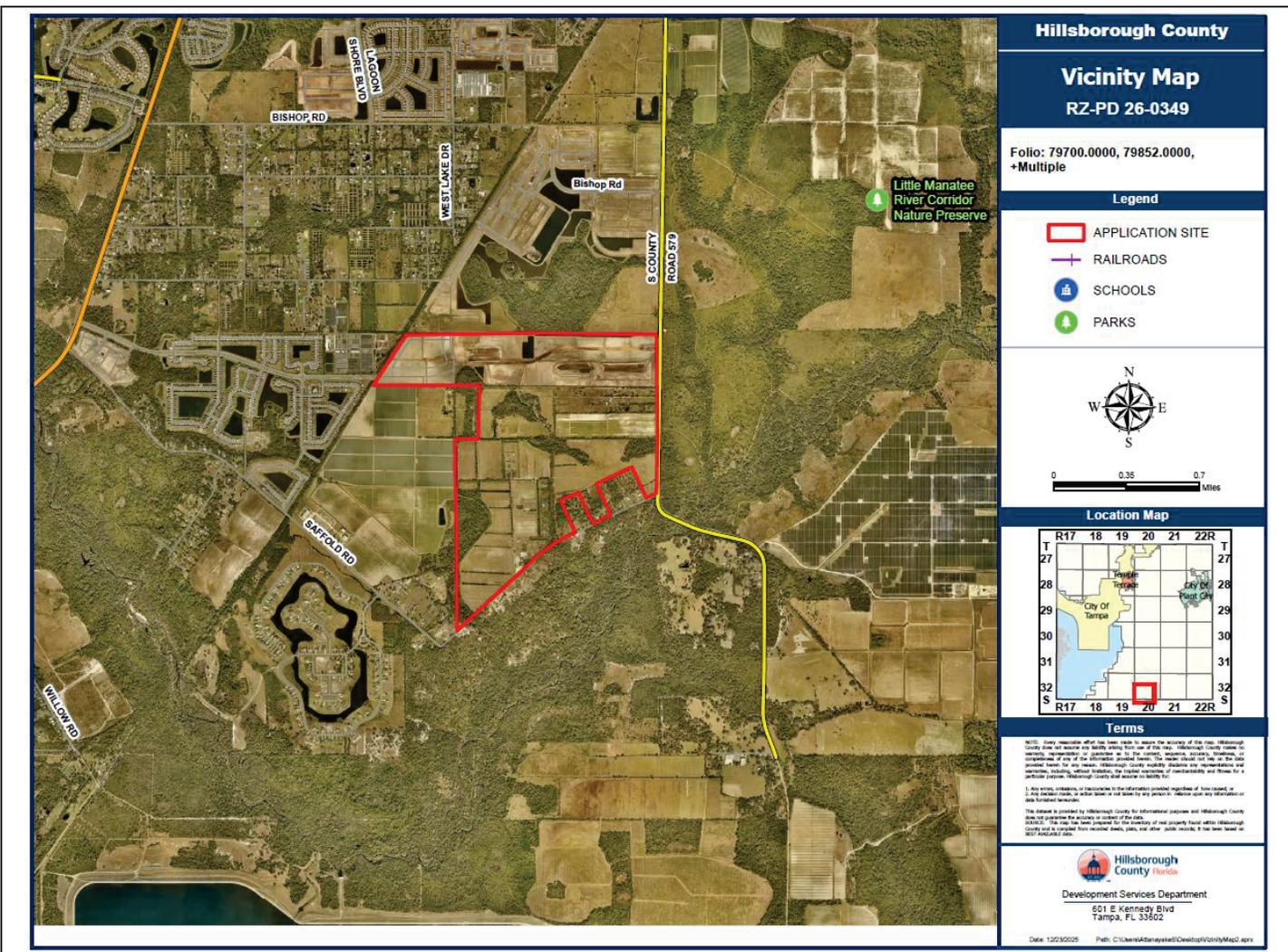
**Additional Information:**

PD Variation(s)	None requested as part of this application
Waiver(s) to the Land Development Code	None requested as part of this application

<b>Planning Commission Recommendation:</b> Consistent	<b>Development Services Recommendation:</b> Approvable, subject to the proposed conditions
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## 2.0 LAND USE MAP SET AND SUMMARY DATA

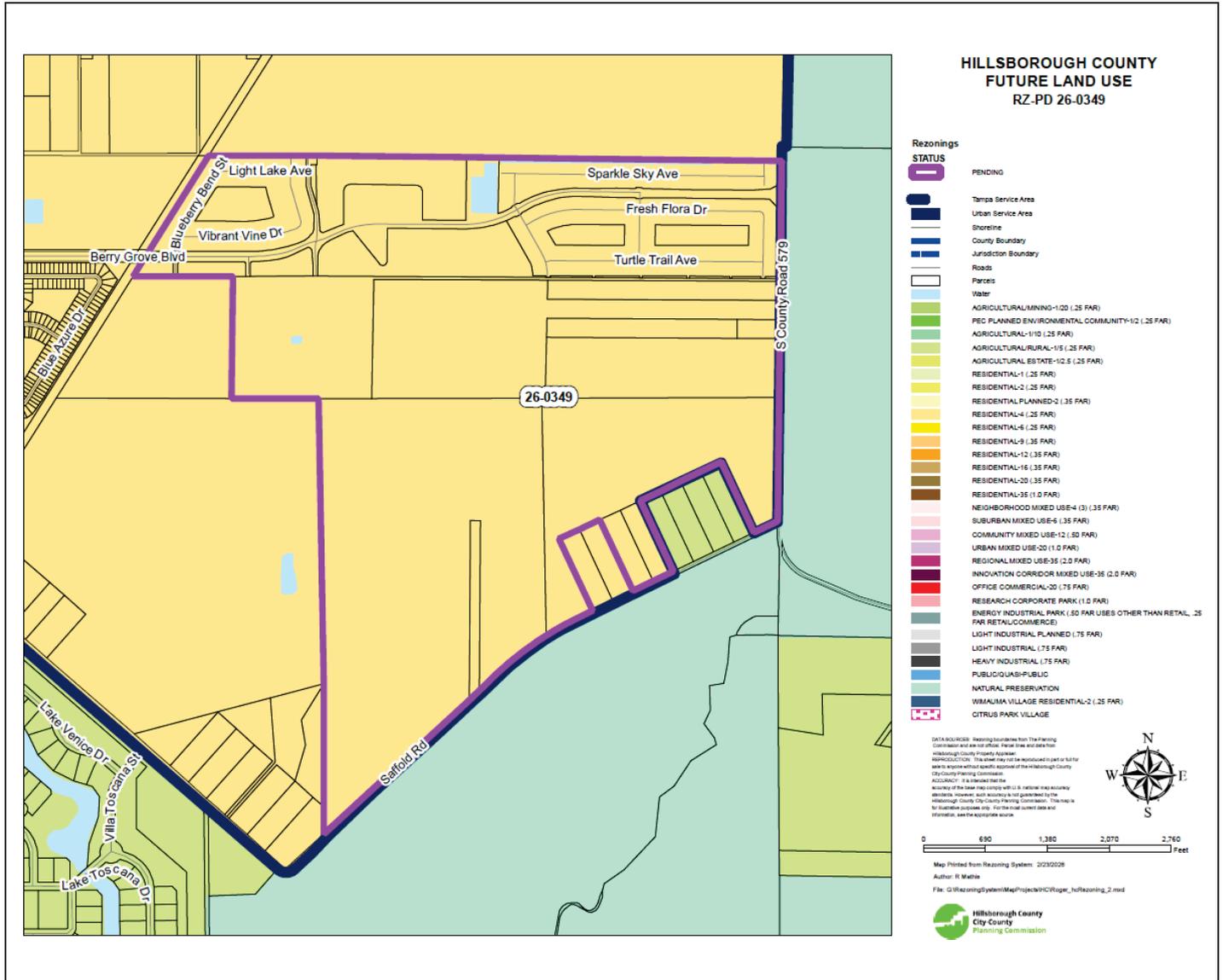
### 2.1 Vicinity Map



**Context of Surrounding Area:**  
Subject site is located predominately in a residential and agricultural area. Adjacent properties are zoned for either residential or agricultural, with several being currently vacant or have existing agricultural uses. Nearby residential uses are located to the west and this site will be an expansion of the “Berry Bay” development.

2.0 LAND USE MAP SET AND SUMMARY DATA

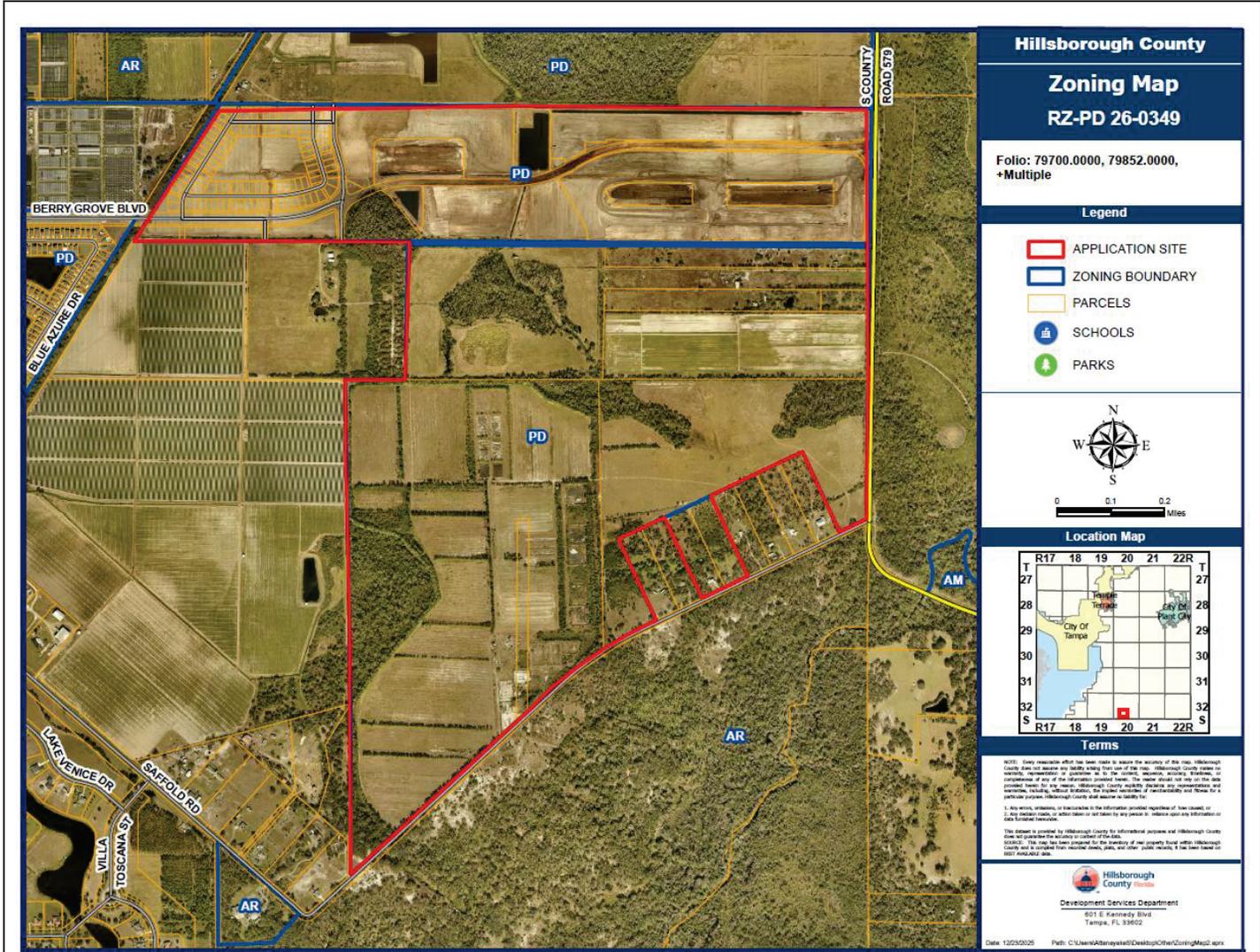
2.2 Future Land Use Map



Subject Site Future Land Use Category:	Residential- 4 (RES-4)
Maximum Density/F.A.R.:	4.0 dwelling units per gross acre / 0.25 FAR
Typical Uses:	Agricultural, residential, neighborhood commercial, office uses, multi-purpose projects and mixed-use development.

2.0 LAND USE MAP SET AND SUMMARY DATA

2.3 Immediate Area Map



Adjacent Zonings and Uses

Location:	Zoning:	Maximum Density/F.A.R. Permitted by Zoning District:	Allowable Use:	Existing Use:
North	PD 18-1048	3.84 dwelling units per acre	Single-Family Conventional	Undeveloped
South	AR	1 dwelling unit per 5 acres	Agriculture/Single-Family Conventional	Residential, Undeveloped
East	AR	1 dwelling unit per 5 acres	Agriculture/Single-Family Conventional	Undeveloped
West	AR	1 dwelling unit per 5 acres	Agriculture/Single-Family Conventional	Agriculture, residential
	PD 24-0044	1.79 dwelling units per acre	Residential detached and attached / public service uses	Agriculture, residential



**3.0 TRANSPORTATION SUMMARY (FULL TRANSPORTATION REPORT IN SECTION 9.0 OF STAFF REPORT)**

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Adjoining Roadways (check if applicable)			
Road Name	Classification	Current Conditions	Select Future Improvements
Berry Grove Blvd.	County Collector - Urban	2 Lanes <input type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width	<input type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other – Roadway Extension
CR 579	County Collector - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width (along project frontage)	<input checked="" type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input checked="" type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other
W. Lake Rd.	County Collector - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road (Existing Portions) <input checked="" type="checkbox"/> Sufficient ROW Width (Within Project)	<input type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input checked="" type="checkbox"/> Substandard Road Improvements <input checked="" type="checkbox"/> Other –Roadway Extension
Saffold Rd.	County Collector - Rural	2 Lanes <input checked="" type="checkbox"/> Substandard Road <input checked="" type="checkbox"/> Sufficient ROW Width (along project frontage)	<input type="checkbox"/> Corridor Preservation Plan <input checked="" type="checkbox"/> Site Access Improvements <input checked="" type="checkbox"/> Substandard Road Improvements <input type="checkbox"/> Other

Project Trip Generation <input type="checkbox"/> Not applicable for this request			
	Average Daily Trips	A.M. Peak Hour Trips	P.M. Peak Hour Trips
Existing	20,386	2,028	1,996
Proposed	20,386	2,028	1,996
Difference (+/-)	No Change	No Change	No Change

\*Trips reported are based on gross external trips unless otherwise noted.

Connectivity and Cross Access <input type="checkbox"/> Not applicable for this request				
Project Boundary	Primary Access	Additional Connectivity/Access	Cross Access	Finding
North	X	Pedestrian & Vehicular	None	Meets LDC
South	X	Pedestrian & Vehicular	None	Meets LDC
East	X	Pedestrian & Vehicular	None	Meets LDC
West	X	Pedestrian & Vehicular	None	Meets LDC
Notes:				

Design Exception/Administrative Variance <input type="checkbox"/> Not applicable for this request		
Road Name/Nature of Request	Type	Finding
CR 579/ Substandard Road	Deminimis Design Exception Requested	Previously Approved
W. Lake Dr./ Substandard Road and New Road	Design Exception Revision Needed	Conceptually Approvable
Saffold Rd./ Substandard Road	Design Exception Revision Needed	Conceptually Approvable
	Choose an item.	Choose an item.
Notes:		

**4.0 ADDITIONAL SITE INFORMATION & AGENCY COMMENTS SUMMARY**

<b>INFORMATION/REVIEWING AGENCY</b>				
<b>Environmental:</b>	<b>Comments Received</b>	<b>Objections</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
Environmental Protection Commission	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Wetlands present
Natural Resources	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Conservation & Environ. Lands Mgmt.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Check if Applicable: <ul style="list-style-type: none"> <li><input checked="" type="checkbox"/> Wetlands/Other Surface Waters</li> <li><input type="checkbox"/> Use of Environmentally Sensitive Land Credit</li> <li><input type="checkbox"/> Wellhead Protection Area</li> <li><input type="checkbox"/> Surface Water Resource Protection Area</li> <li><input type="checkbox"/> Potable Water Wellfield Protection Area</li> <li><input checked="" type="checkbox"/> Significant Wildlife Habitat</li> <li><input type="checkbox"/> Coastal High Hazard Area</li> <li><input type="checkbox"/> Urban/Suburban/Rural Scenic Corridor</li> <li><input checked="" type="checkbox"/> Adjacent to ELAPP property</li> <li><input type="checkbox"/> Other</li> </ul>				
<b>Public Facilities:</b>	<b>Comments Received</b>	<b>Objections</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
<b>Transportation</b> <input checked="" type="checkbox"/> Design Exc./Adm. Variance Requested <input checked="" type="checkbox"/> Off-site Improvements Provided	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See Staff Report
<b>Service Area/ Water &amp; Wastewater</b> <input checked="" type="checkbox"/> Urban <input type="checkbox"/> City of Tampa <input type="checkbox"/> Rural <input type="checkbox"/> City of Temple Terrace	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
<b>Hillsborough County School Board</b> Adequate <input type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A Inadequate <input checked="" type="checkbox"/> K-5 <input type="checkbox"/> 6-8 <input type="checkbox"/> 9-12 <input checked="" type="checkbox"/> N/A	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input type="checkbox"/> No	
<b>Impact/Mobility Fees</b> Townhouse (Fee estimate is based on a 1,500 s.f., 1-2 story) Mobility: \$9,445 * 300 = \$2,833,500 Parks: \$1,957 * 300 = \$587,100 School: \$7,027 * 300 = \$2,108,100 Fire: \$249 * 300 = \$74,700				
Single Family Detached (Fee estimate is based on a 2,000 s.f.) Mobility: \$13,038 * 2,000 = \$26,076,000 Parks: \$2,145 * 2,000 = \$4,290,000 School: \$8,227 * 2,000 = \$16,454,000 Fire: \$335 * 2,000 = \$670,000				
<b>Comprehensive Plan:</b>	<b>Comments Received</b>	<b>Findings</b>	<b>Conditions Requested</b>	<b>Additional Information/Comments</b>
<b>Planning Commission</b> <input type="checkbox"/> Meets Locational Criteria <input checked="" type="checkbox"/> N/A <input type="checkbox"/> Locational Criteria Waiver Requested <input type="checkbox"/> Minimum Density Met <input checked="" type="checkbox"/> N/A	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Inconsistent <input checked="" type="checkbox"/> Consistent	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	

## **5.0 IMPLEMENTATION RECOMMENDATIONS**

### **5.1 Compatibility**

The current Planned Development, 25-0371, was approved to allow a gross density of 3.6 dwelling units per acre. Two developments were proposed. Development Option One allowed 2,000 single-family detached units and 300 townhome units with a public school. Development Option 2 did not include the public school and allowed 2,192 single-family detached units and 300 townhomes.

This new request seeks to keep the same conditions of the previous approval, besides some amendments to Transportation’s conditions, but add approximately 13.1 acres to the project. This added area includes two residential lots adjacent to the south and a long, narrow strip located in the middle of the site.

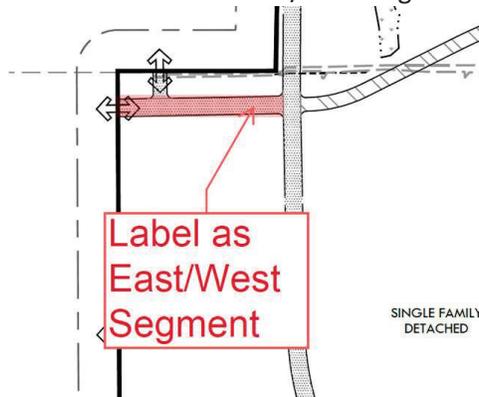
Development Services does not foresee any compatibility concerns with the proposed Planned Development.

### **5.2 Recommendation**

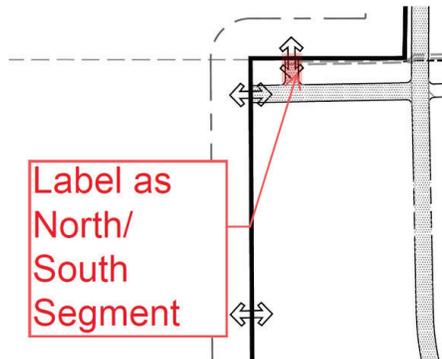
Based on the above considerations, staff finds the proposed Planned Development district, subject to the conditions, approvable.

**Requirements for Certification:**

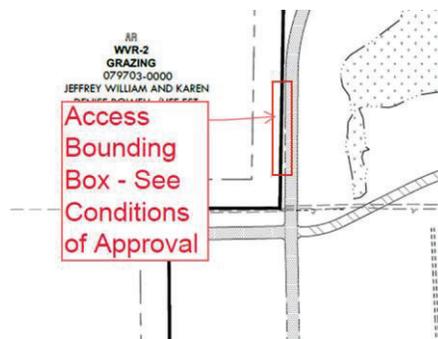
1. Prior to certification of the General Development Plan (GDP), the applicant shall revise the PD site plan to:
  - i. Modify the label reading “Proposed W. Lake Dr. Extension” to instead state “Proposed North/South Collector Road – See Conditions of Approval.”
  - ii. Remove the three internal access arrows along the Berry Grove Blvd. Ext. for consistency. Staff notes that not all connections are shown, and access will be subject to the LDC and conditions of approval.
  - iii. Label the section shown below as “East/West Segment – See Conditions of Approval.”



- iv. Label the section shown below as “North/South Segment – See Conditions of Approval”; and,



- v. Add a bounding box as shown below and label “Access Bounding Box – See Conditions of Approval” .



6.0 PROPOSED CONDITIONS

Approval - Approval of the request, subject to the conditions listed below, is based on the general site plan submitted January 29, 2026.

- 1. The Development shall be limited to: Option A: 2,000 single family detached lots, 300 townhome lots, and a 1,000 student, K-5 public elementary school; or Option B: 2,192 single family detached lots and 300 townhome lots.
- 2. Single-family and townhome lots shall be developed in accordance with the following:

Single-Family Detached Lots:

Minimum Lot Size:	4,400 square feet
Minimum Lot Width:	40 feet
Minimum Lot Depth:	110 feet
Minimum front yard setback:	20 feet
Minimum side yard setback:	5 feet**
Minimum rear yard setback:	10 feet
Maximum building coverage:	75%
Maximum building height:	35 feet (1-3 stories)

\*\*Corner lots shall require a front yard functioning as a side yard setback of 10 feet. If the corner side yard is used for access, the required setback shall be 20 feet.

Townhome Lots:

Minimum Lot Size:	1,200 square feet
Minimum Lot Width:	15 feet
Minimum Lot Depth:	80 feet
Minimum front yard setback:	20 feet
Minimum side yard setback:	5 feet (Corner: 15 feet)
Minimum rear yard setback:	10 feet
Maximum building coverage:	75%
Maximum building height:	35 feet

- 2.1 Any single-family detached lot developed at a lot width of less than 50 feet shall require a 2-car garage.
- 2.2 Any single-family detached lot developed at a lot width of less than 50 feet shall have the home’s primary door face the roadway.
- 3. Under Option A, a 1,000 student K-5 public school is permissible where depicted on the general site plan.
  - 3.1 The school site shall be a minimum of 14 upland acres in size.

- 3.2 The School District and the Developer will use their best efforts to reach a mutually agreeable dedication agreement within three (3) years of the zoning approval for PD 25-0371 (the "Agreement Period").
- 3.3 Any and all roadways within the Planned Development serving and/or providing access to the public school parcel shall be platted to the public school parcel's property line(s) as a public road(s). In no event shall there be any intervening land restriction access to the public school parcel.
- 3.4 Should the School District and the developer not reach a mutually agreeable dedication agreement within the Agreement Period, or should the School District advise the developer that the site is no longer being considered, the developer may develop the area with single-family detached or townhome units in accordance with the development standards found in Condition 2.
- 3.5 If a school is developed, the site shall have access to/from both the Berry Grove Blvd. Ext. and the North/South Collector Roadway.
4. The subject application is adjacent to ELAPP preserves, the Little Manatee River Corridor and the Upper Little Manatee River Corridor. Per LDC 4.01.11, compatibility of the development with the preserve will be ensured with a compatibility plan that addresses issues related to the development such as, but not necessarily limited to, access, prescribed fire, and landscaping. The compatibility plan shall be proposed by the developer, reviewed and approved by the Conservation and Environmental Lands Management Department, and shall be required as a condition of granting a Natural Resources Permit.
5. Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
6. The construction and location of any proposed wetland impacts are not approved by this correspondence but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.
7. Prior to the issuance of any building or land alteration permits or other development, the approved wetland/other surface water (OSW) line must be incorporated into the site plan. The wetland/OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County land Development Code (LDC).
8. Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determination of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.

9. Natural Resources staff identified a number of significant trees on the site including potential Grand Oaks. Every effort must be made to avoid the removal of and design the site around these trees. The site plan may be modified from the Certified Site Plan to avoid tree removal.
10. Wetlands or other surface waters are considered Environmentally Sensitive Areas and are subject to Conservation Area and Preservation Area setbacks. A minimum setback must be maintained around these areas which shall be designated on all future plan submittals. Only items explicitly stated in the condition of approval or items allowed per the LDC may be placed within the wetland setback. Proposed land alterations are restricted within the wetland setback areas.
9. Approval of this petition by Hillsborough County does not constitute a guarantee that Natural Resources approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impacts to trees, natural plant communities or wildlife habitat, and does not grant any implied or vested right to environmental.
10. The construction and location of any proposed environmental impacts are not approved by this correspondence, but shall be reviewed by Natural Resources staff through the site and subdivision development plan process pursuant to the Land Development Code.
11. For the purposes of these zoning conditions:
  - a. The portion of the PD lying north of the northern boundaries of folios 79703.0000 and 79705.0000 is hereafter referred to as "Northern Development Area".
  - b. The portion of folio 79703.0000 within the PD boundary and portion of the PD lying south of the southern boundaries of folios 79703.0000 and 79705.0000 is hereafter referred to as "Southern Development Area".
12. Development shall be limited as follows:
  - a. Under Development Option A, development shall be to a maximum of 2,000 single-family detached dwelling units, 300 townhomes, and a 1,000-student maximum non-charter public school with grade levels K-5 as further described in Condition 13.
  - b. Under Development Option B, development shall be to a maximum of 2,192 single-family detached dwelling units, and 300 townhomes.
  - c. Irrespective of which option is chosen:
    - i. Townhomes shall be constructed in buildings with 3 or more attached dwelling units within each building; and,
    - ii. The above development maximums shall be further restricted by the additional maximum trip generation thresholds within the Northern Development Area and Southern Development Area, as further detailed below.

- d. Notwithstanding anything herein these conditions to the contrary, no development shall be permitted that causes cumulative development to exceed the following thresholds:
    - i. Within the Northern Development Area, no development shall be permitted that causes cumulative development to exceed 8,436 gross average daily trips, 1,171 gross a.m. peak hour trips, or 747 gross p.m. peak hour trips, nor shall development be permitted which exceeds 7,183 net average daily trips, 762 net a.m. peak hour trips, or 659 net p.m. peak hour trips.
    - ii. Within the Southern Development Area, no development shall be permitted that causes cumulative development to exceed 11,950 gross average daily trips, 857 gross a.m. peak hour trips, or 1,249 gross p.m. peak hour trips, nor shall development be permitted which exceeds 11,287 net average daily trips, 640 net a.m. peak hour trips, or 1,203 net p.m. peak hour trips.
    - iii. Concurrent with each increment of development, the developer shall provide a list of existing and previously approved uses within the PD. The list shall contain data including gross floor area, number of students, type of use, date the use was approved by Hillsborough County, references to the site subdivision Project Identification number (or if no project identification number exists, a copy of the permit or other official reference number), calculations detailing individual and cumulative gross and net trip generation impacts for that increment of development, and source(s) for the data used to develop such estimates. Calculations showing the remaining number of available trips for each analysis period (i.e. averaged daily, a.m. peak and p.m. peak) shall also be provided).
13. The Option A school shall be limited to a non-charter public facility serving grade levels K through 5, and with a maximum of 1,000 students. Notwithstanding the exemptions provided in LDC Sec. 6.03.10 which are specifically applicable to public schools, the property owner shall provide adequate on-site vehicular queuing and take other actions to limit off-site impacts as further described herein. Additionally, the school shall provide for onsite vehicular queuing for the number of students who are projected to be ineligible for busing (hereafter referred to as "Non-Bussed Students"). Specifically:
- i. The queue shall provide for the uninterrupted stacking of vehicles within the subject site;
  - ii. The minimum length of queue for the school shall be determined by multiplying the number of Non-Bussed Students by 0.196, then multiplied by 25 feet, and then multiplied by 1.25; and,
  - iii. The school shall take all actions necessary to ensure that students are not dropped off or picked up outside of school property (i.e. within adjacent parcels or along roadways along the school frontage or proximate to the school).
- The school shall not permit students to be dropped off outside of the school property, including along the property's roadway frontages. In such instance, the school shall take all actions necessary to ensure such violations of the conditions of approval are cured.
14. Notwithstanding anything on the PD site plan or herein these conditions to the contrary, bicycle and pedestrian access, or access connections to continue/extend the Multi-Use Trail (MUT), may be permitted anywhere along the PD boundaries.

15. The project shall be served by and limited to the following vehicular access connections:
- a. Within the Northern Development Area:
    - i. One (1) connection to CR 579;
    - ii. One (1) connection to US 301 via an extension of Berry Grove Blvd.;
    - iii. Three (3) stubouts along the northern boundary of the Northern Development Area;and,
    - iv. Three (3) stubouts along the southern boundary of the Northern Development Area.
  - b. Within the Southern Development Area:
    - i. Two (2) connections to CR 579;
    - ii. One (1) connection to Saffold Rd.;
    - iii. Two (2) stubouts along the western boundary of the Southern Development Area; and,
    - iv. One (1) stubout along the northern boundary of the Southern Development Area.
  - c. There shall be a minimum of three (3) connections between the Northern Development Area and the Southern Development Area, the westernmost of which is shown on the site plan as the North/South Collector Roadway.
16. With respect to project roadways:
- a. The developer shall construct the extension of Berry Grove Blvd. (i.e. the east-west collector roadway within the Northern Development Area between US 301 and CR 579) as a 2-lane, collector roadway utilizing the Typical Section standards shown on the PD site plan. The roadway shall be constructed as a divided facility, expandable to 4-lanes west of the internal roundabout, and as an undivided 2-lane facility east of the internal roundabout. The roadway shall be constructed prior to or concurrent with the initial increment of development. Additionally:
    - i. Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC; and,
    - ii. The developer shall dedicate and convey to Hillsborough County sufficient right-of-way necessary to accommodate construction of eastbound right and westbound left turn lanes (by others) at the two (2) westernmost access connection along Berry Grove Blvd. (within the PD).
  - b. With respect to the W. Lake Dr. Extension and substandard roadway improvements, and North/South Collector roadway:
    - i. The design and permitting for the W. Lake Dr. substandard roadway improvements shall be completed by the developer prior to issuance of the 601<sup>st</sup> residential building permit.

- ii. The W. Lake Dr. substandard roadway improvements between Bishop Rd. and Janes Dr., shall be constructed utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. This shall be constructed by the developer prior to issuance of the 1,000th residential building permit;
- iii. The W. Lake Dr. Extension within the adjacent PD located north of the Northern Development Area, as well as portions of the extension between the boundary of that PD and Janes Dr., shall be constructed utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. This shall be constructed by the developer prior to issuance of the 1,000th residential building permit;
- iv. The W. Lake Dr. Extension within the Northern Development Area (both north and south of the proposed internal roundabout) shall be constructed as a 2-lane collector roadway utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. This shall be constructed by the developer with the first increment of development within the Northern Development Area. Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC;
- v. The North/South Collector Roadway (within the Northern and Southern Development Areas) shall be constructed as a 2-lane collector roadway utilizing the C3-2U Typical Section standard as contained in the Hillsborough County Transportation Design Manual (TDM); however, the Shared Use Path shall be constructed on both sides of the roadway. This shall be constructed by the developer with the first increment of development within the Southern Development Area. Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC.

Furthermore, the roadway shall be designed and constructed to accommodate certain site access improvements anticipated to be constructed in the future (by others), and the developer shall be required to dedicate and convey additional right-of-way to Hillsborough County as necessary to permit construction of such turn lanes by others, specifically;

- 1. A southbound to westbound right turn lane on the North/South Collector Roadway onto the east/west segment; and,
- 2. A northbound to westbound left turn lane on the North/South Collector Roadway onto the east/west segment.

Lastly, subject to compliance with applicable access management standards, the County may permit additional access to adjacent properties along that section of the North/South Collector Roadway identified on the PD side plan by a bounding box without further modifications of this PD.

- c. With respect to the CR 579 substandard roadway improvements:
  - i. The design and permitting for CR 579 substandard roadway improvements shall be completed prior to the issuance the 601<sup>st</sup> building permit.

- ii. For the first 1,000 units within the Northern and Southern Development Areas, the developer shall undertake improvements which include construction of a 10-foot-wide multi-purpose pathway along its CR 579 frontage and which includes a crossing of CR 579 to the Little Manatee Corridor Nature Preserve trailhead entrance road as described in Condition 17.a., concurrent with the construction of the site development improvements associated with this first increment of development;
- iii. Prior or concurrent with the issuance of the 1,000<sup>th</sup> residential building permit within the Northern and Southern Development Areas, the developer shall make certain improvements within each of the three (3) discrete sections (A, B and C) of the roadway, as described below.
- iv. Within the Southern Development Area, the developer shall be permitted to obtain building permits for development north of (but not to include) the east-west roadway nor to include any development south of the east-west roadway, provided the following improvements are in place. Specifically, either items 1 through 4 (inclusive) shall be required, or item 5 shall be required, as further detailed below:
  - 1. The Berry Grove Blvd. Ext. to US 301; and,
  - 2. The W. Lake Dr. improvements between Bishop Rd. and Berry Grove Blvd.; and,
  - 3. The northernmost connection to CR 579 within the Southern Development Area; and,
  - 4. The two (2) easternmost local roadway connections along the northern boundary of the Southern Development Area, shall be completed such that they connect the Northern Development Area and Southern Development Area through adjacent folio 79703.0000. Notwithstanding anything herein to the contrary, the third (westernmost) local roadway connection shall be constructed concurrently with the East-West Roadway or adjacent residential development within the Southern Development Area, whichever occurs earlier; or,
  - 5. The CR 579 improvements within Segments A, B and C have been completed and are open for beneficial use.
- v. Notwithstanding the above and solely with respect to development within the Northern Development Area, the developer shall not be required to make improvements identified within Sections A and B, if the W. Lake Dr. improvements between Bishop Rd. and the Berry Grove Blvd. Ext. have been completed and are open for beneficial use (consistent with those improvements referenced in Condition 16.b.i through 16.b.iii.).
- vi. Notwithstanding the above and solely with respect to development within the Southern Development Area, the developer shall not be required to make improvements identified within Sections A and B, if the W. Lake Dr. improvements referenced in Condition 16.b.iii.1. through 4. together with a continuous extension of W. Lake Dr. between the southern boundary of the Northern Development Area and the Proposed East-West Road within the Southern Development Area (i.e. through adjacent folio 79703.0000) is constructed and open for beneficial use.
- vii. Specifically, and subject to the clarifications and requirements provided above:

1. Improvements to CR 579 have been broken into three (3) segments. Segment A is defined as the section of CR 579 between SR 674 and the southern property boundary of folio 79456.0000 (i.e. the southern boundary of PD 26-0008). Segment B is defined as that portion of CR 579 between the southern property boundary of folio 79456.0000 and the southern boundary of adjacent PD 24-1033 (i.e. the northern boundary of the subject PD). Segment C is defined at that portion of CR 579 between the northern boundary of the subject PD and the southern boundary of the subject PD.
  2. Within Segment A, the developer shall:
    - a. Construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway; and,
    - b. Construct a 10-foot-wide sidewalk along the west side of the roadway. The 10-foot-wide sidewalk will transition to a 5-footwide sidewalk north of proposed PD 26-0008, where right-of-way does not exist to permit construction of the wider sidewalk.
  3. Within Segment B, the developer shall construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway.
  4. Within Segment C, the developer shall:
    - a. Dedicate and convey to Hillsborough County sufficient right-of-way to accommodate proposed improvements (a minimum of 48-feet east of the existing centerline). This shall be in addition to any right-of-way necessary to accommodate site access improvements or additional preservation required pursuant to the Hillsborough County Corridor Preservation Plan as further described herein;
    - b. Construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway; and,
  5. Construct a 10-foot-wide sidewalk along the west side of the roadway.
- d. With respect to the Saffold Rd. substandard roadway improvements:
- i. Prior to or concurrent with connection to Saffold Road, whichever occurs earlier, the developer shall make certain improvements within each of two (2) discreet sections of the roadway. Segment B is defined as that portion of Saffold Rd. along the frontages of folios 79700.0400, 79700.0300, 79700.0200, 79700.0250, 79700.0050, and 79700.0150. Segment A is defined as those portions of Saffold Rd. between CR 579 and the western project boundary, excluding the area defined as Segment B. Specifically:
  - ii. Within Segment B, the developer shall:
    1. Construct 6-foot-wide stabilized shoulders along both sides of the roadway; and,
    2. Construct a 5-foot-wide sidewalk along the north side of the roadway.

- iii. Within Segment A, the developer shall:
  - 1. Construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway; and,
  - 2. Construct a 10-foot-wide sidewalk along the north side of the roadway. This will take the place of the 5-foot-wide sidewalk that would normally be constructed along the west side of the roadway.
- iv. Notwithstanding anything herein to the contrary, the Saffold Rd. connection and substandard road improvements shall be completed prior to issuance of the 1,500<sup>th</sup> building permit.
- e. The roadway between the western boundary of the Southern Development Area (i.e. the boundary with folio 79707.0000) and the North/South Collector Roadway, together with the north/south stubout to the southern boundary of folio 79703.0000, shall be constructed as 2-lane urban collector roadways utilizing the standards described below.
  - 1. The roadway shall be constructed to the C3-2U of Hillsborough County Transportation Design Manual (TDM); however, the developer shall be required to construct the Shared Use Path on both sides of the road. Additionally, the roadway shall be designed and constructed to accommodate certain site access improvements anticipated to be constructed in the future (by others), and the developer shall be required to dedicate and convey additional right-of-way to Hillsborough County as necessary to permit construction of such turn lanes by others, specifically:
    - i. An eastbound to northbound left turn lane on the east/west segment onto the North/South Collector Roadway; and,
    - ii. An eastbound to northbound left turn lane on the east/west segment onto the north/south segment.
  - 2. The north/south segment shall be constructed by the developer of the subject PD with the first increment of development of folio 79703.0000 or any development west of the North/South Collector Roadway, whichever occurs earlier. The roadway shall be constructed utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. Additionally, the roadway shall be designed and constructed to accommodate certain site access improvements anticipated to be constructed in the future (by others), and the developer shall be required to dedicate and convey additional right-of-way to Hillsborough County as necessary to permit construction of such turn lanes by others, specifically:
    - i. A southbound to westbound right turn lane on the east/west segment onto the North/South Collector Roadway.
- f. The Proposed East-West Road identified on the PD site plan within the Southern Development Area (i.e. between the W. Lake Dr. Ext. and CR 579) shall be constructed as a 2-lane urban collector roadway utilizing C3-2U standards as found within the Transportation Design Manual

- (TDM); however, the Shared Use Path shall be constructed along both sides of the roadway. Notwithstanding the above, the developer shall have the option of submitting a transportation analysis together with the initial increment of development within the Southern Development Area that demonstrates a collector roadway design is not warranted or otherwise necessary. Such study will be subject to review and approval by Hillsborough County. If approved, the developer shall be permitted to construct the Proposed East-West Road as a 2-lane urban local roadway utilizing the Typical Section – 3 (TS-3) standards as found within the 2021 Transportation Technical Manual (TTM). If the roadway remains a collector roadway, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC.
- g. Other internal project roadways shall be constructed to an appropriate urban typical section as described in the 2021 Hillsborough County TTM and/or last edition of the Hillsborough County Transportation Design Manual (TDM), as applicable. Designation of appropriate typical sections shall occur at the time of plat/site/construction plan review and be based upon anticipated traffic volumes within each segment.
- h. The total right-of-way widths shown in the Design Exception and on the PD site plan are minimum widths. Additionally:
- i. The developer shall preserve a minimum of +/- 46 feet of right-of-way west of the westernmost proposed internal roundabout or as otherwise necessary to accommodate the future expansion of Berry Grove Blvd. as a future 4-lane roadway, expandable to the inside. The intent of these conditions is to require the developer to secure the dedication, conveyance and preservation of certain rights-of-way to the County as described above, both within the project and through adjacent folios 79710.0585 and 79702.0010.
  - ii. In accordance with the Hillsborough County Corridor Preservation Plan, the developer shall preserve sufficient right-of-way along the project's CR 579 frontages such that 107 feet of right-of-way is available for future improvements west of the existing eastern right-of-way boundary (i.e. to accommodate a future 2-lane enhanced rural roadway). Only those interim uses allowed by the Hillsborough County LDC shall be permitted within the preserved right-of-way. The right-of-way preservation area shall be shown on all future site plans, and building setback shall be calculated from the future right-of-way line.
  - iii. Notwithstanding anything shown on the PD site plan to the contrary, the developer shall dedicate and convey to Hillsborough County sufficient right-of-way as necessary to accommodate the proposed/required project traffic signals and/or roundabouts, as well as required site access improvements and associated drainage, both within and external to the project. Where necessary, such right-of-way shall be dedicated and conveyed in addition to right-of-way dedication or preservation requirements listed herein these conditions.
  - iv. The amount and location of right-of-way dedication for roundabouts shall be based upon Transportation Technical Manual (TTM) and/or Transportation Design Manual (TDM) and roundabout design requirements, as applicable, and shall be reviewed and approved by the appropriate agencies including Hillsborough County Development Services and/or Public Works.

17. With respect to other site access and required improvements:
  - a. Prior to or concurrent with the initial increment of development within the Northern Development Area, the developer shall construct the roundabout with MUT connection to the Little Manatee Corridor Nature Preserve Trailhead.
  - b. Prior to or concurrent with the initial increment of development within the Southern Development Area, the developer shall:
    - i. Construct southbound to westbound right turn lanes on CR 579 at each project access serving the Southern Development Area;
    - ii. Construct northbound to westbound left turn lanes on CR 579 at each project access serving the Southern Development Area; and,
    - iii. The developer shall perform a signal warrant analysis at the intersection of the southernmost project access along CR 579, which shall be reviewed by and subject to the approval of Hillsborough County Public Works. If such signal is found not to be warranted the developer shall have no further obligation with respect to the signal. If such signal is found to be warranted, the developer shall install the signal. Alternatively, at the developer's option, the developer may construct a roundabout at the access. If the developer constructs a roundabout, the traffic signal and turn lanes serving such access (i.e. as described in Condition 17.b.i. and 17.b.ii.), above, shall not be required.
  - c. Prior to or concurrent with the increment of development within the Southern Development Area connecting to Saffold Rd., the developer shall:
    - i. Provide a trip generation and site access analysis to determine whether construct of a westbound to north bound right turn land on Saffold Rd. into the project's access is warranted (if warranted the developer shall construct the improvement); and
    - ii. Construct an eastbound to northbound left turn lane on Saffold Rd. into the project's access.
18. Notwithstanding anything shown on the PD site plan to the contrary, the number and spacing of access points along collector and arterial roadways (whether internal or external to the PD) shall be governed by LDC Sec. 6.04.03.I and 6.04.07, unless otherwise varied through the Sec. 6.04.02.B. Administrative Variance process at the time of plat/site/construction plan review.
19. The project shall provide a Multi-Use Trail (MUT) where depicted on the general site plan. With respect to Multi-Use Trail (MUT):
  - a. That portion of the MUT running alongside the Berry Grove Blvd. extension. shall be constructed utilizing the Typical Section standards depicted on the PD site plan together with the initial increment of development.
  - b. Those portions of MUT running through the internal roundabout and the roundabout to be constructed at the intersection of the Berry Grove Blvd. extension. and CR 579 and along the east side of CR 579, and terminating at the trailhead entrance road located on the east side of

CR 579, shall be constructed with a minimum width of 12-feet; however other features of the typical section shall be dictated by roundabout design requirements, which are subject to the review and approval of Hillsborough County Public Works at the time of plat/site/construction plan review. These portions of the trail shall be constructed concurrently with the roundabout.

- c. The developer shall design and construct slip ramps as necessary to transition between the use of MUTs/wide sidewalks and roadways with on-street bicycle facilities and roadways with no on-street bicycle facilities, as applicable.

- 20. In addition to any temporary end of roadway signage required by the MUTCD, the developer shall install signage at all roadway/MUT access stubouts not connecting to an existing roadway which identifies the stubout as a "Future Roadway Connection" as applicable.
- 21. If PD 26-0349 is approved, the County Engineer will approve a deminimis Design Exception (dated July 9, 2025) which was approved by the County Engineer (on October 15, 2025) for the CR 579 substandard roadway improvements. As CR 579 is a substandard collector roadway, the developer will be required to make certain improvements to CR 579 consistent with the Design Exception (DE) and these conditions of approval. Specific improvements are outlined in Condition 16.c., above.
- 22. Prior to approval of the next plat/site/construction plan approval, the developer shall submit a revised Design Exception (DE), based on a previously approved Design Exception (dated June 12, 2025), which was approved by the County Engineer (on October 15, 2025) for the Saffold Rd. substandard roadway improvements. As Saffold Rd. is a substandard collector roadway, the developer will be required to make certain improvements to Saffold Rd. consistent with the DE and these conditions of approval. Specific improvements are outlined in Condition 16.d., above. The DE revisions shall modify the extent of required Segment A improvements, such that Segment A includes all frontages within the project (including portions added as a part of PD 26-0349).
- 23. If PD 26-0349 is approved, the developer will be required to submit a new Design Exception request for review and approval by the County Engineer prior to issuance of the next plat/site/construction plan approval, and which shall be based on a previously approved Design Exception (dated June 13, 2025) which was approved by the County Engineer (on October 15, 2025) for the W. Lake Dr. improvements. Improvements to W. Lake Dr. shall be broken into three (3) segments. Segment A is defined as the section of W. Lake Dr. between Bishop Rd. and the northern property boundary of the Cypress Ridge Development (i.e. adjacent PRS 24-1033/pending PD 25-1386). Segment B is defined as that portion of W. Lake Dr. between the southern property boundary of the Cypress Ridge Development (i.e. the northern boundary of the subject PD) and Berry Grove Blvd. Segment C is defined at those portions of W. Lake Dr. between Berry Grove Blvd. and the east/west road segment.

The Design Exception shall authorize deviations from the TS-4 Typical Section (for 2-lane, Undivided, Urban Collector Roadways) as found in the 2021 Hillsborough County Transportation Technical Manual (TTM). Specifically:

- a. Within Segment A, the developer shall widen/construct the roadway consistent with the TS-4 Typical Section, except that instead of the 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide asphalt paths along

both sides of the roadway. Additionally, the developer shall be permitted to reduce the required asphalt path separation as outlined below. The developer has proposed two typical sections for this segment (A-1 and A-2). Typical Section A-2 is the section where right-of-way is limited and/or there are design constraints. Use of Typical Section A-2 shall be minimized to the greatest extent feasible. The developer shall be permitted to reduce the required asphalt path separation between the closest edge of the asphalt path and the travel lane from 14-feet to 7-feet (for Typical Section A-1) and from 14-feet to 2-feet (for Typical Section A-2).

- b. Within Segment B, the developer shall construct the roadway consistent with the TS-4 Typical Section, except that instead of the 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide sidewalks along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required sidewalk separation between the closest edge of the sidewalk and the travel lane from 14-feet to 6-feet. The developer shall also be permitted to eliminate the required 2-foot pedestrian clearance area between the back of sidewalk and edge of right-of-way.
- c. Within Segment C, the developer shall construct the roadway consistent with the TS-4 Typical Section, except that instead of the 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide asphalt paths along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required asphalt path separation between the closest edge of the sidewalk and the travel lane from 14-feet to 7-feet.

- 24. If the notes and/or graphics on the site plan conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
- 25. In accordance with LDC Section 5.03.07.C, the certified PD general site plan shall expire for the internal transportation network and external access points, as well as for any conditions related to the internal transportation network and external access points, if site construction plans, or equivalent thereof, have not been approved for all or part of the subject Planned Development within 5 years of the effective date of the PD unless an extension is granted as provided in the LDC. Upon expiration, re-certification of the PD General Site Plan shall be required in accordance with provisions set forth in LDC Section 5.03.07.C.

**Zoning Administrator Sign Off:**



**SITE, SUBDIVISION AND BUILDING CONSTRUCTION IN ACCORDANCE WITH HILLSBOROUGH COUNTY SITE DEVELOPMENT PLAN & BUILDING REVIEW AND APPROVAL.**

Approval of this re-zoning petition by Hillsborough County does not constitute a guarantee that the project will receive approvals/permits necessary for site development as proposed will be issued, nor does it imply that other required permits needed for site development or building construction are being waived or otherwise approved. The project will be required to comply with the Site Development Plan Review approval process in addition to obtain all necessary building permits for on-site structures.

**7.0 ADDITIONAL INFORMATION AND/OR GRAPHICS (See following pages)**

Property Violation History

Agency	Number	Violation	Status
<b>Code Enforcement*</b>			
<input checked="" type="checkbox"/> None current or pending			
<input type="checkbox"/> Violation(s)			
<b>Building Code Compliance*</b>			
<input checked="" type="checkbox"/> None current or pending			
<input type="checkbox"/> Violation(s)			
<b>Natural Resources*</b>			
<input checked="" type="checkbox"/> None current or pending			
<input type="checkbox"/> Violation(s)			
<b>EPC*</b>			
<input checked="" type="checkbox"/> None current or pending			
<input type="checkbox"/> Violation(s)			

\*past 12 months from intake date



**9.0 FULL TRANSPORTATION REPORT (see following pages)**

**AGENCY REVIEW COMMENT SHEET**

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TO: Zoning Technician, Development Services Department

DATE: 3/16/2026

REVIEWER: James Ratliff, AICP, PTP, Principal Planner

AGENCY/DEPT: Transportation

PLANNING AREA/SECTOR: WM/ South

PETITION NO: PD 26-0349

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- This agency has no comments.
  
- This agency has no objection.
  
- This agency has no objection, subject to listed or attached conditions.
  
- This agency objects for the reasons outlined below.

**CONDITIONS OF APPROVAL**

*The below conditions represent the existing conditions for 25-1373, which (with respect to Transportation related issues) are being brought forward as-is except for necessary changes due to the addition of land or to correct scrivener's error(s); however, staff has proposed such conditions in the form of strikethrough and underline form the previous PD to aid readers in determining how these conditions are different from the last approved PD, despite all conditions below being technically new conditions (given this is a new PD application).*

1. The Development shall be limited to: Option A: 2,000 single family detached lots, 300 townhome lots, and a 1,000 student, K-5 public elementary school; or Option B: 2,192 single family detached lots and 300 townhome lots.
  
2. Single-family and townhome lots shall be developed in accordance with the following:

Single-Family Detached Lots:

Minimum Lot Size:	4,400 square feet
Minimum Lot Width:	40 feet
Minimum Lot Depth:	110 feet
Minimum front yard setback:	20 feet
Minimum side yard setback:	5 feet**
Minimum rear yard setback:	10 feet
Maximum building coverage:	75%
Maximum building height:	35 feet (1-3 stories)

\*\*Corner lots shall require a front yard functioning as a side yard setback of 10 feet. If the corner side yard is used for access, the required setback shall be 20 feet.

Townhome Lots:

Minimum Lot Size:	1,200 square feet
Minimum Lot Width:	15 feet
Minimum Lot Depth:	80 feet
Minimum front yard setback:	20 feet

Minimum side yard setback:	5 feet (Corner: 15 feet)
Minimum rear yard setback:	10 feet
Maximum building coverage:	75%
Maximum building height:	35 feet

- 2.1 Any single-family detached lot developed at a lot width of less than 50 feet shall require a 2-car garage.
- 2.2 Any single-family detached lot developed at a lot width of less than 50 feet shall have the home's primary door face the roadway.
3. Under Option A, a 1,000 student K-5 public school is permissible where depicted on the general site plan.
  - 3.1 The school site shall be a minimum of 14 upland acres in size.
  - 3.2 The School District and the Developer will use their best efforts to reach a mutually agreeable dedication agreement within three (3) years of the zoning approval for PD 25-0371 (the "Agreement Period").
  - 3.3 Any and all roadways within the Planned Development serving and/or providing access to the public school parcel shall be platted to the public school parcel's property line(s) as a public road(s). In no event shall there be any intervening land restriction access to the public school parcel.
  - 3.4 Should the School District and the developer not reach a mutually agreeable dedication agreement within the Agreement Period, or should the School District advise the developer that the site is no longer being considered, the developer may develop the area with single-family detached or townhome units in accordance with the development standards found in Condition 2.
  - 3.5 If a school is developed, the site shall have access to/from both the Berry Grove Blvd. Ext. and the North/South Collector Roadway.
4. The subject application is adjacent to ELAPP preserves, the Little Manatee River Corridor and the Upper Little Manatee River Corridor. Per LDC 4.01.11, compatibility of the development with the preserve will be ensured with a compatibility plan that addresses issues related to the development such as, but not necessarily limited to, access, prescribed fire, and landscaping. The compatibility plan shall be proposed by the developer, reviewed and approved by the Conservation and Environmental Lands Management Department, and shall be required as a condition of granting a Natural Resources Permit.
5. Approval of this zoning petition by Hillsborough County does not constitute a guarantee that the Environmental Protection Commission of Hillsborough County (EPC) approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any impact to wetlands, and does not grant any implied or vested right to environmental approvals.
6. The construction and location of any proposed wetland impacts are not approved by this correspondence but shall be reviewed by EPC staff under separate application pursuant to the EPC Wetlands rule detailed in Chapter 1-11, Rules of the EPC, (Chapter 1-11) to determine whether such impacts are necessary to accomplish reasonable use of the subject property.
7. Prior to the issuance of any building or land alteration permits or other development, the approved wetland/other surface water (OSW) line must be incorporated into the site plan. The wetland/OSW line must appear on all site plans, labeled as "EPC Wetland Line", and the wetland must be labeled as "Wetland Conservation Area" pursuant to the Hillsborough County land Development Code (LDC).
8. Final design of buildings, stormwater retention areas, and ingress/egresses are subject to change pending formal agency jurisdictional determination of wetland and other surface water boundaries and approval by the appropriate regulatory agencies.
9. Approval of this petition by Hillsborough County does not constitute a guarantee that Natural Resources approvals/permits necessary for the development as proposed will be issued, does not itself serve to justify any

impacts to trees, natural plant communities or wildlife habitat, and does not grant any implied or vested right to environmental.

10. The construction and location of any proposed environmental impacts are not approved by this correspondence, but shall be reviewed by Natural Resources staff through the site and subdivision development plan process pursuant to the Land Development Code.

11. For the purposes of these zoning conditions:

- a. The portion of the PD lying north of the northern boundaries of folios 79703.0000 and 79705.0000 is hereafter referred to as “Northern Development Area”.
- b. The portion of [folio 79703.0000 within the PD boundary and portion of](#) the PD lying south of the ~~northern~~-southern boundaries of folios 79703.0000 and 79705.0000 is hereafter referred to as “Southern Development Area”.

12. Development shall be limited as follows:

- a. Under Development Option A, development shall be to a maximum of ~~2,300~~[2,000](#) single-family detached dwelling units, 300 townhomes, and a 1,000-student maximum non-charter public school with grade levels K-5 as further described in Condition 13.
- b. Under Development Option B, development shall be to a maximum of 2,192 single-family detached dwelling units, and 300 townhomes.
- c. Irrespective of which option is chosen:
  - i. Townhomes shall be constructed in buildings with 3 or more attached dwelling units within each building; and,
  - ii. The above development maximums shall be further restricted by the additional maximum trip generation thresholds within the Northern Development Area and Southern Development Area, as further detailed below.
- d. Notwithstanding anything herein these conditions to the contrary, no development shall be permitted that causes cumulative development to exceed the following thresholds:
  - i. Within the Northern Development Area, no development shall be permitted that causes cumulative development to exceed 8,436 gross average daily trips, 1,171 gross a.m. peak hour trips, or 747 gross p.m. peak hour trips, nor shall development be permitted which exceeds 7,183 net average daily trips, 762 net a.m. peak hour trips, or 659 net p.m. peak hour trips.
  - ii. Within the Southern Development Area, no development shall be permitted that causes cumulative development to exceed 11,950 gross average daily trips, 857 gross a.m. peak hour trips, or 1,249 gross p.m. peak hour trips, nor shall development be permitted which exceeds 11,287 net average daily trips, 640 net a.m. peak hour trips, or 1,203 net p.m. peak hour trips.
  - iii. Concurrent with each increment of development, the developer shall provide a list of existing and previously approved uses within the PD. The list shall contain data including gross floor area, number of students, type of use, date the use was approved by Hillsborough County, references to the site subdivision Project Identification number (or if no project identification number exists, a copy of the permit or other official reference number), calculations detailing individual and cumulative gross and net trip generation impacts for that increment of development, and source(s) for the data used to develop such estimates. Calculations showing the remaining number of available trips for each analysis period (i.e. averaged daily, a.m. peak and p.m. peak) shall also be provided).

13. The Option A school shall be limited to a non-charter public facility serving grade levels K through 5, and with a maximum of 1,000 students. Notwithstanding the exemptions provided in LDC Sec. 6.03.10 which are specifically applicable to public schools, the property owner shall provide adequate on-site vehicular queuing and take other actions to limit off-site impacts as further described herein. Additionally, the school shall provide for onsite

vehicular queuing for the number of students who are projected to be ineligible for busing (hereafter referred to as “Non-Bussed Students”). Specifically:

- i. The queue shall provide for the uninterrupted stacking of vehicles within the subject site;
- ii. The minimum length of queue for the school shall be determined by multiplying the number of Non-Bussed Students by 0.196, then multiplied by 25 feet, and then multiplied by 1.25; and,
- iii. The school shall take all actions necessary to ensure that students are not dropped off or picked up outside of school property (i.e. within adjacent parcels or along roadways along the school frontage or proximate to the school).

The school shall not permit students to be dropped off outside of the school property, including along the property’s roadway frontages. In such instance, the school shall take all actions necessary to ensure such violations of the conditions of approval are cured.

14. Notwithstanding anything on the PD site plan or herein these conditions to the contrary, bicycle and pedestrian access, or access connections to continue/extend the Multi-Use Trail (MUT), may be permitted anywhere along the PD boundaries.

15. The project shall be served by and limited to the following vehicular access connections:

- a. Within the Northern Development Area:
  - i. One (1) connection to CR 579;
  - ii. One (1) connection to US 301 via an extension of Berry Grove Blvd.;
  - iii. Three (3) stubouts along the northern boundary of the Northern Development Area; and,
  - iv. ~~Five-Three (53)~~ stubouts along the southern boundary of the Northern Development Area.
- b. Within the Southern Development Area:
  - i. Two (2) connections to CR 579;
  - ii. One (1) connection to Saffold Rd.;
  - iii. ~~One-Two (+2)~~ stubouts along the western boundary of the Southern Development Area; and,
  - iv. ~~Four-One (41)~~ stubouts along the northern boundary of the Southern Development Area, ~~the westernmost of which is shown on the site plan as the W. Lake Dr. Extension.~~
- c. There shall be a minimum of three (3) connections between the Northern Development Area and the Southern Development Area, the westernmost of which is shown on the site plan as the North/South Collector Roadway.

16. With respect to project roadways:

- a. The developer shall construct the extension of Berry Grove Blvd. (i.e. the east-west collector roadway within the Northern Development Area between US 301 and CR 579) as a 2-lane, collector roadway utilizing the Typical Section standards shown on the PD site plan. The roadway shall be constructed as a divided facility, expandable to 4-lanes west of the internal roundabout, and as an undivided 2-lane facility east of the internal roundabout. The roadway shall be constructed prior to or concurrent with the initial increment of development. Additionally:
  - i. Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC; and,
  - ii. The developer shall dedicate and convey to Hillsborough County sufficient right-of-way necessary to accommodate construction of eastbound right and westbound left turn lanes (by others) at the two (2) westernmost access connections along Berry Grove Blvd. (within the PD).
- b. With respect to the W. Lake Dr. Extension and substandard roadway improvements, and North/South Collector roadway:

- i. [The design and permitting for the W. Lake Dr. substandard roadway improvements shall be completed by the developer prior to issuance of the 601<sup>st</sup> residential building permit.](#)
- ~~ii.~~ [The W. Lake Dr. substandard roadway improvements between Bishop Rd. and Janes Dr., shall be constructed utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. This shall be constructed by the developer prior to issuance of the 1,000<sup>th</sup> residential building permit;](#)
- ~~iii.~~ [The W. Lake Dr. Extension within the adjacent PD located north of the Northern Development Area, as well as portions of the extension between the boundary of that PD and Janes Dr., shall be constructed utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. This shall be constructed by the developer prior to issuance of the 1,000<sup>th</sup> residential building permit;](#)
- ~~iii.~~ [iv.](#) [The W. Lake Dr. Extension within the Northern Development Area \(both north and south of the proposed internal roundabout\) shall be constructed as a 2-lane collector roadway utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. This shall be constructed by the developer with the first increment of development within the Northern Development Area. Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC;](#)
- ~~iv.~~ [v.](#) [The ~~W. Lake Dr. Extension~~North/South Collector Roadway \(within the Northern and Southern Development Areas shall be constructed as a 2-lane collector roadway utilizing the ~~Typical Section standards described within the Design Exception referenced in Condition 23~~C3-2U Typical Section standard as contained in the Hillsborough County Transportation Design Manual \(TDM\); however, the Shared Use Path shall be constructed on both sides of the roadway. This shall be constructed by the developer with the first increment of development within the Southern Development Area. Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC.](#)

[Furthermore, the roadway shall be designed and constructed to accommodate certain site access improvements anticipated to be constructed in the future \(by others\), and the developer shall be required to dedicate and convey additional right-of-way to Hillsborough County as necessary to permit construction of such turn lanes by others, specifically:](#)

- [1. A southbound to westbound right turn lane on the North/South Collector Roadway onto the east/west segment; and,](#)
- [2. A northbound to westbound left turn lane on the North/South Collector Roadway onto the east/west segment.](#)

[Lastly, subject to compliance with applicable access management standards, the County may permit additional access to adjacent properties along that section of the North/South Collector Roadway identified on the PD site plan by a bounding box without further modification of this PD.](#)

c. With respect to the CR 579 substandard roadway improvements:

- i. [The design and permitting for CR 579 substandard roadway improvements shall be completed prior to the issuance the 601<sup>st</sup> building permit.](#)
- ~~i.~~ [ii.](#) [For the first ~~phase of development within the Northern Development Area consisting of 600 dwelling~~ 1,000 units within the Northern and Southern Development Areas, the developer shall undertake improvements which include construction of a 10-foot-wide multi-purpose](#)

pathway along its CR 579 frontage and which includes a crossing of CR 579 to the Little Manatee Corridor Nature Preserve trailhead entrance road as described in Condition 17.a., concurrent with the construction of the site development improvements associated with this first increment of development;

~~iii.~~ iii. Prior or concurrent with the issuance of the ~~601st~~ 1,000<sup>th</sup> residential building permit within the Northern and Southern Development Areas, ~~and prior to the issuance of any nonresidential building permit in the Southern Development Area,~~ the developer shall make certain improvements within each of the three (3) discrete sections (A, B and C) of the roadway, as described below.

~~iii.~~ iv. Within the Southern Development Area, the developer shall be permitted to obtain building permits for development north of (but not to include) the east-west roadway nor to include any development south of the east-west roadway, provided the following improvements are in place. Specifically, either items 1 through 4 (inclusive) shall be required, or item 5 shall be required, as further detailed below:

1. The Berry Grove Blvd. Ext. to US 301; and,
2. The W. Lake Dr. improvements between Bishop Rd. and Berry Grove Blvd.; and,
3. The northernmost connection to CR 579 within the Southern Development Area; and,
4. The two (2) easternmost local roadway connections along the northern boundary of the Southern Development Area, shall be completed such that they connect the Northern Development Area and Southern Development Area through adjacent folio 79703.0000. Notwithstanding anything herein to the contrary, the third (westernmost) local roadway connection shall be constructed concurrently with the ~~east~~ East-west ~~West roadway~~ Roadway or adjacent residential development within the Southern Development Area, whichever occurs earlier; or,
5. The CR 579 improvements within Segments A, B and C have been completed and are open for beneficial use.

~~iv.~~ v. Notwithstanding the above and solely with respect to development within the Northern Development Area, the developer shall not be required to make improvements identified within Sections A and B, if the W. Lake Dr. improvements between Bishop Rd. and the Berry Grove Blvd. Ext. have been completed and are open for beneficial use (consistent with those improvements referenced in Condition 16.b.i through 16.b.iii.).

~~v.~~ vi. Notwithstanding the above and solely with respect to development within the Southern Development Area, the developer shall not be required to make improvements identified within Sections A and B, if the W. Lake Dr. improvements referenced in Condition 16.b.iii.1. through 4. together with a continuous extension of W. Lake Dr. between the southern boundary of the Northern Development Area and the Proposed East-West Road within the Southern Development Area (i.e. through adjacent folio 79703.0000) is constructed and open for beneficial use.

~~vi.~~ vii. Specifically, and subject to the clarifications and requirements provided above:

1. Improvements to CR 579 have been broken into three (3) segments. Segment A is defined as the section of CR 579 between SR 674 and the southern property boundary of folio 79456.0000 (i.e. the southern boundary of ~~pending PD 25-046926-0008~~). Segment B is defined as that portion of CR 579 between the southern property boundary of folio 79456.0000 and the southern boundary of adjacent PD ~~18-104824-1033~~ (i.e. the northern boundary of the subject PD).

Segment C is defined at that portion of CR 579 between the northern boundary of the subject PD and the southern boundary of the subject PD.

2. Within Segment A, the developer shall:
    - a. Construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway; and,
    - b. Construct a 10-foot-wide sidewalk along the west side of the roadway. The 10-foot-wide sidewalk will transition to a 5-foot-wide sidewalk north of proposed PD ~~25-0469~~26-0008, where right-of-way does not exist to permit construction of the wider sidewalk.
  3. Within Segment B, the developer shall construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway.
  4. Within Segment C, the developer shall:
    - a. Dedicate and convey to Hillsborough County sufficient right-of-way to accommodate proposed improvements (a minimum of 48-feet east of the existing centerline). This shall be in addition to any right-of-way necessary to accommodate site access improvements or additional preservation required pursuant to the Hillsborough County Corridor Preservation Plan as further described herein;
    - b. Construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway; and,
  5. Construct a 10-foot-wide sidewalk along the west side of the roadway.
- d. With respect to the Saffold Rd. substandard roadway improvements:
- i. Prior to or concurrent with ~~any development south of the East/West Road or~~ connection to Saffold Road, whichever occurs earlier, the developer shall make certain improvements within each of two (2) discreet sections of the roadway. Segment B is defined as that portion of Saffold Rd. along the frontages of folios 79700.0400, 79700.0300, ~~79700.0350, 79700.0100,~~ 79700.0200, 79700.0250, 79700.0050, and 79700.0150. Segment A is defined as those portions of Saffold Rd. between CR 579 and the western project boundary, excluding the area defined as Segment B. Specifically:
    1. Construct 6-foot-wide stabilized shoulders along both sides of the roadway; and,
    2. Construct a 5-foot-wide sidewalk along the north side of the roadway.
  - ii. Within Segment B, the developer shall:
    1. Construct 6-foot-wide stabilized shoulders along both sides of the roadway; and,
    2. Construct a 5-foot-wide sidewalk along the north side of the roadway.
  - iii. Within Segment A, the developer shall:
    1. Construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway; and,
    2. Construct a 10-foot-wide sidewalk along the north side of the roadway. This will take the place of the 5-foot-wide sidewalk that would normally be constructed along the west side of the roadway.
  - iv. Notwithstanding anything herein to the contrary, the Saffold Rd. connection and substandard road improvements shall be completed prior to issuance of the 1,500<sup>th</sup> building permit.

- e. The roadway between the western boundary of the Southern Development Area (i.e. the boundary with folio 79707.0000) and the ~~W. Lake Dr. Extension~~North/South Collector Roadway, together with the north/south stubout to the southern boundary of folio 79703.0000, shall be constructed as a 2-lane urban collector roadways utilizing the ~~Typical Section —4 (TS 4) standards as found within the 2021 Transportation Technical Manual (TTM)~~standards described below.

1. This-The east/west segment shall be constructed by the developer of the subject PD with the first increment of development of folio 79707.0000 or any development west of the ~~W. Lake Dr. Ext.~~North/South Collector Roadway, whichever occurs earlier. The roadway shall be constructed to the C3-2U of Hillsborough County Transportation Design Manual (TDM); however, the developer shall be required to construct the Shared Use Path on both sides of the road. Additionally, the roadway shall be designed and constructed to accommodate certain site access improvements anticipated to be constructed in the future (by others), and the developer shall be required to dedicate and convey additional right-of-way to Hillsborough County as necessary to permit construction of such turn lanes by others, specifically:

i. An eastbound to northbound left turn lane on the east/west segment onto the North/South Collector Roadway; and,

ii. An eastbound to northbound left turn lane on the east/west segment onto the north/south segment.

2. The north/south segment shall be constructed by the developer of the subject PD with the first increment of development of folio 79703.0000 or any development west of the North/South Collector Roadway, whichever occurs earlier. The roadway shall be constructed utilizing the Typical Section standards described within the Design Exception referenced in Condition 23. Additionally, the roadway shall be designed and constructed to accommodate certain site access improvements anticipated to be constructed in the future (by others), and the developer shall be required to dedicate and convey additional right-of-way to Hillsborough County as necessary to permit construction of such turn lanes by others, specifically:

i. A southbound to westbound right turn lane on the east/west segment onto the North/South Collector Roadway.

~~Additionally, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC.~~

- f. The Proposed East-West Road identified on the PD site plan within the Southern Development Area (i.e. between the W. Lake Dr. Ext. and CR 579) shall be constructed as a 2-lane urban collector roadway utilizing the C3-2U standards as found within the Transportation Design Manual (TDM); however, the Shared Use Path shall be constructed along both sides of the roadway. ~~Typical Section —4 (TS 4) standards as found within the 2021 Transportation Technical Manual (TTM).~~ Notwithstanding the above, the developer shall have the option of submitting a transportation analysis together with the initial increment of development within the Southern Development Area that demonstrates a collector roadway design is not warranted or otherwise necessary ~~only if connections between the Northern Development Area and Southern Development Area and the W. Lake Dr. Extension through adjacent folio 79703.000 has been completed and are open for beneficial use.~~ Such study will be subject to review and approval by Hillsborough County. If approved, the developer shall be permitted to construct the Proposed East-West Road as a 2-lane urban local roadway utilizing the Typical Section – 3 (TS-3) standards as found within the 2021 Transportation Technical Manual (TTM). If the roadway remains a collector roadway, the developer will be required to construct any auxiliary turn lanes which may be warranted along this section pursuant to Sec. 6.04.04.D. of the LDC.

- g. Other internal project roadways shall be constructed to an appropriate urban typical section as described in the 2021 Hillsborough County TTM and/or last edition of the Hillsborough County Transportation Design Manual (TDM), as applicable. Designation of appropriate typical sections shall occur at the time of plat/site/construction plan review and be based upon anticipated traffic volumes within each segment.

- h. The total right-of-way widths shown in the Design Exception and on the PD site plan are minimum widths. Additionally:
  - i. The developer shall preserve a minimum of +/- 46 feet of right-of-way west of the westernmost proposed internal roundabout or as otherwise necessary to accommodate the future expansion of Berry Grove Blvd. as a future 4-lane roadway, expandable to the inside. The intent of these conditions is to require the developer to secure the dedication, conveyance and preservation of certain rights-of-way to the County as described above, both within the project and through adjacent folios 79710.0585 and 79702.0010.
  - ii. In accordance with the Hillsborough County Corridor Preservation Plan, the developer shall preserve sufficient right-of-way along the project's CR 579 frontages such that 107 feet of right-of-way is available for future improvements west of the existing eastern right-of-way boundary (i.e. to accommodate a future 2-lane enhanced rural roadway). Only those interim uses allowed by the Hillsborough County LDC shall be permitted within the preserved right-of-way. The right-of-way preservation area shall be shown on all future site plans, and building setback shall be calculated from the future right-of-way line.
  - iii. Notwithstanding anything shown on the PD site plan to the contrary, the developer shall dedicate and convey to Hillsborough County sufficient right-of-way as necessary to accommodate the proposed/required project traffic signals and/or roundabouts, as well as required site access improvements and associated drainage, both within and external to the project. Where necessary, such right-of-way shall be dedicated and conveyed in addition to right-of-way dedication or preservation requirements listed herein these conditions.
  - iv. The amount and location of right-of-way dedication for roundabouts shall be based upon Transportation Technical Manual (TTM) and/or Transportation Design Manual (TDM) and roundabout design requirements, as applicable, and shall be reviewed and approved by the appropriate agencies including Hillsborough County Development Services and/or Public Works.

17. With respect to other site access and required improvements:

- a. Prior to or concurrent with the initial increment of development within the Northern Development Area, the developer shall construct the roundabout with MUT connection to the Little Manatee Corridor Nature Preserve Trailhead.
- b. Prior to or concurrent with the initial increment of development within the Southern Development Area, the developer shall:
  - ~~i. Provide a trip generation and site access analysis to determine whether construct of a westbound to northbound right turn lane on Saffold Rd. into the project's access is warranted (if warranted the developer shall construct the improvement);~~
  - ~~ii.i. Construct an eastbound to northbound left turn lane on Saffold Rd. into the project's access;~~
  - ~~iii.i.~~ iii.i. Construct southbound to westbound right turn lanes on CR 579 at each project access serving the Southern Development Area;
  - ~~iv.ii.~~ iv.ii. Construct northbound to westbound left turn lanes on CR 579 at each project access serving the Southern Development Area; and,
  - ~~v.iii.~~ v.iii. The developer shall perform a signal warrant analysis at the intersection of the southernmost project access along CR 579, which shall be reviewed by and subject to the approval of Hillsborough County Public Works. If such signal is found not to be warranted the developer shall have no further obligation with respect to the signal. If such signal is found to be warranted, the developer shall install the signal. Alternatively, at the developer's



~~and which shall be based on a previously~~ ~~will~~ approved ~~a de minimis~~ Design Exception (dated June 13, 2025) which was approved by the County Engineer (on October 15, 2025) for the W. Lake Dr. improvements. Improvements to W. Lake Dr. ~~have shall been~~ broken into three (3) segments. Segment A is defined as the section of W. Lake Dr. between Bishop Rd. and the northern property boundary of the Cypress Ridge Development (i.e. adjacent PRS 24-1033/~~pending PD 25-1386~~). Segment B is defined as that portion of W. Lake Dr. between the southern property boundary of the Cypress Ridge Development (i.e. the northern boundary of the subject PD) and Berry Grove Blvd. Segment C is defined at ~~that those~~ portions of W. Lake Dr. between Berry Grove Blvd. and ~~Saffold Rd.~~ the east/west road segment.

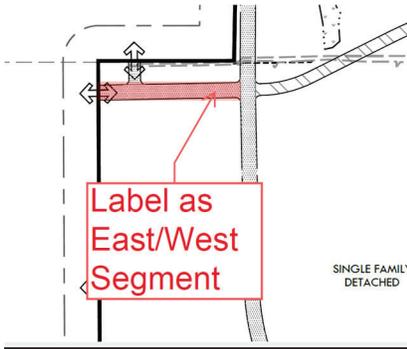
The Design Exception ~~shall~~ authorizes deviations from the TS-4 Typical Section (for 2-lane, Undivided, Urban Collector Roadways) as found in the 2021 Hillsborough County Transportation Technical Manual (TTM). Specifically:

- a. Within Segment A, the developer shall widen/construct the roadway consistent with the TS-4 Typical Section, except that instead of the 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide asphalt paths along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required asphalt path separation as outlined below. The developer has proposed two typical sections for this segment (A-1 and A-2). Typical Section A-2 is the section where right-of-way is limited and/or there are design constraints. Use of Typical Section A-2 shall be minimized to the greatest extent feasible. The developer shall be permitted to reduce the required asphalt path separation between the closest edge of the asphalt path and the travel lane from 14-feet to 7-feet (for Typical Section A-1) and from 14-feet to 2-feet (for Typical Section A-2).
  - b. Within Segment B, the developer shall construct the roadway consistent with the TS-4 Typical Section, except that instead of the 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide sidewalks along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required sidewalk separation between the closest edge of the sidewalk and the travel lane from 14-feet to 6-feet. The developer shall also be permitted to eliminate the required 2-foot pedestrian clearance area between the back of sidewalk and edge of right-of-way.
  - c. Within Segment C, the developer shall construct the roadway consistent with the TS-4 Typical Section, except that instead of the 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide asphalt paths along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required asphalt path separation between the closest edge of the sidewalk and the travel lane from 14-feet to 7-feet.
24. If the notes and/or graphics on the site plan conflict with specific zoning conditions and/or the Land Development Code (LDC) regulations, the more restrictive regulation shall apply, unless specifically conditioned otherwise. References to development standards of the LDC in the above stated conditions shall be interpreted as the regulations in effect at the time of preliminary site plan/plat approval.
25. In accordance with LDC Section 5.03.07.C, the certified PD general site plan shall expire for the internal transportation network and external access points, as well as for any conditions related to the internal transportation network and external access points, if site construction plans, or equivalent thereof, have not been approved for all or part of the subject Planned Development within 5 years of the effective date of the PD unless an extension is granted as provided in the LDC. Upon expiration, re-certification of the PD General Site Plan shall be required in accordance with provisions set forth in LDC Section 5.03.07.C.

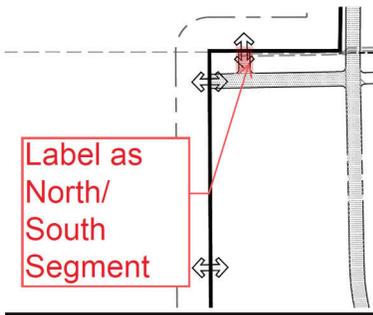
#### Other Conditions

- Prior to certification of the General Development Plan (GDP), the applicant shall revise the PD site plan to:
  - Modify the label reading “Proposed W. Lake Dr. Extension” to instead state “Proposed North/South Collector Road – See Conditions of Approval”.

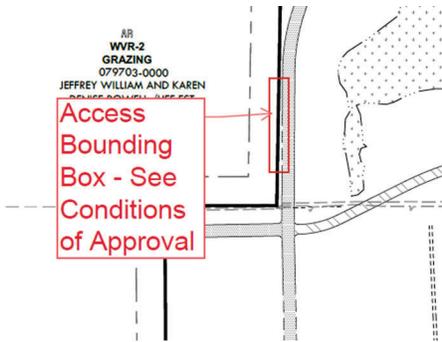
- Remove the three internal access arrows along the Berry Grove Blvd. Ext. for consistency. Staff notes that not all connections are shown, and access will be subject to the LDC and conditions of approval.
- Label the section shown below as “East/West Segment – See Conditions of Approval”



- Label the section shown below as “North/South Segment – See Conditions of Approval”; and,



- Add a bounding box as shown below and label “Access Bounding Box – See Conditions of Approval” .



## **PROJECT SUMMARY AND ANALYSIS**

The applicant is requesting to rezone multiple parcels, totaling +/- 648.8 ac., from Agricultural Rural (AR) (+/- 13.1 ac.) and Planned Development (PD) +/- 635.7 ac. to a new PD (to “add” lands to the PD). The

existing PD is approved via 25-0371, for up to 2,390 dwelling units and a 1,000-student maximum non-charter public elementary school.

The PD has two development options, such that if the school is not constructed (Option B), the maximum number of residential units which could be constructed is 2,390. If the school is constructed (Option A), then the maximum number of residential units would be 2,300.

The existing PD defers certain required substandard road improvements along CR 579 and W. Lake Dr. as outlined in the conditions of approval.

The applicant is proposing to defer a final determination of whether a portion of the east-west road within the Southern Development Area shall be built as a collector roadway. Staff notes that the applicant is still committed to construct site access improvements at the northernmost CR 579 entrances (i.e. a roundabout), as well as the multi-purpose pathways along its CR 579 project boundaries, and the trail connections between the internal trail system and the Little Manatee Corridor Nature Preserve located east of CR 579. The southernmost project access to CR 579 (i.e. the primary access serving that area) may or may not meet signal warrants depending upon final design and whether certain connections between the Southern Development Area and Northern Development Area (i.e. through lands outside of this PD) have been constructed. If required to be signalized, the developer may opt to construct a roundabout in this location instead.

While the applicant is proposing to add land, the previously approved entitlements are not proposed to be modified. The applicant is also proposing to modify the number of secondary project access connections, although the primary access connections to CR 579, W. Lake Dr. (to the north) and the Berry Grove Blvd. Extension remain unchanged.

The applicant is proposing to create a primary north/south collector roadway through the project to the east of the W. Lake Dr. Extension, such that it is located wholly within the PD boundaries (and can be completed together with development of the project). The developer is proposing a second (easternmost) roundabout within the PD where the new roadway will intersect with Berry Grove Blvd. The W. Lake Dr. Extension stubout built to the southern boundary of the Northern Development Area, located south of the westernmost roundabout, will still be available for future continuation upon (re)development of folio 79703.0000.

Staff has carried forward previously approved conditions, with modifications as necessary to address necessary language changes created by the addition of the 13.1 ac. to the PD and the other changes proposed by the applicant.

Consistent with the Development Review Procedures Manual (DRPM), and as required by staff, the developer submitted a trip generation and site access analyses which looked the overall network trip generation; however, the study was not comprehensive in that it did not analyze all internal or secondary external connections. Staff notes the PD conditions require additional analysis to determine the need for additional turn lane warrants on collector roadways within the PD.

Staff notes additional turn lane will be needed to serve other projects, and this concept was included in the previously approved zoning (i.e. the existing requirement to dedicate land for left and right turn lanes along that portion of the Berry Grove Blvd. Extension west of the westernmost roundabout. Staff has analyzed likely development and traffic patterns based on proposed site plan changes, and identified those areas where certain such turn lanes are likely to be needed (and will be constructed by others). Staff has included a condition requiring the developer to dedicate and convey to the County additional right-of-way and design the road and drainage systems to accommodate such additional turn lanes (by others) in the future.

A comparison of the number of trips potentially generated under the existing and proposed zoning designations is presented below, utilizing a generalized worst-case scenario. Data presented below is based on the Institute of Transportation Engineer's (ITE) Trip Generation Manual, 11<sup>th</sup> Edition, and is consistent with data from the 25-0371 transportation staff report.

Existing Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
Residential Development within the Northern Development Area/ 495 SFDUs and 300 Townhomes (LUC 210/215)	6,166	431	587
1,000 Student Non-Charter Public Elementary School (LUC 520)	2,270	740	160
<i>Northern Development Area Subtotal:</i>	<i>8,436</i>	<i>1,171</i>	<i>747</i>
Residential Development within the Southern Development Area/ 1,505 SFDUs (LUC 210)	11,950	857	1,249
<i>Southern Development Area Subtotal:</i>	<i>11,950</i>	<i>857</i>	<i>1,249</i>
<b>Project Totals:</b>	<b>20,386</b>	<b>2,028</b>	<b>1,996</b>

Proposed Zoning:

Land Use/Size	24 Hour Two-Way Volume	Total Peak Hour Trips	
		AM	PM
Residential Development within the Northern Development Area/ 495 SFDUs and 300 Townhomes (LUC 210/215)	6,166	431	587
1,000 Student Non-Charter Public Elementary School (LUC 520)	2,270	740	160
<i>Northern Development Area Subtotal:</i>	<i>8,436</i>	<i>1,171</i>	<i>747</i>
Residential Development within the Southern Development Area/ 1,505 SFDUs (LUC 210)	11,950	857	1,249
<i>Southern Development Area Subtotal:</i>	<i>11,950</i>	<i>857</i>	<i>1,249</i>
<b>Project Totals:</b>	<b>20,386</b>	<b>2,028</b>	<b>1,996</b>

Trip Generation Difference:

Land Use/Size	24 Hour Two-Way Volume	Total Net Peak Hour Trips	
		AM	PM
<b>Difference</b>	<b>No Change</b>	<b>No Change</b>	<b>No Change</b>

**TRANSPORTATION INFRASTRUCTURE SERVING THE SITE**

Berry Grove Blvd. is a 2-lane, divided, collector roadway characterized by 11-foot-wide travel lanes in good condition. There are 7-foot-wide buffered bicycle lanes present along both sides of the facility. There are 5-foot-wide sidewalks present along both sides of the roadway. The roadway has been constructed approximately 300 feet west of the easternmost project boundary of PD 24-0044, as most recently modified via PRS 25-0573. Responsibility to construct the roadway to the eastern PD boundary

is a condition of that zoning's approval. The developer of that project is also required to preserve +/- 46-feet of additional right of way in order to facilitate the future potential 4-laning of Berry Grove Blvd.

CR 579 is a 2-lane, undivided, substandard, collector roadway characterized by +/- 10-foot-wide travel lanes in average condition. The roadway lies within a variable width right-of-way (between +/- 51 and +/- 74 feet in width). There are no existing sidewalks along CR 579 in the vicinity of the proposed project. There are no existing bicycle facilities on CR 579 in the vicinity of the proposed project.

Saffold Rd. is a 2-lane, undivided, substandard, collector roadway characterized by +/- 10-11-foot-wide travel lanes in average condition. The roadway lies within a variable width right-of-way (between +/- 57 and +/- 89 feet in width). There are no existing sidewalks or bicycle facilities along Saffold Rd. in the vicinity of the proposed project; however, there is a 10-foot-wide multi-purpose pathway along the portions of the northern side of Saffold Rd. west of the proposed project (which were constructed by the developer of the above referenced adjacent PD). Additional facilities will be constructed as development progresses by that developer.

CR 579 is shown on the Hillsborough County Corridor Preservation Plan (HCCPP) as a future 2-lane enhanced roadway. In a rural context, 2-lane collector roadways require a minimum of 96 feet pursuant to Typical Section – 7 (TS-7) of the Hillsborough County Transportation Technical Manual, to which we add an additional 11 feet to accommodate future enhancements. As such, the total future right-of-way needed is a minimum of 107 feet. Staff notes that due to the roundabout(s) proposed on CR 579 by the applicant, additional right-of-way will be needed for the roundabout. Since roundabouts are not constructed with additional auxiliary turning lanes, no additional right-of-way to accommodate left or right turning movements will be needed.

The W. Lake Dr. Extension north of the project has not yet been constructed. The developer of adjacent PD 18-1048 (which is currently undergoing a rezoning to a new PD via file 25-1386) is required to construct an extension of W. Lake Dr. between its southern project boundary and the existing terminus of W. Lake Dr. (in the vicinity of Janes Dr.), as well as certain substandard road improvements along W. Lake Dr. (between Bishop Rd. and the existing terminus). This developer (i.e. the developer of the subject PD) also has certain development thresholds which requires construction to occur by this developer (if the other project does not move forward) of segments of W. Lake Dr. south of Bishop (up to a full continuous road between Bishop Rd. and a point within the Southern Development Area before issuance of building permits). This is further discussed in the Design Exception requests section hereinbelow.

### **SITE ACCESS AND CONNECTIVITY**

Access shall be as follows:

1. One (1) access connection along the western project boundary within the Northern Development Area (NDA), representing the extension of Berry Grove Blvd. This roadway will be designed a 4-lane roadway and constructed as a 2-lane roadway, expandable to the inside. The 4-lane segment will extend to W. Lake Dr. where the extra lanes will convert to drop/turn/specialized lanes.
2. Three (3) access connections along the northern property boundary within the NDA, the westernmost representing the extension of W. Lake Dr. and the other two local roadway connections.
3. One (1) access connection along the eastern boundary of the NDA, representing the terminus of the Berry Grove Blvd. Ext. with CR 579.

4. Six (6) access connections along the southern boundary of the NDA, with one representing the new north/south collector roadway, one representing a stubout for the future extension of W. Lake Dr., and the other four representing local roadway connections.
5. Two (2) access connections along the western boundary of the Southern Development Area (SDA), with the northernmost connection providing a collector road stubout to provide future access to large undeveloped properties to the west of the subject PD, with the second connection being a local roadway.
6. Four (4) access connections along the northern boundary of the SDA, with the westernmost connection representing the extension of W. Lake Dr., one for the new north/south collector roadway, and the other two representing local road connections.
7. Two (2) access connections along the eastern boundary of the SDA, with the southernmost connection representing the point of connection for the potential east-west collector roadway as shown on the site plan and further described in the conditions.
8. One (1) access connection along the southern boundary of the SDA, representing the southern terminus of the new north/south road to Saffold Rd.
9. One (1) bounding box along a portion of the North/South Collector roadway, to allow the County to approve access to/from the roadway (if consistent with LDC standards) to adjacent properties where such roadway abuts the PD boundary.

While some site access improvements have been identified in the conditions, given the large scale of the project, lack of detail regarding internal lotting patterns/design, whether the NDA and SDA will be connected, and given other factors, it will be necessary to defer to the plat/site/construction plan review stage the final determination of any improvements, including whether turn lanes are required on external and internal roadways and intersections, and whether roundabouts or traffic signals are warranted to serve the project. Similarly, additional internal road design decisions will be deferred to the plat/site/construction plan review stage.

A graphic has been provided below which demonstrates connectivity in the greater Wimauma Area. The subject project is just outside the southern boundary of the graphic.

**REVISED DESIGN EXCEPTION NEEDED – W. LAKE DR. (SUBSTANDARD ROAD AND NEW ROAD)**

As W. Lake Dr. is a substandard collector roadway between Bishop Rd. and Janes Dr., and that the developer is proposing to extend W. Lake Dr. south of Janes Rd. to the proposed access within adjacent PD 24-1033 (pending PD 25-1386), along that PD's southern project boundary), the applicant is required to make certain improvements within those areas. Also, the developer is proposing to extend W. Lake Dr. south of adjacent PD 24-1033 (pending PD 25-1386), continuing through the internal roundabout within the subject PD, and stubbing out at the southern project boundary of the Northern Development Area.

Improvements to W. Lake Dr. had been broken into three (3) segments. Segment A was defined as the section of W. Lake Dr. between Bishop Rd. and the northern property boundary of the Cypress Ridge Development (i.e. adjacent PRS 24-1033/pending PD 25-1386). Segment B was defined as that portion of W. Lake Dr. between the southern property boundary of the Cypress Ridge Development (i.e. the northern boundary of the subject PD) and Berry Grove Blvd. Segment C was defined as that portion of W. Lake Dr. between Berry Grove Blvd. and Saffold Rd.

Given the above, the applicant's Engineer of Record (EOR) submitted a Design Exception request (dated June 13, 2025) for W. Lake Dr. to determine the specific improvements that would be required by the County Engineer. Based on factors presented in the Design Exception request, the County Engineer found the request approvable (on July 11, 2025) and approved the Design Exception on October 15, 2025. The Design Exception as also the subject of a deminimis approval as a part of the 25-1373 application.

The Design Exception would authorize deviations from the TS-4 Typical Section (for 2-lane, Undivided, Urban Collector Roadways) as found in the 2021 Hillsborough County Transportation Technical Manual (TTM). Specifically:

1. Within Segment A, the developer shall widen/construct the roadway consistent with the TS-4 Typical Section, except that instead of the of 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide asphalt paths along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required asphalt path separation as outlined below. The developer has proposed two typical sections for this segment (A-1 and A-2). Typical Section A-2 is the section where right-of-way is limited and/or there are design constraints. Use of Typical Section A-2 shall be minimized to the greatest extent feasible. The developer shall be permitted to reduce the required asphalt path separation between the closest edge of the asphalt path and the travel lane from 14-feet to 7-feet (for Typical Section A-1) and from 14-feet to 2-feet (for Typical Section A-2).
2. Within Segment B, the developer shall construct the roadway consistent with the TS-4 Typical Section, except that instead of the of 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide sidewalks along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required sidewalk separation between the closest edge of the sidewalk and the travel lane from 14-feet to 6-feet. The developer shall also be permitted to eliminate the required 2-foot pedestrian clearance area between the back of sidewalk and edge of right-of-way.
3. Within Segment C, the developer shall construct the roadway consistent with the TS-4 Typical Section, except that instead of the of 5-foot sidewalks and 7-foot buffered bicycle lanes required per TS-4 the developer shall be permitted to install 10-foot-wide asphalt paths along both sides of the roadway. Additionally, the developer shall be permitted to reduce the required asphalt path separation between the closest edge of the sidewalk and the travel lane from 14-feet to 7-feet.

As a part of this request (26-0349) the developer has proposed an alternative facility through the project (i.e. North/South Collector Road) which is being required by the County Engineer to utilize an approved typical section standard (since it no longer represents a continuation of the W. Lake Dr. Ext.) Given the above, and that the limits and configuration shown/described in the existing Design Exception no longer match and are not appropriate for the new development proposal, staff has included a condition requiring the developer submit and obtain approval of a revised DE prior to approval of the next plat/site/construction plan submittal, and to be based on the previously approved DE, with changes to graphics and limits of the DE such that it terminates with the east/west segment (as labeled on the PD site plan). If the BOCC approves 26-0349, the County Engineer will approve a revised DE as described above, once received.

#### **DEMINIMIS DESIGN EXCEPTION – CR 579 SUBSTANDARD ROAD**

As CR 579 is a substandard collector roadway, the applicant's Engineer of Record (EOR) submitted a Design Exception request (dated July 9, 2025) to determine the specific improvements that would be required by the County Engineer. Based on factors presented in the Design Exception request, the County

Engineer found the request approvable (on July 11, 2025), and approved the request (on October 15, 2025) with a subsequent deminimis approval related to application 25-1373 (on November 26, 2025).

Improvements to CR 579 had been broken into three (3) segments. Segment A was defined as the section of CR 579 between SR 672 and the southern property boundary of folio 79456.0000 (i.e. the southern boundary of PD 26-0008). Segment B was defined as that portion of CR 579 between the southern property boundary of folio 79456.0000 and the southern boundary of adjacent PD 18-1048/pending PD 25-1386 (i.e. the northern boundary of the subject PD). Segment C was defined as that portion of CR 579 between the northern boundary of the subject PD and the southern boundary of the subject PD.

The Design Exception authorized deviations from the 2021 Transportation Technical Manual (TTM) Typical Section – 7 (TS-7) (for 2-Lane, Rural Local and Collector Roadways). Specifically:

1. Within Segment A:

- a. The developer will be permitted to maintain the 10 to 11-foot-wide existing lanes in lieu of the 12-foot-wide lanes required pursuant to the Typical Section – 7 (TS-7) of the Transportation Technical Manual (TTM);
- b. The developer will be permitted to construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway, in lieu of the 8-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway as required pursuant to TS-7 of the TTM; and,
- c. The developer will be required to construct a 10-foot-wide sidewalk along the west side of the roadway. This will take the place of the 5-foot-wide sidewalk that would normally be constructed along the west side of the roadway within this segment per TS-7, but will not be in lieu of the required bicycle facilities, which are being provided on the paved 5-foot shoulders consistent with TS-7. Also, staff notes that the 10-foot-wide sidewalk will transition to a 5-foot-wide sidewalk north of proposed PD 25-0469, where right-of-way does not exist to permit construction of the wider sidewalk.

2. Within Segment B:

- a. The developer will be permitted to maintain the 10-foot-wide to 11-foot-wide existing lanes in lieu of the 12-foot-wide lanes required pursuant to the Typical Section – 7 (TS-7) of the Transportation Technical Manual (TTM); and,
- b. The developer will be permitted to construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway, in lieu of the 8-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway as required pursuant to TS-7 of the TTM.

3. Within Segment C:

- a. The developer will be permitted to maintain the 10-foot-wide lanes in lieu of the 12-foot-wide lanes required pursuant to the Typical Section – 7 (TS-7) of the Transportation Technical Manual (TTM);
- b. The developer shall dedicate and convey to Hillsborough County sufficient right-of-way to accommodate proposed improvements (a minimum of 48-feet east of the existing centerline). Staff notes this is in addition to any right-of-way necessary to

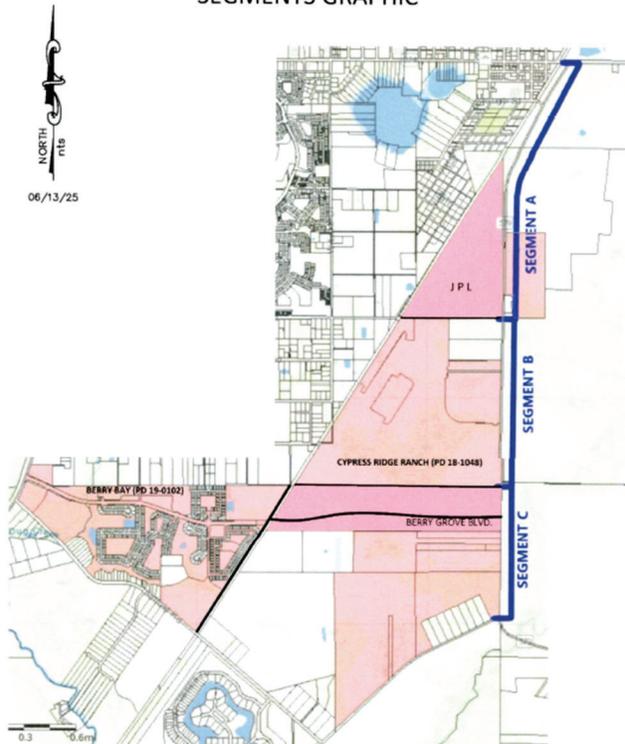
accommodate site access improvements or additional preservation required pursuant to the Hillsborough County Corridor Preservation Plan as further described herein;

- c. The developer will be permitted to construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway, in lieu of the 8-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway as required pursuant to TS-7 of the TTM; and,
- d. The developer will be required to construct a 10-foot-wide sidewalk along the west side of the roadway. This will take the place of the 5-foot-wide sidewalk that would normally be constructed along the west side of the roadway within this segment per TS-7, but will not be in lieu of the required bicycle facilities, which are being provided on the paved 5-foot shoulders consistent with TS-7.

The developer of the subject PD noted that the developer of PD 26-0008 is required to make the same improvements within Segments A and B (in addition to 5-foot-wide sidewalks/asphalt paths along their frontages except where replaced by a wider facility); however, in the event that development does not move forward, this developer will be required to make those improvements in certain circumstances. Although the Design Exception doesn't specifically mention the Southern Development Area, staff and the applicant's team discussed that certain connections between the Southern Development Area and Northern Development Area needed to be in place. There are various triggers which have been proposed in the conditions included hereinabove to "unlock" certain levels of development (and contingent upon certain improvements).

The applicant has requested to move this previously approved Design Exception forward through the deminimis review process. The County Engineer reviewed the new PD request and determined any changes would not impact the Design Exception as currently proposed. Given the above, if PD 26-0349 is approved by the BOCC, the County Engineer will approve the above referenced deminimis request.

SEGMENTS GRAPHIC



**DESIGN EXCEPTION REVISION NEEDED – SAFFOLD RD. SUBSTANDARD ROAD**

As Saffold Rd. is a substandard collector roadway, the applicant’s Engineer of Record (EOR) submitted a Design Exception request (dated June 12, 2025) to determine the specific improvements that would be required by the County Engineer. Based on factors presented in the Design Exception request, the County Engineer found the request approvable (on July 11, 2025) and approved the Design Exception request (on November 26, 2025).

Improvements to Saffold Rd. have had broken into two (2) segments. Segment B was defined as that portion of Saffold Rd. along the frontages of folios 79700.0400, 79700.0300, 79700.0350, 79700.0100, 79700.0200, 79700.0250, 79700.0050, and 79700.0150. Segment A was defined as those portions of Saffold Rd. between CR 579 and the western project boundary, excluding the area defined as Segment B.

The Design Exception would authorize deviations from the 2021 Transportation Technical Manual (TTM) Typical Section – 7 (TS-7) (for 2-Lane, Rural Local and Collector Roadways). Specifically:

1. Within Segment B:

- a. The developer will be permitted to maintain the 10-foot-wide lanes in lieu of the 12-foot-wide lanes required pursuant to the Typical Section – 7 (TS-7) of the Transportation Technical Manual (TTM);
- b. The developer will be permitted to construct 6-foot-wide stabilized shoulders along both sides of the roadway, in lieu of the 8-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway as required pursuant to TS-7 of the TTM, thereby eliminating required bicycle facilities within this segment; and,
- c. The developer will be required to construct a 5-foot-wide sidewalk along the north side of the roadway.

2. Within Segment A:

- a. The developer will be permitted to maintain the 10-foot-wide lanes in lieu of the 12-foot-wide lanes required pursuant to the Typical Section – 7 (TS-7) of the Transportation Technical Manual (TTM);
- b. The developer shall dedicate and convey to Hillsborough County sufficient right-of-way to accommodate proposed improvements (a minimum of 48-feet north of the existing centerline). Staff notes this is in addition to any right-of-way necessary to accommodate site access improvements or additional preservation required pursuant to the Hillsborough County Corridor Preservation Plan as further described herein;
- c. The developer will be permitted to construct 6-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway, in lieu of the 8-foot-wide stabilized shoulders of which 5-feet is paved along both sides of the roadway as required pursuant to TS-7 of the TTM; and,
- d. The developer will be required to construct a 10-foot-wide sidewalk along the north side of the roadway. This will take the place of the 5-foot-wide sidewalk that would normally be constructed along the west side of the roadway within this segment per TS-7, but will not be in lieu of the required bicycle facilities, which are being provided on the paved 5-foot shoulders consistent with TS-7.

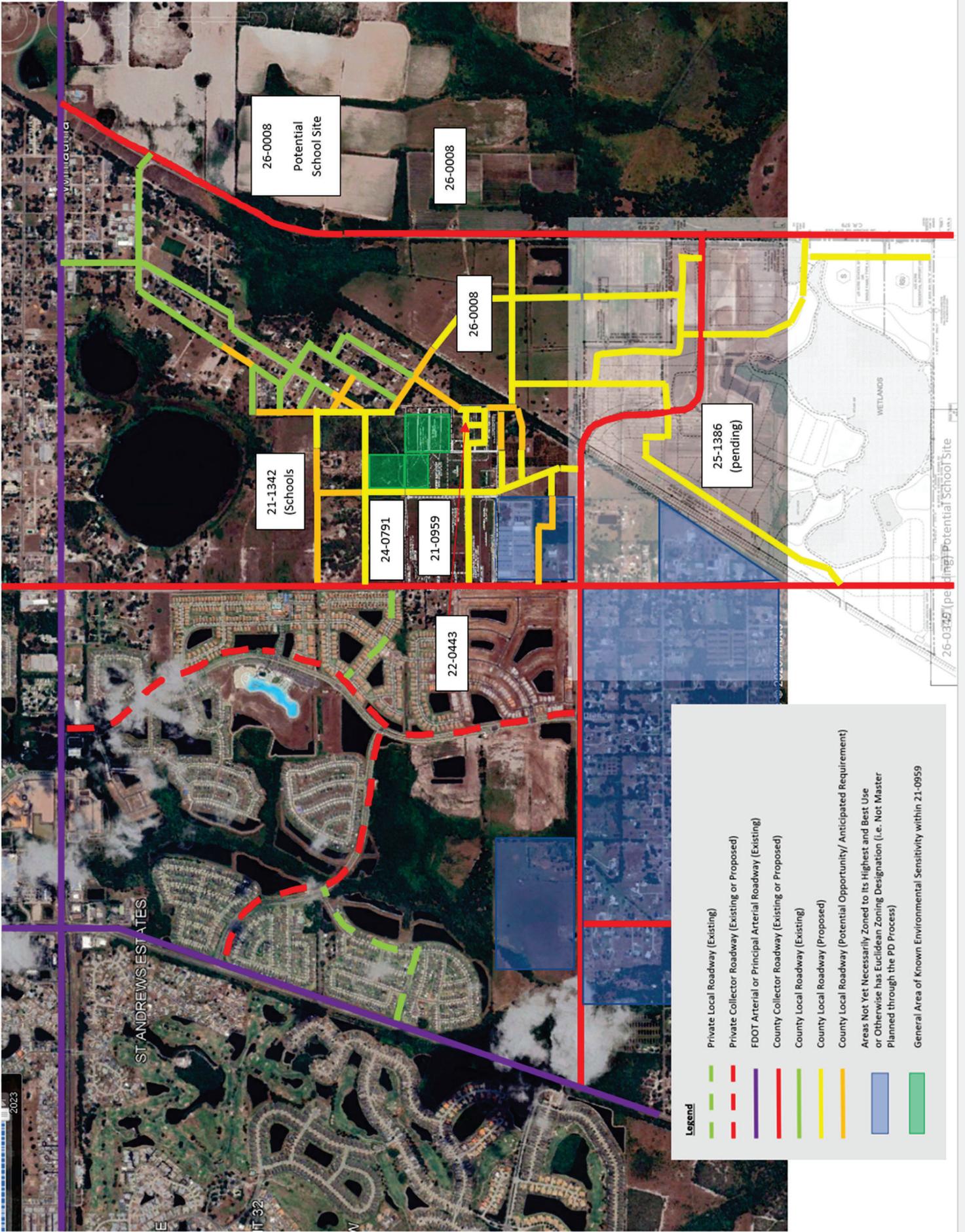
Given that the proposed PD seeks to add land, the limits of Segment A need to be modified include all areas within the PD. Staff has included a condition requiring the developer submit and obtain approval of a revised DE, based on the previously approved DE, with the changes as noted above. If the BOCC approves 26-0349, the County Engineer will approve a revised DE as described above, once received.

**ROADWAY LEVEL OF SERVICE (LOS) INFORMATION**

Information for pertinent roadways is included below. Saffold Rd., Berry Grove Blvd. and W. Lake Dr. (south of SR 674) were not included in the 2024 LOS report. As such, information for these facilities cannot be provided.

<b>Roadway</b>	<b>From</b>	<b>To</b>	<b>LOS Standard</b>	<b>Peak Hour Directional LOS</b>
US 301	Manatee County Line	River Rd.	D	C
US 301	River Rd.	Bonita Dr.	D	C
CR 579	Manatee County Line	SR 674	D	B

Source: Hillsborough County 2024 Level of Service Report.



## Ratliff, James

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**From:** Williams, Michael  
**Sent:** Friday, July 11, 2025 2:42 PM  
**To:** Steven Henry  
**Cc:** Kami Corbett; kelly.love@clearviewland.com; Follin, Jared; Ratliff, James; Drapach, Alan; Tirado, Sheida; De Leon, Eleonor; PW-CEIntake  
**Subject:** FW: RZ-PD 25-0371 - Design Exception Review (1 of 2)  
**Attachments:** 25-0371 Rev DEReq 06-13-25.pdf; 25-0371 DEAd 07-10-25\_3.pdf

Steve,

I have found the attached three Design Exceptions (DE) for PD 25-0371 APPROVABLE. Please note these are being sent over two emails due to file size.

Please note that it is you (or your client's) responsibility to follow-up with my administrative assistant, Eleonor De Leon ([DeLeonE@hcfl.gov](mailto:DeLeonE@hcfl.gov) or 813-307-1707) after the BOCC approves the PD zoning or PD zoning modification related to below request. This is to obtain a signed copy of the DE/AV.

If the BOCC denies the PD zoning or PD zoning modification request, staff will request that you withdraw the AV/DE. In such instance, notwithstanding the above finding of approvability, if you fail to withdraw the request, I will deny the AV/DE (since the finding was predicated on a specific development program and site configuration which was not approved).

Once I have signed the document, it is your responsibility to submit the signed AV/DE(s) together with your initial plat/site/construction plan submittal. If the project is already in preliminary review, then you must submit the signed document before the review will be allowed to progress. Staff will require resubmittal of all plat/site/construction plan submittals that do not include the appropriate signed AV/DE documentation.

Lastly, please note that it is critical to ensure you copy all related correspondence to [PW-CEIntake@hcfl.gov](mailto:PW-CEIntake@hcfl.gov)

Mike

**Michael J. Williams, P.E.**

**Director, Development Review**

**County Engineer**

Development Services Department

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P: (813) 307-1851

M: (813) 614-2190

E: [Williamsm@HCFL.gov](mailto:Williamsm@HCFL.gov)

W: HCFLGov.net

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**Hillsborough County**

601 E. Kennedy Blvd., Tampa, FL 33602

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Please note: All correspondence to or from this office is subject to Florida's Public Records law.

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**From:** Tirado, Sheida <[TiradoS@hcfl.gov](mailto:TiradoS@hcfl.gov)>  
**Sent:** Friday, July 11, 2025 1:34 PM  
**To:** Williams, Michael <[WilliamsM@hcfl.gov](mailto:WilliamsM@hcfl.gov)>; Steven Henry <[shenry@lincks.com](mailto:shenry@lincks.com)>  
**Cc:** Ratliff, James <[RatliffJa@hcfl.gov](mailto:RatliffJa@hcfl.gov)>  
**Subject:** RE: RZ-PD 25-0371 - Design Exception Review (1 of 2)

Hello Mike,

The attached Design Exceptions are Approvable to me, please include the following people in your response email:

[shenry@lincks.com](mailto:shenry@lincks.com)  
[kami.corbett@hwlaw.com](mailto:kami.corbett@hwlaw.com)  
[kelly.love@clearviewland.com](mailto:kelly.love@clearviewland.com)  
[follinj@hcfl.gov](mailto:follinj@hcfl.gov)  
[ratliffja@hcfl.gov](mailto:ratliffja@hcfl.gov)  
[drapacha@hcfl.gov](mailto:drapacha@hcfl.gov)

Best Regards,

**Sheida L. Tirado, PE**  
**Transportation Review & Site Intake Manager**  
Development Services Department

E: [TiradoS@HCFL.gov](mailto:TiradoS@HCFL.gov)  
P: (813) 276-8364 | M: (813) 564-4676

601 E. Kennedy Blvd., Tampa, FL 33602  
[HCFL.gov](http://HCFL.gov)

## Hillsborough County Florida

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**From:** Williams, Michael <[WilliamsM@hcfl.gov](mailto:WilliamsM@hcfl.gov)>  
**Sent:** Tuesday, July 8, 2025 10:43 AM  
**To:** Steven Henry <[shenry@lincks.com](mailto:shenry@lincks.com)>  
**Cc:** Tirado, Sheida <[TiradoS@hcfl.gov](mailto:TiradoS@hcfl.gov)>; Ratliff, James <[RatliffJa@hcfl.gov](mailto:RatliffJa@hcfl.gov)>  
**Subject:** FW: RZ-PD 25-0371 - Design Exception Review (1 of 2)

Steve,

The DE for Saffold and West Lake Drive are as discussed and fine. For CR 579, the DE will need to cover Segments A and B, as well as C, until such time as West Lake is completed per the current zoning conditions. In the current zoning up to 600 units are allowed prior to West Lake.

Mike

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**From:** Tirado, Sheida <[TiradoS@hcfl.gov](mailto:TiradoS@hcfl.gov)>  
**Sent:** Thursday, July 3, 2025 5:57 PM  
**To:** Williams, Michael <[WilliamsM@hcfl.gov](mailto:WilliamsM@hcfl.gov)>  
**Cc:** Ratliff, James <[RatliffJa@hcfl.gov](mailto:RatliffJa@hcfl.gov)>; Drapach, Alan <[DrapachA@hcfl.gov](mailto:DrapachA@hcfl.gov)>  
**Subject:** RZ-PD 25-0371 - Design Exception Review (1 of 2)

Hello Mike,

The attached Design Exceptions are Approvable to me, please include the following people in your response email:

[shenry@lincks.com](mailto:shenry@lincks.com)  
[kami.corbett@hwhlaw.com](mailto:kami.corbett@hwhlaw.com)  
[kelly.love@clearviewland.com](mailto:kelly.love@clearviewland.com)  
[follinj@hcfl.gov](mailto:follinj@hcfl.gov)  
[ratliffja@hcfl.gov](mailto:ratliffja@hcfl.gov)  
[drapacha@hcfl.gov](mailto:drapacha@hcfl.gov)

Best Regards,

**Sheida L. Tirado, PE**  
**Transportation Review & Site Intake Manager**  
Development Services Department

E: [TiradoS@HCFL.gov](mailto:TiradoS@HCFL.gov)  
P: (813) 276-8364 | M: (813) 564-4676

601 E. Kennedy Blvd., Tampa, FL 33602  
[HCFL.gov](http://HCFL.gov)

[Facebook](#) | [X](#) | [YouTube](#) | [LinkedIn](#) | [Instagram](#) | [HCFL Stay Safe](#)

**Hillsborough County Florida**

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# Supplemental Information for Transportation Related Administrative Reviews

**Instructions:**

- This form must be provided separately for each request submitted (including different requests of the same type).
- This form must accompany all requests for applications types shown below. Staff will not log in or assign cases that are not accompanied by this form, or where the form is partially incomplete.
- A response is required in every field. Blank fields or non-responsive answers will result in your application being returned.
- All responses must be typed.
- Please contact Eleonor de Leon at [deleone@HCFL.gov](mailto:deleone@HCFL.gov) or via telephone at (813) 307-1707 if you have questions about how to complete this form.

<b>Request Type</b> (check one)	<input type="checkbox"/> Section 6.04.02.B. Administrative Variance <input checked="" type="checkbox"/> Technical Manual Design Exception Request <input type="checkbox"/> Alternative Parking Plan Request (Reference LDC Sec. 6.05.02.G3.) <input type="checkbox"/> Request for Determination of Required Parking for Unlisted Uses (Reference LDC Sec. 6.05.02.G.1. and G.2.)
<b>Submittal Type</b> (check one)	<input type="checkbox"/> New Request <input checked="" type="checkbox"/> Revised Request <input type="checkbox"/> Additional Information
<b>Submittal Number and Description/Running History</b> (check one and complete text box using instructions provided below)	<input checked="" type="checkbox"/> 1. CR 579 - Substandard Road <input type="checkbox"/> 4. <input checked="" type="checkbox"/> 2. CR 579 - Substandard Road <input type="checkbox"/> 5. <input type="checkbox"/> 3. <input type="checkbox"/> 6.
<p><b>Important:</b> To help staff differentiate multiple requests (whether of the same or different type), please use the above fields to assign a unique submittal number/name to each separate request. Previous submittals relating to the same project/phase shall be listed using the name and number previously identified. It is critical that the applicant reference this unique name in the request letter and subsequent filings/correspondence. If the applicant is revising or submitting additional information related to a previously submitted request, then the applicant would check the number of the previous submittal.</p>	
<b>Project Name/ Phase</b>	Council Growers
<p><b>Important:</b> The name selected must be used on all future communications and submittals of additional/revised information relating to this variance. If request is specific to a discrete phase, please also list that phase.</p>	
<b>Folio Number(s)</b>	<input checked="" type="checkbox"/> Check This Box If There Are More Than Five Folio Numbers
<p><b>Important:</b> List all folios related to the project, up to a maximum of five. If there are additional folios, check the box to indicate such. Folio numbers must be provided in the format provided by the Hillsborough County Property Appraiser's website (i.e. 6 numbers, followed by a hyphen, followed by 4 additional numbers, e.g. "012345-6789"). Multiple records should be separated by a semicolon and a space e.g. "012345-6789; 054321-9876").</p>	
<b>Name of Person Submitting Request</b>	Steven J. Henry, P.E.
<p><b>Important:</b> All Administrative Variances (AV) and Design Exceptions (DE) must be Signed and Sealed by a Professional Engineer (PE) licensed in the State of Florida.</p>	
<b>Current Property Zoning Designation</b>	
<p><b>Important:</b> For Example, type "Residential Multi-Family Conventional – 9" or "RMC-9". This is not the same as the property's Future Land Use (FLU) Designation. Typing "N/A" or "Unknown" will result in your application being returned. This information may be obtained via the Official Hillsborough County Zoning Atlas, which is available at <a href="https://maps.hillsboroughcountv.org/maphillsborough/maphillsborough.html">https://maps.hillsboroughcountv.org/maphillsborough/maphillsborough.html</a>. For additional assistance, please contact the <a href="#">Zoning Counselors</a> at the Center for Development Services at (813) 272-5600 Option 3.</p>	
<b>Pending Zoning Application Number</b>	MM 25-0371
<p><b>Important:</b> If a rezoning application is pending, enter the application number proceeded by the case type prefix, otherwise type "N/A" or "Not Applicable". Use PD for PD rezoning applications, MM for major modifications, PRS for minor modifications/personal appearances.</p>	
<b>Related Project Identification Number (Site/Subdivision Application Number)</b>	N/A
<p><b>Important:</b> This 4-digit code is assigned by the Center for Development Services Intake Team for all Certified Parcel, Site Construction, Subdivision Construction, and Preliminary/Final Plat applications. If no project number exists, please type "N/A" or "Not Applicable".</p>	



July 9, 2025

Mr. Michael Williams, PE  
County Engineer Development Review Director  
Hillsborough County Government  
601 East Kennedy Blvd., 20<sup>th</sup> Floor  
Tampa, FL 33602

Re: Council Growers  
MM 25-0371

Folio Numbers: 079702.0002, 079702.0000, 279691.0000, 079691.0010,  
279693.0000, 079692.0000, 079698.0010, 079698.0000,  
079699.0000, 079852.0000, 079852.0010, 079700.0000

Lincks Project # 19119

The purpose of this letter is to request a Design Exception to the Hillsborough County Transportation Technical Manual per Section 1.7.2 to meet the Land Development Code Section 6.04.03L— Substandard Roadways of the Hillsborough County Land Development for CR 579 from Saffold Road to CR 674. The subject project is located west of CR 579 and north of Saffold Road. The developer proposes to modify the existing Planned Development for the property to allow the following land uses:

- 2,000 Single Family Homes
- 300 Townhomes
- 1,000 Student Elementary School

Tables 1, 2, and 3 provides the trip generation for the project.

The access to serve the project is proposed to be as follows:

- Two (2) full accesses to CR 579
- The extension of the Berry Grove Boulevard from its current terminus to CR 579 (Roundabout)
- One (1) full access to Saffold Road
- Extension of West Lake Drive north to tie into West Lake Drive north of the TECO easement

According to the Hillsborough County Roadways Functional Classification Map, CR 579 is a collector road.

Based on the evaluation of CR 579, there is not sufficient right of way to improve CR 579 to TS-7 standards. Therefore, a Design Exception is requested for CR 579 along the

5023 West Laurel Street  
Tampa, FL 33607  
813 289 0039 Telephone  
8133 287 0674 Telefax  
[www.Lincks.com](http://www.Lincks.com) Website

Mr. Mike Williams  
July 9, 2025  
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property frontage. The JPL Development is to improve Segments A and B along CR 579 and the subject project is to improve Segment C. Except for the first phase of the development consisting of 600 dwelling units, the subject property will also be responsible for Segments A and B until such time that the West Lake Drive improvements north of Berry Grove Boulevard are constructed. The segments are shown in the attached graphic.

#### Segment A

This section is from SR 674 to the southern property line of the JPL project. See Typical Section A for the section proposed along the segment.

1. Right of Way – The right of way along the segment of CR 579 varies between approximately 51 feet to 74 feet. The developer of the JPL development has committed to providing the right of way on each side of CR 579 to provide a total of 48 feet of right of way from the existing centerline of CR 579 within the limits of the property they own.
2. Lane Width – TS-7 has 12 foot lanes. The proposed section is to maintain the existing lanes at 10 to 11 feet. Due to limited right of way, 12 foot lanes cannot be provided.
3. Shoulder – TS-7 has 8 foot shoulders with 5 feet paved. The proposed section has 6 foot shoulders with 5 feet paved.
4. Sidewalk – TS-7 has 5 foot sidewalk on both sides of the roadway. A 10 foot sidewalk is proposed along the west side of CR 579 within the property controlled by the JPL developer. North of the property the 10 foot sidewalk is to transition to a 5 foot sidewalk.

#### Segment B

This segment is from the southern property line of the JPL development to the northern property line of the Council Growers project along the Cypress Ridge Development, as shown in the attached graphic. See Typical Section B for the section proposed along this segment of the roadway.

1. Right of Way – The right of way along the segment of CR 579 varies between approximately 59 feet to 90 feet. The developer of PD 18-1048 is required to dedicate an additional 21.5 feet of right of way on the west side of CR 579.
2. Lane Width – TS-7 has 12 foot lanes. The proposed section is to maintain the existing lanes at 10 to 11 feet. Due to limited right of way, 12 foot lanes cannot be provided.

Mr. Mike Williams  
July 9, 2025  
Page 3

3. Shoulder – TS-7 has 8 foot shoulders with 5 feet paved. The proposed section has 6 foot shoulder with 5 feet paved.
4. Sidewalk – TS-7 has 5 feet on both sides of the roadway. The developer of PD 18-1048 is required to provide a 5 foot sidewalk along the property frontage.

### Segment C

This segment is along the subject property frontage of CR 579, as shown in the attached exhibit. See Typical Section C for the section along this segment.

1. Right of Way – The right of way along this segment of CR 579 varies between approximately 74 feet to 90 feet. As shown in Figure 1, the developer does own property along the portion of the segment and has committed to providing 48 feet of right way on the west side to accommodate the proposed improvements.
2. Lane Width – TS-7 has 12 foot lanes. The existing lanes are 10 feet. This section proposes to maintain the existing lane width.
3. Shoulder – TS-7 has 8 foot shoulders with 5 feet paved. The proposed section has 6 foot stabilized shoulder with five feet paved.
4. Sidewalk – TS-7 has 5 foot sidewalk on both sides of the road. The proposed section provides a 10 foot sidewalk on the west side of CR 579.

The proposed Design Exception for CR 579 protects and furthers the public health, safety and welfare based on the following:

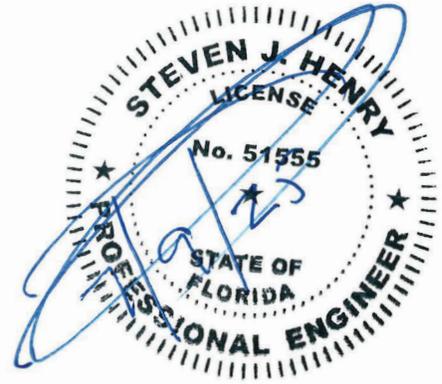
1. Five (5) foot paved shoulders/bike lanes are proposed along the entire length of the roadway. These will provide shoulders/bike lanes that do not currently exist on the roadway.
2. A continuous five (5) to ten (10) foot sidewalk along this section of the roadway is to be provided. This increases the pedestrian safety along the roadway and furthers the Vision Zero goals for Hillsborough County.

Please do not hesitate to contact us if you have any questions or require any additional information.

Mr. Mike Williams  
July 9, 2025  
Page 4

Best Regards,

Steven J Henry  
President  
Lincks & Associates, LLC  
A TMC Company  
P.E. #51555



Based on the information provided by the applicant, this request is:

- Disapproved
- Approved
- Approved with Conditions

If there are any further questions or you need clarification, please contact Sheida L. Tirado, P.E., (813) 276-8364, [TiradoS@hillsboroughcounty.org](mailto:TiradoS@hillsboroughcounty.org).

Date \_\_\_\_\_

**Michael J.  
Williams**

Digitally signed by  
Michael J. Williams  
Date: 2025.10.15  
09:59:10 -04'00'

Sincerely,

**Michael J. Williams**  
Hillsborough County Engineer

**The County Engineer has reviewed zoning modification application # 26-0349 and determined the changes to be de minimis. As such, the previous approval shall stand.**

\_\_\_\_\_  
**Michael J. Williams, P.E.**  
Hillsborough County Engineer on \_\_\_\_\_

TABLE 1  
 ESTIMATED DAILY TRIP ENDS

<u>Location</u>	<u>Land Use</u>	<u>Code</u>	<u>Size</u>	<u>Daily Trip Ends</u>	<u>Internal Trip Ends</u>	<u>External Trip Ends</u>
North	Single Family	210	495 DU's	3,930	218	3,712
	Townhomes	215	300 DU's	2,236	77	2,159
	Elementary	520	1,000 Students	<u>2,270</u>	<u>958</u>	<u>1,312</u>
			Sub-Total	8,436	1,253	7,183
South	Single Family	210	1,505 DU's	<u>11,950</u>	<u>663</u>	<u>11,287</u>
			Total	20,386	1,916	18,470

(1) Source: ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.  
 (2) Internal Capture  
 • School Internal  
 422/1,000 x 2,270 = 958

TABLE 2  
 ESTIMATED AM PEAK HOUR PROJECT TRIP ENDS

Location	Land Use	ITE Land Use Code	Size	AM Peak Hour Trip Ends (1)			Internal Trip Ends			New External AM Peak Hour Trip Ends		
				In	Out	Total	In	Out	Total	In	Out	Total
North	Single Family	210	495 DU's	70	211	281	32	38	70	38	173	211
	Townhomes	215	300 DU's	38	112	150	12	14	26	26	98	124
	Elementary	520	1,000 Students	400	340	740	169	144	313	231	196	427
			Sub-Total	508	663	1,171	213	196	409	295	467	762
South	Single Family	210	1,505 DU's	215	642	857	100	117	217	115	525	640
			Total	723	1,305	2,028	313	313	626	410	992	1,402

(1) Source: ITE Trip Generation Manual, 11<sup>th</sup> Edition.

(2) Internal Capture  
 • School Internal  
 In - 422/1000 x 400 = 169  
 Out - 422/1000 x 340 = 144

TABLE 3  
 ESTIMATED PM PEAK HOUR PROJECT TRIP ENDS

Location	Land Use	ITE Land Use Code	Size	PM Peak Hour Trip Ends (1)			Internal Trip Ends			New External PM Peak Hour Trip Ends		
				In	Out	Total	In	Out	Total	In	Out	Total
North	Single Family	210	495 DU's	259	152	411	8	7	15	251	145	396
	Townhomes	215	300 DU's	104	72	176	3	3	6	101	69	170
	Elementary	520	1,000 Students	74	86	160	31	36	67	43	50	93
			Sub-Total	437	310	747	42	46	88	395	264	659
South	Single Family	210	1,505 DU's	787	462	1,249	25	21	46	762	441	1,203
	Total			1,224	772	1,996	67	67	134	1,157	705	1,862

(1) Source: ITE Trip Generation Manual, 11<sup>th</sup> Edition.

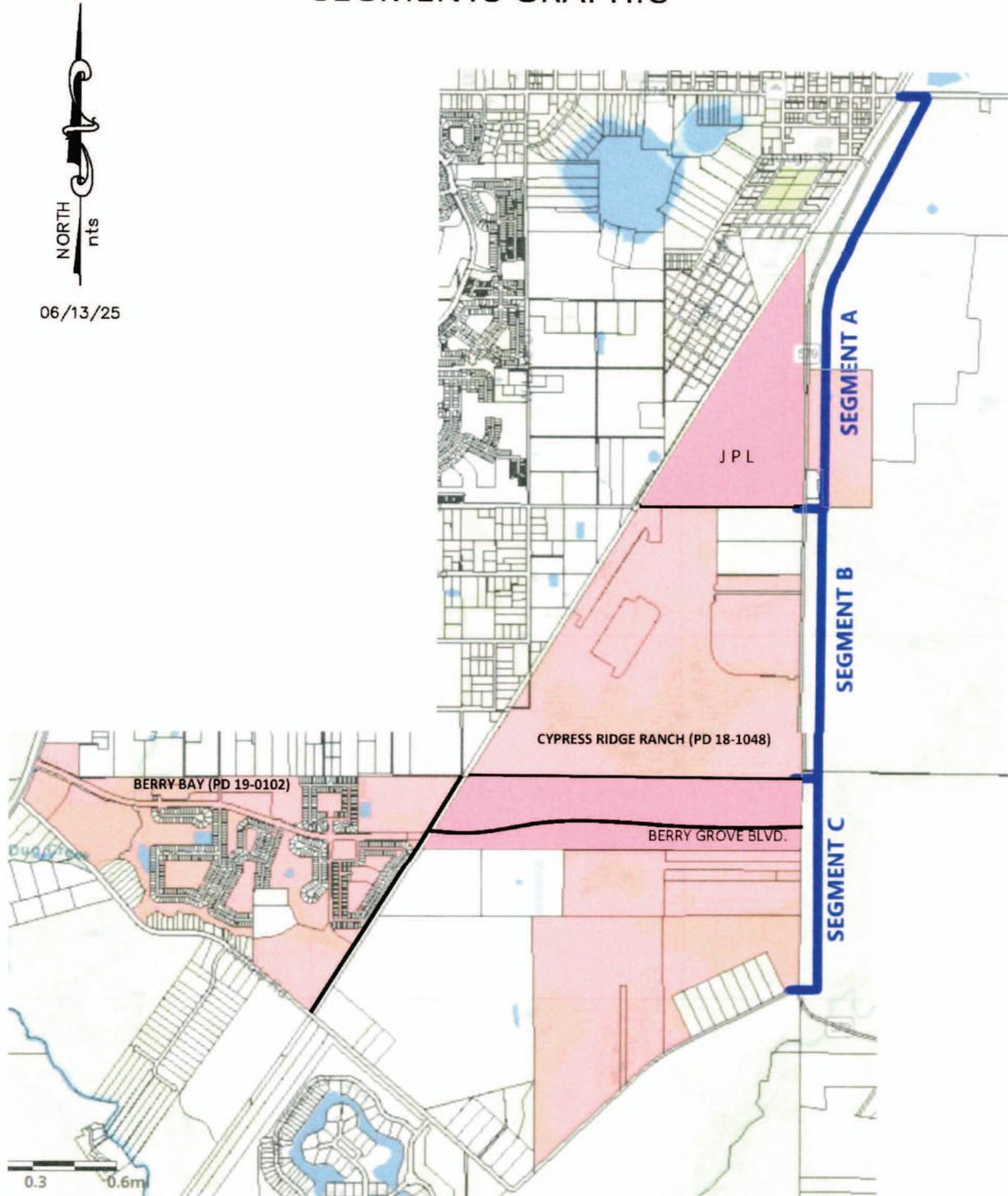
(2) Internal Capture

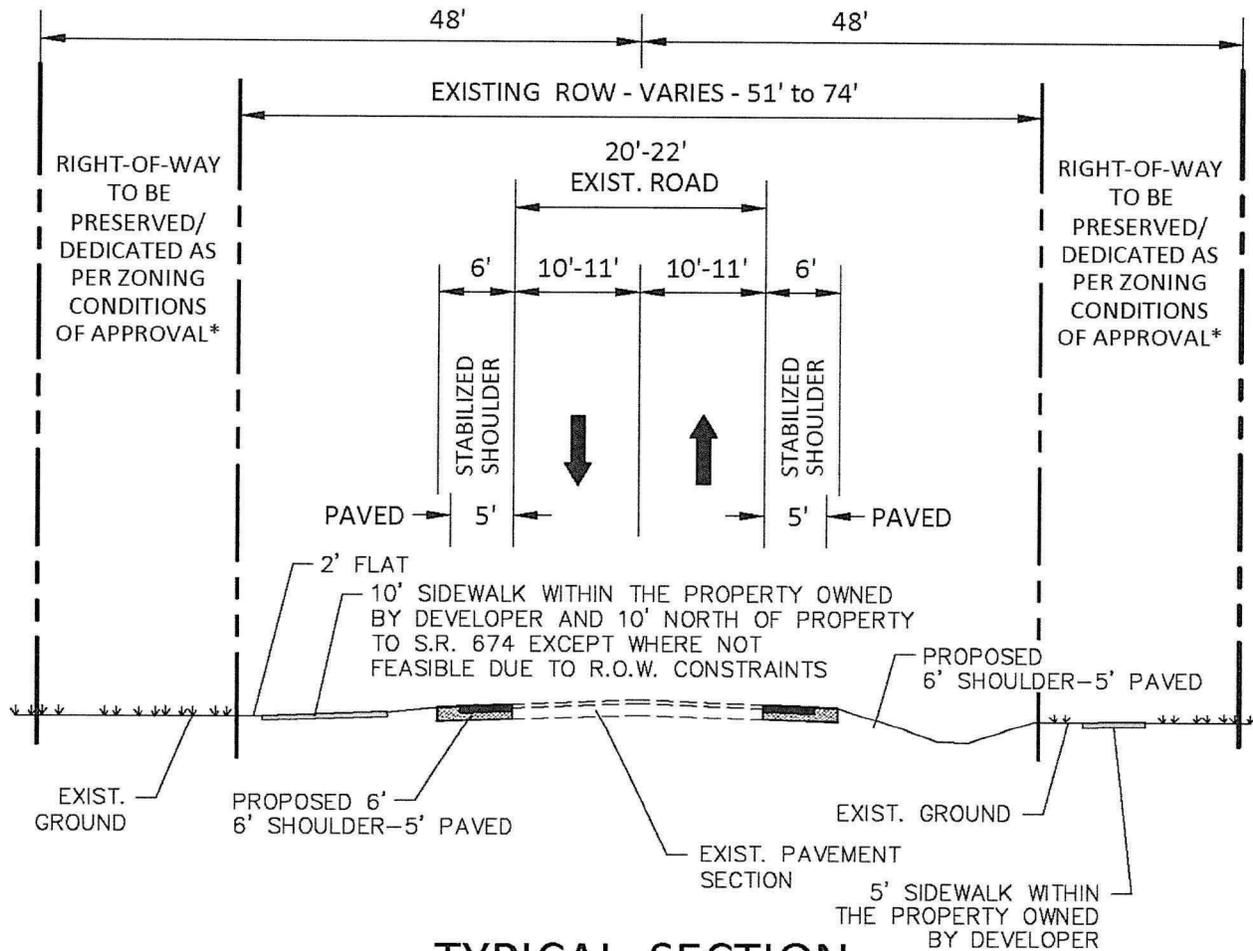
• School Internal

In - 422/1000 x 74 = 31

Out - 422/1000 x 86 = 36

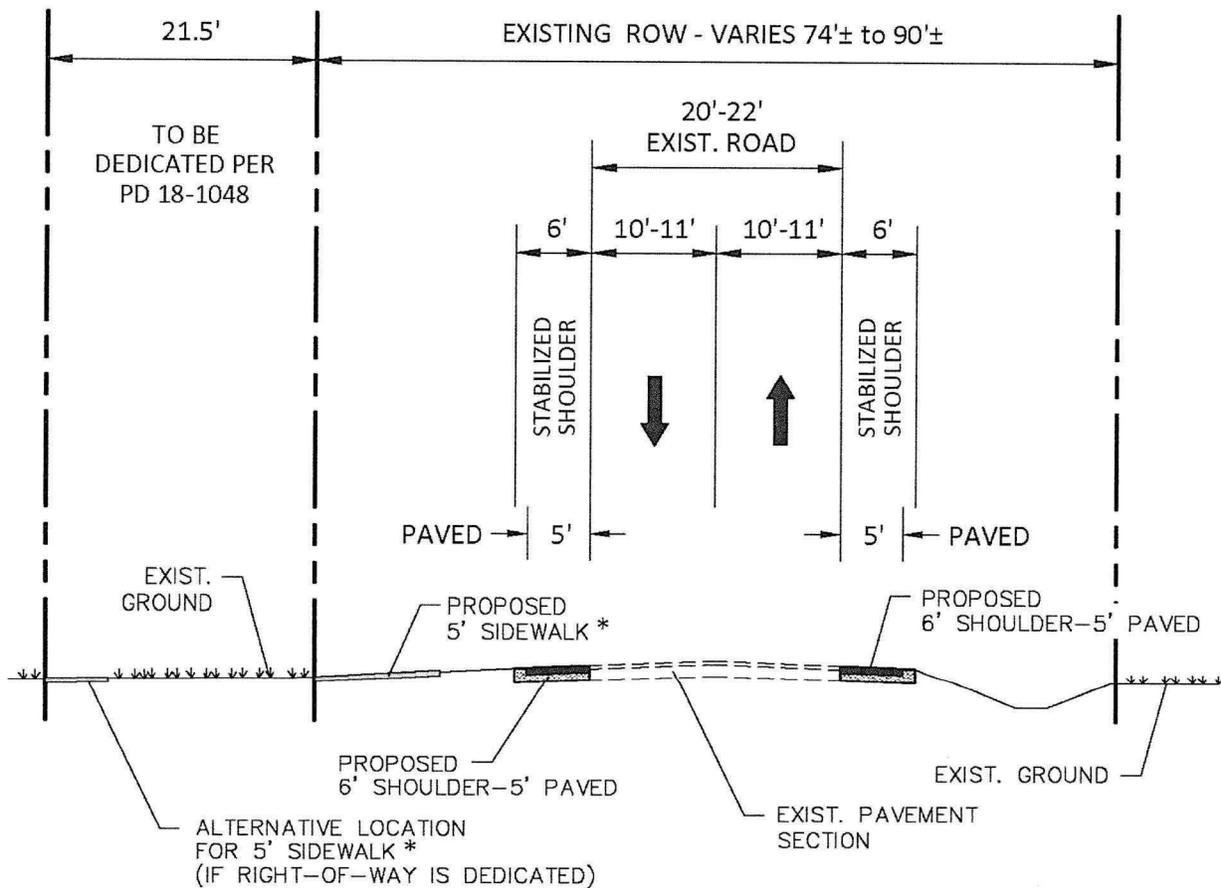
# SEGMENTS GRAPHIC





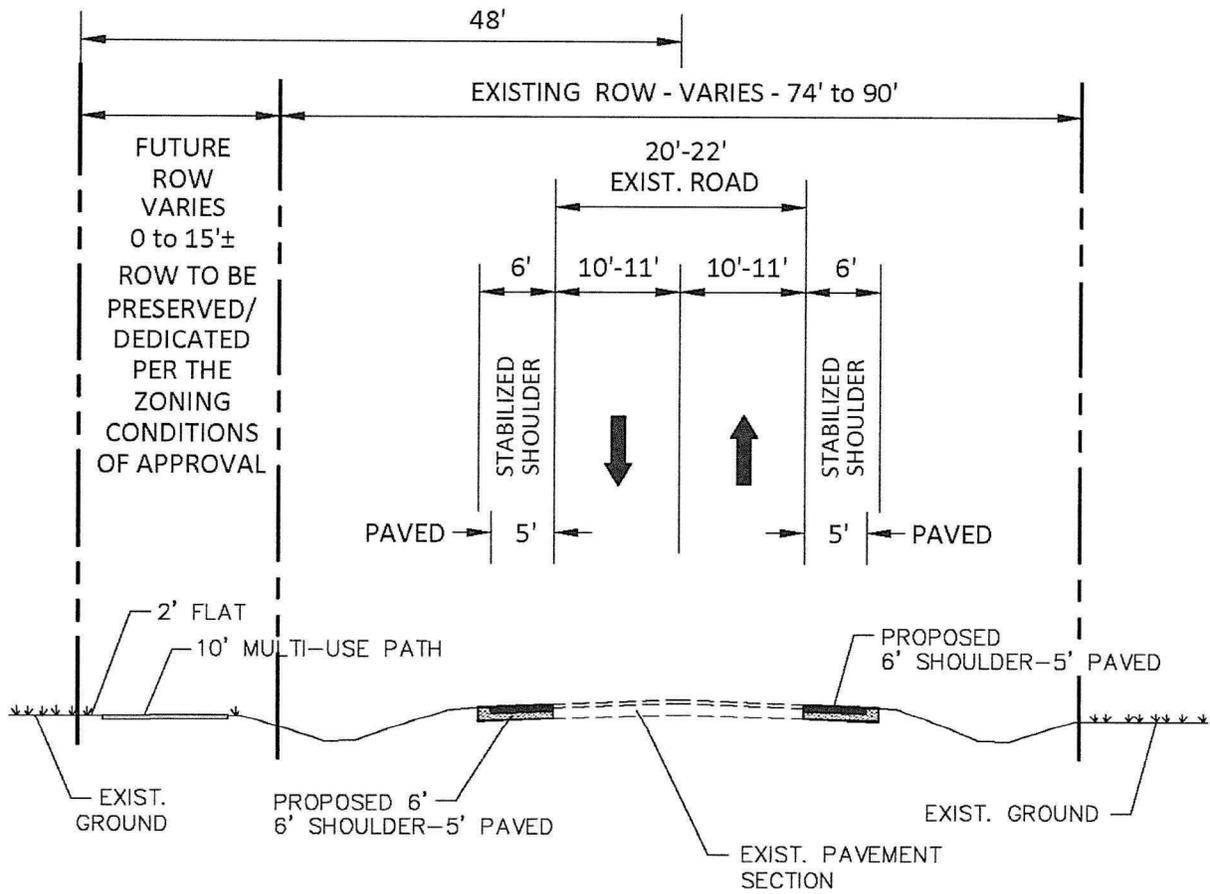
**TYPICAL SECTION  
SEGMENT A  
C.R. 579**

\* THE RIGHT-OF-WAY IS TO BE PRESERVED/DEDICATED WITHIN THE LIMITS OF THE PROPERTY OWNED BY THE DEVELOPER.



### TYPICAL SECTION SEGMENT B C.R. 579

\*TO BE CONSTRUCTED BY THE DEVELOPER OF THE  
CYPRESS RIDGE DEVELOPMENT



TYPICAL SECTION  
SEGMENT C  
C.R. 579

APPENDIX



PD PLAN





HILLSBOROUGH COUNTY ROADWAYS  
FUNCTIONAL CLASSIFICATION



# HILLSBOROUGH COUNTY ROADWAYS FUNCTIONAL CLASSIFICATION

Infrastructure & Development Services



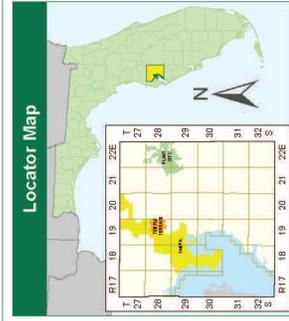
## Legend

- Functional Classifications  
Authority, Classification
- State, Principal Arterial
  - State, Arterial
  - Hillsborough, Arterial
  - Hillsborough, Collector
  - Urban Service Area Boundary
  - City Limits

The Hillsborough County Roadway Functional Map will be used in all matters in the Hillsborough County Land Development Code (LDC) that relate to functional classification of roads. Some, but not all, examples of those matters are as follows:

- PART 3.02.00 INTERSTATE-75 PLANNED DEVELOPMENT DISTRICTS
- PART 3.03.00 PLANNED DEVELOPMENT DISTRICTS
- PART 3.04.00 PLANNED DEVELOPMENT DISTRICTS
- PART 6.02.00 SUBDIVISION STANDARDS AND GUIDELINES
- PART 6.07.00 FENCES AND WALLS
- PART 12.01.00 DEFINITIONS - SPECIAL USES
- OTHER PARTS OF THE LDC NOT LISTED ABOVE

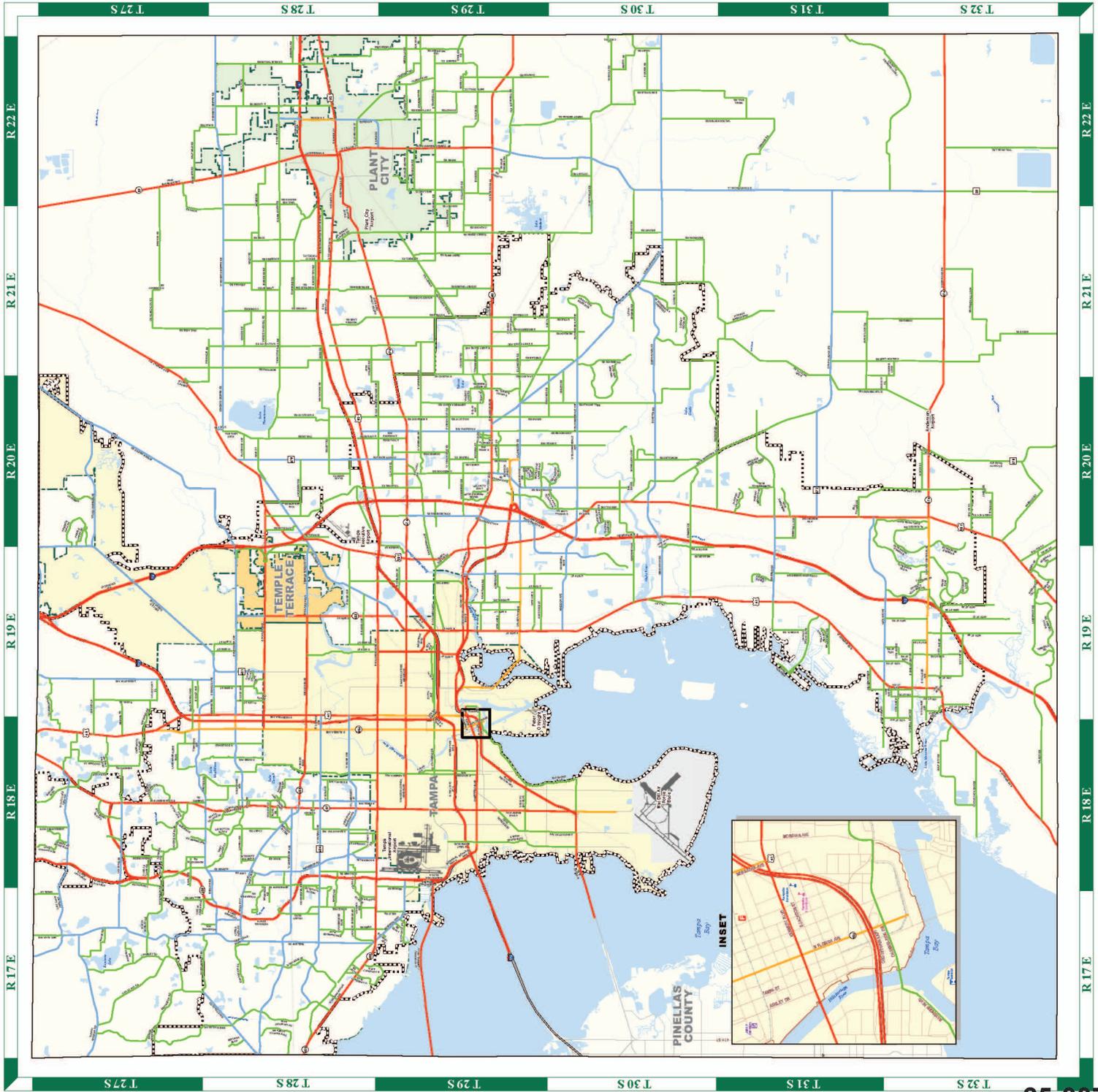
In addition to subject matters in the LDC, Functional Classification of roads plays an important role in administering the Residential Traffic Control Program and the Neighborhood Traffic Calming Program.



NOTE: Every reasonable effort has been made to assure the accuracy of this map. Hillsborough County does not warrant the accuracy of the map, and the user assumes all responsibility for the use of the map. The map is for informational purposes only and is not intended to be used for any other purpose, including, but not limited to, the implied warranties of merchantability and fitness for a particular purpose.

SOURCE: This map has been prepared for the inventory of real property parcels within Hillsborough County and is complete for the year 2024. The map is based on GIS/Aerial Data. The user assumes all responsibility for the accuracy of the information contained on this map. Primary information sources should be consulted for verification of the information contained on this map.

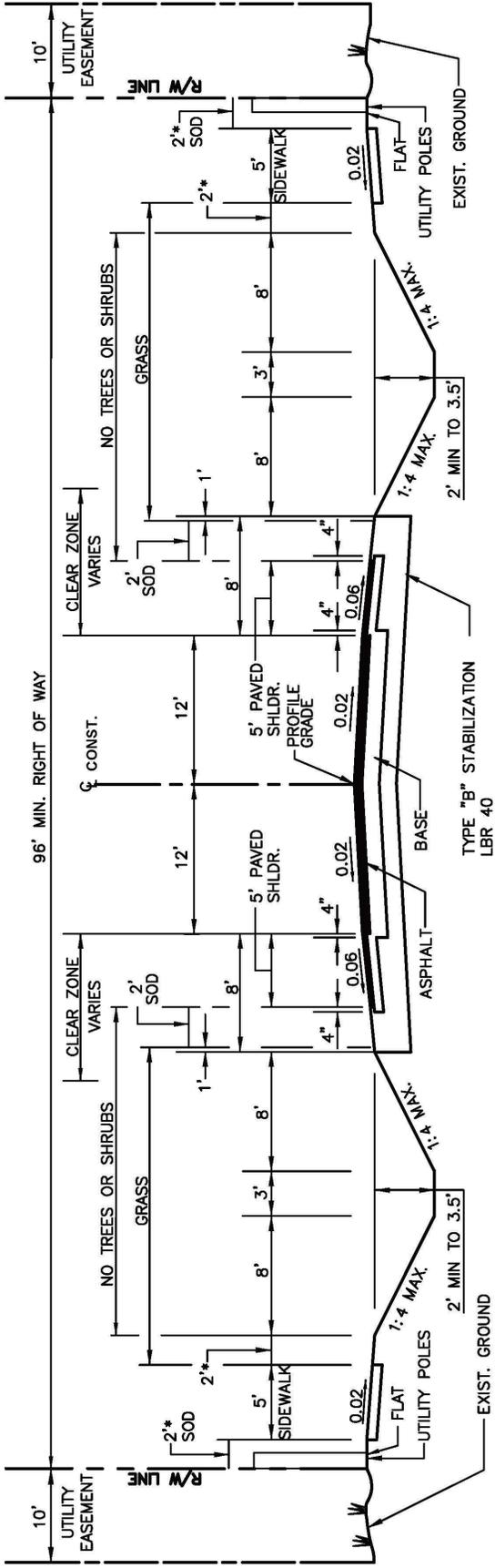
881 E. Kennedy Blvd  
Tampa, FL 33602  
(813) 272-5810  
printroom@hillsboroughcounty.org



TS-7



DRAWING NO. TS-7  
SHEET NO. 1 OF



**TYPICAL SECTION**

N.T.S.

FOR LESS THAN 10,000 AADT

MAX. ALLOWABLE DESIGN SPEED - 50 MPH

1. ALL DIMENSIONS SHOWN ARE MINIMUM.
2. SEE APPROPRIATE SECTIONS OF TECHNICAL MANUAL FOR DESIGN PARAMETERS.
- \* 3. PROVIDE 2' MINIMUM CLEARANCE FROM FENCES, WALLS, HEDGES, ABOVEGROUND UTILITIES OR IMPROVEMENTS, DROP OFFS, OR FROM THE TOPS OF BANKS WITH SLOPES STEEPER THAN 1 TO 4, THAT INTERFERE WITH THE SAFE, FUNCTIONAL USE OF THE SIDEWALK. INTERMITTENT ABOVEGROUND UTILITIES, OR MATURE TREES, 2' OR LESS IN DIAMETER MAY BE PLACED IN THIS 2' STRIP AS FAR FROM THE SIDEWALK AS POSSIBLE, IF NOT IN THE CLEAR ZONE.
4. SOD SHALL BE PLACED IN TWO ROWS STAGGERED. (BOTH TEMPORARY AND PERMANENT)
5. PAVED SHOULDER TO BE STRIPED AS A DESIGNATED BIKE LANE, AS APPROPRIATE.

**LOCAL & COLLECTOR RURAL ROADS  
(2 LANE UNDIVIDED)  
TYPICAL SECTION**



**TRANSPORTATION  
TECHNICAL  
MANUAL**

REVISION DATE:  
10/17

CR 579 FIELD ASSESSMENT



# CR 579

## Special Field Survey for Substandard Road Assessment

Limits of Survey: Scaffold Road to CR 674

Date of Survey: 11-05-22

By: WLR & DZS

Type of Road: Two lane, crown, asphalt

Pav't cond.: Fair to good to very good

Shoulder cond.: Good to poor, some erosion

Swales: swales both sides, most of the segment

**Notes:**

1. Left and right slopes are measured away from the pavement line, crown, invert crown, centerline or median that separates opposing traffic. Slopes down to the left and right from any of those dividing features are negative, slopes up are positive.
2. Measured Lane Pavement Width is edge of pavement to edge of pavement, including any paved shoulders. Minimum, Maximum and Average Lane Width values are lane widths without shoulders
3. Nominal dimensions for shoulders are when there is no discrete separation between shoulder and front slope and the minimum required shoulder is used as a nominal shoulder.
4. Most traffic signs are 8' to 10' from EOP and are breakaway.
5. AADT is 800. Requirement for shoulder is 8'. FDOT greenbook allows a max. of 12% slope. See Summary Page for existing shoulder widths and slopes.
6. CR 579 classified as Major on Hillsborough County Map and Minor Collector RURAL on FDOT map.
7. Hillsborough Transportation Manual for Subdivision and Site Development Projects Section 3.1 requires 12' lanes for commercial rural roads without bike lanes or paved shoulders. See Summary Page for existing lane widths
8. There are no Traffic Control Poles or devices. All Light Poles, Utility Poles, and Trees are outside of the Clear Zone. Some mailboxes, guardrails and drainage culvert headwalls are within the Clear Zone. See Field Survey.

**Speed Limits and Clear Zone Distances**

Road Jurisdiction:  FDOT  Hillsborough County  Pasco County

Type of Lane: Through (T), Through Curbed (TC), Auxiliary (A), or Auxiliary Curbed (AC)	Left Lanes				Right Lanes				
	Begin Station	End Station	Speed Limit (mph)	Clear Zone	Type of Lane: Through (T), Through Curbed (TC), Auxiliary (A), or Auxiliary Curbed (AC)	Begin Station	End Station	Speed Limit (mph)	Clear Zone
T	0+00	5+15	45	14'	T	0+00	5+75	45	14'
T	5+15	169+30	55	18'	T	5+75	169+30	55*	18'

\* Presumed speed as speed limit sign missing

**Summaries of Widths and Slopes for Pavement, Shoulders and Side Slopes**

Lane Pavement Width and Slopes		
Left Slope	Width	Right Slope
Minimum:	20.0'	-4.3%
Maximum:	22.7'	-1.3%
Average:	20.5'	-2.6%

Shoulders Width and Slopes			
Left Width	Left Slope	Right Width	Right Slope
Minimum:	4.0'	2.0%	0.0%
Maximum:	8.0'	16.0%	17.0%
Average:	6.7'	10.2%	9.5%

Average values calculated from trimmed mean values (without outliers)

**LEFT Side Slopes**

Front Slope 1 Width	Front Slope 1 Slope	Front Slope 2 Width	Front Slope 2 Slope	Bottom Width	Back Slope 1 Width	Back Slope 1 Slope	Back Slope 2 Width	Back Slope 2 Slope
4'	2%	7'	5%	0'	4'	10%	~	3%
11'	37%	7'	25%	8'	12'	100%	~	10%
7'	16.7%	7'	15.0%	3'	8'	43.6%	~	6.5%

~ = Slope continues beyond limits of survey

**RIGHT Side Slopes**

Front Slope 1 Width	Front Slope 1 Slope	Front Slope 2 Width	Front Slope 2 Slope	Bottom Width	Back Slope 1 Width	Back Slope 1 Slope	Back Slope 2 Width	Back Slope 2 Slope
4'	9%			0'	1'	2%	8'	10%
14'	30%			10'	15'	160%	14'	13%
7'	19.0%			3'	10'	40.5%	11'	11.5%

~ = Slope continues beyond limits of survey

**LEFT Slope Maximums**

Clear Zone	Front slope		Back slope	
	Inside	Outside	Inside	Outside
25%	33%	33%	33%	50%
14	14	14	14	14
1	0	0	3	3
7.1%	0.0%	0.0%	21.4%	21.4%

Maximum Allowed:  
Number of Sections:  
Sections Exceeding:  
Percent Exceeding:

**RIGHT Slope Maximums**

Clear Zone	Front slope		Back slope	
	Inside	Outside	Inside	Outside
25%	33%	33%	33%	50%
14	14	14	14	14
0	0	0	2	2
0.0%	0.0%	0.0%	14.3%	14.3%

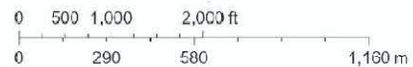
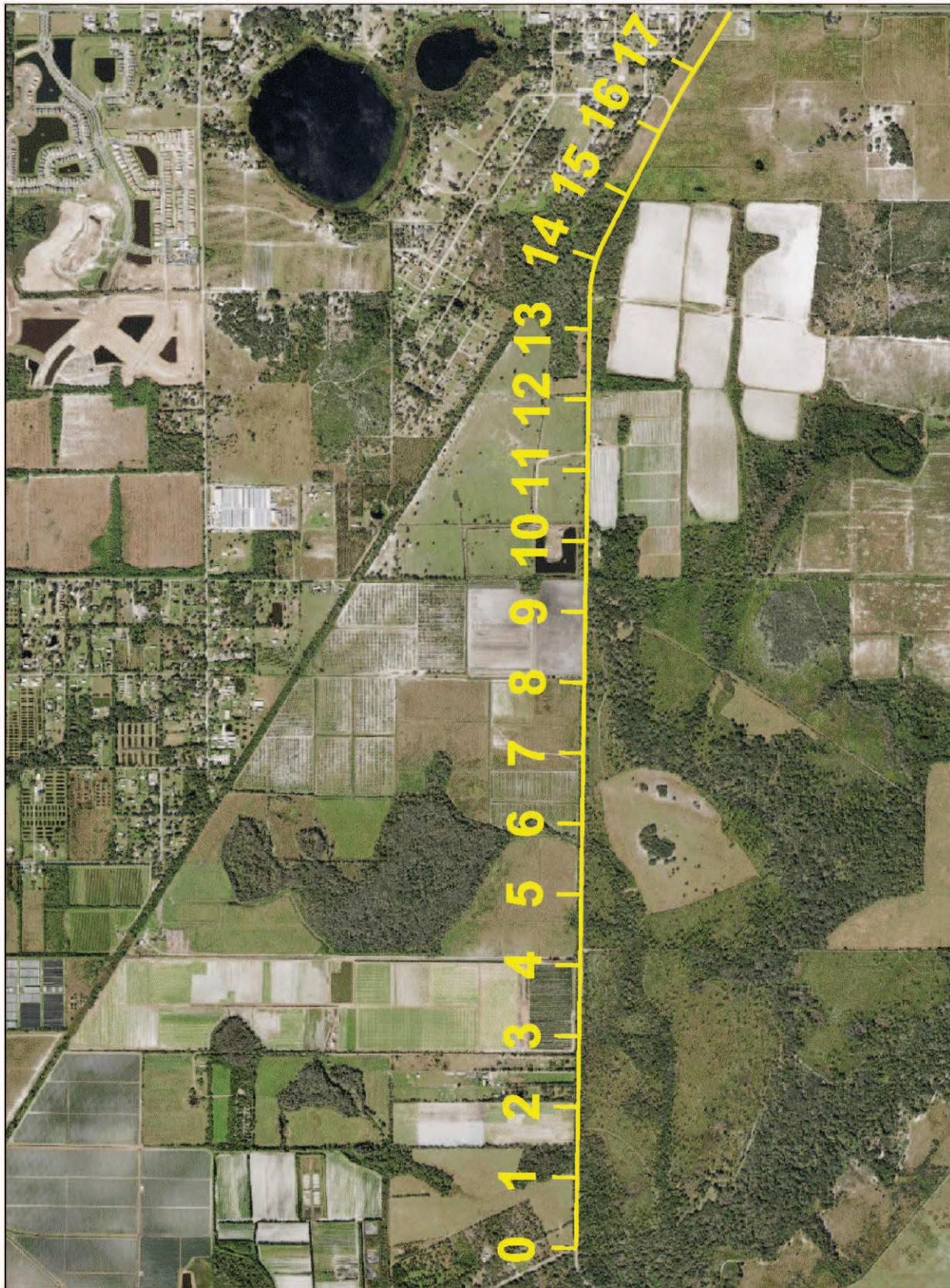
Maximum Allowed:  
Number of Sections:  
Sections Exceeding:  
Percent Exceeding:

# Field Survey

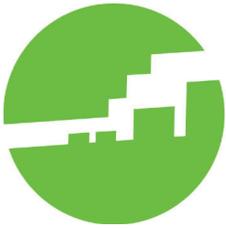
Station	Left Slopes and Swales			Lane Pavement		Right Shoulder		Right Slopes and Swales		
	Back Slope (width/slope)	Bottom (width)	Front Slope (width/slope)	Left Slope	Width	Right Slope	Total/Paved/ Slope	Front Slope (width/slope)	Bottom (width)	Back Slope (width/slope)
1+00	11'/12%, ~1/3%	0'	4'/-6%	-3.2%	22.7'	-3.4%	*8'/0'/-14%	4'/-14%	4'	7'/22%
LT 4' BWF 39' LT, 5+15 45 mph South										
RT Trees 20' RT, 5+75 55 mph North, RT side shoulder erosion, 3+23 72" CMP culvert, HDW 16' LT, 16' RT										
Notes 0+00 set at centerline intersection with Saffols Road										
8+00	15'18%	0'	5'/-12%	-3.0%	20.0'	-2.7%	*8'/0'/-15%	5'/-15%	0'	10'/16%
LT 4' BWF 28' LT										
RT Trees 35' RT										
Notes 10+70 (3) 48" CMP Culvert, HDW 18' LT, 20' RT										
20+00	4'/40%, ~10%	7'	5'/-37%	-3.8%	20.0'	-2.6%	4'/0'/0%	6'/-25%	6'	1'/160%
LT M.B.'s 6'-8' LT, U.P. 20' LT										
RT Trees 17' RT										
Notes										
28+00	8'/45%	5'	8'/-10%, 7'/-25%	-2.6%	20.6'	-2.7%	6'/0'/-8%	8'/-25%	4'	5'/130%
LT Trees 20' LT										
RT Trees 18' RT										
Notes										
36+00	8'/24%	0'	8'/-10%	-3.5%	20.4'	-3.0%	*8'/0'/-17%	6'/-17%	0'	10'/22%, 8'/-13%
LT Trees 20' LT, U.P. 24' LT										
RT Trees 30' RT, 4' BWF 32' RT										
Notes 45+27 24" RCP Culvert, HDW LT 15', RT 16'										
52+00	12'/16%	0'	8'/-15%	-2.3%	21.0'	-1.3%	8'/0'/-12%	7'/-17%	0'	11'/20%, 14'/-10%
LT Trees 25' LT										
RT Trees 26' RT, 56+70 (3) 36" RCP culvert, HDW 20' LT, 19' RT										
Notes										
68+00	6'/100%	8'	10'/-25%	-3.7%	21.0'	-1.8%	6'/0'/-9%	14'/-15%	4'	5'/100%
LT Trees 26' LT										
RT Trees 20' RT										
Notes Pav't Good, 71+00 24" RCP culvert, HDW 19' LT, 20' RT										
84+00	7'/23%	4'	5'/-14%	-3.2%	20.4'	-1.6%	7'/0'/-10%	8'/-14%	4'	15'/10%
LT U.P. 20' LT, GR 94+23 to 95+80 8' LT										
RT Trees 25' RT, BWF 34' RT, GR 93+60 to 94+74 8' RT										
Notes 94+50 36" RCP Culvert, HDW 15' LT, 17' RT										

Station	Left Slopes and Swales			Lane Pavement			Right Slopes and Swales			
	Back Slope (width/slope)	Bottom (width)	Front Slope (width/slope)	Left Slope	Width	Right Slope	Right Shoulder Total/Paved/Slope	Front Slope (width/slope)	Bottom (width)	Back Slope (width/slope)
100+00	10'/10%	0'	5'/-16%	-1.6%	20.4'	-1.4%	6'/0'/-12%	7'/-16%	0'	13'/8%
LT	Trees 24' LT, GR LT 101+60 to 104+00 8' LT									
RT	Trees 25' RT, 6' CLF 66' RT, GR RT 101+30 to 102+55 8' RT									
Notes	Notes 102+30 (2) 48" Box Culverts, HDW 16' LT, 12' RT									
120+00	8'/100%	6'	6'/25%	-2.1%	20.0'	-2.9%	6'/0'/-6%	7'/-27%	6'	8'/15%
LT	Trees 24' LT									
RT										
Notes										
134+00	5'/100%	2'	9'/-25%	-0.6%	21.0'	-2.1%	6'/0'/-5%	4'/-30%	10'	4'/30%
LT	MB's 4' LT, U.P. 18' LT, Trees 20'									
RT										
Notes										
150+00	~10%	7'	5'/-21%	-2.9%	20.1'	-2.3%	5'/0'/-6%	6'/-28%	6'	15'/25%
LT	Trees 16' LT, 163+12 Centerline Hillsborough St									
RT	4' WF 32' RT									
Notes										
166+00			11'/-16%, ~/-5%	2.2%	22.0'	-4.3%	*8'/0'/-9%	7'/-9%	0'	15'/2%
LT										
RT	U.P. 28' RT, 5' WF 30' RT									
Notes										
172+00			~/-2%	-2.6%	21.0'	-3.7%	5'/0'/-10%	12'/-14%	0'	12'/7%
LT										
RT	5' WF 25' RT									
Notes										
LT										
RT	179+30 End of Segment at EOP S.R. 674									
Notes										
LT										
RT										
Notes										
LT										
RT										
Notes										

# CR 579 Aerial & Stationing



Stations in 1000 ft increments



**Hillsborough County  
City-County  
Planning Commission**

**Plan Hillsborough**  
planhillsborough.org  
planner@plancom.org  
813 – 272 – 5940  
601 E Kennedy Blvd  
18<sup>th</sup> floor  
Tampa, FL, 33602

<b>Unincorporated Hillsborough County Rezoning Consistency Review</b>	
<b>Hearing Date:</b> March 23, 2026 <b>Report Prepared:</b> March 12, 2026	<b>Case Number:</b> PD 26-0349 <b>Folio(s):</b> 79700.0100, 79700.0350, 79691.0010, 79700.0000, 79702.0050, 79702.0002, 79702.0000, 79691.0000, 79692.0000, 79693.0000, 79698.0000, 79698.0010, 79699.0000, 79852.0000, 79852.0010, and a portion of 79703.0000 <b>General Location:</b> North of Saffold Road and west of County Road 579
<b>Comprehensive Plan Finding</b>	<b>CONSISTENT</b>
<b>Adopted Future Land Use</b>	Residential-4 (4 du/ga; 0.25 FAR)
<b>Service Area</b>	Urban
<b>Community Plan(s)</b>	Southshore Areawide Systems Plan and Wimauma Village Community Plan
<b>Rezoning Request</b>	Planned Development to add an additional 13.1 acres to an existing Planned Development (PD 25-0371) with no changes to the previously approved options
<b>Parcel Size</b>	+/- 648.79 acres
<b>Street Functional Classification</b>	Saffold Road – <b>County Collector</b> County Road 579 – <b>County Collector</b>

<b>Commercial Locational Criteria</b>	Not applicable
<b>Evacuation Area</b>	N/A

<b>Table 1: COMPARISON OF SURROUNDING PROPERTIES</b>			
<b>Vicinity</b>	<b>Future Land Use Designation</b>	<b>Zoning</b>	<b>Existing Land Use</b>
<b>Subject Property</b>	Residential-4	AR + PD	Agricultural, Public/Quasi-Public/Institutions + Single Family
<b>North</b>	Residential-4	AR + AS-1 + PD	Single Family + Public/Quasi-Public/Institutions + Vacant
<b>South</b>	Natural Preservation + Agricultural/Rural-1/5	AR	Public/Quasi-Public/Institutions
<b>East</b>	Natural Preservation + Agricultural/Rural-1/5 + Agricultural/Mining-1/20	AR + A + AM	Single Family, Public/Quasi-Public/Institutions + Agricultural
<b>West</b>	Natural Preservation + Agricultural/Rural-1/5 + Residential-1	AR + AS-1 + PD	Single Family, Agricultural Public/Quasi-Public/Institutions

**Staff Analysis of Goals, Objectives, and Policies:**

The 648.79 ± acre subject site is located north of Saffold Road and west of County Road 579. The site is in the Urban Service Area (USA) and is located within the limits of the Southshore Areawide Systems Plan and Wimauma Village Community Plan. The applicant is requesting a Planned Development to add an additional 13.1 acres to an existing Planned Development (PD 25-0371) with no changes to the previously approved options. The approved options under PD 25-0371 are as follows: (1) 2,000 single family detached units, 300 townhome units, and a 1,000-student elementary school; OR (2) as a second option in the event that Hillsborough County School District does not acquire the school site, 2,192 single family detached units and 300 townhome units.

The subject site is in the Urban Service Area where, according to Objective 1.1 of the Future Land Use Section (FLUS), 80 percent of the county's growth is to be directed. The subject site is surrounded by the Residential-4 (RES-4), Natural Preservation (NP), Agricultural/Rural-1/5 (A/R-1/5), and Agricultural/Mining-1/20 (A/M-1/20) Future Land Use categories. The current area is largely residential with natural and agricultural areas to the east, west, and south of the site. The residential uses are all single-family developments.

FLUS Goal 2 and FLUS Objective 2.1, and each of their respective policies, establish the Future Land Use Map (FLUM) as well as the allowable range of uses for each Future Land Use category. The character of each land use category is defined by building type, residential density, functional use, and the physical composition of the land. The integration of these factors sets the general atmosphere and character of each land use category. Each category has a range of potentially permissible uses, which are not exhaustive, but are intended to be illustrative of the character of uses permitted within the land use designation. The site has a Future Land Use designation of Residential-4. The RES-4 Future Land Use category allows for the consideration of agricultural, residential, neighborhood commercial, office uses and multi-purpose projects. This category allows for a maximum of 4 dwelling units per gross acre or a 0.25 Floor Area Ratio (FAR). With the 648.79 acres, the site may be considered for a maximum of 2,594 dwelling units (648.79 acres X 4 dwelling unit/acre = 2,594 dwelling units). Both options the applicant is proposing with the Planned Development do not exceed the maximum allowable density or uses allowed within the RES-4 Future Land Use; therefore, the proposed rezoning is consistent with FLUS Goal 2 and Objective 2.1.

FLUS Policy 3.1.3 requires all new developments to be compatible with the surrounding area, noting that "Compatibility does not mean 'the same as.' Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development." Additionally, the Neighborhood Protection policies in the Future Land Use Section under Objective 4.4 and Policy 4.4.1 require new development to be compatible with the surrounding neighborhood. The proposed Planned Development will provide additional single-family housing within a predominantly residential and agricultural area. The addition of homes within the area and the placement of the townhome-style homes north away from the edge of the Urban Service Area and agricultural uses provides a gradual transition of density and intensity within the proposed development area. The proposed road provides additional connectivity within the area while also preserving the existing wetlands and natural surface water areas located on the site. The proposed Planned Development is consistent with FLUS Objective 4.4, Policy 4.4.1, and Policy 3.1.3.

The Comprehensive Plan requires that all development meet or exceed the land development regulations in Hillsborough County (FLUS Policy 4.1.1, 4.1.2, and 4.1.6). However, at the time of uploading this report, Hillsborough County Development Services Department and Transportation Division comments were not yet available in Optix and thus were not taken into consideration for analysis of this request.

FLUS Objective 3.2 and Policy 3.2.4 require community plans throughout the county to be consistent with the Comprehensive Plan. The site is within the limits of the Wimauma Village Community Plan and Southshore Areawide Systems Plan. Goal 1 of the Cultural/Historic Objective of the SouthShore Areawide Systems Plan seeks to promote sustainable growth and development that is clustered and well planned to preserve the area's environment, cultural identity, and livability. Goal 1.a. under the Economic Objective within the SouthShore Areawide Systems Plan intends to analyze, identify, and market lands that are available for economic development, including: residential, commercial, office, industrial, agricultural (i.e., lands that already have development orders or lands that are not developable). As Goal 1.b. is to recognize preferred development patterns as described in individual community plans, and

implement the communities' desires to the greatest extent possible (including codification into the Land Development Code). I.e., activity center, compatibility, design and form, pedestrian and bicycle/trail connectivity. Goal 5 of the Wimauma Village Community Plan, Affordable Housing and Neighborhoods, seeks to encourage housing to accommodate a diverse population and range of income levels. The proposed rezoning increases the type of housing allowed on the subject site, as well as allows for housing on a smaller lot size, which can be more affordable for the residents in the Wimauma Village area, and clusters residential development on land suitable for economic development without impacting the existing natural and agricultural uses in the area. The proposed amendment meets the intent of the SouthShore Areawide Systems Community Plan and the Wimauma Village Plan, located in the Livable Communities Element of the Comprehensive Plan.

Overall, staff finds that the proposed Planned Development is compatible with the existing development pattern found within the surrounding area and supports the goals of the Wimauma Village Community Plan and SouthShore Areawide Systems Plan. The proposed Planned Development would allow for development that is consistent with the Goals, Objectives and Policies of the *Unincorporated Hillsborough County Comprehensive Plan*.

### **Recommendation**

Based upon the above considerations and the following Goals, Objectives and Policies, Planning Commission staff finds the proposed Planned Development **CONSISTENT** with the *Unincorporated Hillsborough County Comprehensive Plan*, subject to the conditions proposed by the Development Services Department.

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### **Staff Identified Goals, Objectives and Policies of the *Unincorporated Hillsborough County Comprehensive Plan* Related to the Request:**

#### **FUTURE LAND USE SECTION**

##### ***Urban Service Area***

***Objective 1.1:*** Direct at least 80% of new population growth into the USA and adopted Urban expansion areas through 2045. Building permit activity and other similar measures will be used to evaluate this objective.

##### ***Land Use Categories***

***Goal 2:*** Ensure that the character, compatibility and location of land uses optimize the combined potential for economic benefit, fiscal sustainability, protection of natural resources and maintaining viable agriculture. Ensure density and intensities are maintained through the Future Land Use Map.

***Objective 2.1:*** The Future Land Use Map is a regulatory tool governing the pattern of development in unincorporated Hillsborough County through the year 2045.

***Policy 2.1.1:*** The Future Land Use Map shall identify Future Land Use categories, summarized in Table 2.2 and further described in Appendix A, that establish permitted land uses and maximum densities and intensities.

**Objective 2.2:** *The Future Land Use Map (FLUM) Shall identify Land Use Categories, summarized in table 2.2 of the Future Land Use Element.*

**Policy 2.2.1:** *The character of each land use category is defined by building type, residential density, functional use, and the physical composition of the land. The integration of these factors sets the general atmosphere and character of each land use category. Each category has a range of potentially permissible uses which are not exhaustive, but are intended to be illustrative of the character of uses permitted within the land use designation. Not all of those potential uses are routinely acceptable anywhere within that land use category.*

### **Community Context and Compatibility**

**Policy 3.1.3:** *Compatibility is defined as the characteristics of different uses or activities or design which allow them to be located near or adjacent to each other in harmony. Some elements affecting compatibility include the following: height, scale, mass and bulk of structures, pedestrian or vehicular traffic, circulation, access and parking impacts, landscaping, lighting, noise, odor and architecture. Compatibility does not mean “the same as.” Rather, it refers to the sensitivity of development proposals in maintaining the character of existing development.*

### **Community Planning**

**Objective 3.2:** *Hillsborough County is comprised of many diverse communities and neighborhoods. The comprehensive plan is effective in providing an overall growth management strategy for development within the entire County. Strategies shall be developed that ensure the long-range viability of its communities through a community and special area studies planning effort.*

#### *Relationship to Land Development Regulations*

**Policy 3.2.4:** *The County shall assist the Hillsborough County City-County Planning Commission in developing community plans for each planning area that are consistent with and further the Goals, Objectives and Policies of the Comprehensive Plan. The community plans will be adopted as part of the Comprehensive Plan in the Livable Communities Element. These community specific policies will apply in guiding the development of the community. Additional policies regarding community planning and the adopted community plans can be found in the Livable Communities Element*

### **Development**

**Objective 4.1:** *All existing and future land development regulations shall be made consistent with the Comprehensive Plan, and all development approvals shall be consistent with those development regulations as per the timeframe provided for within Chapter 163, Florida Statutes. Whenever feasible and*

*consistent with Comprehensive Plan policies, land development regulations shall be designed to provide flexible, alternative solutions to problems.*

**Policy 4.1.1:** *Each land use plan category shall have a set of zoning districts that may be permitted within that land use plan category, and development shall not be approved for zoning that is inconsistent with the plan.*

**Policy 4.1.2:** *Developments must meet or exceed the requirements of all land development regulations as established and adopted by Hillsborough County, the state of Florida and the federal government unless such requirements have been previously waived by those governmental bodies.*

**Policy 4.1.6:** *Existing and future land development regulations shall be made consistent with the Comprehensive Plan, and all development approvals shall be consistent with those development regulations per the timeframe provided for within Chapter 163, Florida Statutes. Whenever feasible and consistent with Comprehensive Plan policies, land development regulations shall be designed to provide flexible, alternative solutions to problems.*

### **Neighborhood and Community Development**

**Objective 4.4: Neighborhood Protection** – *Enhance and preserve existing neighborhoods and communities. Design neighborhoods which are related to the predominant character of their surroundings.*

**Policy 4.4.1:** *Any density or intensity increases shall be compatible with existing, proposed or planned surrounding development. Development and redevelopment shall be integrated with the adjacent land uses through:*

- a) the creation of like uses; and*
- b) creation of complementary uses; and*
- c) mitigation of adverse impacts; and*
- d) transportation/pedestrian connections; and*
- e) Gradual transitions of intensity*

## LIVABLE COMMUNITIES ELEMENT: SOUTHSHORE AREAWIDE SYSTEMS PLAN

### **Cultural/Historic Objective**

*The SouthShore region of Hillsborough County supports a diverse population with people living in unique communities, interspersed with farms, natural areas, open spaces and greenways that preserve and enhance the natural and cultural heritage.*

*The community desires to:*

- 1. Promote sustainable growth and development that is clustered and well planned to preserve the area's environment, cultural identity and livability.*

### **Economic Development Objective**

*The SouthShore community encourages activities that benefit residents, employers, employees, entrepreneurs, and businesses that will enhance economic prosperity and improve quality of life.*

*The community desires to pursue economic development activities in the following areas:*

- 1. Land Use/Transportation*
  - a. Analyze, identify and market lands that are available for economic development, including: residential commercial, office, industrial, agricultural (i.e., lands that already have development orders or lands that are not developable.)*
  - b. Recognize preferred development patterns as described in individual community plans, and implement the communities' desires to the greatest extent possible (including codification into the land development code). I.e., activity center, compatibility, design and form, pedestrian and bicycle/trail connectivity.*

## LIVABLE COMMUNITIES ELEMENT: WIMAUMA VILLAGE PLAN

### **Goal 5 – Affordable Housing and Neighborhoods**

- Implement incentives to encourage affordable housing*
- Encourage housing to accommodate a diverse population and a range of income levels*
- Implement housing rehabilitation assistance to lower income homeowners and mobile homes*
- Allow stacking of affordable housing bonus densities and the Transfer of Development Rights within the Wimauma Downtown TDR Receiving Zone*
- Gated subdivisions will not be permitted in order to foster an economically integrated community*
- Repair local streets within existing subdivisions*
- Encourage mixed-use residential with commercial development within the Wimauma Downtown district*
- Increase enforcement of the "Hillsborough County Property Maintenance Code"*
- Orient residential development to the conceptual Cross County Greenway Trail-Wimauma*

# HILLSBOROUGH COUNTY FUTURE LAND USE RZ-PD 26-0349

## Rezoning STATUS



PENDING



Tampa Service Area



Urban Service Area



Shoreline



County Boundary



Jurisdiction Boundary



Roads



Parcels



Water



AGRICULTURAL/MINING-120 (.25 FAR)



PEC PLANNED ENVIRONMENTAL COMMUNITY-1/2 (.25 FAR)



AGRICULTURAL-1/10 (.25 FAR)



AGRICULTURAL/RURAL-1/5 (.25 FAR)



AGRICULTURAL ESTATE-1/2.5 (.25 FAR)



RESIDENTIAL-1 (.25 FAR)



RESIDENTIAL-2 (.25 FAR)



RESIDENTIAL PLANNED-2 (.35 FAR)



RESIDENTIAL-4 (.25 FAR)



RESIDENTIAL-6 (.25 FAR)



RESIDENTIAL-9 (.35 FAR)



RESIDENTIAL-12 (.35 FAR)



RESIDENTIAL-16 (.35 FAR)



RESIDENTIAL-20 (.35 FAR)



RESIDENTIAL-35 (1.0 FAR)



NEIGHBORHOOD MIXED USE-4 (3) (.35 FAR)



SUBURBAN MIXED USE-6 (.35 FAR)



COMMUNITY MIXED USE-12 (.50 FAR)



URBAN MIXED USE-20 (1.0 FAR)



REGIONAL MIXED USE-35 (2.0 FAR)



INNOVATION CORRIDOR MIXED USE-35 (2.0 FAR)



OFFICE COMMERCIAL-20 (.75 FAR)



RESEARCH CORPORATE PARK (1.0 FAR)



ENERGY INDUSTRIAL PARK (.50 FAR USES OTHER THAN RETAIL, .25 FAR RETAIL/COMMERCE)



LIGHT INDUSTRIAL (75 FAR)



HEAVY INDUSTRIAL (75 FAR)



PUBLIC/QUASH-PUBLIC



NATURAL PRESERVATION



WIMAUMA VILLAGE RESIDENTIAL-2 (.25 FAR)



CITRUS PARK VILLAGE

DATA SOURCES: Rezoning boundaries from The Planning Commission and are not official. Parcel lines and data from Hillsborough County Property Appraiser. Shorelines and water bodies are based on the most current data available. The map is for informational purposes only. For the most current data and information, visit the appropriate agency.



Map Printed from Rezoning System: 2/23/2026

Author: R. Mathie

File: G:\Rezoning\System\MapProjects\HC\Regre\_jhc\Rezoning\_2.mxd

